

REVISIONS			
NO.	DESCRIPTION	DATE	BY
1	ADDED SEDIMENT CONTROL PLANS S.P.810	9-5-85	M.J.

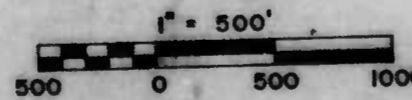
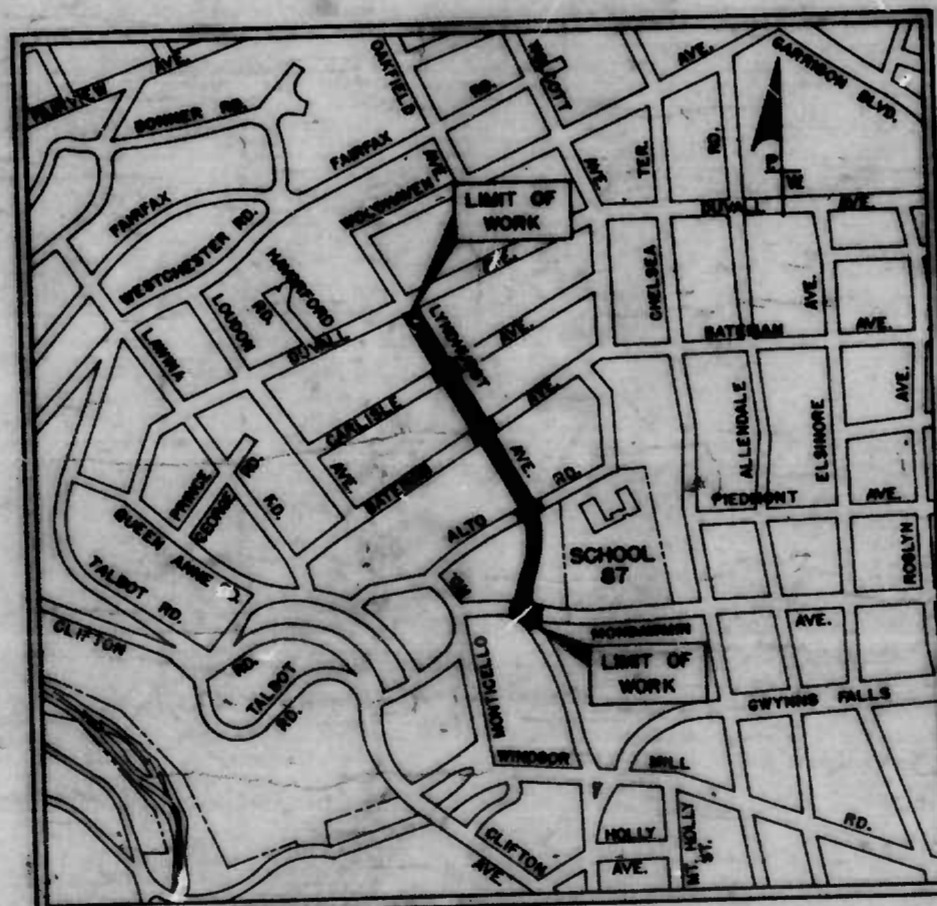
**CITY OF BALTIMORE**  
**DEPARTMENT OF PUBLIC WORKS**  
**BUREAU OF HIGHWAYS**  
**LYNDHURST AVENUE**  
**MONDAWMIN AVENUE TO DUVALL AVENUE**



**CITY OF BALTIMORE BUREAU OF HIGHWAYS CONTRACT NO. 2913**

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	TYPICAL SECTIONS
3	ROADWAY PLAN MONDAWMIN TO ALTO
4	ROADWAY PLAN BATEMAN TO DUVALL
5	ROADWAY PROFILES
6	INTERSECTION GRADING PLANS
7	MAINTENANCE OF TRAFFIC PLAN
8	SEDIMENT CONTROL PLAN (PHASE 1)
9	SEDIMENT CONTROL PLAN (PHASE 2)
10	SOIL EROSION/SEDIMENT CONTROL DETAIL

- LEGEND**
- - TREE TO BE REMOVED BY THE CONTRACTOR
  - ▭ - PEDESTRIAN RAMP
  - ▨ - MILL 1" OF EXISTING PAVEMENT AND RESURFACE WITH 1" BITUMINOUS CONCRETE SURFACE COURSE-USING SAND 5M
  - ▭ - FULL DEPTH PAVING
  - Ⓐ - UTILITY TO BE ADJUSTED BY THE CONTRACTOR
  - Ⓡ - UTILITY TO BE REMOVED BY THE CONTRACTOR
  - ▲ - PROPOSED STREET LIGHT POLES TO BE INSTALLED BY OTHERS
  - Ⓡ - REMOVE EXISTING STREET LIGHT BASE
  - ▭ - 5" CONCRETE WALK
  - ▨ - 7" CONCRETE DRIVEWAY PAVING



FIELD BOOKS		
NO.	TITLE	DESCRIPTION
X875	Cross Sections and Topo	Lyndhurst Ave, Duvall Ave to Bateman Ave/Alto Road to Mondawmin Ave
X842	Driveway Cross Sections	Lyndhurst Ave, Duvall Ave to Bateman Ave/Alto Road to Mondawmin Ave

**DEPARTMENT OF PUBLIC WORKS**

BUREAU OF HIGHWAYS  
 APPROVED *[Signature]*  
 CHIEF, HIGHWAY ENGINEERING DIVISION

APPROVED *[Signature]*  
 CHIEF, ENVIRONMENTAL SERVICES DIVISION

OFFICE OF TRANSPORTATION  
 APPROVED *[Signature]* 4/25/85  
 DIRECTOR OF OFFICE OF TRANSPORTATION

APPROVED

*[Signature]*  
 DIRECTOR OF PUBLIC WORKS

APPROVED

*[Signature]*  
 HEAD, BUREAU OF HIGHWAYS



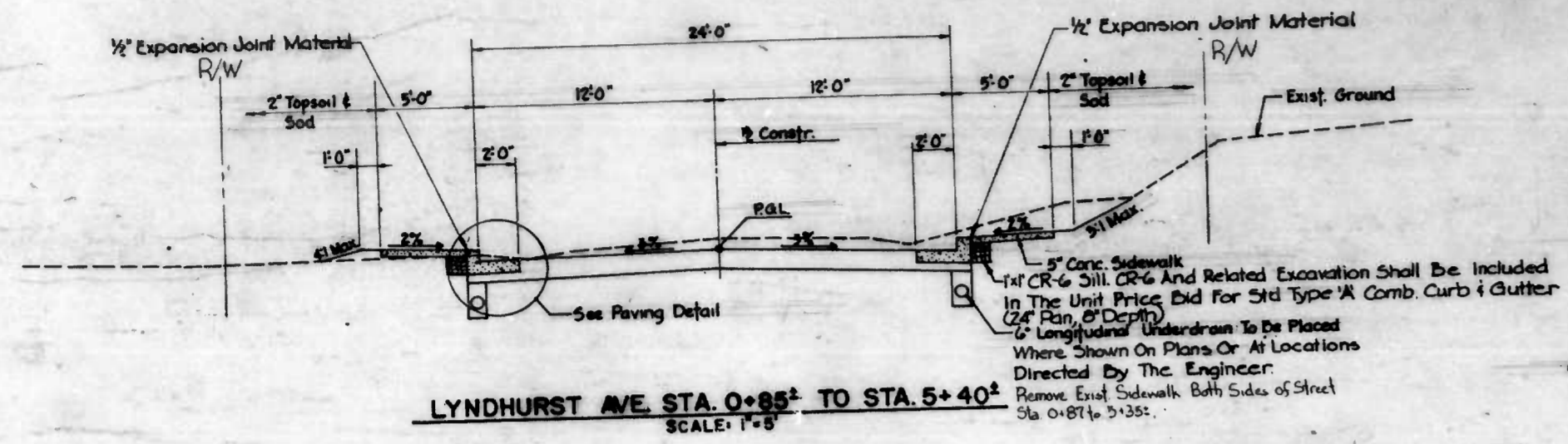
JOHNSON, MIMMINEN & THOMPSON  
 ENGINEERS *David F. Cheng*  
 DATE 10-24-85 DC NO 10497

# OF H REVIEW	R/W RELEASE	GRADE ESTD	ROADWAY DESIGN	STRUCTURAL	DRAINAGE	LIGHTING	CONDUIT	SEDIMENTATION AND EROSION CONTROL	OFFICE OF TRANSPORTATION	WASTE WATER ENGINEERING	WATER ENGINEERING
	10/24/85	4/24/85	4/24/85	-	11	8/24/85	BPR	B.G	1/16/85	12/2/85	2/11/85
DATE	4/24/85	4/24/85	4/24/85	-	5-23-85	4/24/85	4/23/85	6-10-85	1/05/85	12/2/85	1/24/85

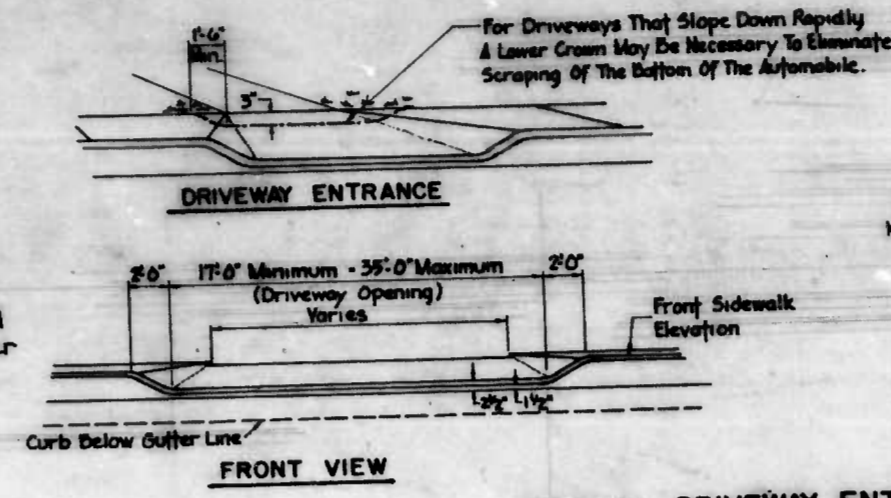
**Johnson, Mimminen & Thompson, P.A.**  
 ENGINEERS, ARCHITECTS, INTERIORS, LANDSCAPE ARCHITECTS  
 100 BALTIMORE STREET, BALTIMORE, MARYLAND 21201

FILE REF. 9 OF 84

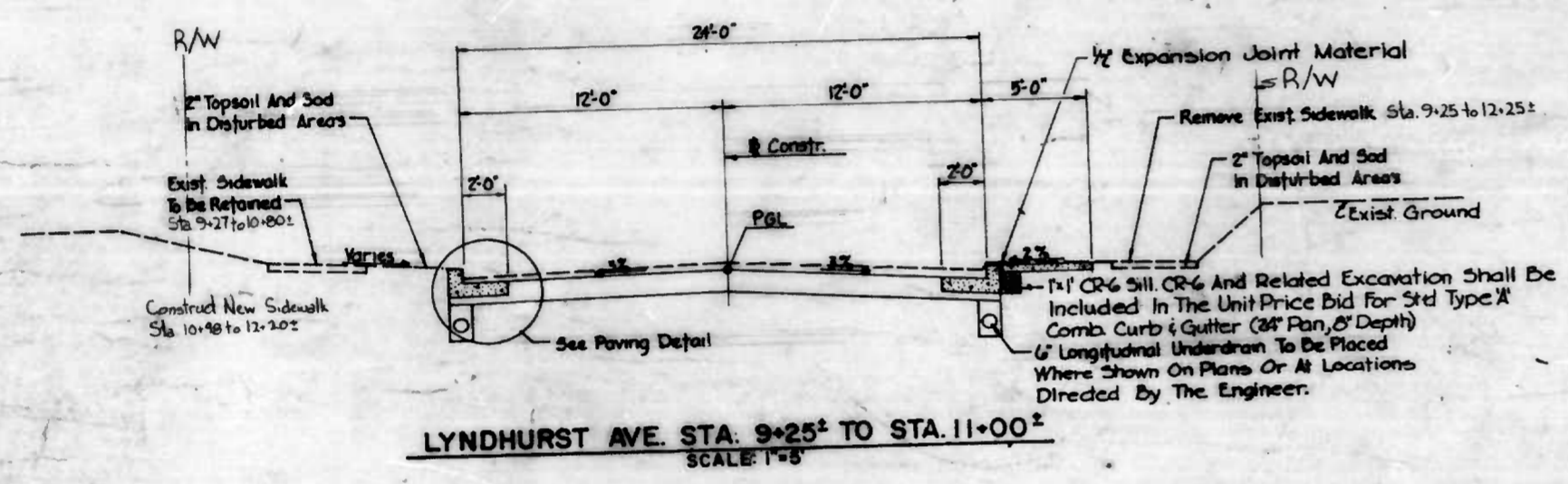
REVISIONS		
NO.	DESCRIPTION	DATE
1	ADDED SEDIMENT CONTROL PLANS 8, 9, 9.D	8-5-88 M.J.



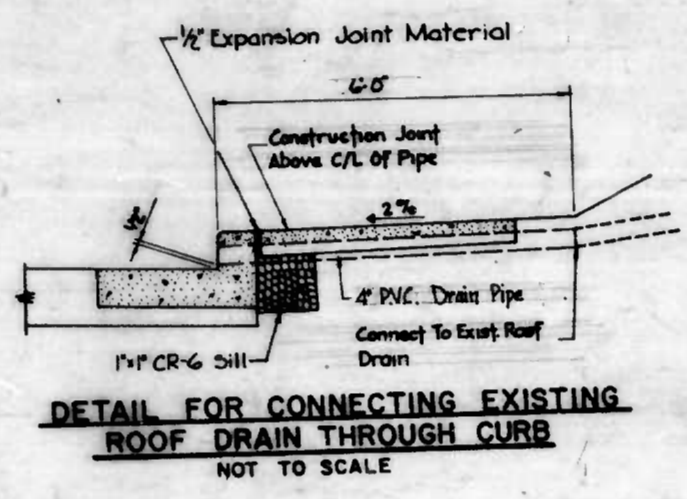
LYNDHURST AVE. STA. 0+85<sup>±</sup> TO STA. 5+40<sup>±</sup>  
SCALE: 1"=8'



SPECIAL DRIVEWAY ENTRANCE DETAIL  
NOT TO SCALE



LYNDHURST AVE. STA. 9+25<sup>±</sup> TO STA. 11+00<sup>±</sup>  
SCALE: 1"=8'



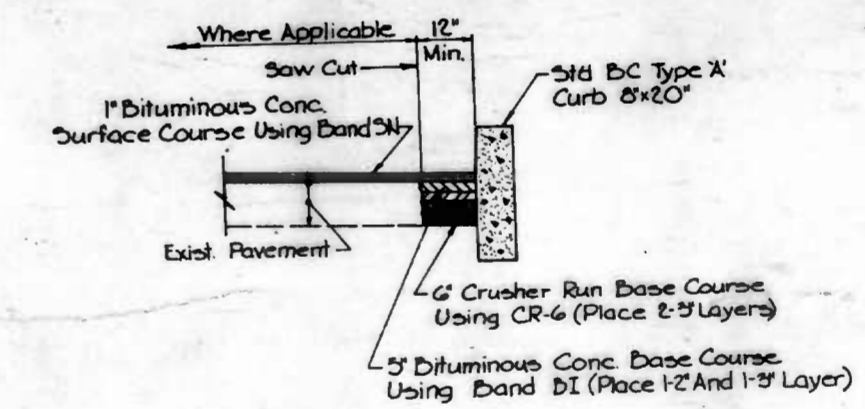
DETAIL FOR CONNECTING EXISTING ROOF DRAIN THROUGH CURB  
NOT TO SCALE

LYNDHURST AVE. - ALTO RD. TO BATEMAN AVENUE

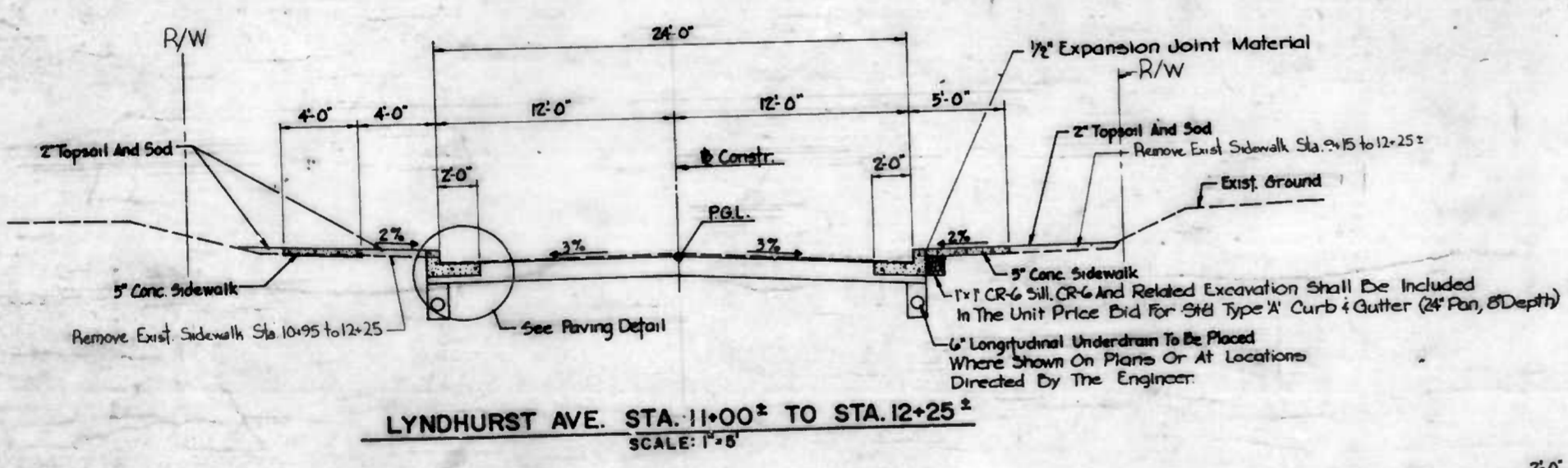
Mill 1" Of Existing Pavement And Resurface With 1" Bituminous Concrete Surface Course - Using Band 5N, From Alto Rd. To Bateman Ave.

CURB REPAIRS #		
LOCATION	LENGTH	
Sta. 5+88.5 Rt Lyndhurst Ave.	3L.F.	
Sta. 6+14.5 Rt Lyndhurst Ave.	15L.F.	
Sta. 7+55 Rt Lyndhurst Ave.	5L.F.	
Sta. 7+95 Rt Lyndhurst Ave.	6L.F.	
Sta. 8+27 Lt Lyndhurst Ave.	7L.F.	
Sta. 8+57 Rt Lyndhurst Ave.	35L.F.	

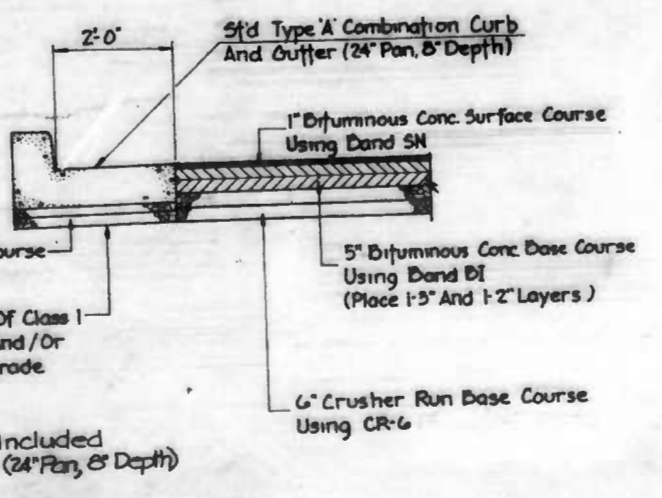
\* Use BC Type 'X' Curb 8"x20" For These Repairs See Baltimore City Std NO. B.C. 620.02



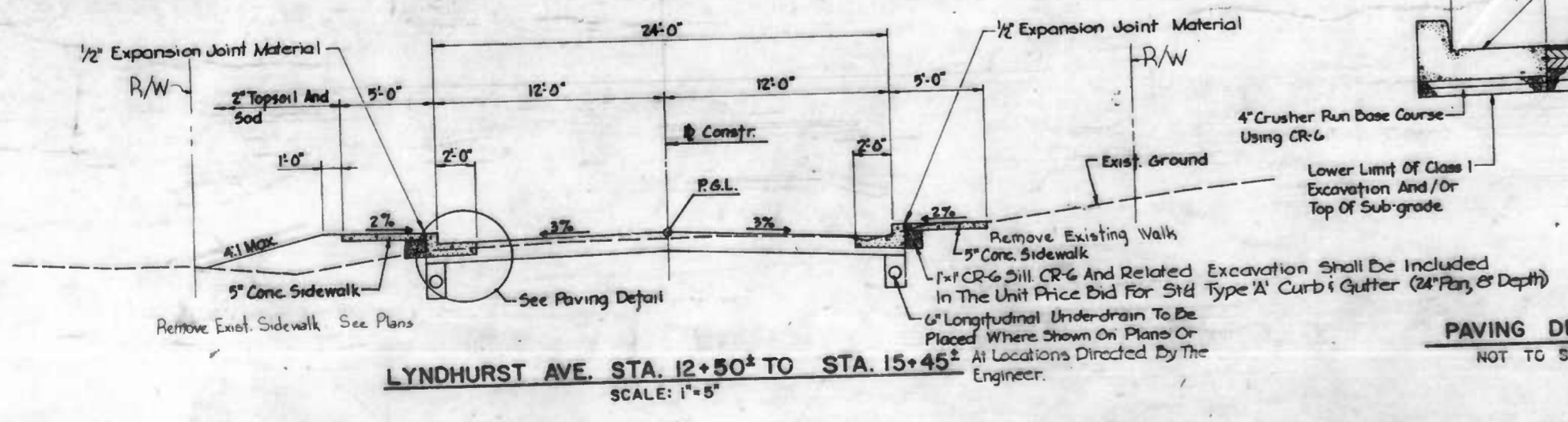
DETAIL FOR REPLACEMENT OF CURB IN EXISTING PAVEMENT  
NOT TO SCALE



LYNDHURST AVE. STA. 11+00<sup>±</sup> TO STA. 12+25<sup>±</sup>  
SCALE: 1"=8'



PAVING DETAIL  
NOT TO SCALE



LYNDHURST AVE. STA. 12+50<sup>±</sup> TO STA. 15+45<sup>±</sup>  
SCALE: 1"=8'

CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
BUREAU OF HIGHWAYS

CONTRACT NO. 2913  
LYNDHURST AVENUE  
TYPICAL SECTIONS

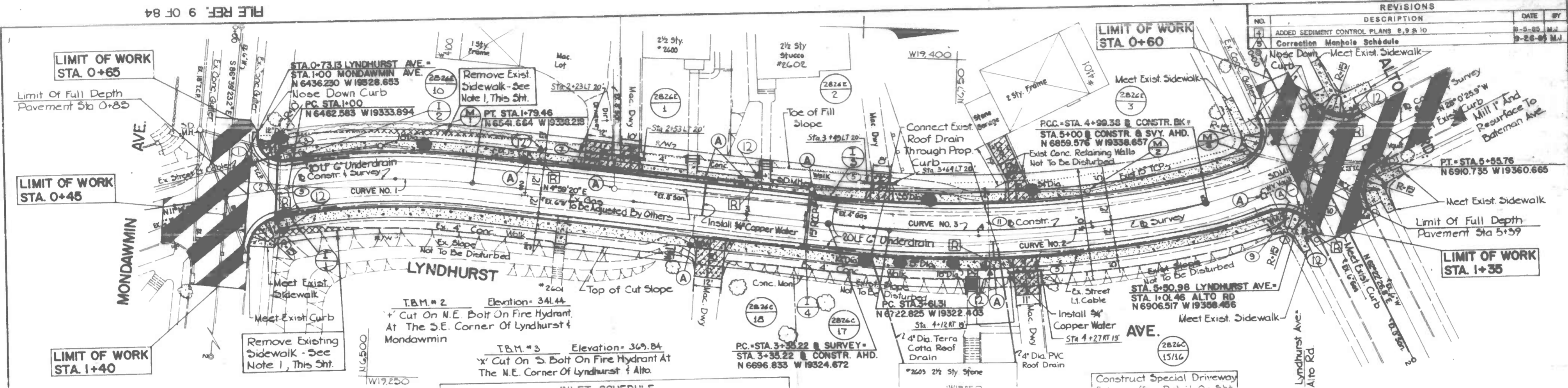
SCALE AS SHOWN DATE 4-17-85  
SHEET 2 OF 10

DRAWN BY R. CORINGRATO  
EXAMINED BY B. BROSIUS

FILE REF. 9 OF 84

FILE REF. 9 OF 84

REVISIONS		
NO.	DESCRIPTION	DATE BY
1	ADDED SEDIMENT CONTROL PLANS 8, 9 & 10	8-9-83 M.J.
2	Correction Manhole Schedule	8-28-83 M.J.



NO.	Δ	Dc	R	T	L	E
1	16°14'15.2"	20°15'59.3"	250.41'	40.00'	79.46'	2.54'
2	22°57'45.7"	12°58'10.2"	327.64'	100.00'	194.44'	14.50'
3	17°48'4.2"	17°05'44.2"	475.00'	82.91'	164.16'	7.18'

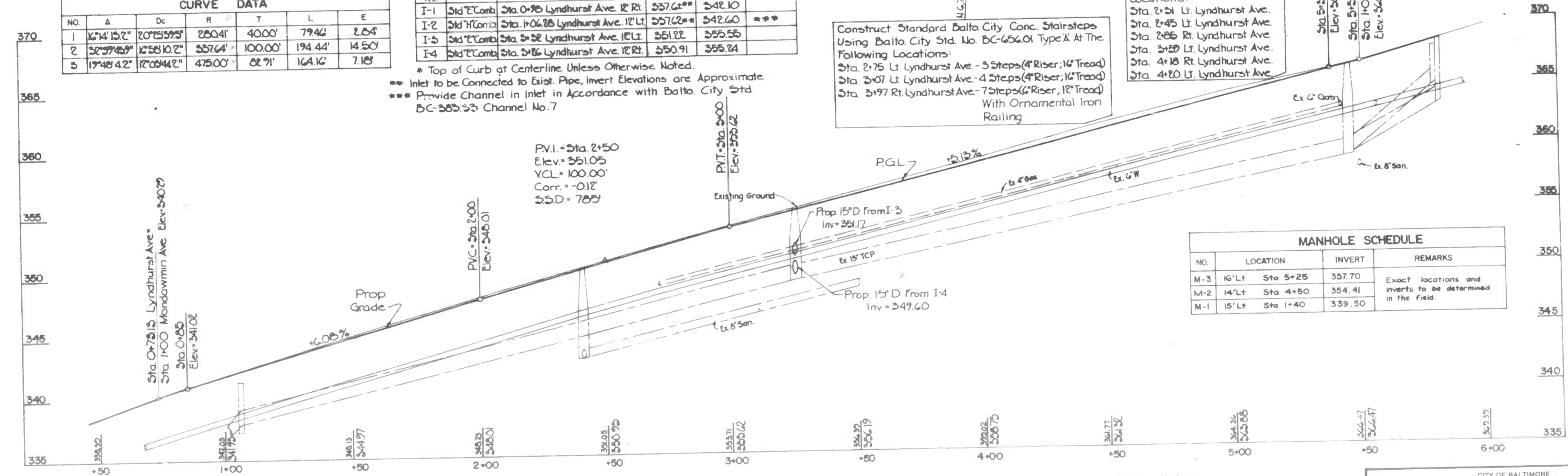
INLET NO.	TYPE	LOCATION	INVERT	TOP ELEVATION	REMARKS
I-1	Std 18" Comb	Sta 0+75 Lyndhurst Ave. 12' Rt.	337.62	342.10	
I-2	Std 18" Comb	Sta 1+06.25 Lyndhurst Ave. 12' Lt.	337.62	342.60	***
I-3	Std 18" Comb	Sta 3+52 Lyndhurst Ave. 12' Lt.	351.22	355.55	
I-4	Std 18" Comb	Sta 3+86 Lyndhurst Ave. 12' Rt.	350.91	355.24	

\* Top of Curb at Centerline Unless Otherwise Noted.  
 \*\* Inlet to be Connected to Exist. Pipe, Invert Elevations are Approximate.  
 \*\*\* Provide Channel in Inlet in Accordance with Balto. City Std. B.C. 355.33 Channel No. 7

**PLAN**  
Scale 1" = 20'

Construct Standard Balto City Conc. Stairsteps Using Balto City Std. No. B.C. 656.01 Type 'A' At The Following Locations:  
 Sta. 2+75 Lt Lyndhurst Ave. - 5 Steps (4" Riser, 14" Tread)  
 Sta. 3+07 Lt Lyndhurst Ave. - 4 Steps (4" Riser, 16" Tread)  
 Sta. 3+97 Rt Lyndhurst Ave. - 7 Steps (6" Riser, 12" Tread) With Ornamental Iron Railing

Construct Special Driveway Entrance (See Detail On Sht. 2 of 3) At The Following Locations:  
 Sta. 2+51 Lt Lyndhurst Ave.  
 Sta. 2+45 Lt Lyndhurst Ave.  
 Sta. 2+85 Rt Lyndhurst Ave.  
 Sta. 3+59 Lt Lyndhurst Ave.  
 Sta. 4+18 Rt Lyndhurst Ave.  
 Sta. 4+20 Lt Lyndhurst Ave.



NO.	LOCATION	INVERT	REMARKS
M-3	16" Lt Sta 5+25	357.70	Exact locations and inverts to be determined in the field.
M-2	14" Lt Sta 4+25	354.41	
M-1	15" Lt Sta 1+40	339.50	

LOCATION	SIZE
Sta. 1+00	22' Lt 12" Dia.
Sta. 1+61	15' Lt 18" Dia.
Sta. 3+52	15' Rt 10" Dia.
Sta. 3+67	15' Rt 3" Dia.
Sta. 3+78	13' Lt 33" Dia.
Sta. 3+87	15' Lt 18" Dia.
Sta. 4+25	17' Lt 34" Dia.

**PROFILE**  
Scale 1" = 20' Horiz  
1" = 4' Vert

For Street Light Notes and Charts For Hand Boxes and Street Light Bases, See Sheet 5 of 7.

**General Notes**  
 1. Areas of Removal of Existing 4" Conc. Sidewalks Shall be Backfilled, and Receive 2" Topsoil and Sodded.  
 2. See Sheet 5 of 7 for "Intersecting Street Profiles".  
 3. See Sheet 6 of 7 for "Intersection Grading Plans".

CITY OF BALTIMORE  
 DEPARTMENT OF PUBLIC WORKS  
 BUREAU OF HIGHWAYS

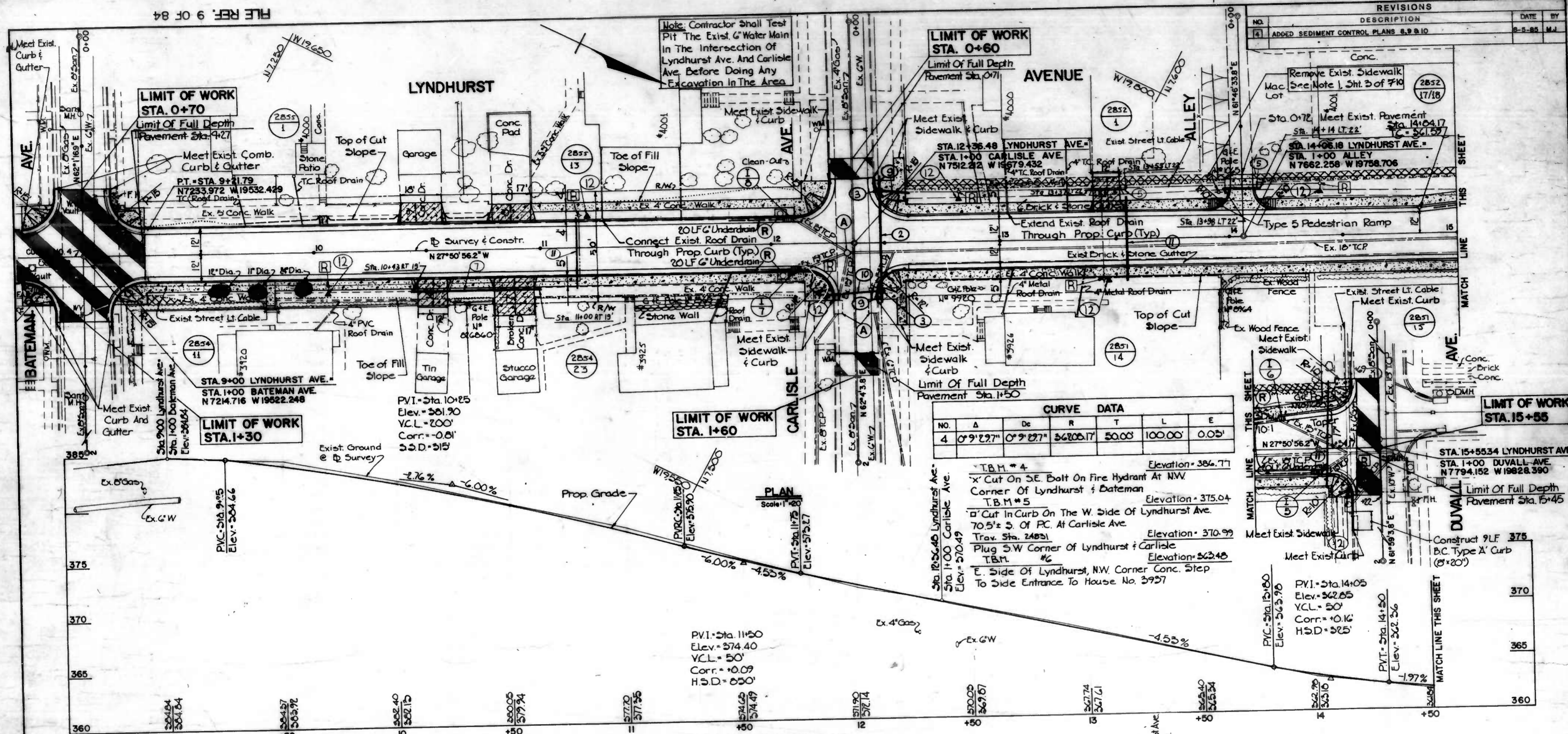
CONTRACT NO. 2913  
**LYNDHURST AVENUE**  
 MONDAWMIN AVE. TO ALTO ROAD

SCALE AS SHOWN  
 DATE 4-17-85  
 SHEET 3 OF 10

DRAWN BY: E. BREWER  
 EXAMINED BY: B. BROSIUS

FILE REF. 9 OF 84

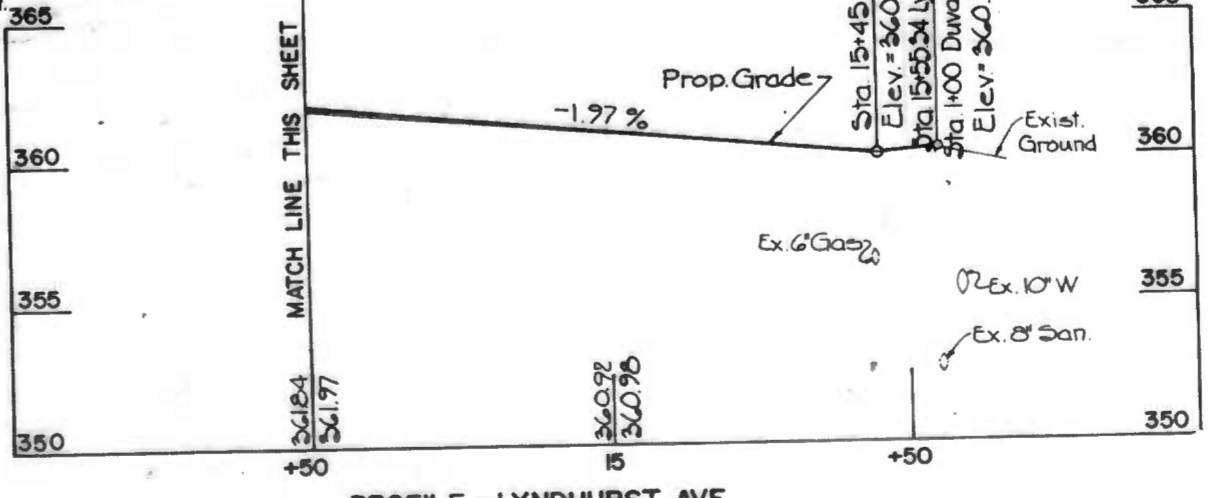
REVISIONS			
NO.	DESCRIPTION	DATE	BY
1	ADDED SEDIMENT CONTROL PLANS 8.9.810	8-5-85	MJ



NO.	Δ	Dc	R	T	L	E
4	0° 9' 27"	0° 9' 27"	36206.17'	50.00'	100.00'	0.05'

PLAN Scale 1" = 20'

PROFILE - LYNDHURST AVE. Scale 1" = 20' Horiz. 1" = 4' Vert.



PROFILE - LYNDHURST AVE. Scale 1" = 20' Horiz. 1" = 4' Vert.

INLET NO.	TYPE	LOCATION	INVERT OUT	TOP # ELEVATION	REMARKS
I-5	Std T Comb	Sta. 15+25 Lyndhurst Ave. 12" Rt.	355.94**	360.72	
I-6	Std T Comb	Sta. 15+12 Lyndhurst Ave. 14" Rt.	355.12**	361.03	
I-7	Std T Comb	Sta. 12+11.5 Lyndhurst Ave. 12" Rt.	368.72**	371.91	
I-8	Std T Comb	Sta. 12+08.5 Lyndhurst Ave. 12" Lt.	368.52**	372.04	

\* Top of Curb at Centerline Unless Otherwise Noted.  
 \*\* Inlet to be Connected to Exist. Pipe, Invert Elevations are Approximate.

Construct Typical Driveway Entrance Balto. City Std. No. DC-500.01 At The Following Locations:  
 Sta. 10+50 Lt. Lyndhurst Ave.  
 Sta. 10+88 Lt. Lyndhurst Ave.

Construct Special Driveway Entrance (See Detail On Sht 2 of 7) At The Following Locations:  
 Sta. 10+50 Rt. Lyndhurst Ave.  
 Sta. 10+88 Rt. Lyndhurst Ave.  
 Sta. 13+46 Lt. Lyndhurst Ave.

LOCATION	SIZE
Sta. 9+58	17" Rt. 12" Dia.
Sta. 9+75	17" Rt. 17" Dia.
Sta. 9+95	16" Rt. 24" Dia.

For Street Light Notes, And Charts For Hand Boxes And Street Light Bases, See Sheet 5 of 7.10

- 1. See Sheet 6 of 7 for "Intersection Grading Plan".
- 2. See Sheet 5 of 7 for "Intersecting Street Profiles".

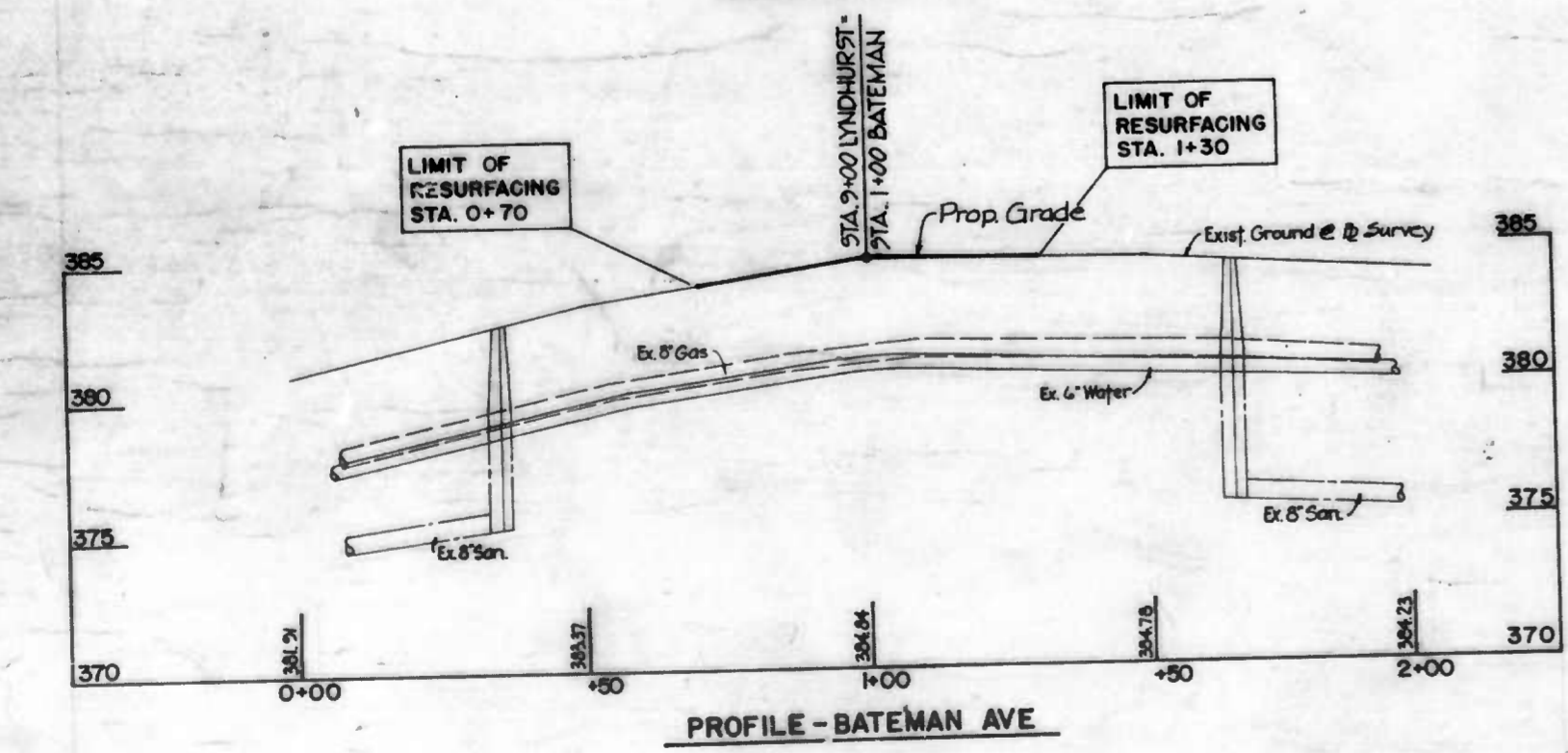
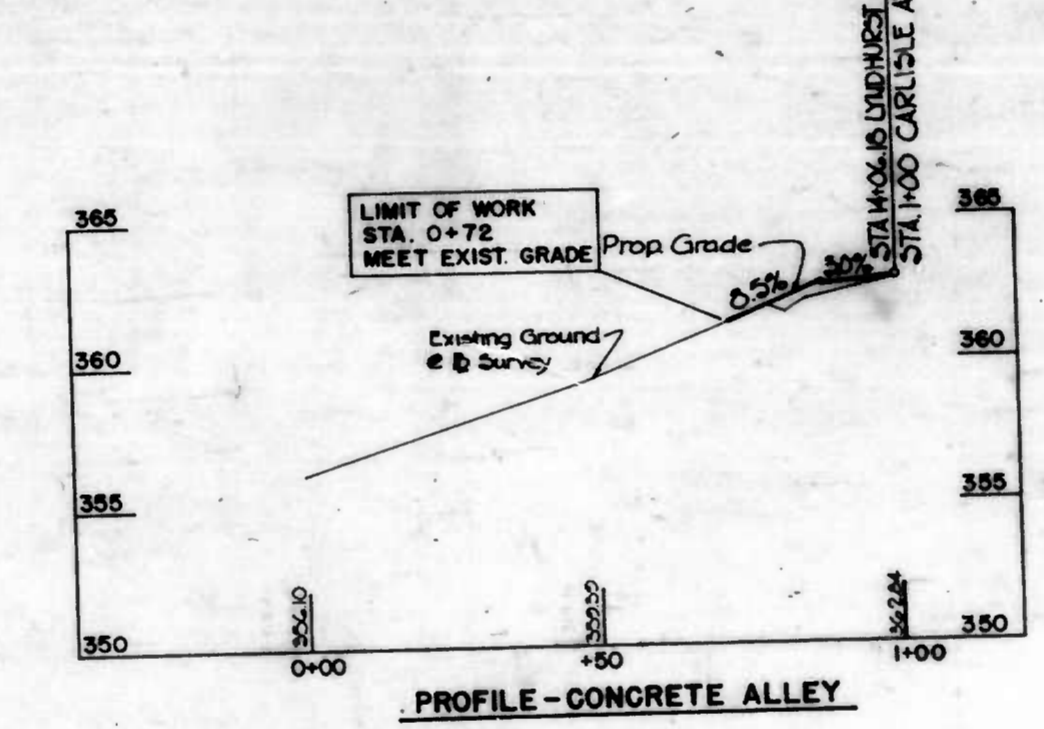
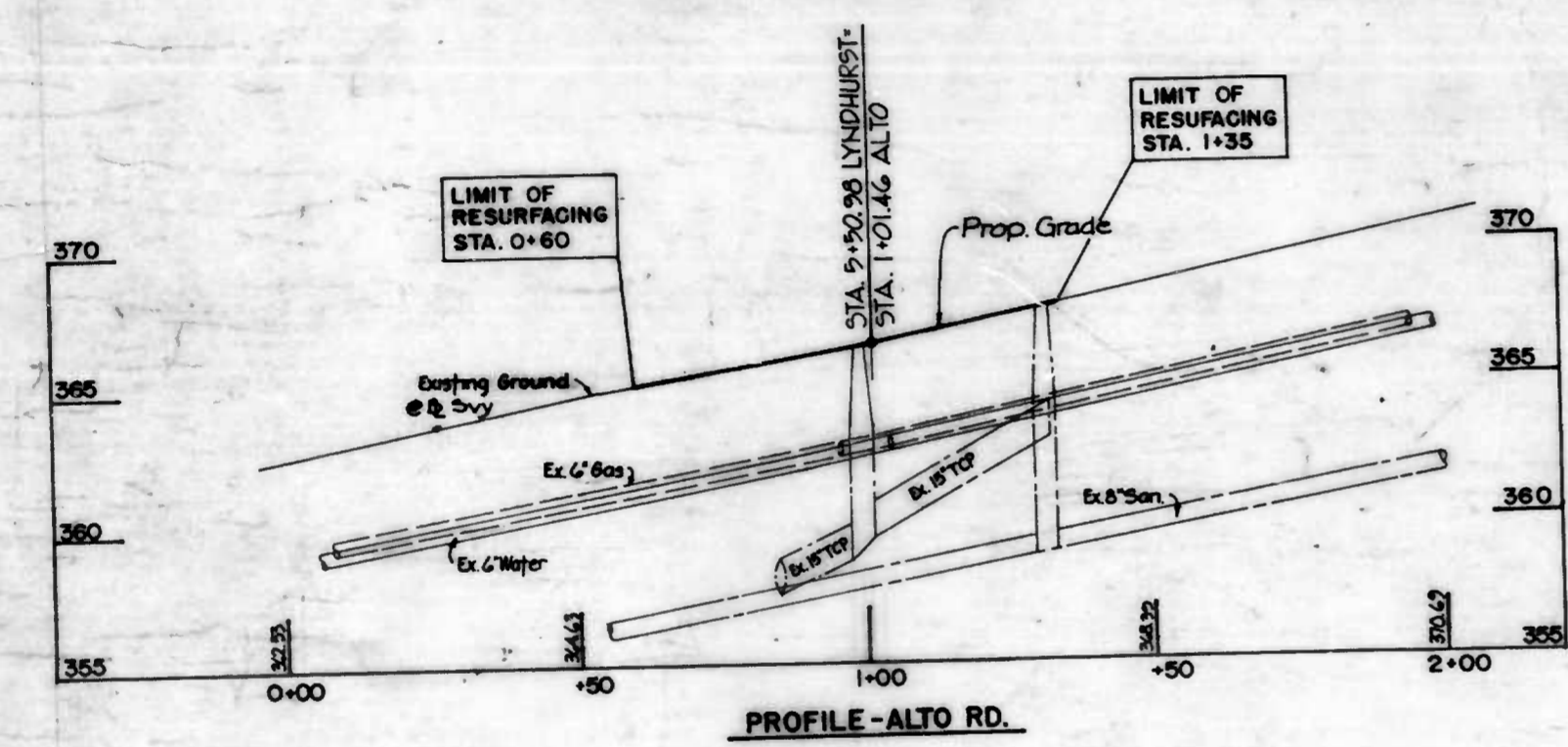
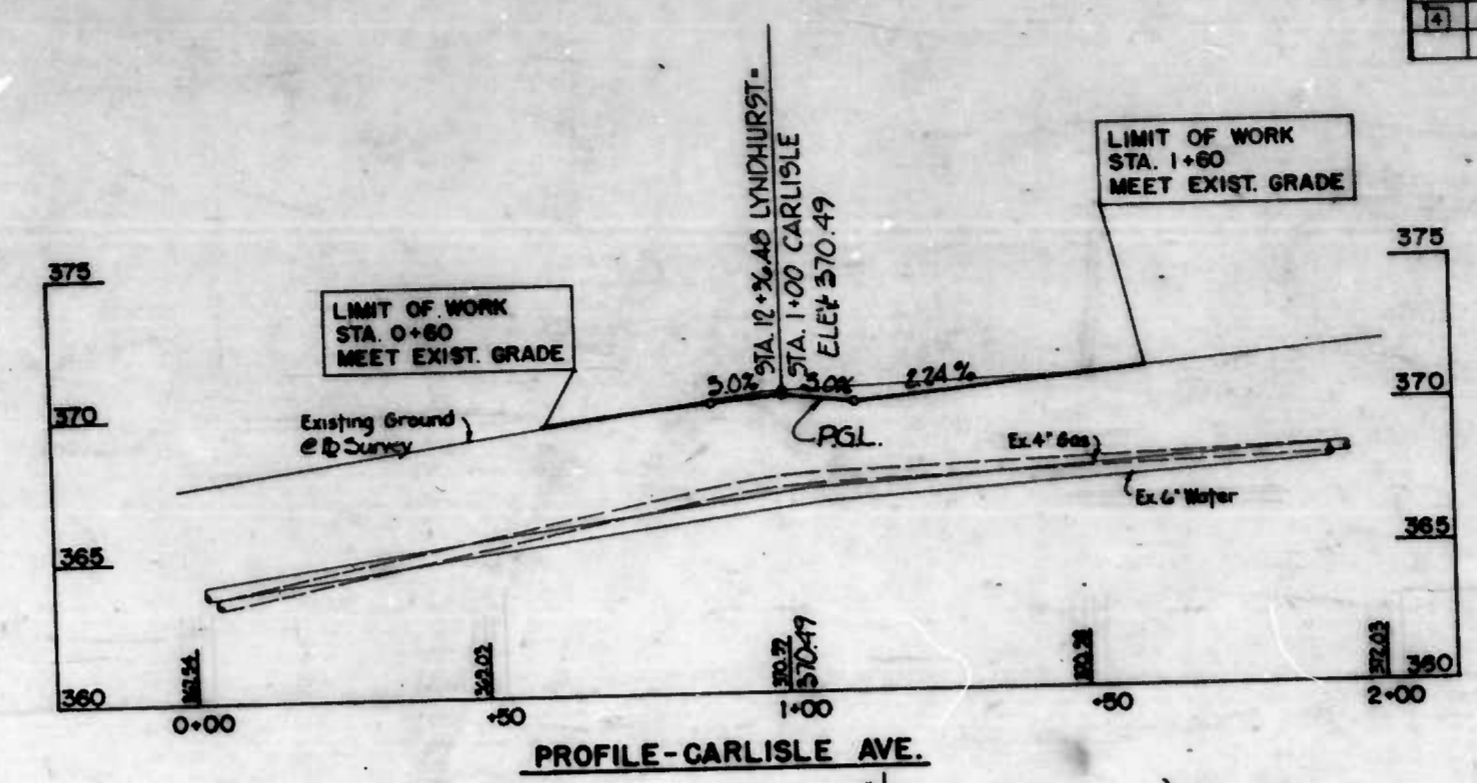
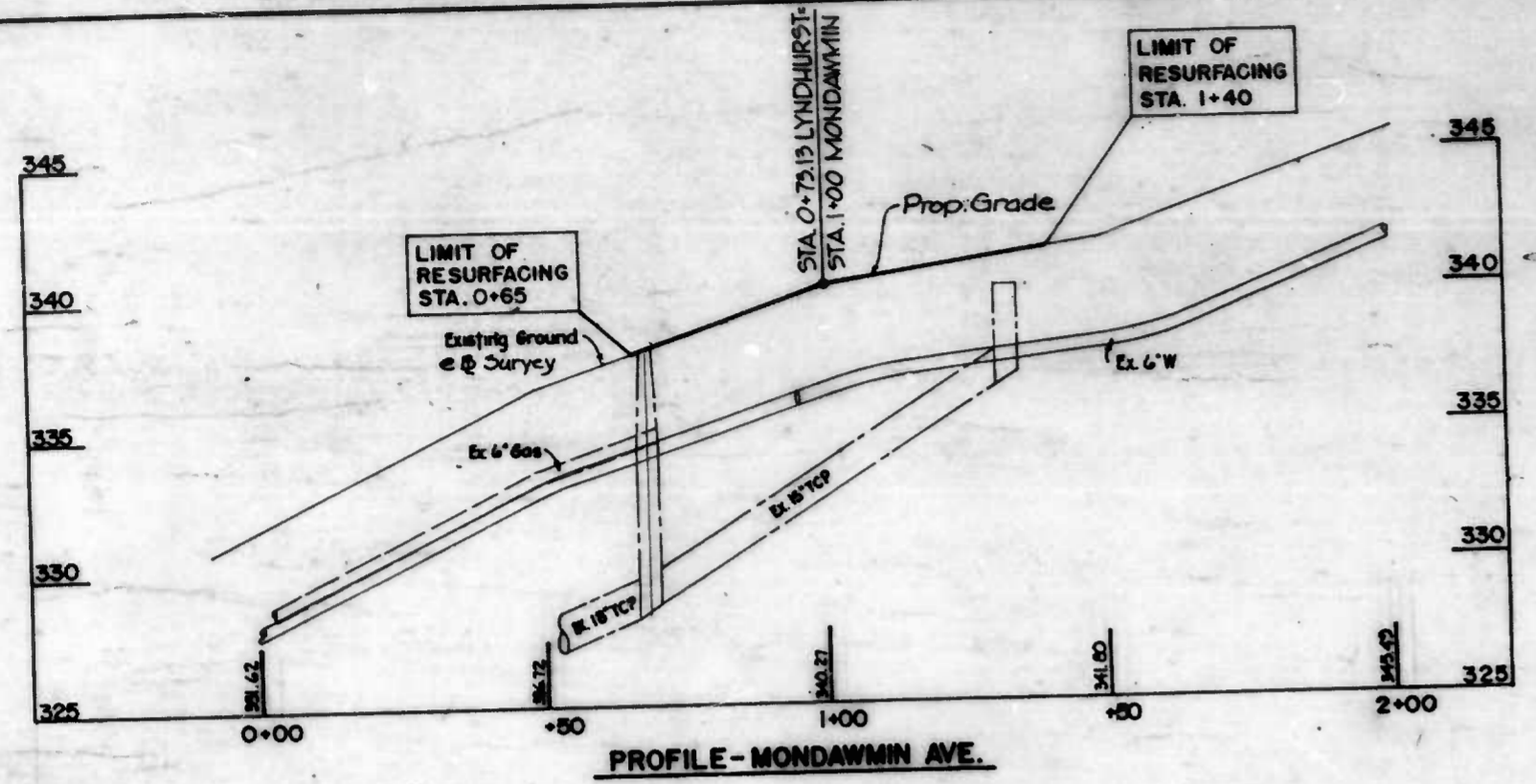
CITY OF BALTIMORE  
 DEPARTMENT OF PUBLIC WORKS  
 BUREAU OF HIGHWAYS  
 CONTRACT NO. 2913  
 LYNDHURST AVENUE  
 BATEMAN AVE. TO DUVAL AVE.

SCALE AS SHOWN DATE 4-17-85 SHEET 4 OF 10

DRAWN BY E. BREWER  
 EXAMINED BY R. BROSIUS

FILE REF. 9 OF 84

REVISIONS		
NO.	DESCRIPTION	DATE BY
15	ADDED SEDIMENT CONTROL PLANS 8, 9 & 10	3-5-85 M.J.



- Street Lighting Notes
- ① Install new hand box on existing duct line. For location, see chart.
  - ② Install 1-3" duct from new hand box to new hand box.
  - ③ Install new hand box. For location, see chart.
  - ④ Install 1-3" duct crossing from new hand box as shown. Dead end and cap 3' inside new curb.
  - ⑤ Install 1-3" duct across new driveway as shown. Dead end and cap duct 3' past edge of drive.
  - ⑥ Install 1-3" duct crossing as shown. Dead end and cap duct 3' inside new curb.
  - ⑦ Install 1-3" duct crossing across both driveways as shown. Install a continuous duct run until both driveways are cleared. Dead end and cap duct 3' past the edge of driveway as shown.
  - ⑧ Install 1-3" duct from new hand box to 3' inside of new curb as shown. Dead end and cap.
  - ⑨ Install 1-3" duct from hand box to 2' outside of new sidewalk dead end and cap as shown.
  - ⑩ Install 1-3" duct from hand box to pole base as shown.
  - ⑪ Install 1-3" duct crossing. Extend 2' past new sidewalk on each side as shown.
  - ⑫ Light bases to be installed by others. See chart for location.

General Notes

For hand box installation and standards, see B.C. 804.01 - .09.

For duct installation, see B.C. 824.01.

Light base to be installed by BG+E. Co.

Station	Offset
0+95	18' RT.
2+06	18' LT.
3+03	18' LT.
4+05	18' RT.
5+34	26' RT.
5+77	18' LT.
10+06	18' RT.
11+20	18' LT.
12+24	24' RT.
12+82	18' LT.
13+47	18' RT.
14+41	18' LT.
15+40	20' RT.

Station	Offset
0+87	13' LT.
0+96	15' LT.
5+35	16' LT.
5+38	15' RT.
12+49	22' RT.
12+49	23' LT.

Duct Crossing	
Carlisle Ave Station	
1+23	
Lyndhurst Ave. Station	
0+95	
4+03	
11+16	
12+48	
13+54	
15+32	

CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
BUREAU OF HIGHWAYS

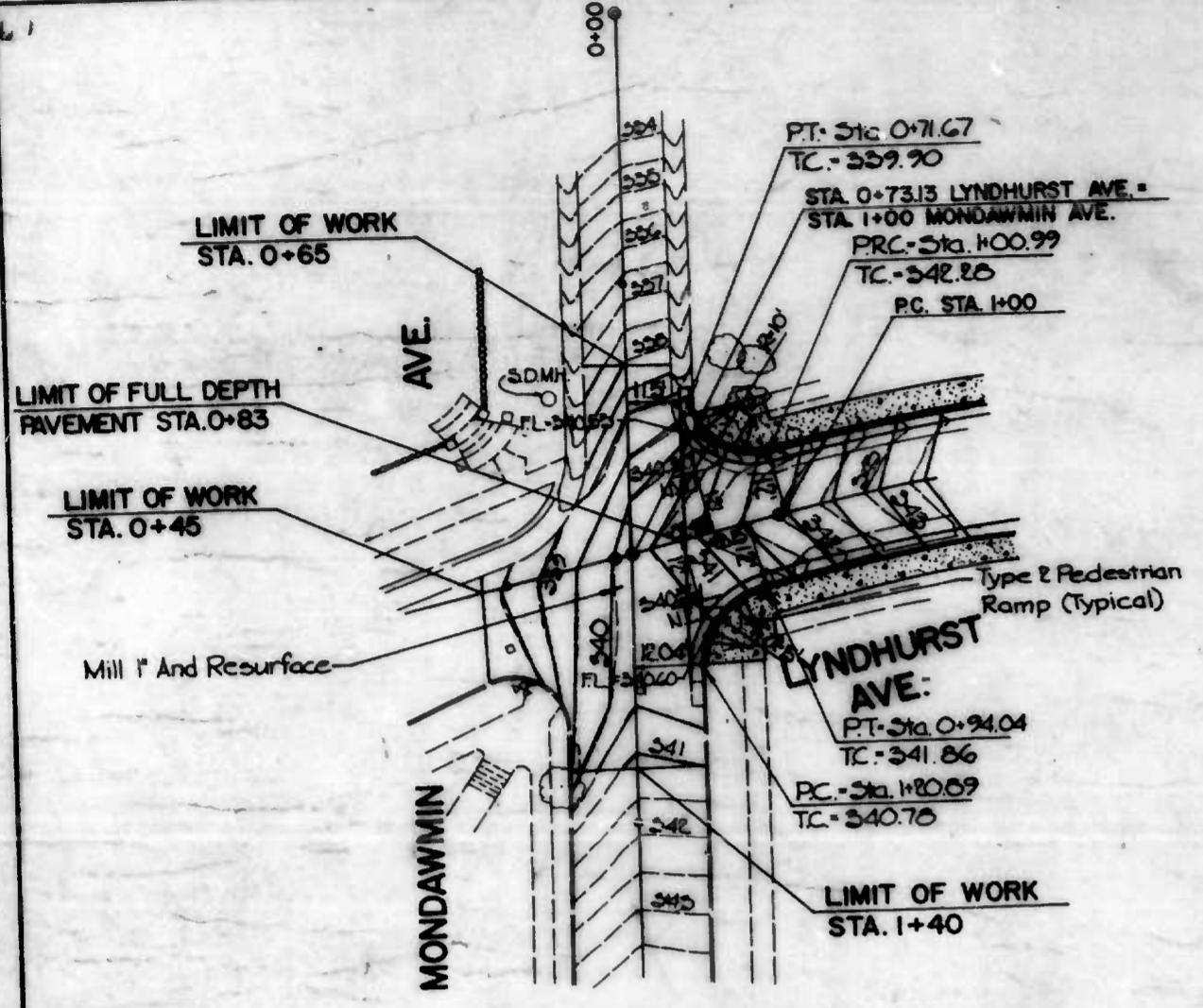
CONTRACT NO. 2913  
**LYNDHURST AVENUE**  
ROADWAY PROFILES  
INTERSECTING STREETS

SCALE: 1"=20' HORIZ. 1"=4' VERT. DATE: 4-17-85  
SHEET 5 OF 10

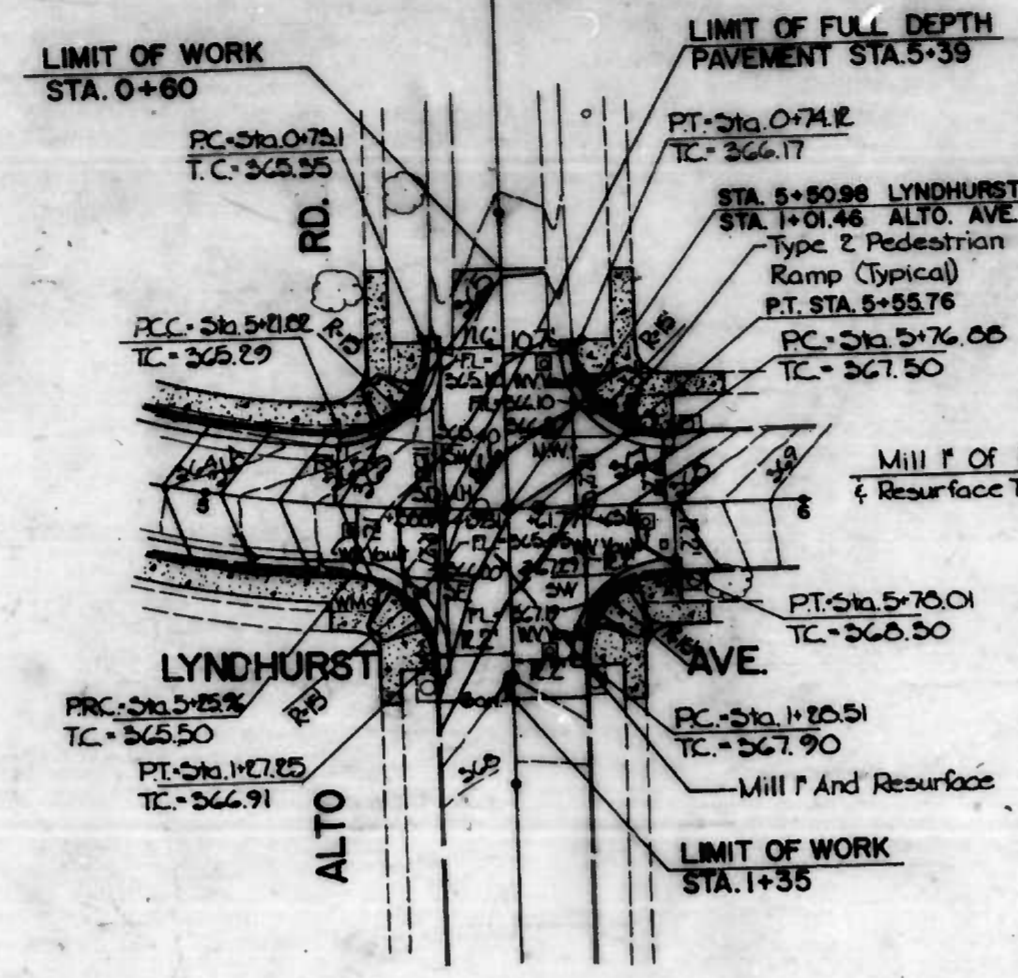
DRAWN BY: E. BREWER  
EXAMINED BY: B. BROSIUS

FILE REF. 9 OF 84

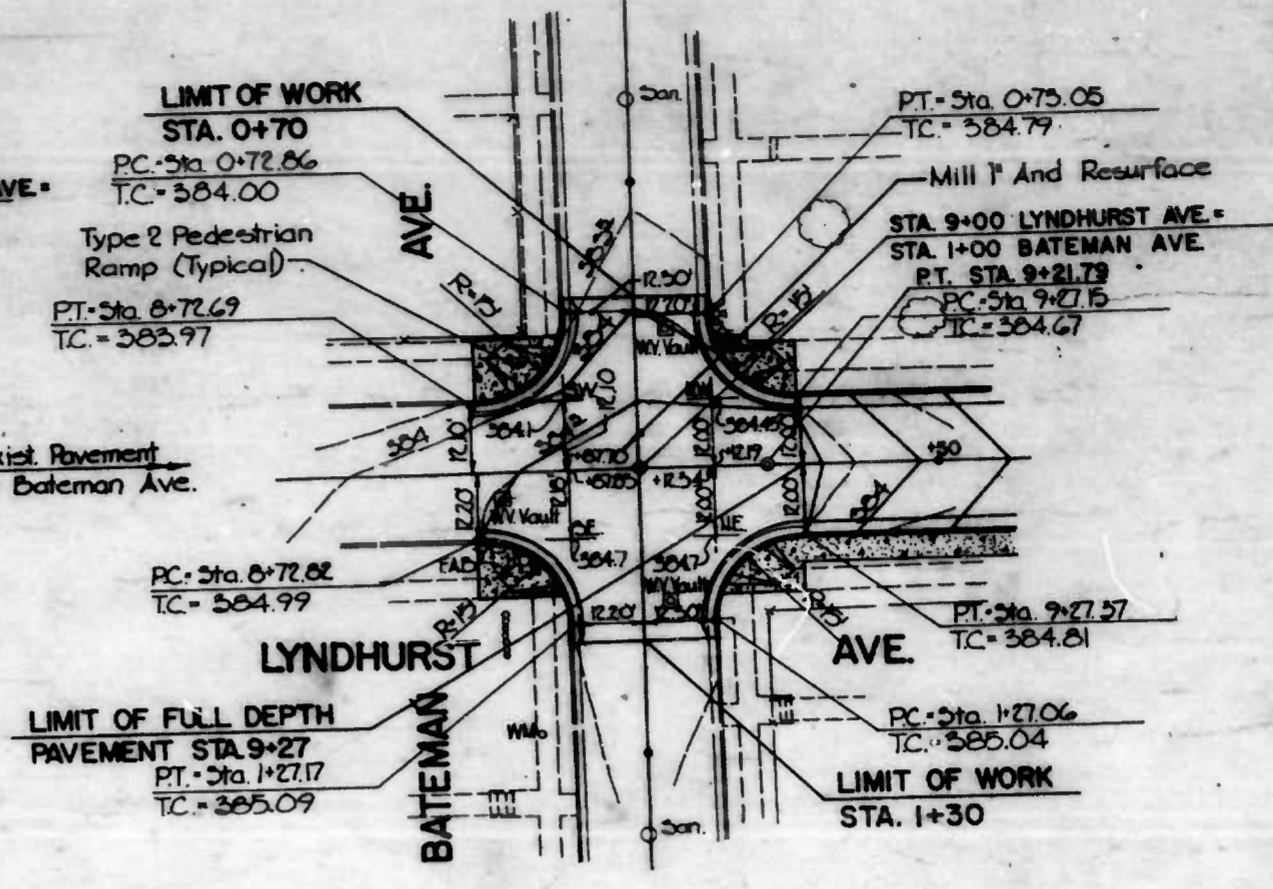
REVISIONS		
NO.	DESCRIPTION	DATE BY
1	ADDED SEDIMENT CONTROL PLANS	8-5-85 M.J.



INTERSECTION DETAIL  
MONDAWMIN AVE. - LYNDHURST AVE.  
Scale: 1" = 20'

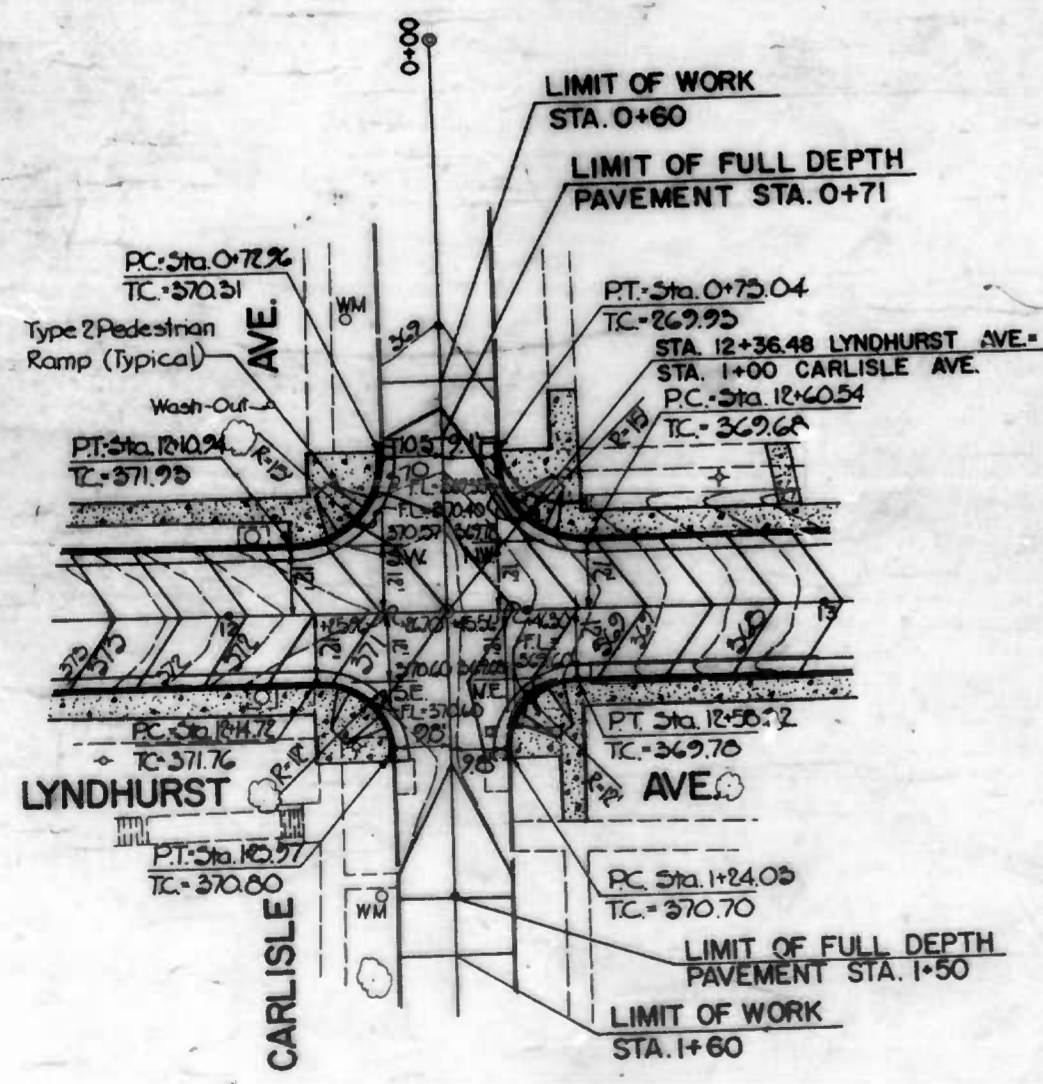


INTERSECTION DETAIL  
ALTO RD. - LYNDHURST AVE.  
Scale: 1" = 20'

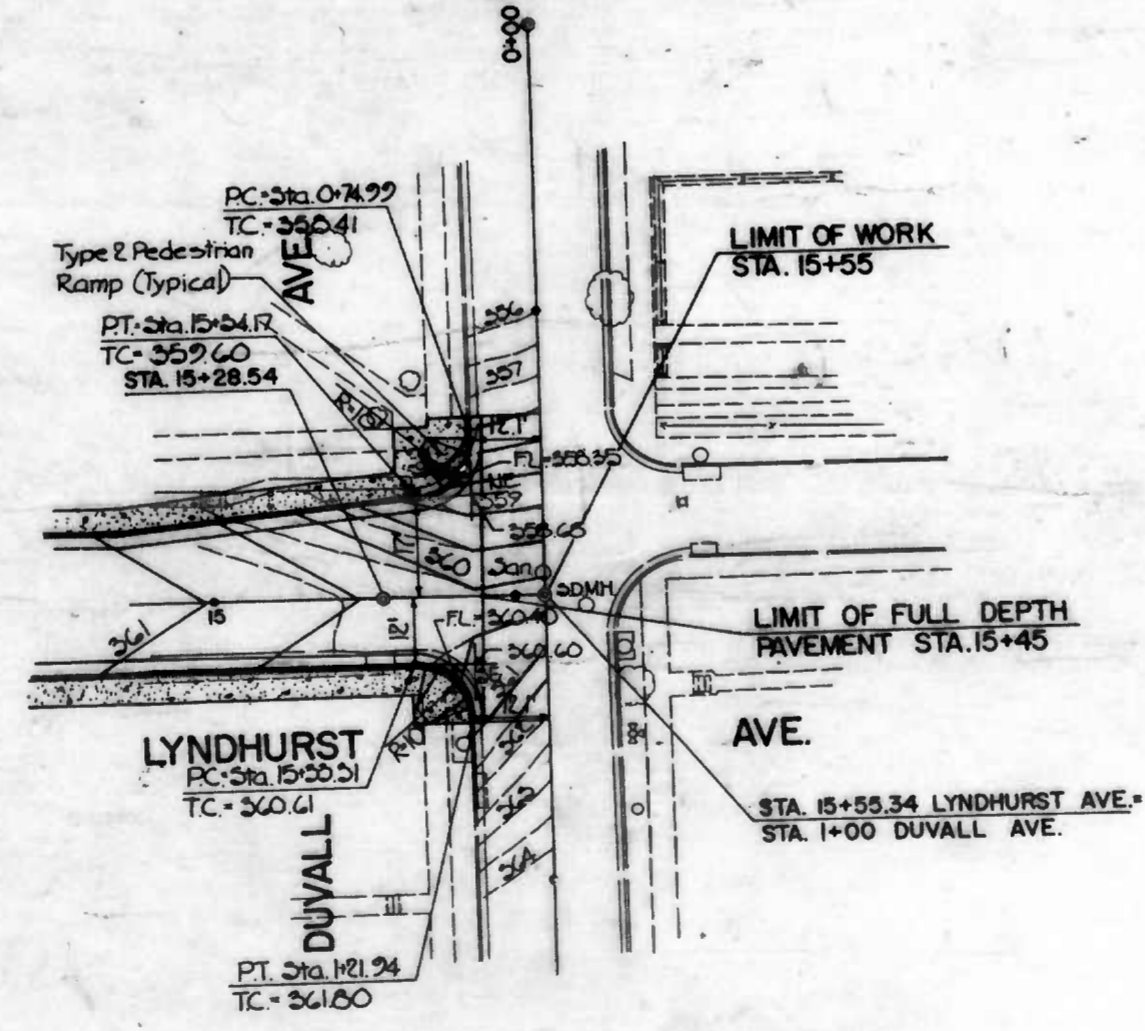


INTERSECTION DETAIL  
BATEMAN AVE. - LYNDHURST AVE.  
Scale: 1" = 20'

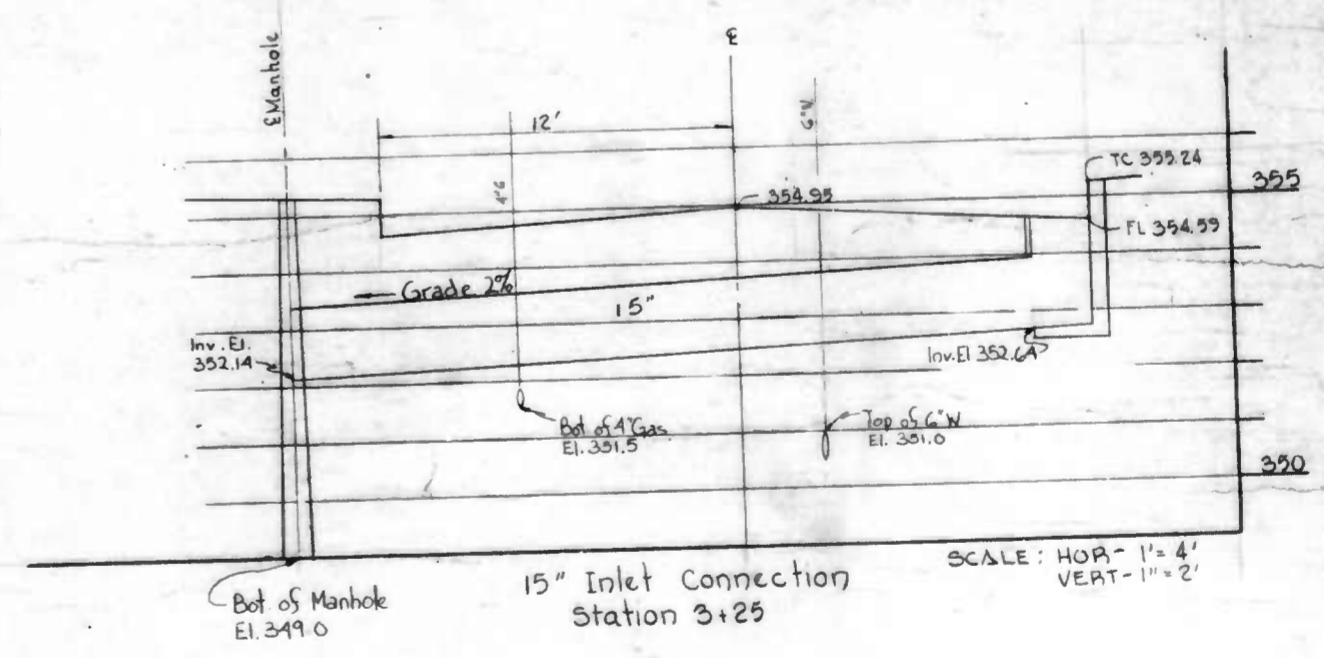
Note: Mill # And Resurface Lyndhurst Ave. From Alto Rd. To Bateman Ave. As Shown On Plans.



INTERSECTION DETAIL  
CARLISLE AVE. - LYNDHURST AVE.  
Scale: 1" = 20'



INTERSECTION DETAIL  
DUVALL AVE. - LYNDHURST AVE.  
Scale: 1" = 20'



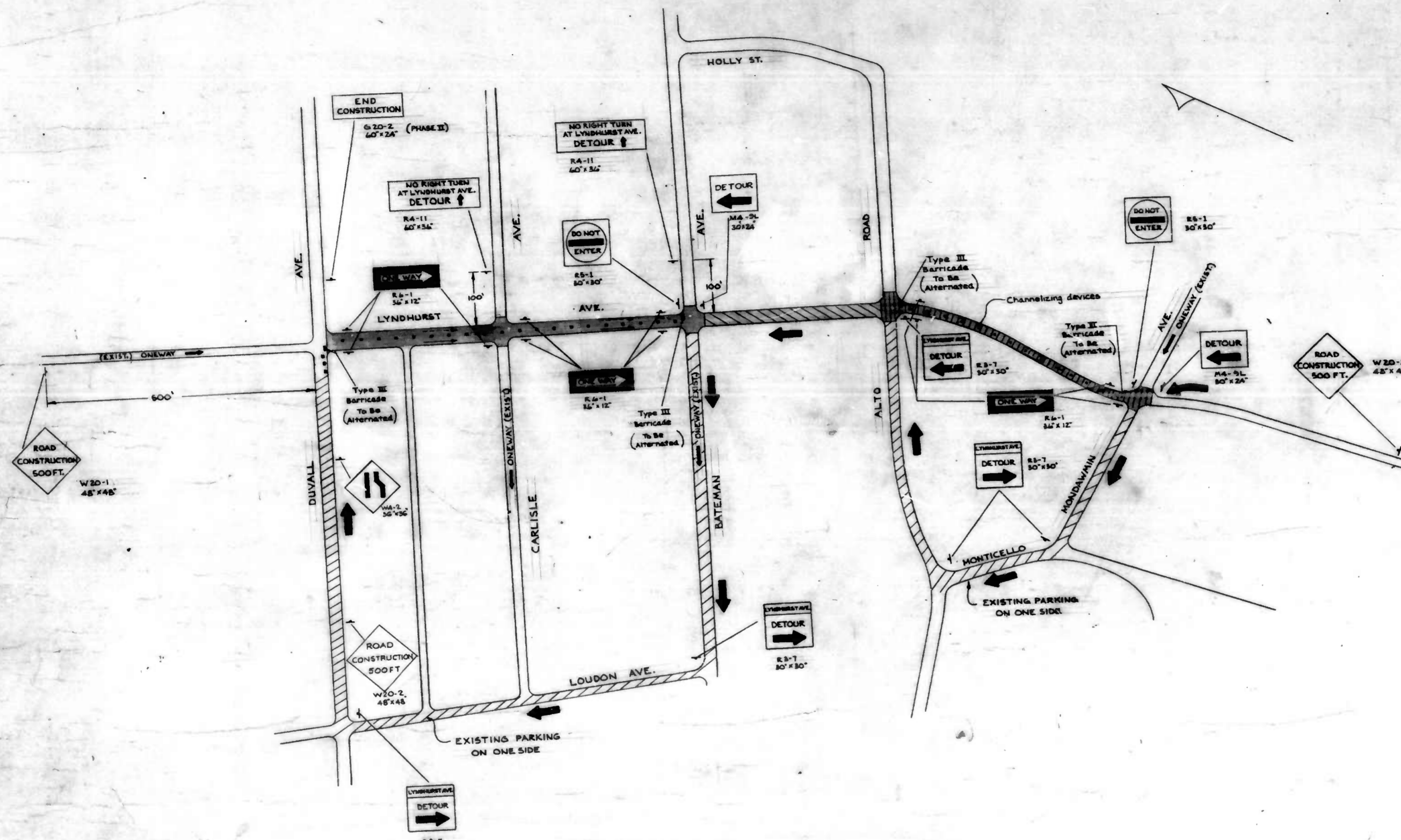
CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
BUREAU OF HIGHWAYS

CONTRACT NO. 2913  
LYNDHURST AVENUE  
INTERSECTION GRADING  
PLANS

SCALE: 1" = 20'      DATE: 4-17-85  
SHEET 6 OF 10

DRAWN BY: E. BREWER  
EXAMINED BY: B. BROSIUS

REVISIONS			
NO.	DESCRIPTION	DATE	BY
1	ADDED SEDIMENT CONTROL PLANS 8,9,10	8-5-85	J



NOTES:

1. "ROAD CONSTRUCTION AHEAD" and "END CONSTRUCTION" signs are to be installed when construction begins for each phase.
2. "ONEWAY", "DO NOT ENTER" and "TURN PROHIBITION" signs are to be furnished and installed by the Department of Transit and Traffic upon two weeks notice.
3. "DETOUR" signs are to be installed or uncovered by the contractor no more than one whole day in advance of one-way operation.
4. Advance detour advisory signs are to be covered until one-way operation commences.
5. Access and parking shall be provided for all local residents.
6. Streets will revert back to two-way operation after construction.

SEQUENCE OF CONSTRUCTION

PHASE I: CONSTRUCT ROAD BETWEEN MONDAWMIN AND ALTO AND RESURFACE BETWEEN ALTO AND BATEMAN

- a. Do all utility work such as Drainage, Lighting etc.
- b. Construct new roadway, curb and gutter and side walk area between Mondawmin and Alto on east side while maintaining traffic on the west side.
- c. Reverse procedure and construct west side while maintaining traffic on the east side.
- d. Do the milling operation between Alto Road and Bateman Ave alternately on west side and east side while maintaining two way operation within the segment by flagging operation.
- e. Perform the surfacing operation between Mondawmin Ave and Bateman Ave on a lane by lane basis while maintaining traffic on the other side.

TRAFFIC MAINTENANCE PROVISIONS

- a. Northbound traffic shall be detoured via Mondawmin, Monticello and Alto as shown during the construction between Mondawmin Ave and Alto Road. During the milling and resurfacing operation, between Alto and Bateman two way traffic shall be maintained by flagging operation.

PHASE II CONSTRUCT BETWEEN BATEMAN AND DUVALL AVENUE.

Follow the construction sequence of Phase I.

TRAFFIC MAINTENANCE PROVISIONS

- Northbound traffic shall be detoured via Bateman Ave, Loudon Ave and Duvall Avenue.

LEGEND

- Phase I Milling and resurfacing (Alto to Bateman)
- Phase I Construction (Mondawmin to Alto)
- Phase II Construction (Bateman to Duvall)
- Southbound one-way movement during construction
- Detour Roadway
- Proposed detour direction
- Channelization devices with steady burn lights
- Signs on standard fixed supports

CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
BUREAU OF HIGHWAYS

CONTRACT NO. 2913

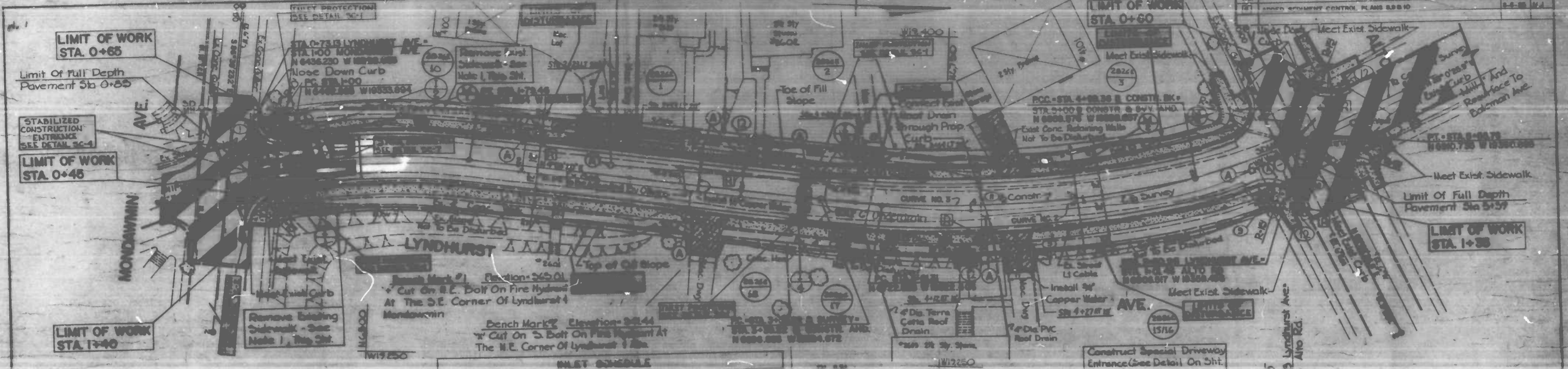
LYNDHURST AVENUE  
MONDAWMIN AVE TO DUVALL AVE  
TRAFFIC CONTROL PLAN

SCALE 1" = 100' DATE: 4-17-85  
SHEET 7 OF 10

DRAWN BY: A.J.L.  
EXAMINED BY: K.M.D.

FILE REF. 9 OF 84

NO.	DESCRIPTION	DATE
101	AMENDMENT CONTROL PLANS 8.8.8.10	8-8-85 W.J.

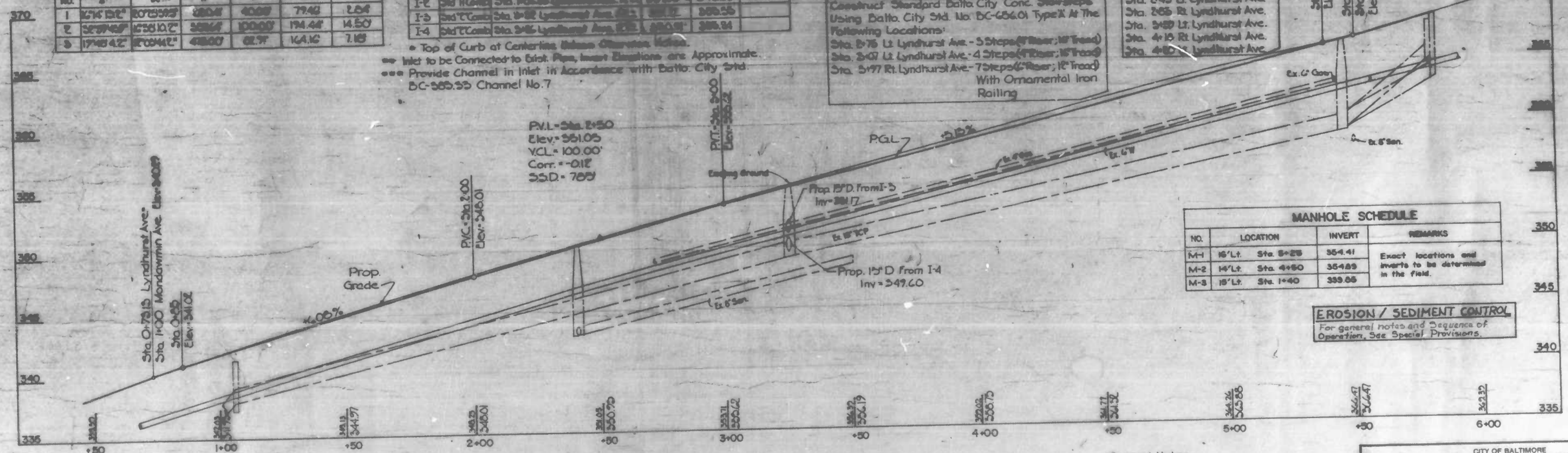


NO.	A	B	C	D	E
1	1274.132'	1078.997'	2804'	4008'	7742'
2	3279.66'	1758.102'	3284'	10000'	17444'
3	1748.42'	1700.412'	4200'	617'	14.16'

INLET NO.	TYPE	LOCATION	INVERT	TOP OF CURB	REMARKS
I-1	Std 18" Comb	Sta 0+85 Lyndhurst Ave. R. 21	357.65	342.10	
I-2	Std 18" Comb	Sta 1+05 Lyndhurst Ave. R. 15	357.65	342.60	***
I-3	Std 18" Comb	Sta 3+82 Lyndhurst Ave. R. 15	357.65	342.55	
I-4	Std 18" Comb	Sta 3+82 Lyndhurst Ave. R. 15	357.65	342.55	

Construct Special Driveway Entrance (See Detail On Sht. 2 of 7) At The Following Locations:  
 Sta 2+31 Lt. Lyndhurst Ave.  
 Sta 2+45 Lt. Lyndhurst Ave.  
 Sta 2+55 Rt. Lyndhurst Ave.  
 Sta 3+89 Lt. Lyndhurst Ave.  
 Sta 4+15 Rt. Lyndhurst Ave.  
 Sta 4+80 Lyndhurst Ave.

Construct Standard Balto City Conc. Stairsteps Using Balto City Std. No. DC-656.01 Type X At The Following Locations:  
 Sta. 2+75 Lt. Lyndhurst Ave. - 5 Steps (4' Rise; 16" Tread)  
 Sta. 3+07 Lt. Lyndhurst Ave. - 4 Steps (4' Rise; 16" Tread)  
 Sta. 3+77 Rt. Lyndhurst Ave. - 7 Steps (4' Rise; 16" Tread) With Ornamental Iron Railing



NO.	LOCATION	INVERT	REMARKS
M-1	15' Lt. Sta. 3+25	354.41	Exact locations and inverts to be determined in the field.
M-2	15' Lt. Sta. 4+50	354.89	
M-3	15' Lt. Sta. 1+40	359.65	

**EROSION / SEDIMENT CONTROL**  
 For general notes and Sequence of Operation, See Special Provisions.

LOCATION	SIZE
Sta 1+00	22' Lt 12" Dia.
Sta 1+61	15' Lt 18" Dia.
Sta 3+52	15' Rt 10" Dia.
Sta 3+67	15' Rt 8" Dia.
Sta 3+78	15' Lt 58" Dia.
Sta 3+89	15' Rt 18" Dia.
Sta 4+25	17' Lt 54" Dia.

**PROFILE**  
 Scale: 1" = 20' Horiz.  
 1" = 4' Vert.

For Street Light Notes, And Charts For Hand Boxes And Street Light Bases, See Sheet 5 of 7.10

- General Notes:**
1. Areas of Removal of Existing 4' Conc. Sidewalks Shall Be Backfilled, and Receive 2" Topsoil and Sodded.
  2. See Sheet 5 of 7 for "Intersecting Street Profiles".
  3. See Sheet 6 of 7 for "Intersection Grading Plans".

CITY OF BALTIMORE  
 DEPARTMENT OF PUBLIC WORKS  
 BUREAU OF HIGHWAYS

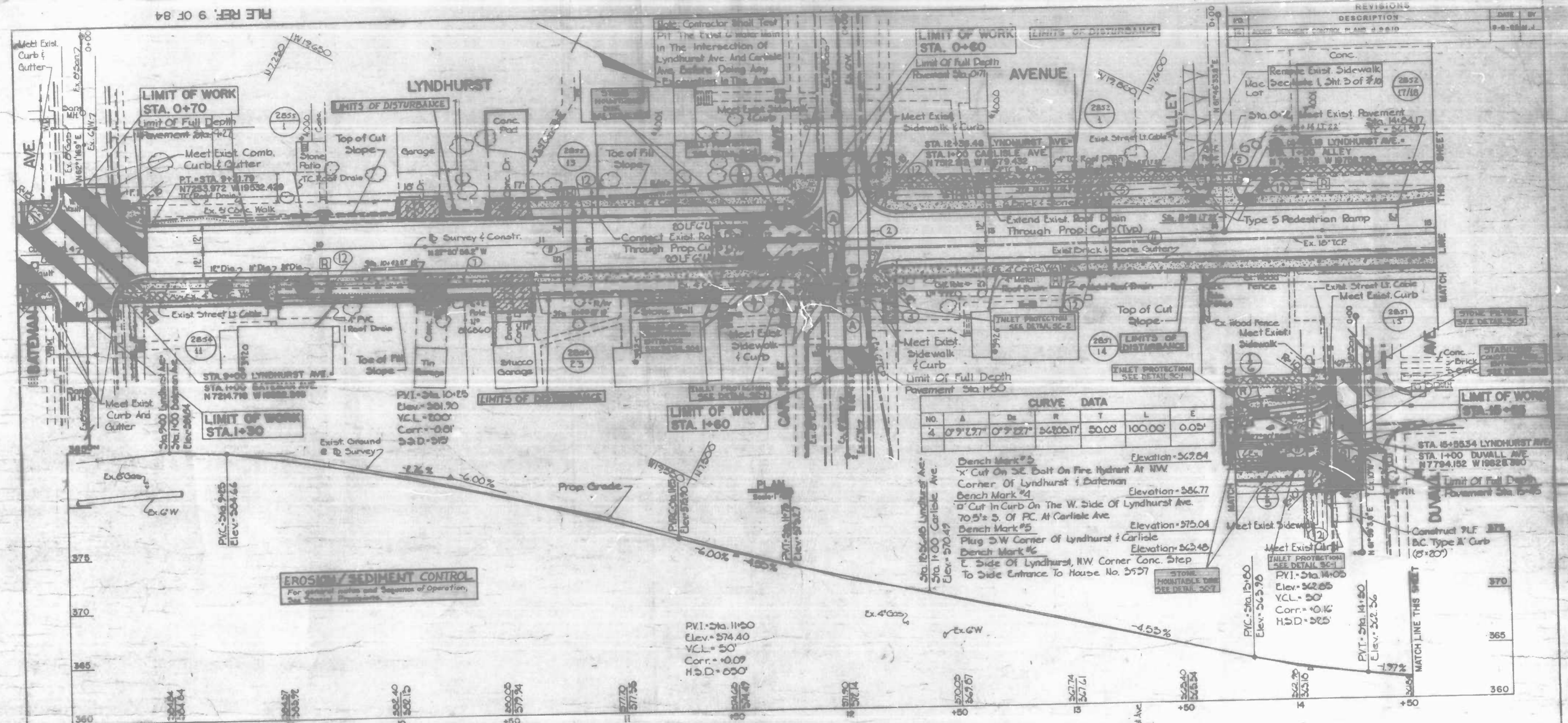
CONTRACT NO. 2913  
**LYNDHURST AVENUE**  
 MONDAWMIN AVE. TO ALTO ROAD  
**SEDIMENT CONTROL PLAN**  
 (PHASE 1)

SCALE AS SHOWN DATE 4-17-85  
 HIGHWAY ENGINEERING DIVISION SHEET 8 OF 10

DRAWN BY E. BREWER  
 EXAMINED BY S. BROSIUS



FILE REF. 9 OF 84



NO.	A	B	R	T	L	E
4	0+72.77	0+72.77	54205.17	50.03	100.00	0.05

Bench Mark #5 Elevation = 567.64  
 "X" Cut On SE Bolt On Fire Hydrant At NW Corner Of Lyndhurst & Bateman  
 Bench Mark #4 Elevation = 566.77  
 "O" Cut In Curb On The W. Side Of Lyndhurst Ave. 70.5' S. Of P.C. At Carville Ave.  
 Bench Mark #5 Elevation = 575.04  
 Plug SW Corner Of Lyndhurst & Carville  
 Bench Mark #6 Elevation = 562.40  
 E Side Of Lyndhurst, NW Corner Conc. Strip To Side Entrance To House No. 3537

**EROSION / SEDIMENT CONTROL**  
 For general nature and sequence of operation, see Special Provisions.

Note: Top of Curb Elevations are 0.25' ft. Above Profile Grade Elevations Shown.

INLET NO.	TYPE	LOCATION	INVERT OUT.	TOP OF ELEVATION	REMARKS
I-5	Std T'Comb	Sta. 15+25 Lyndhurst Ave. Rt. Lt.	555.91**	560.72	
I-6	Std T'Comb	Sta. 15+12 Lyndhurst Ave. Lt. Rt.	555.12**	561.03	
I-7	Std T'Comb	Sta. 12+15 Lyndhurst Ave. Rt. Lt.	568.72**	571.91	
I-8	Std T'Comb	Sta. 12+05 Lyndhurst Ave. Lt. Lt.	568.51**	572.04	

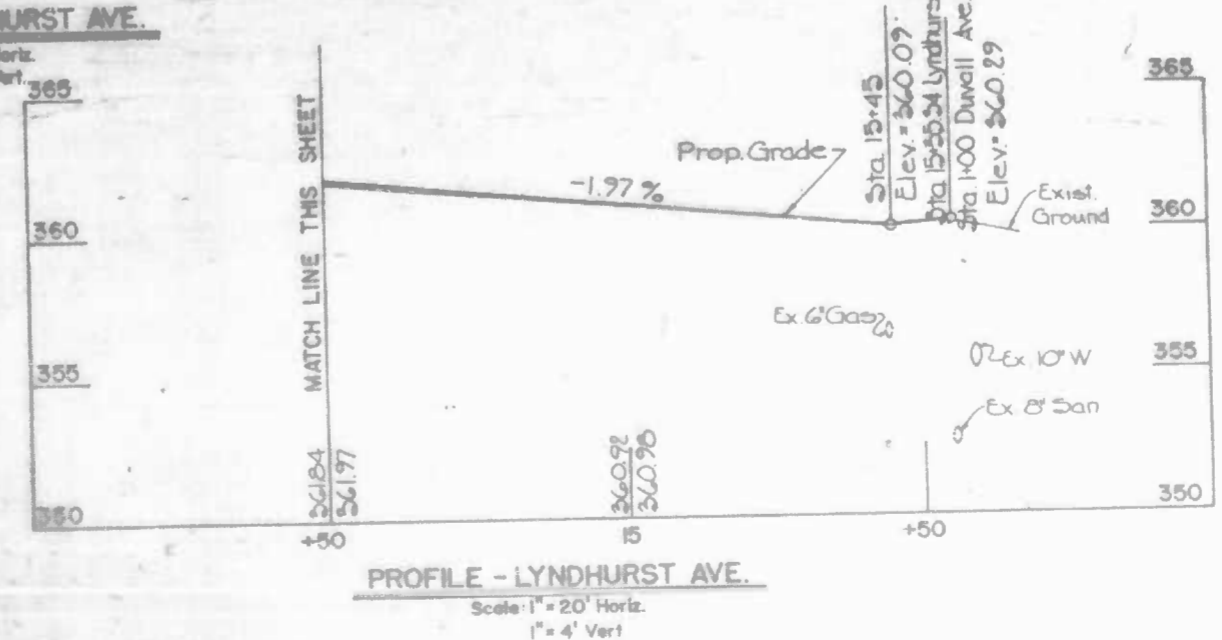
\* Top of Curb at Centerline Unless Otherwise Noted  
 \*\* Inlet to be Connected to Exist Pipe, Invert Elevations are Approximate.

Construct Typical Driveway Entrance Balto City Std. No. BC-500.01 At The Following Locations:  
 Sta. 10+50 Lt. Lyndhurst Ave.  
 Sta. 10+88 Lt. Lyndhurst Ave.

Construct Special Driveway Entrance (See Detail On Sheet 2 of 10) At The Following Locations:  
 Sta. 10+50 Rt. Lyndhurst Ave.  
 Sta. 10+88 Rt. Lyndhurst Ave.  
 Sta. 12+46 Lt. Lyndhurst Ave.

LOCATION	SIZE
Sta. 9+50	17" Rt.
Sta. 9+75	17" Rt.
Sta. 9+95	16" Rt.

For Street Light Notes, and Charts For Hand Boxes and Street Light Bases, See Sheet 5 of 7.10



Notes:  
 1. See Sheet 6 of 7 For "Intersection Grading Plan".  
 2. See Sheet 5 of 7 For "Intersecting Street Profiles".

CITY OF BALTIMORE  
 DEPARTMENT OF PUBLIC WORKS  
 BUREAU OF HIGHWAYS

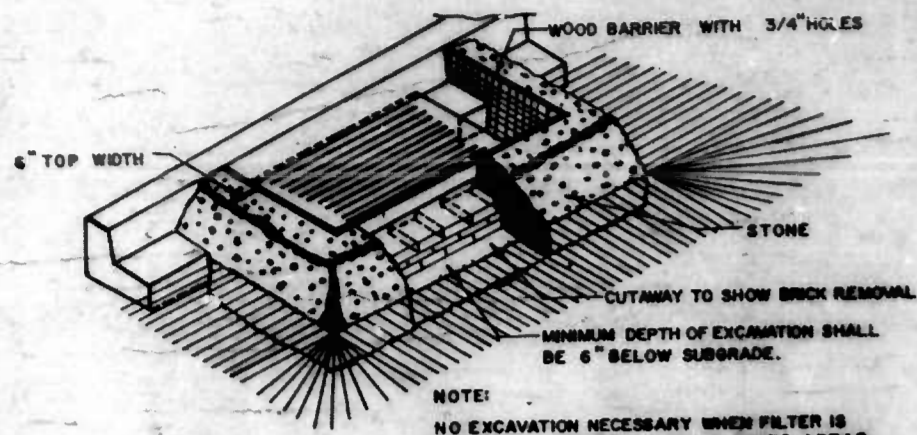
CONTRACT NO. 2913  
**LYNDHURST AVENUE**  
 BATEMAN AVE. TO DUVALL AVE.  
**SEDIMENT CONTROL PLAN**  
 (PHASE 2)

SCALE AS SHOWN DATE: 4-17-85  
 HIGHWAY ENGINEERING DIVISION SHEET 9 OF 10

DRAWN BY E. BREWER  
 EXAMINED BY B. BROSIUS

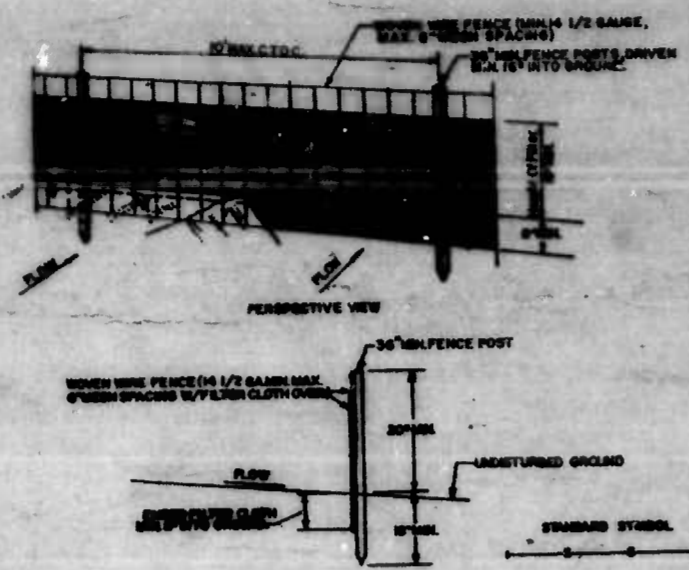
FILE REF.

NO.	REVISIONS	DATE	BY
4	ADDED SEDIMENT CONTROL PLANS B.9.8.10	8-6-85	M.J.



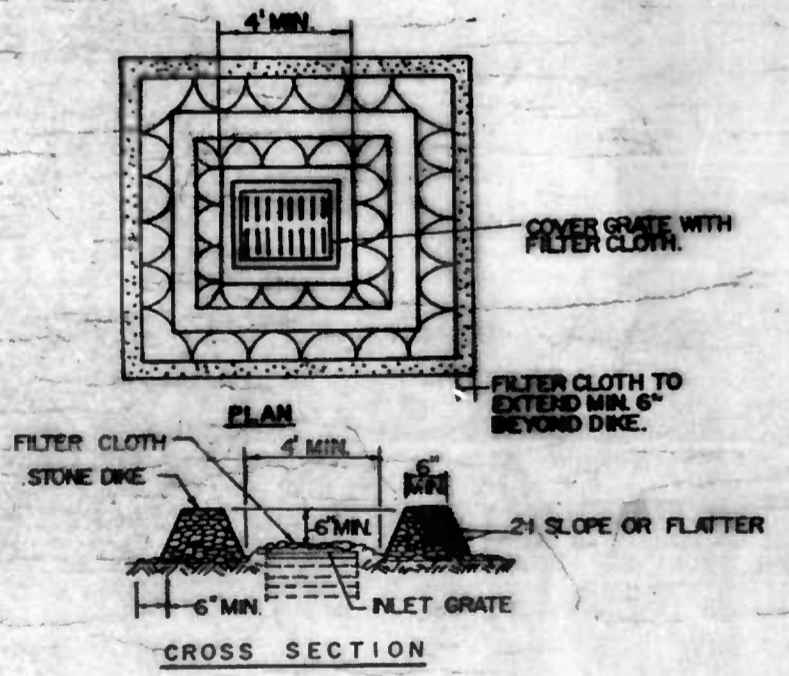
- CONSTRUCTION SPECIFICATIONS**
- Structure shall be inspected after each storm and repairs made as needed.
  - Construction Operations shall be carried out in such a manner that erosion and water pollution is minimized.
  - The structure shall be removed when drainage area has been properly stabilized.
  - The crushed stone used in the outlet shall meet ASTM Designation M3 size no. 2 or 24 or the equivalent such as M3A size no. 2. Gravel meeting the above gradation may be used if crushed stone is not available. Crusher Run is not acceptable.

**INLET PROTECTION  
DETAIL SC-1**



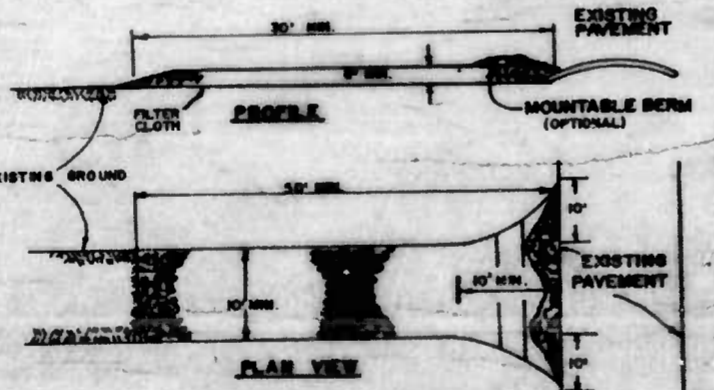
- CONSTRUCTION NOTES FOR FABRICATED SILTY FENCE**
- |   |   |
|---|---|
| 1. Woven Wire Fence to be fastened securely to fence posts with wire ties or staples.                     | POSTS: Steel either T or U Type or 2\"                                  |
| 2. Filter Cloth to be fastened securely to Woven Wire Fence with ties spaced every 24\"                   | FENCE: Woven Wire, 26 Gr. 6\"   |
| 3. When two sections of Filter Cloth adjoin each other they shall be overlapped by six inches and folded. | FILTER CLOTH: Filter 1, Harsco Lock, stabilizer T140 or approved equal. |
| 4. Maintenance shall be performed as needed and material removed when "bulges" develop in the silt fence. | PRE-FABRICATED UNIT: GEOTAS, INTERFENCE, OR Equal.                      |

**SILTY FENCE  
DETAIL SC-3**



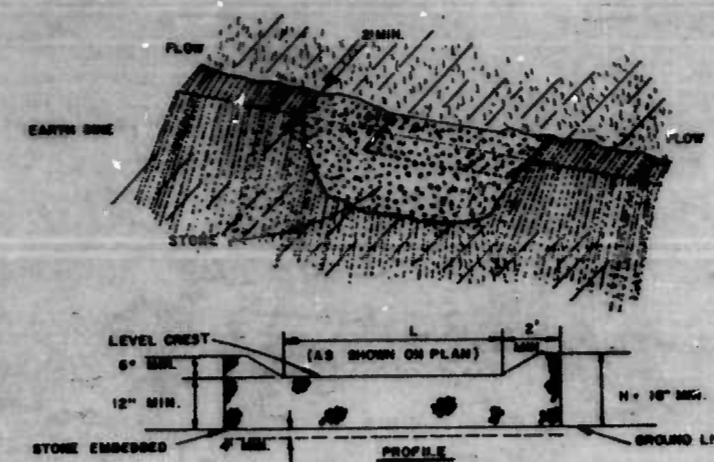
- CONSTRUCTION SPECIFICATIONS**
- Stone for the Dike shall be crushed stone. Gravel may be used if crushed stone is not available. The stone shall meet M3A size no. 2 or ASTM designation M3 size no. 2 or 24. Crusher Run is not acceptable.
  - Filter cloth shall be Poly-Filter X or approved equal.
  - The structure shall be inspected after each rain and repairs made as needed.

**INLET PROTECTION  
DETAIL SC-2**



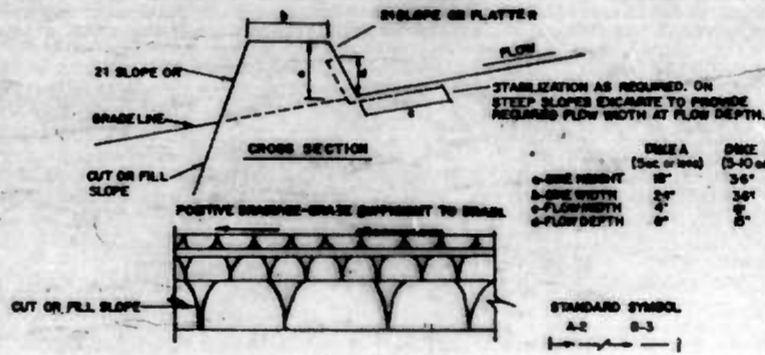
- CONSTRUCTION SPECIFICATIONS**
- Stone Size - No 2" stone, or washed or recycled concrete equivalent.
  - Length - As required, but not less than 50 feet (except on a single residential lot where a 30 foot minimum length would apply).
  - Thickness - Not less than six (6) inches.
  - Width - Ten (10) feet minimum, but not less than the full width at points where ingress or egress occurs.
  - Filter Cloth - Will be placed over the entire area prior to placing of stone. Filter will not be required on a single family residence lot.
  - Surface Water - All surface water flowing or directed toward construction activities shall be piped across the entrance. If piping is impractical, a mountable berm with 3:1 slopes will be permitted.
  - Maintenance - The entrance shall be maintained in a condition which will prevent tracking or flowing of sediment onto public right-of-way. This may require periodic law mowing with additional stone as conditions demand and repair and/or placement of any materials used to trap sediment. All sediment spilled, dropped, or tracked onto public right-of-way must be removed immediately.
  - Washing - Wheels shall be cleaned to remove sediment prior to entrance onto public right-of-way. When washing is required, it shall be done on an area stabilized with stone and which drains into an approved sediment trapping device.
  - Periodic inspection and needed maintenance shall be provided after each rain.

**STABILIZED CONSTRUCTION ENTRANCE  
DETAIL SC-4**



- CONSTRUCTION SPECIFICATIONS**
- The stone shall be crushed stone. Gravel may be used if crushed stone is not available. The stone shall meet M3A size no. 2 or ASTM designation M3 size no. 2 or 24.
  - The crest of the stone dike shall be at least six inches lower than the lowest elevation of the top of the earth dike and shall be level.
  - The stone outlet structure shall be embedded into the soil a minimum of four inches.
  - The minimum length, in feet, of the crest of the stone outlet structure shall be equal to six times the number of acres of contributing drainage area.
  - The stone outlet structure shall be inspected after each rain, and the stone shall be replaced when the structure ceases to function as intended due to soil accumulation among the stones, washout, construction traffic damage, etc.

**STONE OUTLET STRUCTURE  
DETAIL SC-5**



- CONSTRUCTION SPECIFICATIONS**
- All Earth Dikes shall be compacted by earth moving equipment.
  - All Dikes shall have positive drainage to an outlet.
  - Top width may be wider and side slopes may be flatter if desired to facilitate crossing by construction traffic.
  - Final location should be adjusted as needed to utilize a stabilized, safe outlet.
  - Dikes shall have an outlet that functions with a minimum of erosion. Runoff shall be conveyed to a sediment trapping device such as a sediment basin where either the dike channel or the drainage area above the dike are not adequately stabilized.
  - Stabilization shall be (a) in accordance with standard specifications for seed and straw mulch or (b) as per the chart below. Stone Dike need not be stabilized.

**FLOW CHANNEL STABILIZATION**

TYPE OF STABILIZATION	CHANNEL GRADE	AREA A	AREA B
1	0.5-3.0%	Seed and Straw Mulch	Seed and Straw Mulch
2	3.1-5.0%	Seed and Straw Mulch	Seed using Jute, or Excelsior; Sod; 2\"
3	5.1-8.0%	Seed with Jute, or Sod; 2\"	Lined Rip-Rap 4-8\"
4	8.1-20%	Lined Rip-Rap 4-8\"	Engineering Design

- Stone to be 2 inch stone, or recycled concrete equivalent, in a layer at least 3 inches in thickness and be processed into the soil with construction equipment.
  - Rip-Rap to be 4-8 inches in a layer at least 8 inches thickness and processed into the soil.
  - Approved equivalents can be substituted for any of the above materials.
7. Periodic inspection and required maintenance must be provided after each rain event.

**EARTH / STONE DIKE  
DETAIL SC-6/DETAIL SC-7**

**LEGEND**

- LIMITS OF DISTURBANCE
- [Symbol] INLET PROTECTION (SC-1 OR SC-2 AS NOTED)
- S-S- SILTY FENCE (SC-3)
- [Symbol] STABILIZED CONSTRUCTION ENTRANCE (SC-4)
- [Symbol] STONE OUTLET STRUCTURE (SC-5)
- [Symbol] EARTH/STONE DIKE (SC-6 OR SC-7 AS NOTED)

**STOCKPILE / STAGING AREA**  
Contractor must submit to the Baltimore City Sediment Control Section designated location for related stockpile and/or Contractor's staging operations prior to any construction operation.

**Owner/Developer Certification**  
"I/We hereby certify that any clearing, grading, construction and/or development will be done pursuant to this plan and that any responsible personnel involved in the construction project will have a certification of attendance at a Department of Natural Resources approved training program for the control of sediment and erosion before beginning the project."

Signature: *James A. Lito*      Date: 8-11-85  
Print Name: James A. Lito  
Address: 204 Municipal Bldg.      Phone: 396-3075

**Engineer's Certificate**  
I certify that this plan for Erosion and Sediment Control represents a practical, workable plan based on personal knowledge of the site conditions and that it was prepared in accordance with the requirements of the Baltimore City Erosion and Sediment Control Section.  
Signature: *Frederick Marc*      Date: Please Print Name  
Address: 39 Municipal Bldg.      Phone: 396-3175

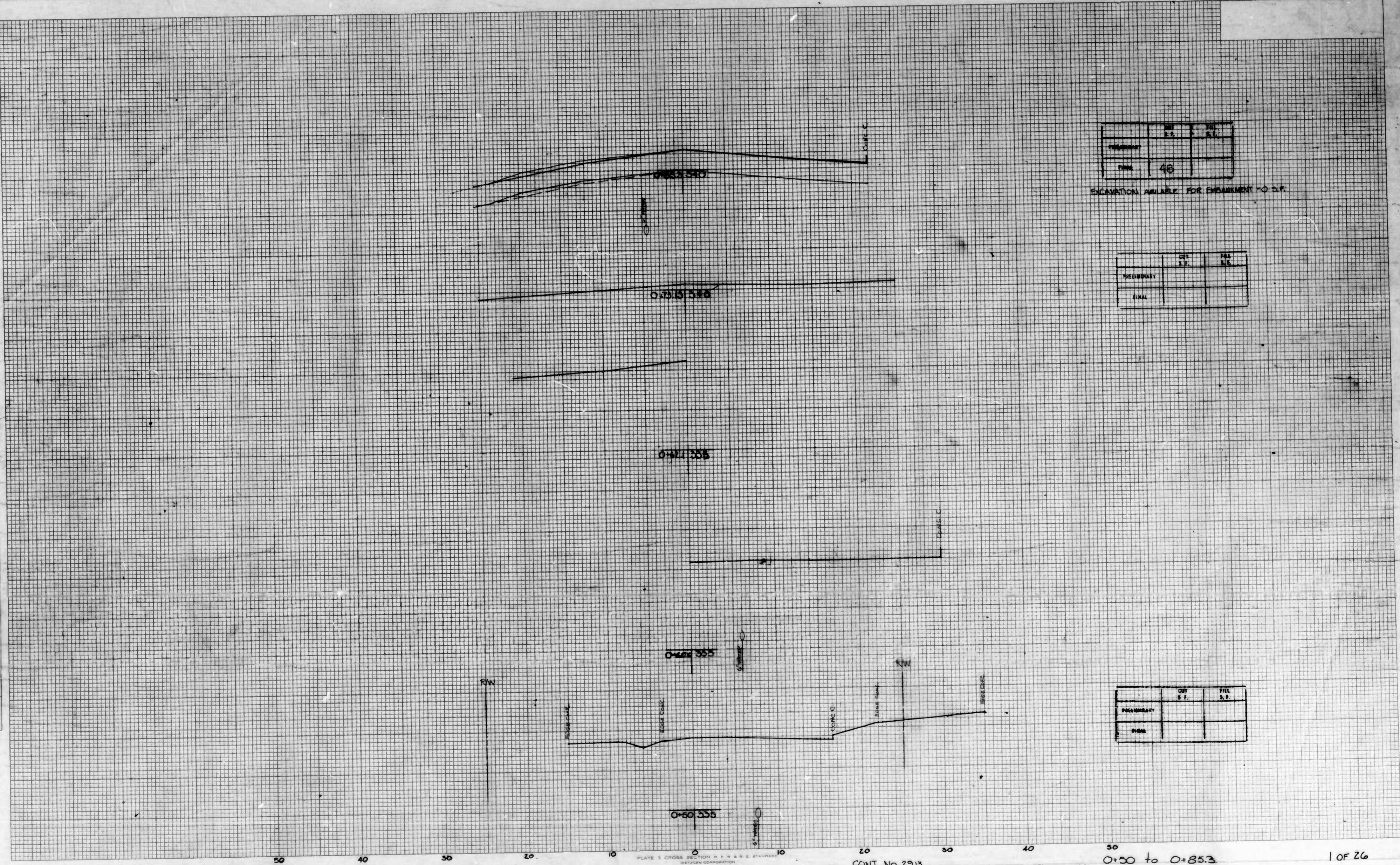
CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
BUREAU OF HIGHWAYS  
  
CONTRACT NO 2913  
LYNDHURST AVENUE  
BATEMAN AVE. TO DUVAL AVE  
SOIL EROSION / SEDIMENT CONTROL DETAILS

SCALE: NO SCALE      DATE:      SHEET 10 OF 10

DRAWN BY: A. MOSCATO  
EXAMINED BY:

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 PLAN SURVEY \_\_\_\_\_  
 NOTE BOOK \_\_\_\_\_  
 NO. \_\_\_\_\_

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 ORIGINAL SURVEY \_\_\_\_\_  
 NOTE BOOK \_\_\_\_\_  
 NO. \_\_\_\_\_



	OFF S.F.	FILL S.F.
PRELIMINARY		
FINAL	48	

EXCAVATION AVAILABLE FOR EMBANKMENT = 0 S.F.

	OFF S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	OFF S.F.	FILL S.F.
PRELIMINARY		
FINAL		

Lyndhurst Ave.

PLATE 3 CROSS SECTION D.P.R. & R.E. STANDA  
 DIST'COEN CORPORATION

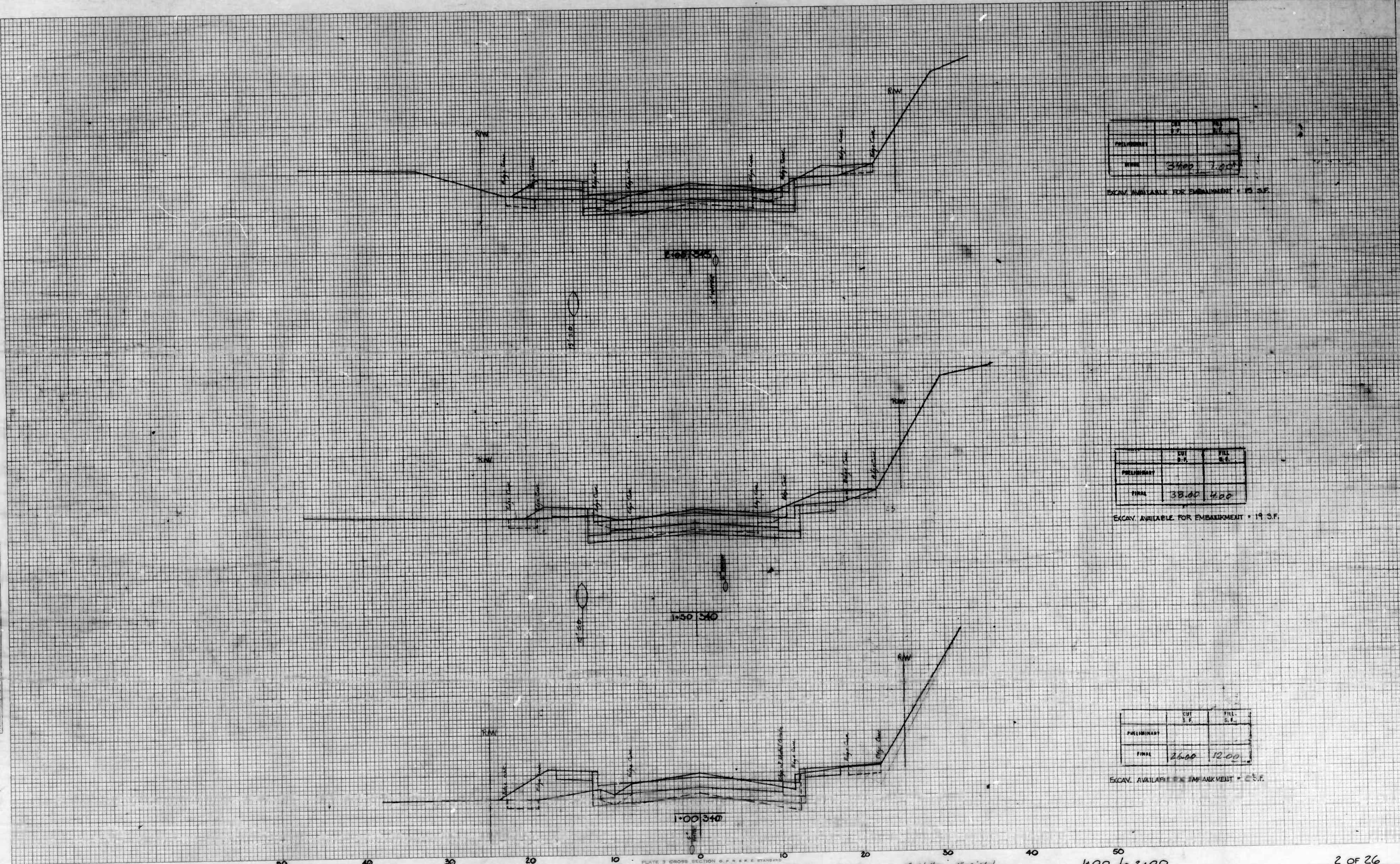
CONT. No. 2913

0+50 to 0+85.3

1 OF 26

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 ORIGINAL SURVEY NOTE BOOK NO. \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 ORIGINAL SURVEY NOTE BOOK NO. \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_



	CUT	FILL
PRELIMINARY		
FINAL	3400	1200

EXCAV. AVAILABLE FOR EMBANKMENT = 10 S.F.

	CUT	FILL
PRELIMINARY		
FINAL	38100	1200

EXCAV. AVAILABLE FOR EMBANKMENT = 19 S.F.

	CUT	FILL
PRELIMINARY		
FINAL	2600	1200

EXCAV. AVAILABLE FOR EMBANKMENT = 0 S.F.

Lynnhurst Ave.

PLATE 3 CROSS SECTION OF R.R.E. STANDARD  
 DETROIT CORPORATION

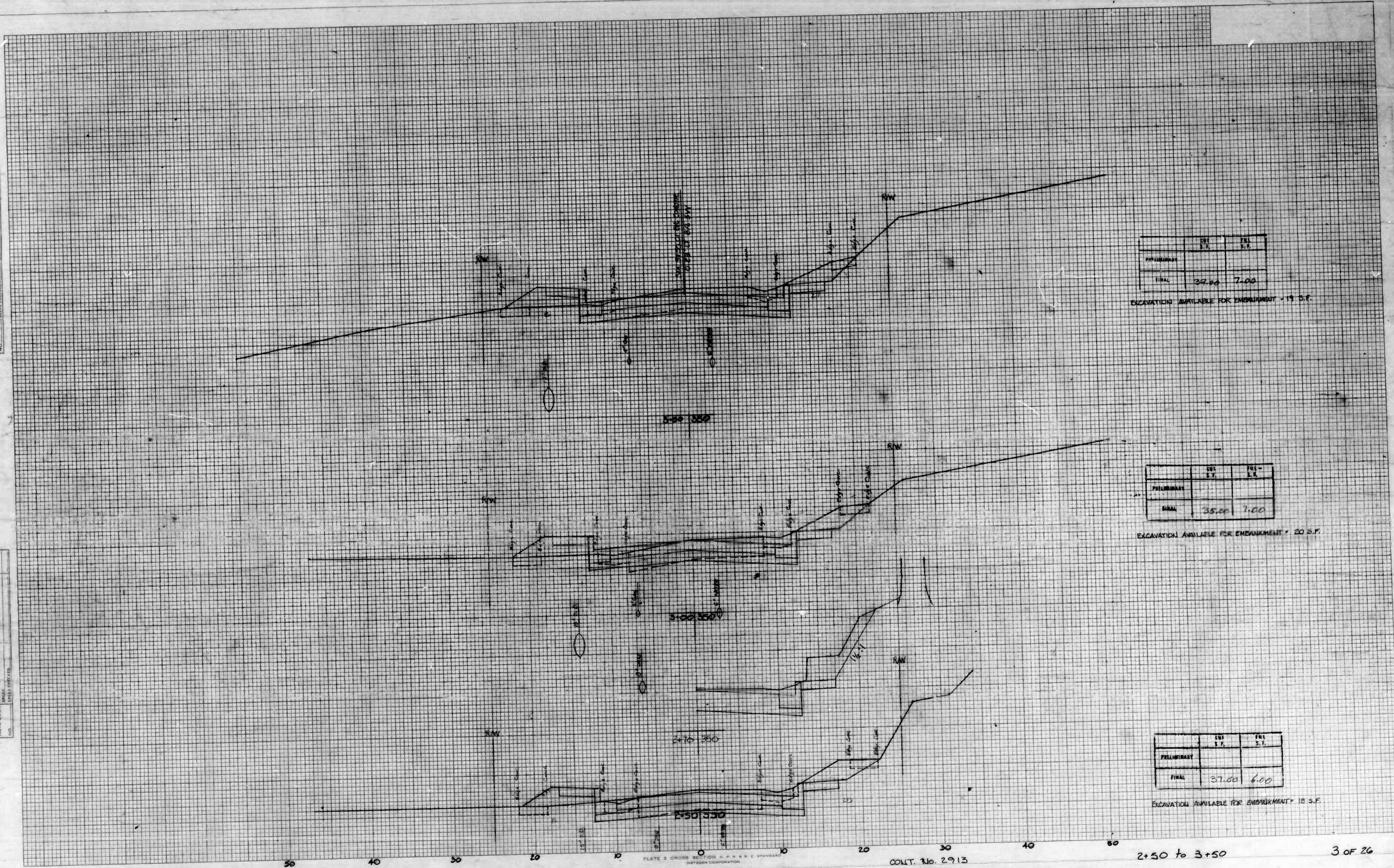
CONT. No. 2913  
 26/11/22

1" = 5' HORIZ. 1" = 2' VERT.

1+00 to 2+00

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 ORIGINAL SURVEY NOTE BOOK NO. \_\_\_\_\_  
 DATE CHECKED: \_\_\_\_\_

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 ORIGINAL SURVEY NOTE BOOK NO. \_\_\_\_\_  
 DATE CHECKED: \_\_\_\_\_



Lyndhurst Ave.

PLATE 3 CROSS SECTION C.P.R.R.C. STANDARD  
 INTERIOR CORPORATION

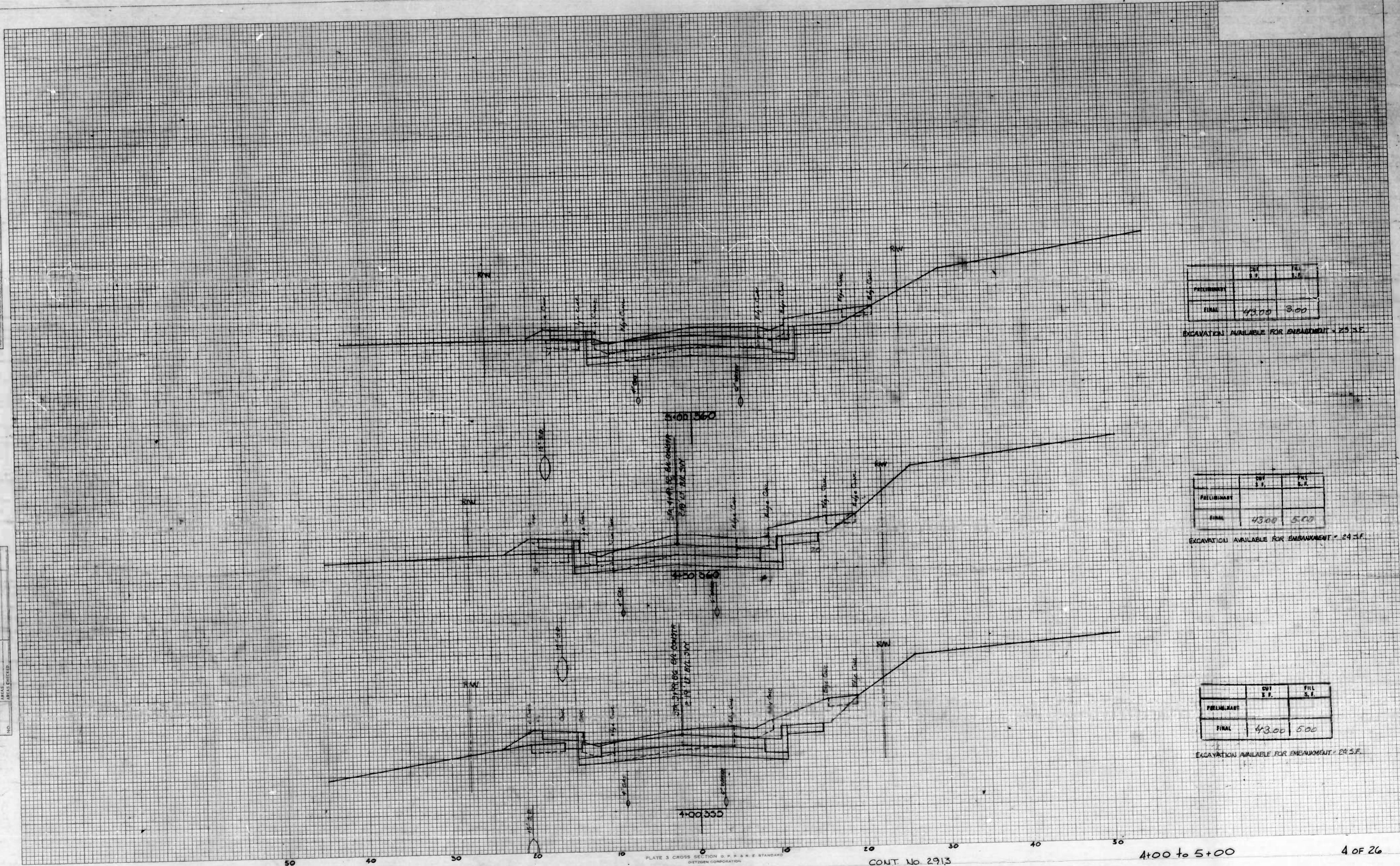
COLT. No. 2913

2+50 to 3+50

3 of 26

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 ORIGINAL SURVEY TEMPLATE: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 ORIGINAL SURVEY TEMPLATE: \_\_\_\_\_  
 NOTE BOOK NO.: \_\_\_\_\_  
 REVISIONS: \_\_\_\_\_



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	43.00	3.00

EXCAVATION AVAILABLE FOR EMBANKMENT = 23.5 F.

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	43.00	5.00

EXCAVATION AVAILABLE FOR EMBANKMENT = 24.5 F.

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	43.00	5.00

EXCAVATION AVAILABLE FOR EMBANKMENT = 24.5 F.

Lyndhurst Ave

PLATE 3 CROSS SECTION 3' x 4' 4" STANDARD  
 CITY OF CHICAGO

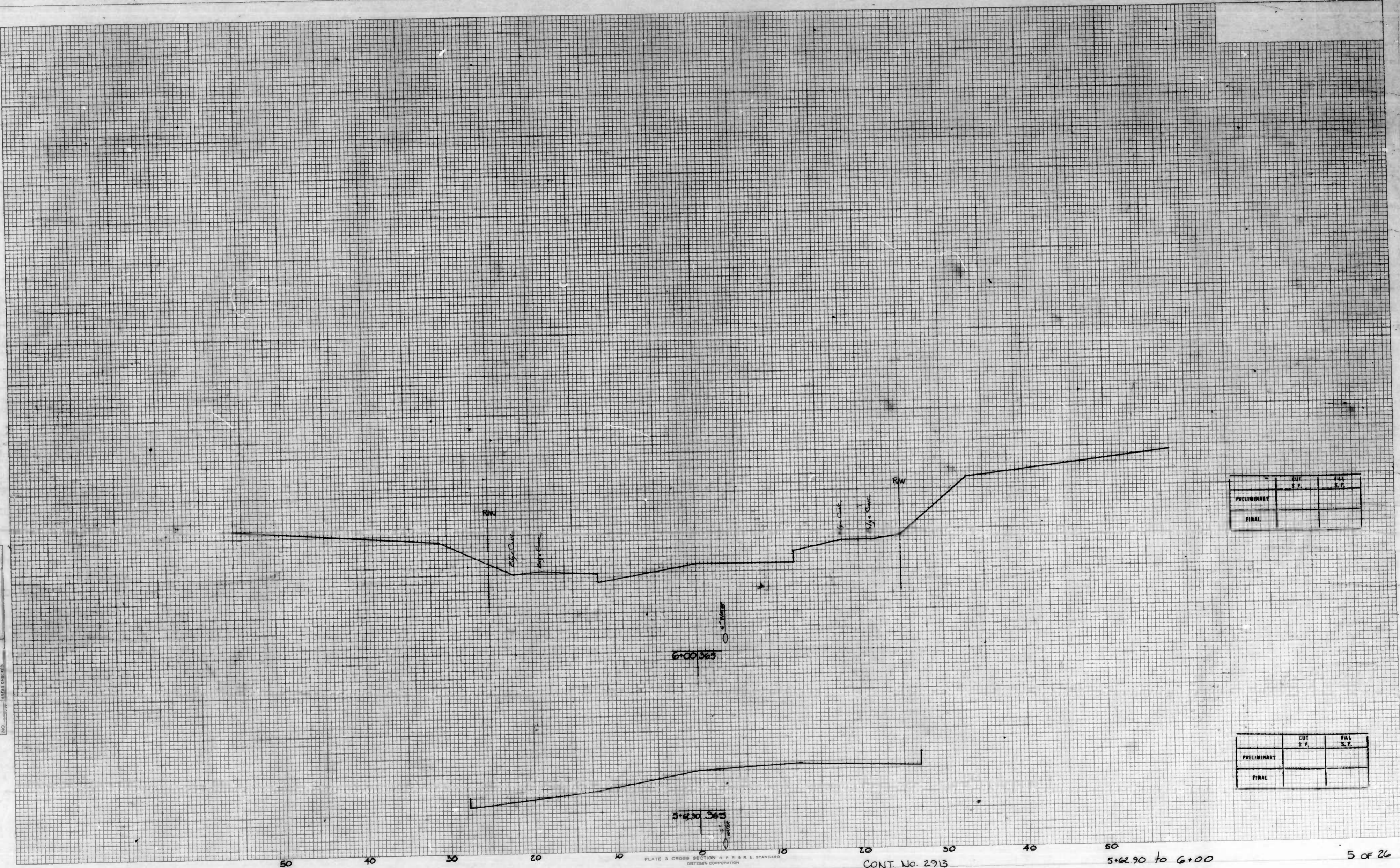
CONT. No 2913

4+00 to 5+00

4 OF 26

DATE	
BY	
PROJECT	
NO. OF SHEETS	
DATE CHECKED	
BY	

DATE	
BY	
PROJECT	
NO. OF SHEETS	
DATE CHECKED	
BY	



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

Lyndhurst Ave.

PLATE 3 CROSS SECTION 6" X 8" S.E. STANDARD  
DETROIT CORPORATION

CONT. No. 2913

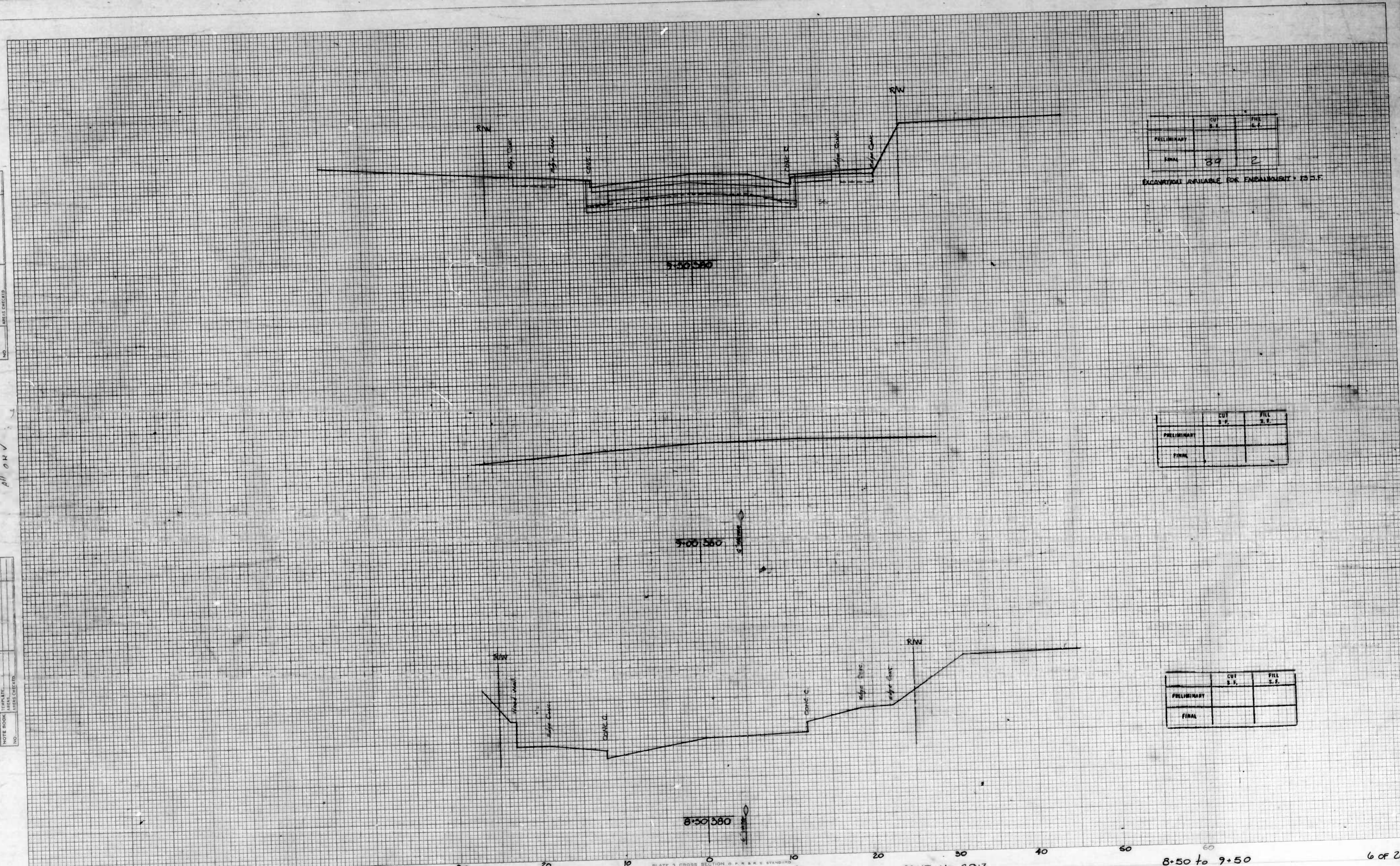
5+62.30 to 6+00

5 of 26

FINAL	DATE	BY
SURVEY	PLOTTED	
NOTE BOOK	REVISION	
NO.		

ORIGINAL	DATE	BY
SURVEY	PLOTTED	
NOTE BOOK	REVISION	
NO.		

all OK ✓



	CUT	FILL
PRELIMINARY	S.F.	S.F.
FINAL	39	2

EXCAVATION AVAILABLE FOR ENCLOSURE TO D.F.

	CUT	FILL
PRELIMINARY	S.F.	S.F.
FINAL		

	CUT	FILL
PRELIMINARY	S.F.	S.F.
FINAL		

Lyndhurst Ave.

PLATE 3 CROSS SECTION O.P.R. & A.E. STANDING DETROIT CORPORATION

CONT. No. 2913

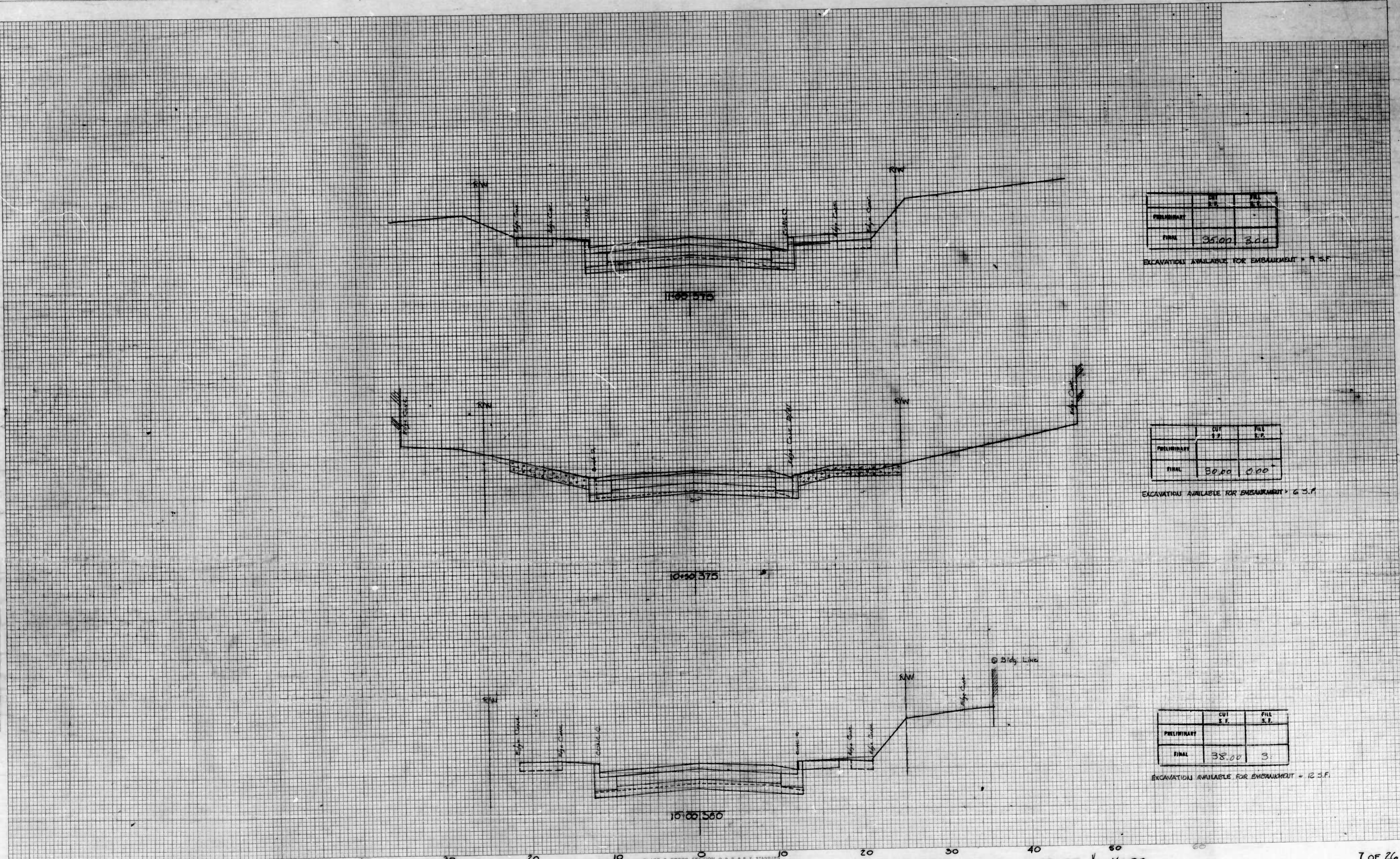
8+50 to 9+50

6 of 26



DATE	BY	SCALE
FINAL	DATE	BY
REVISED	NO.	BY
NOTE BOOK	NO.	BY
NO.		

DATE	BY	SCALE
ORIGINAL	DATE	BY
REVISED	NO.	BY
NOTE BOOK	NO.	BY
NO.		



	CUT	FILL
PRELIMINARY	2.75	2.5
FINAL	35.00	3.00

EXCAVATION AVAILABLE FOR EMBANKMENT = 11.25 F.

	CUT	FILL
PRELIMINARY	1.7	2.6
FINAL	50.00	0.00

EXCAVATION AVAILABLE FOR EMBANKMENT = 6.3 F.

	CUT	FILL
PRELIMINARY	5.6	2.1
FINAL	38.00	3

EXCAVATION AVAILABLE FOR EMBANKMENT = 12.3 F.

Lyndhurst Ave.

PLATE 3 CROSS SECTION P. H. & E. STANBARD  
GETZGEN CORPORATION

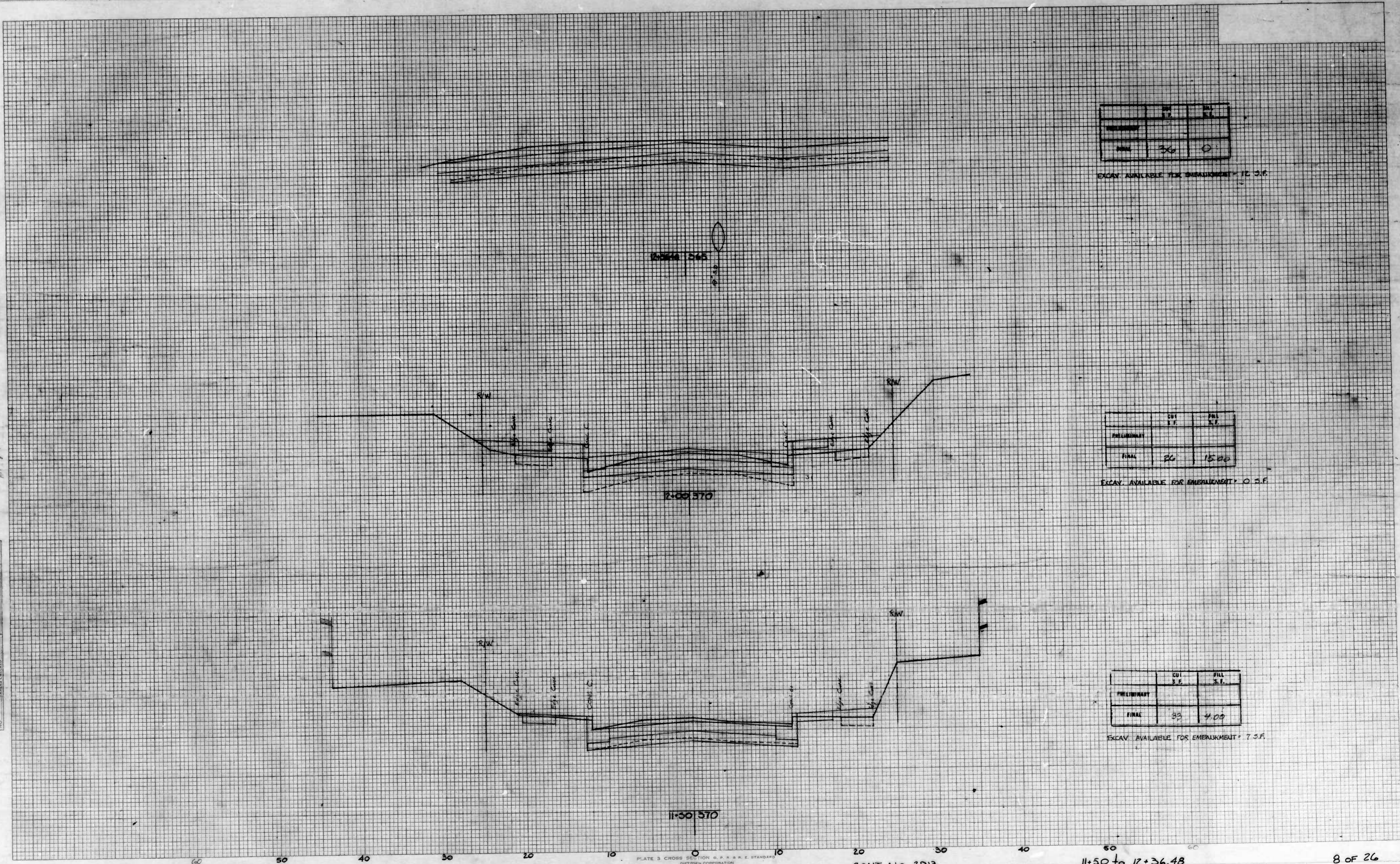
CONT. No. 2913

10+00 to 11+00

7 of 26

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
FIELD CHECKED	

DATE	5/7/68
BY	R.E. Sullivan
ORIGINAL SURVEY	
NOTE BOOK	
FIELD CHECKED	



	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	26	0

EXCAV. AVAILABLE FOR EMBANKMENT = 12.37

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	26	15.00

EXCAV. AVAILABLE FOR EMBANKMENT = 0.26

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	93	4.00

EXCAV. AVAILABLE FOR EMBANKMENT = 7.37

Lyndhurst Ave

PLATE 3 CROSS SECTION & S. & R. E. STANDARD  
DETROIT CORPORATION

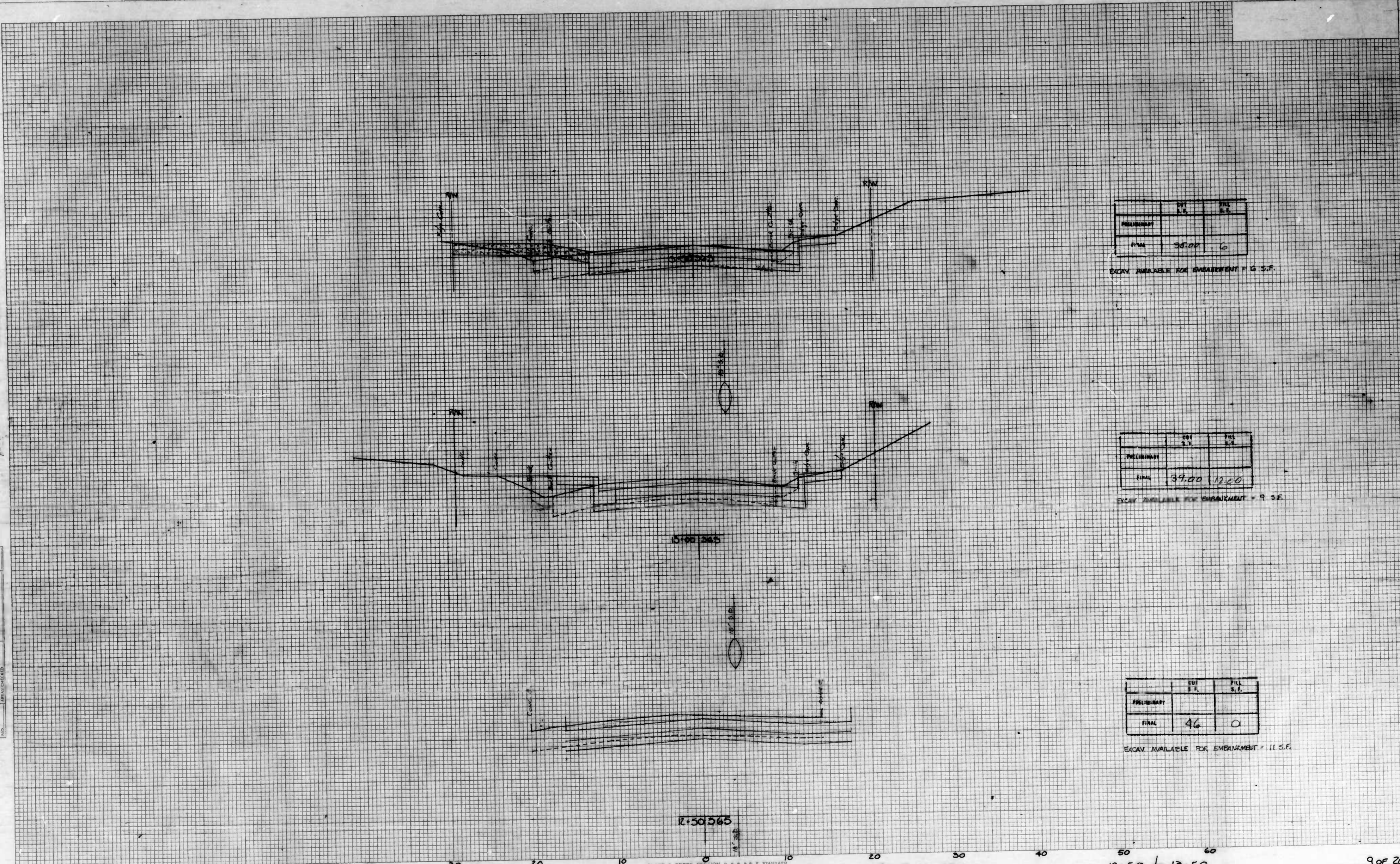
CONT No. 2913

11+50 to 12+36.48

8 of 26

DATE	
BY	
FINAL SURVEY	
NOTED	
NOTED BOOK	
NO.	

DATE	5/1/58
BY	F. J. Cotton
ORIGINAL	
NOTED	
NOTED BOOK	
NO.	



	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	38.00	6

EXCAV AVAILABLE FOR EMBANKMENT = 10 S.F.

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	39.00	12.00

EXCAV AVAILABLE FOR EMBANKMENT = 9 S.F.

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL	46	0

EXCAV AVAILABLE FOR EMBANKMENT = 11 S.F.

Lyndhurst Ave.

PLATE 3 CROSS SECTION S.P.A.B.E. STANDARD  
DETROIT CORPORATION

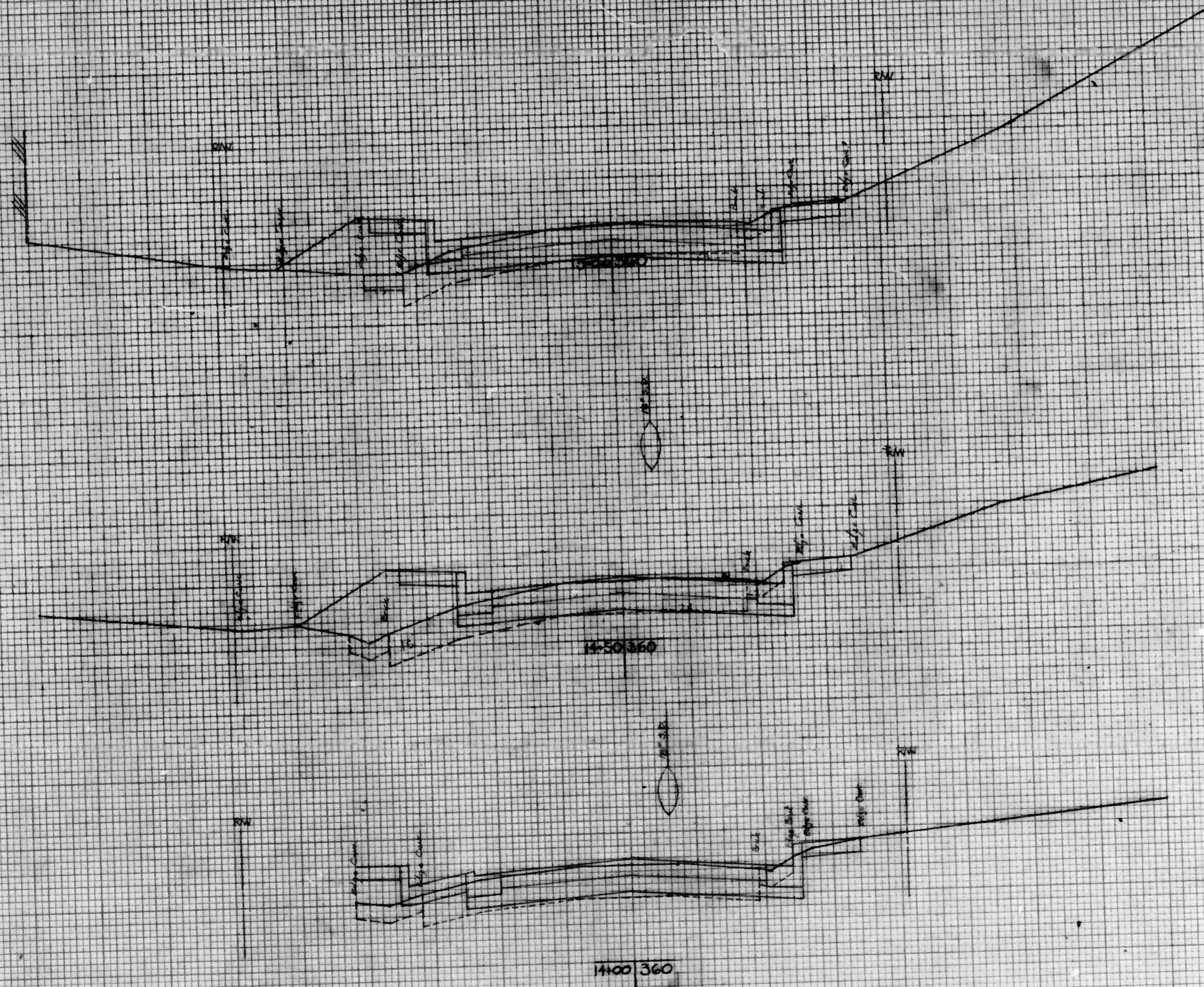
CONT No. 2913

12+50 to 13+50

9 of 26

DATE \_\_\_\_\_  
 BY \_\_\_\_\_  
 CHECKED \_\_\_\_\_  
 FINAL SURVEY NOTED: \_\_\_\_\_  
 NOTE BOOK NO. \_\_\_\_\_  
 AREA CHECKED \_\_\_\_\_

DATE 7/16  
 BY K. B. Sullivan  
 CHECKED \_\_\_\_\_  
 ORIGINAL SURVEY NOTED: \_\_\_\_\_  
 NOTE BOOK NO. \_\_\_\_\_  
 AREA CHECKED \_\_\_\_\_



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	35	17

EXCAV. AVAILABLE FOR EMBANKMENT = 6 S.F.

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	36.00	13.00

EXCAV. AVAILABLE FOR EMBANKMENT = 6 S.F.

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL	30	6

EXCAV. AVAILABLE FOR EMBANKMENT = 0 S.F.

Lyndhurst Ave.

PLATE 3 CROSS SECTION OF R & R E STANDARD  
 DETROIT CORPORATION

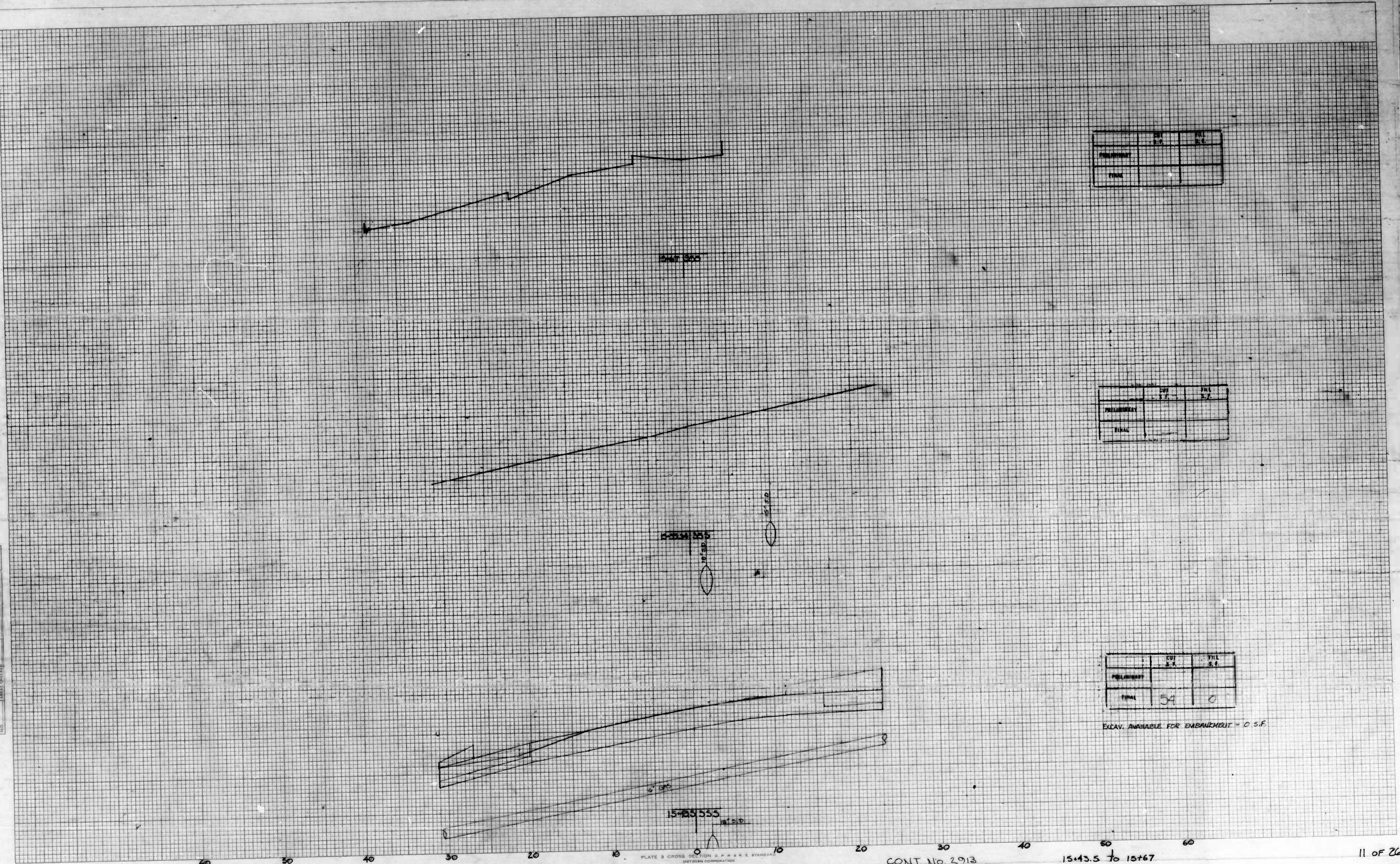
CONT No 2913

14+00 to 15+00

10 of 26

DATE  
BY  
FRANK SURVEY  
NO. 10000  
NOTE BOOK  
NO. 10000  
AREAS CHECKED

DATE  
BY  
Sutton  
NO. 10000  
NOTE BOOK  
NO. 10000  
AREAS CHECKED



Lyndhurst Ave.

PLATE 3 CROSS SECTION S.P.H.R.E. STANDARD  
DETROIT CORPORATION

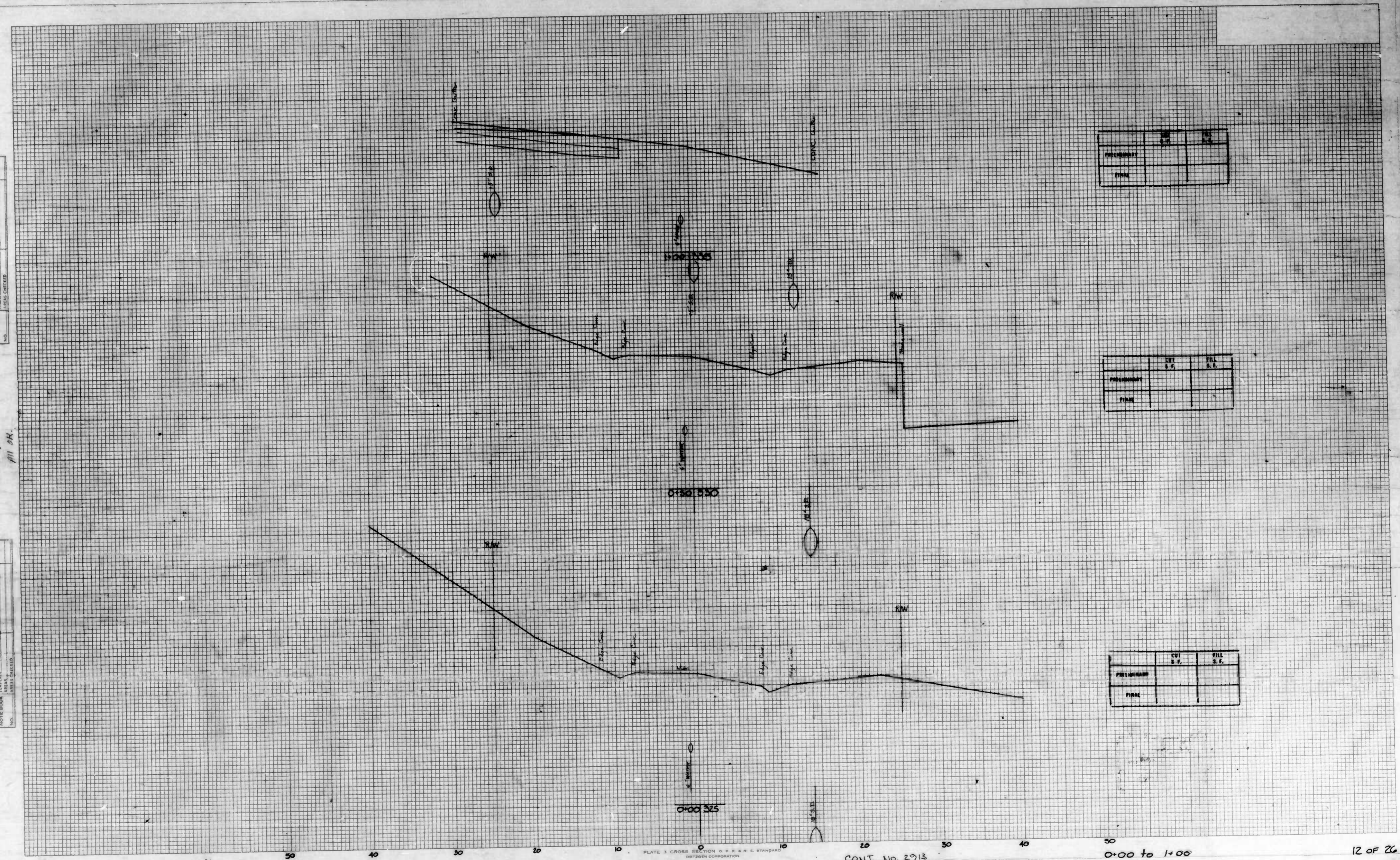
CONT. No. 2913

15+43.5 to 15+67

11 OF 26

DATE	BY
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	BY



PRELIMINARY		
FINAL		

PRELIMINARY	CUT S.F.	FILL S.F.
FINAL		

PRELIMINARY	CUT S.F.	FILL S.F.
FINAL		

Mondawmin Ave.

PLATE 3 CROSS SECTION C.P.R.R. & STANDARD  
DETROIT CORPORATION

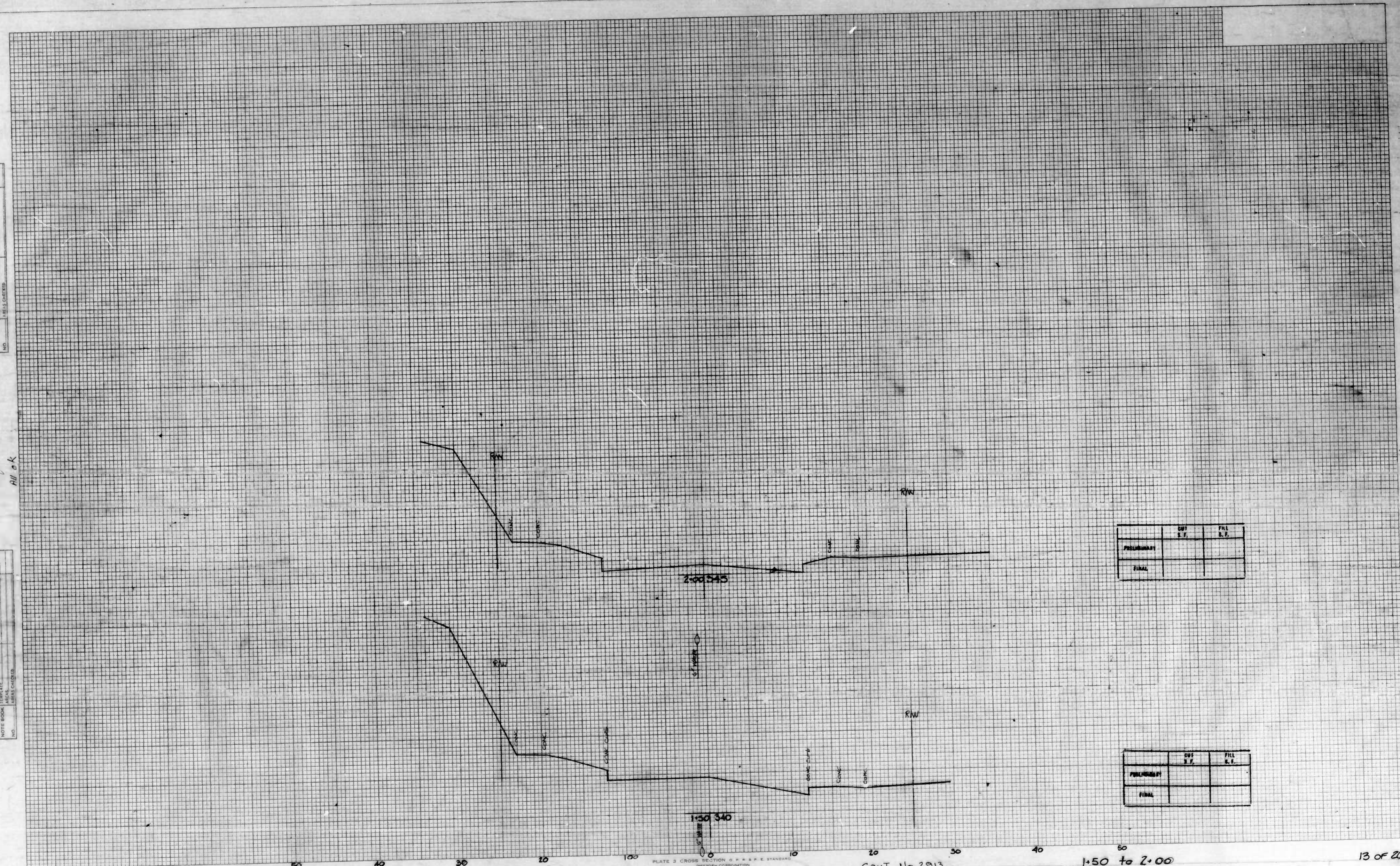
CONT. No. 2913

0+00 to 1+00

12 OF 26

FINAL SURVEY	DATE
NOTED BOOK	
NO. OF SHEETS	
NO. OF SHEETS COVERED	

ORIGINAL SURVEY	DATE
NOTED BOOK	5/7/52
NO. OF SHEETS	
NO. OF SHEETS COVERED	



	DIST	FILL
PRELIMINARY	2.7	2.1
FINAL		

	DIST	FILL
PRELIMINARY	2.7	2.1
FINAL		

Mondawmin Ave.

PLATE 3 CROSS SECTION 2.0 x 2.0 STANDARD  
BY TOP CONSTRUCTION

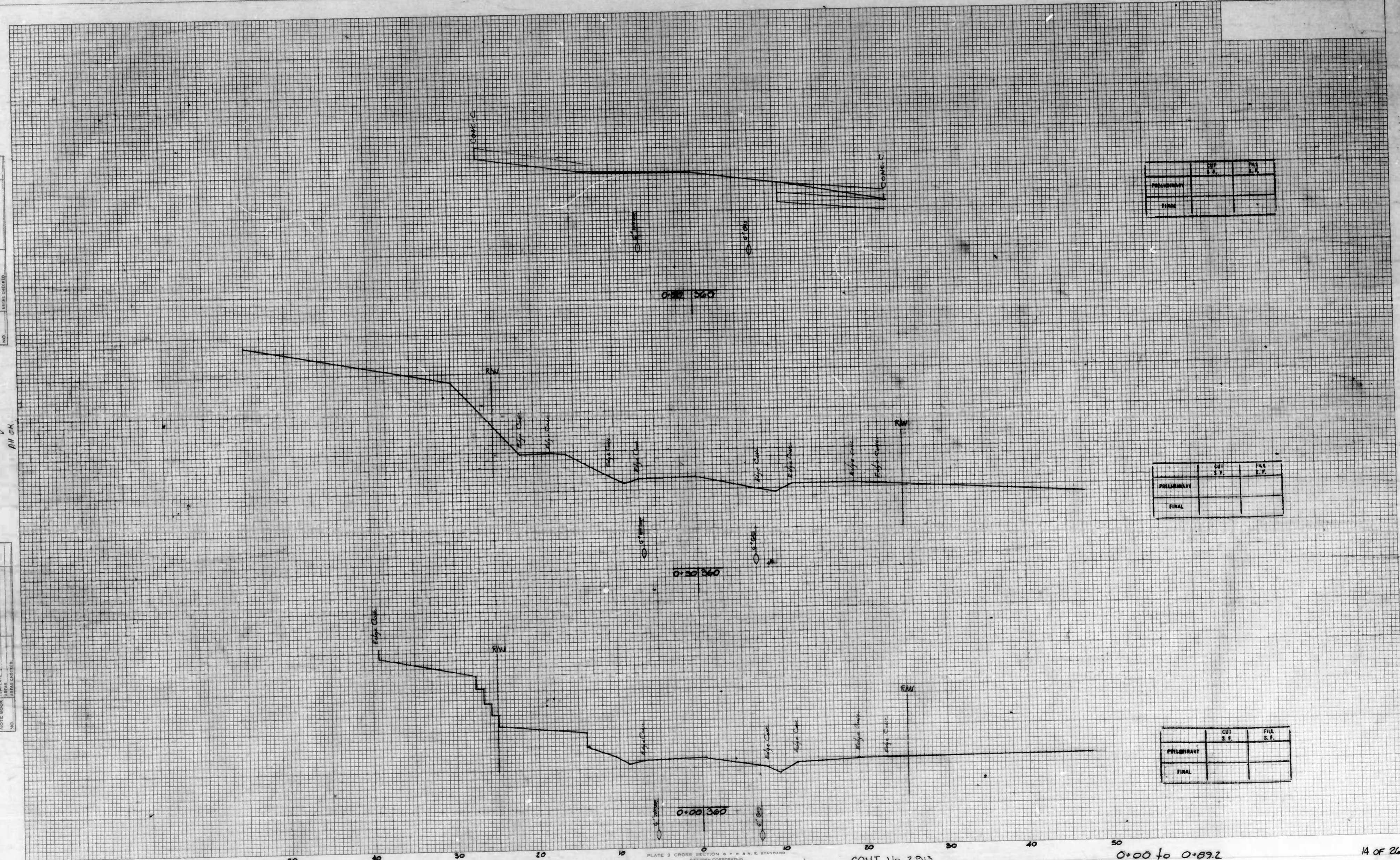
CONT. No. 2913

1:50 to 2:00

13 of 26

FINAL	DATE
SHEET	BY
NO. OF SHEETS	
PROJECT	
NO. OF SHEETS	
AREA	
DATE	

ORIGINAL	DATE
SHEET	BY
NO. OF SHEETS	
PROJECT	
NO. OF SHEETS	
AREA	
DATE	



	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

Alto Rd.

PLATE 3 CROSS SECTION & A.A.E. STANDARD  
DETROIT CORPORATION

CONT No. 2913

0+00 to 0+82

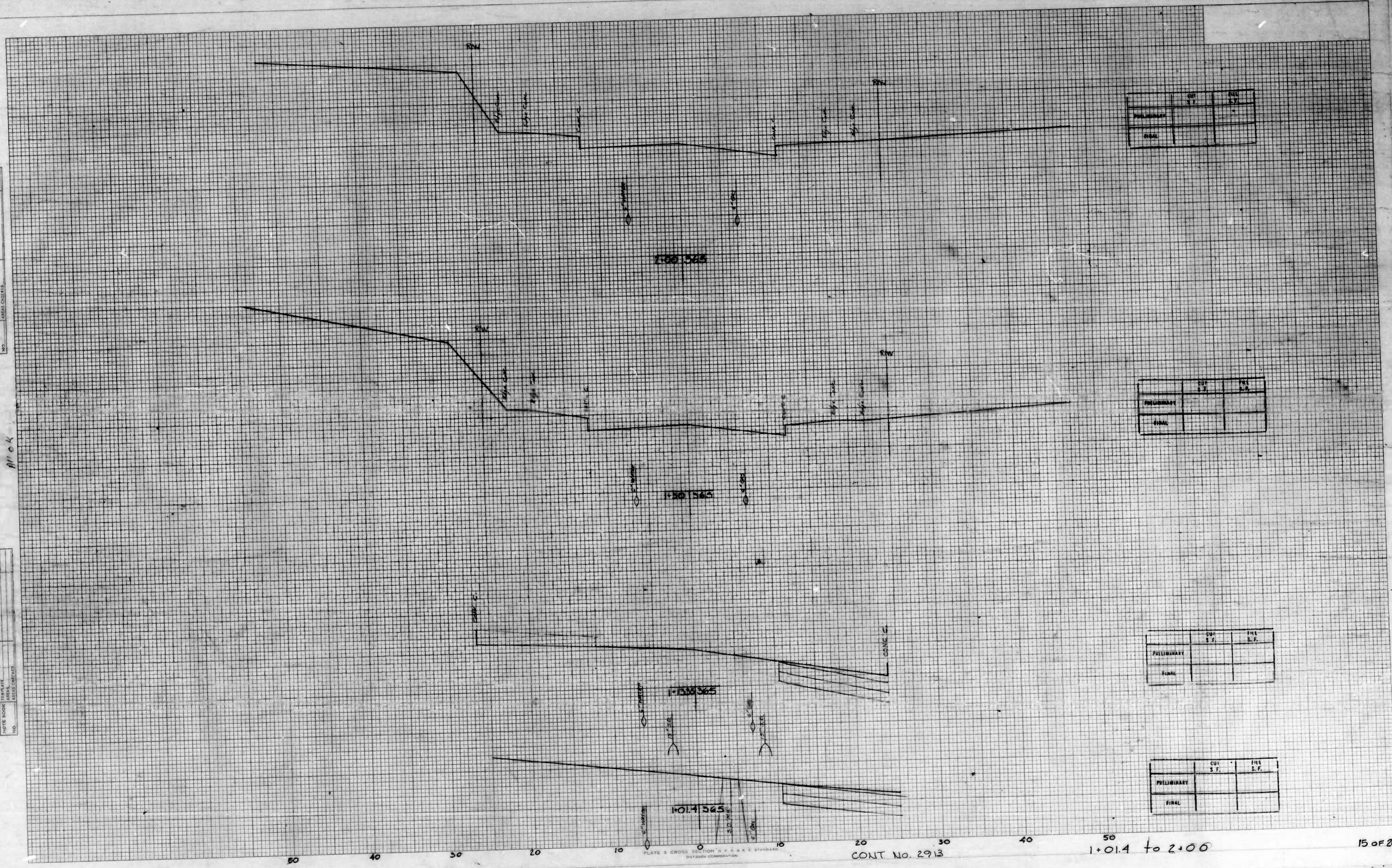
14 OF 26



FINAL SURVEY	DATE
NOTED	BY
NOTE BOOK	
AREA	
NO.	

ORIGINAL SURVEY	DATE
NOTED	BY
NOTE BOOK	
AREA	
NO.	

APPROX



Alto Rd.

PLATE 3 CROSS SECTION "G" 4 x 4 x 4 STANDARD  
DETZEN CORPORATION

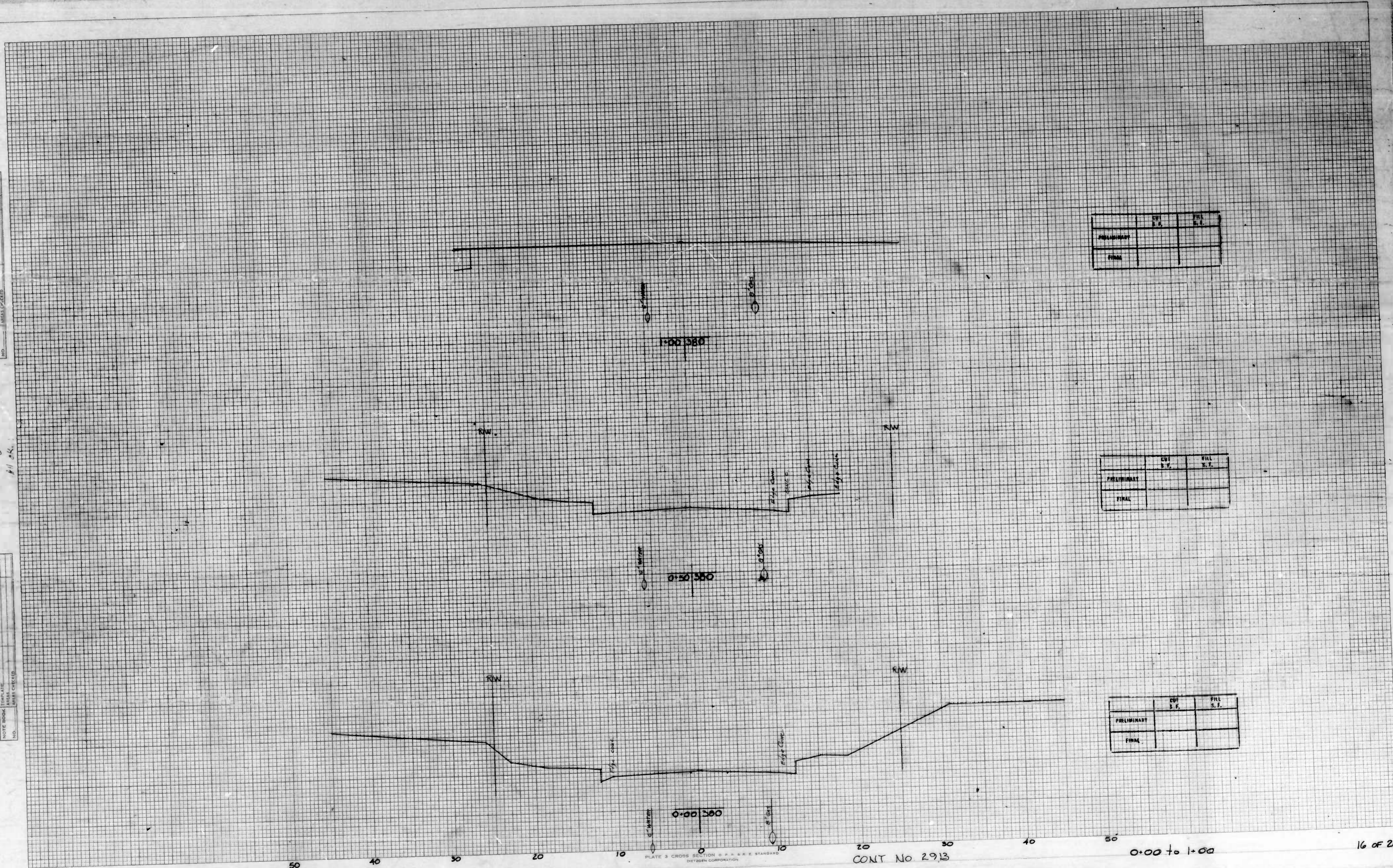
CONT. No. 2913

1+01.4 to 2+00

15 of 26

DATE	
BY	
FINAL SURVEY	
PORTER BOOK	
NO.	
DATE CHECKED	
BY	

DATE	7/1/55
BY	R. E. Sullivan
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
DATE CHECKED	
BY	



	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

	CUT	FILL
	S.F.	S.F.
PRELIMINARY		
FINAL		

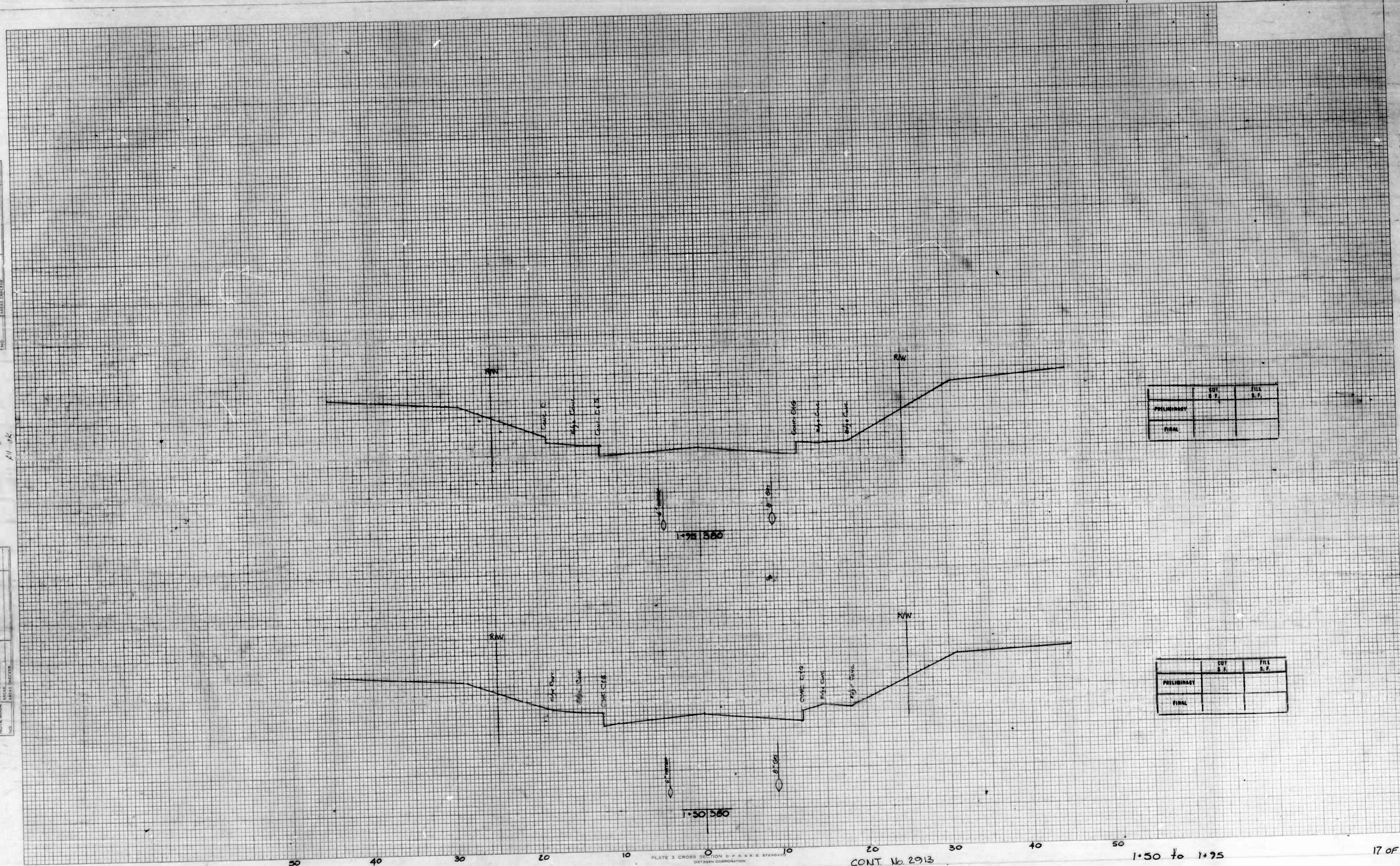
Bateman Ave.

CONT No 2913

0+00 to 1+00

FINAL SURVEY  
 DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_

ORIGINAL SURVEY  
 DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

Batsman Ave.

PLATE 3 CROSS SECTION O.P.A.R.E. STANDARD  
 DITZGEN CORPORATION

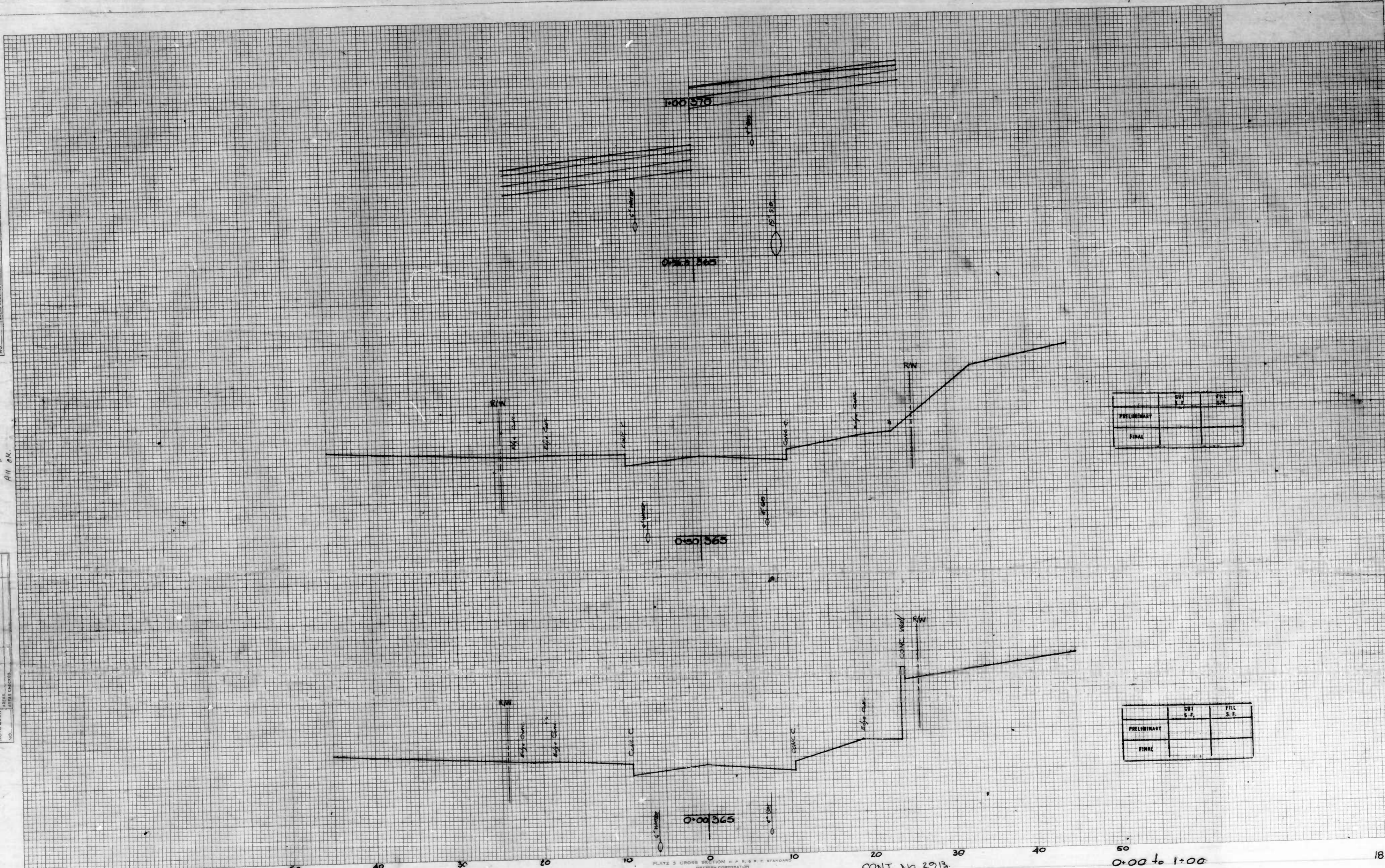
CONT. No. 2913

1+50 to 1+95

17 of 26

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 DIVISION: \_\_\_\_\_  
 PROJECT: \_\_\_\_\_  
 SHEET: \_\_\_\_\_  
 TOTAL SHEETS: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_

DATE: 7/7/52  
 BY: K. S. Sullivan  
 DIVISION: \_\_\_\_\_  
 PROJECT: \_\_\_\_\_  
 SHEET: \_\_\_\_\_  
 TOTAL SHEETS: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_



	DWT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	DWT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

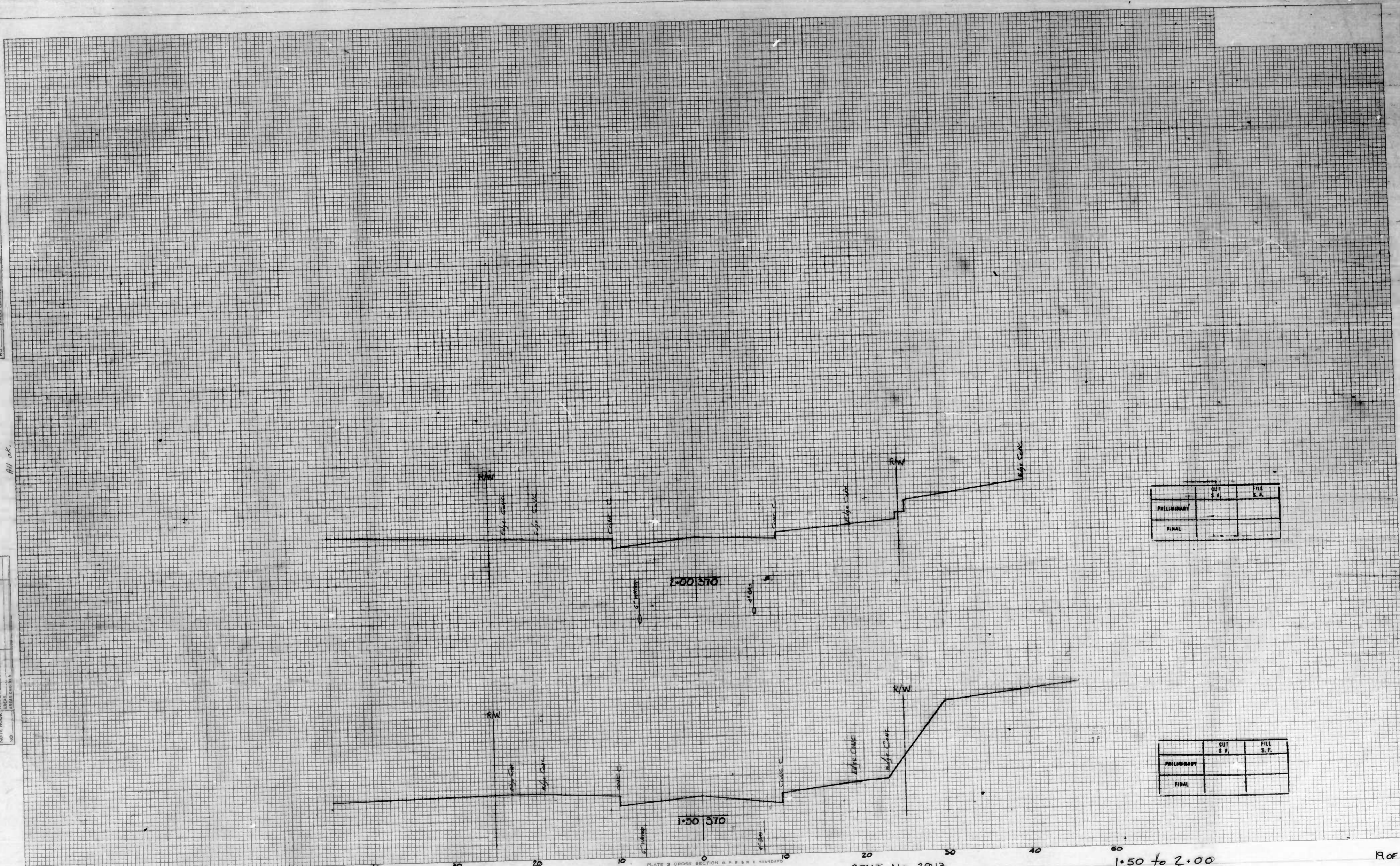
Carlisle Ave.

50 40 30 20 10 0 10 20 30 40 50  
 PLATE 3 CROSS SECTION C.P.R. & R.E. STANDARD  
 DEFENSE CORPORATION  
 CONT. No 2913  
 0+00 to 1+00

DATE	
NO.	
NO. CHECKED	
NO. AT	
NO. SURVEY	
NO. REVISIONS	

DATE	11/75
BY	R. S. [unclear]
NO.	
NO. CHECKED	
NO. AT	
NO. SURVEY	
NO. REVISIONS	

All ok.



	CUT S.F.	FILE S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILE S.F.
PRELIMINARY		
FINAL		

Carlisle Ave.

PLATE 3 CROSS SECTION 6" x 6" S.F. STANDARDS  
SIXTEEN COPIES

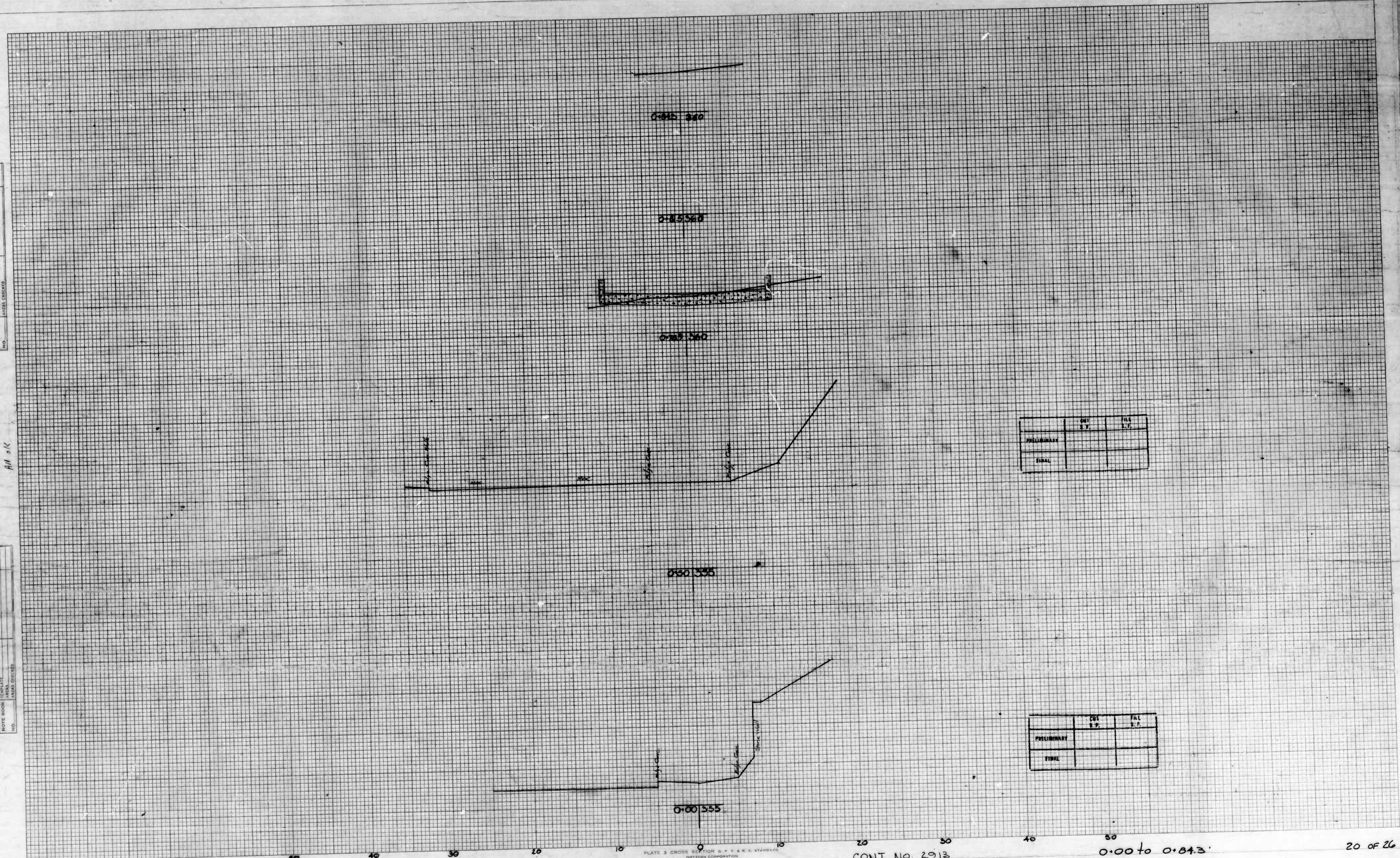
CONT. No. 2913

1.50 to 2.00

R of 26

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 DRAWN: \_\_\_\_\_ CHECKED: \_\_\_\_\_  
 SCALE: \_\_\_\_\_  
 SHEET NO.: \_\_\_\_\_

DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
 DRAWN: \_\_\_\_\_ CHECKED: \_\_\_\_\_  
 SCALE: \_\_\_\_\_  
 SHEET NO.: \_\_\_\_\_



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

Alley

PLATE 3 CROSS SECTION D. P. & R. STANDARD  
DETROIT CORPORATION

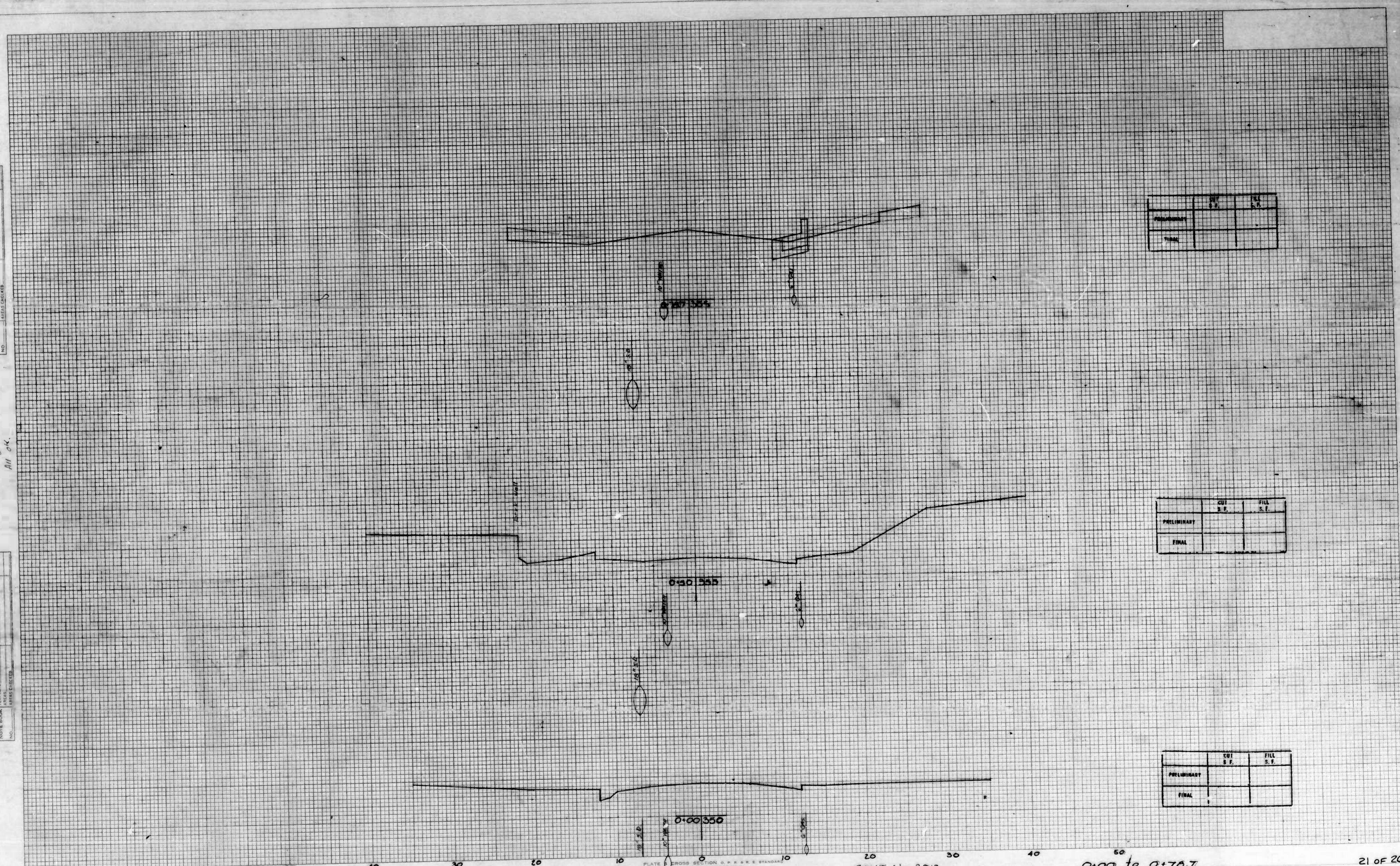
CONT. NO. 2913

0.00 to 0.043

20 of 26

DATE \_\_\_\_\_ BY \_\_\_\_\_  
 FINAL CHECKED \_\_\_\_\_  
 SURVEYED \_\_\_\_\_  
 NOTED \_\_\_\_\_  
 NO. \_\_\_\_\_

ALL BY \_\_\_\_\_  
 ORIGINAL CHECKED \_\_\_\_\_  
 SURVEYED \_\_\_\_\_  
 NOTED \_\_\_\_\_  
 NO. \_\_\_\_\_



	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

	CUT S.F.	FILL S.F.
PRELIMINARY		
FINAL		

Duvall

PLATE CROSS SECTION D. & R. E. STANDARD SET FOR COMPANION

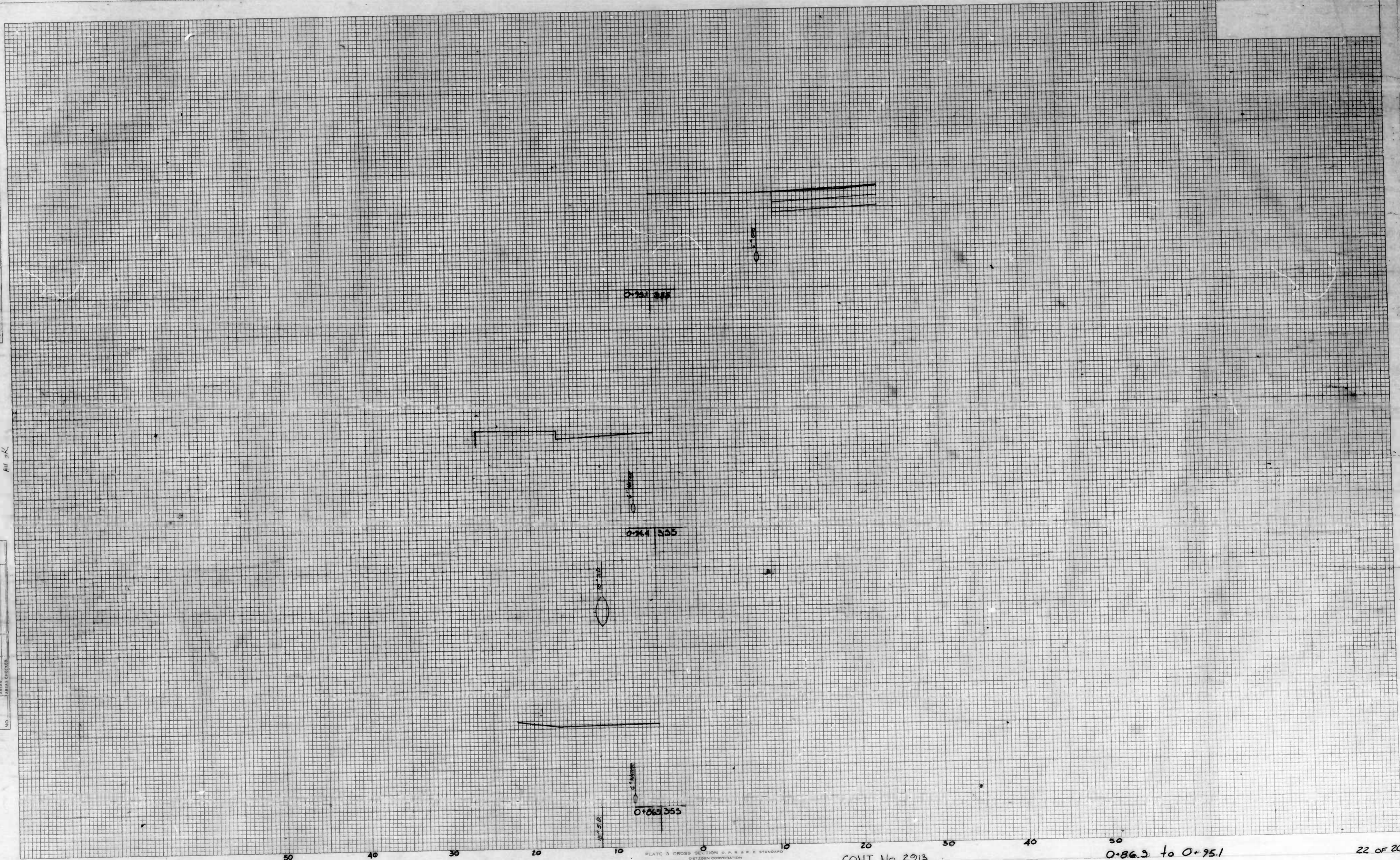
CONT No. 2013

0+00 to 0+70.7

21 OF 26

DATE	
BY	
FINAL SURVEY	
PLATINUM	
NOTE BOOK	
DATE CHECKED	
NO.	

DATE	3/2/68
BY	L.E. Suffern
ORIGINAL SURVEY	
PLATINUM	
NOTE BOOK	
DATE CHECKED	
NO.	



Duvall

PLATE 3 CROSS SECTION O.P.R.R. STANDARDS  
DETROIT CORPORATION

CONT No 2913

0+86.3 to 0+95.1

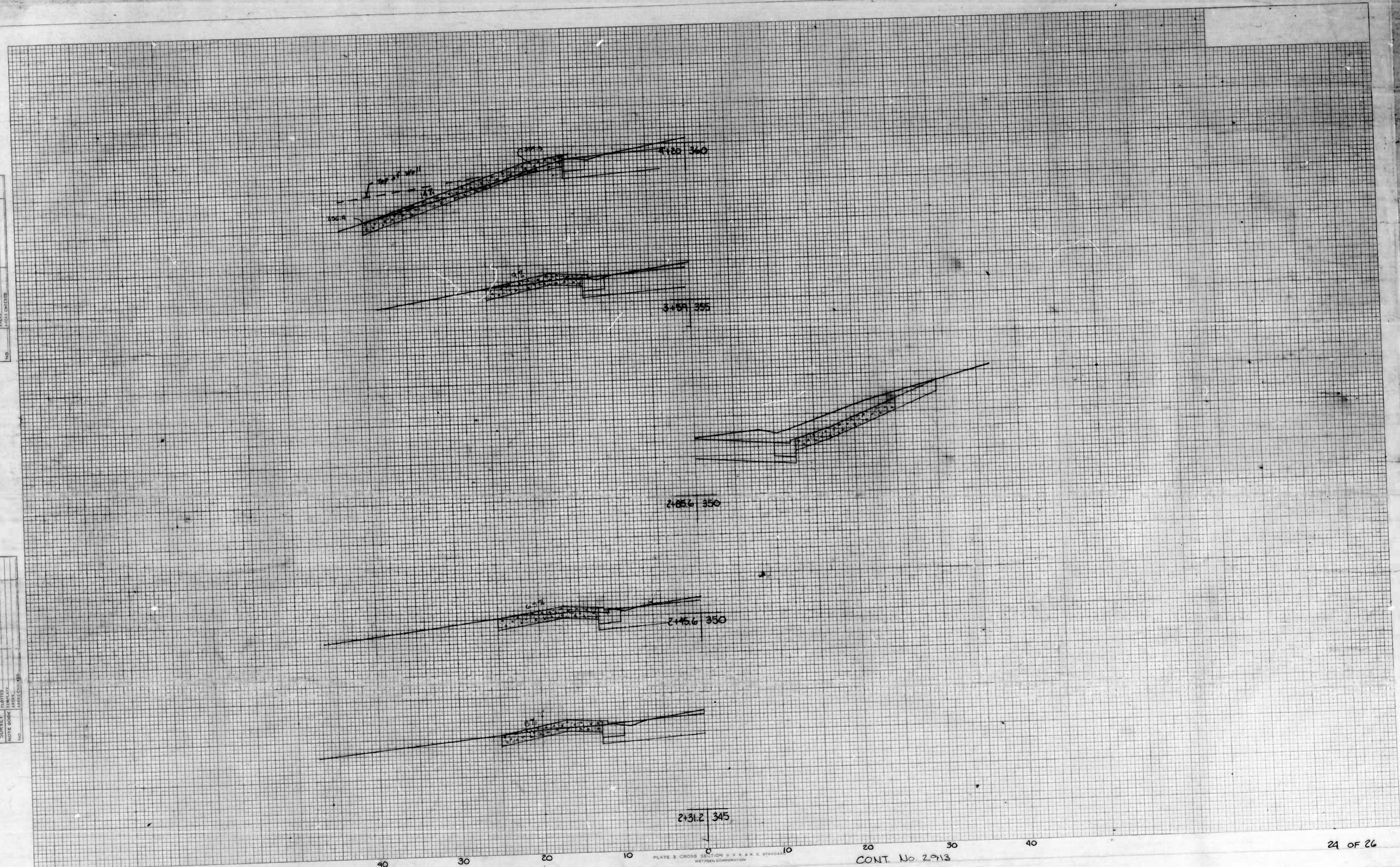
22 OF 26





DATE	
BY	
FINAL SURVEY	
PLAT	
NOTE BOOK	
NO.	

DATE	
BY	
ORIGINAL	
NOTE BOOK	
NO.	

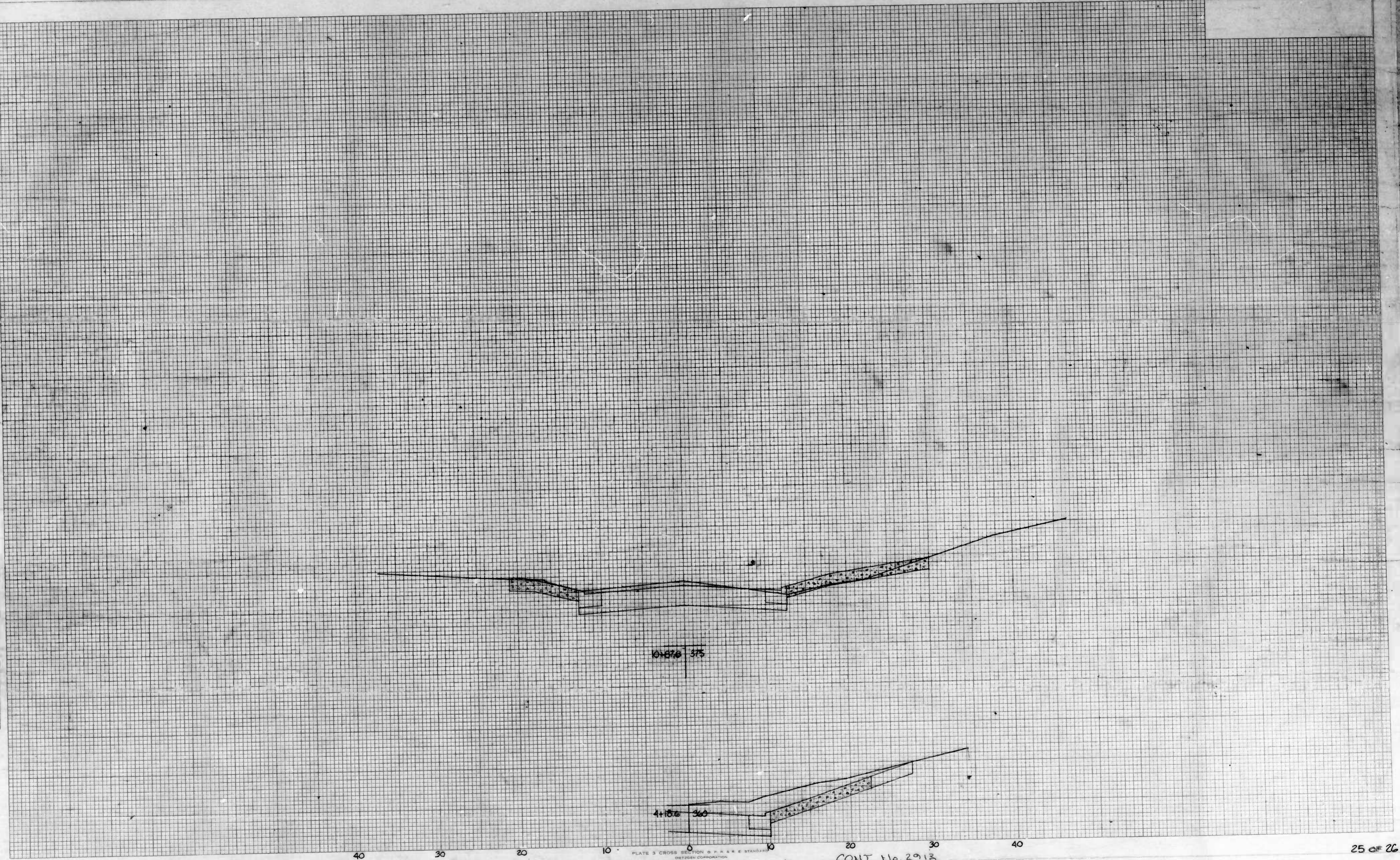


Lyndhurst Ave - Driveways

CONT. No. 2913

FINAL SURVEY	DATE
NO. OF STATIONS	
NOTE BOOK	
AREA CHECKED	
NO.	

ORIGINAL SURVEY	DATE
NO. OF STATIONS	
NOTE BOOK	
AREA CHECKED	
NO.	



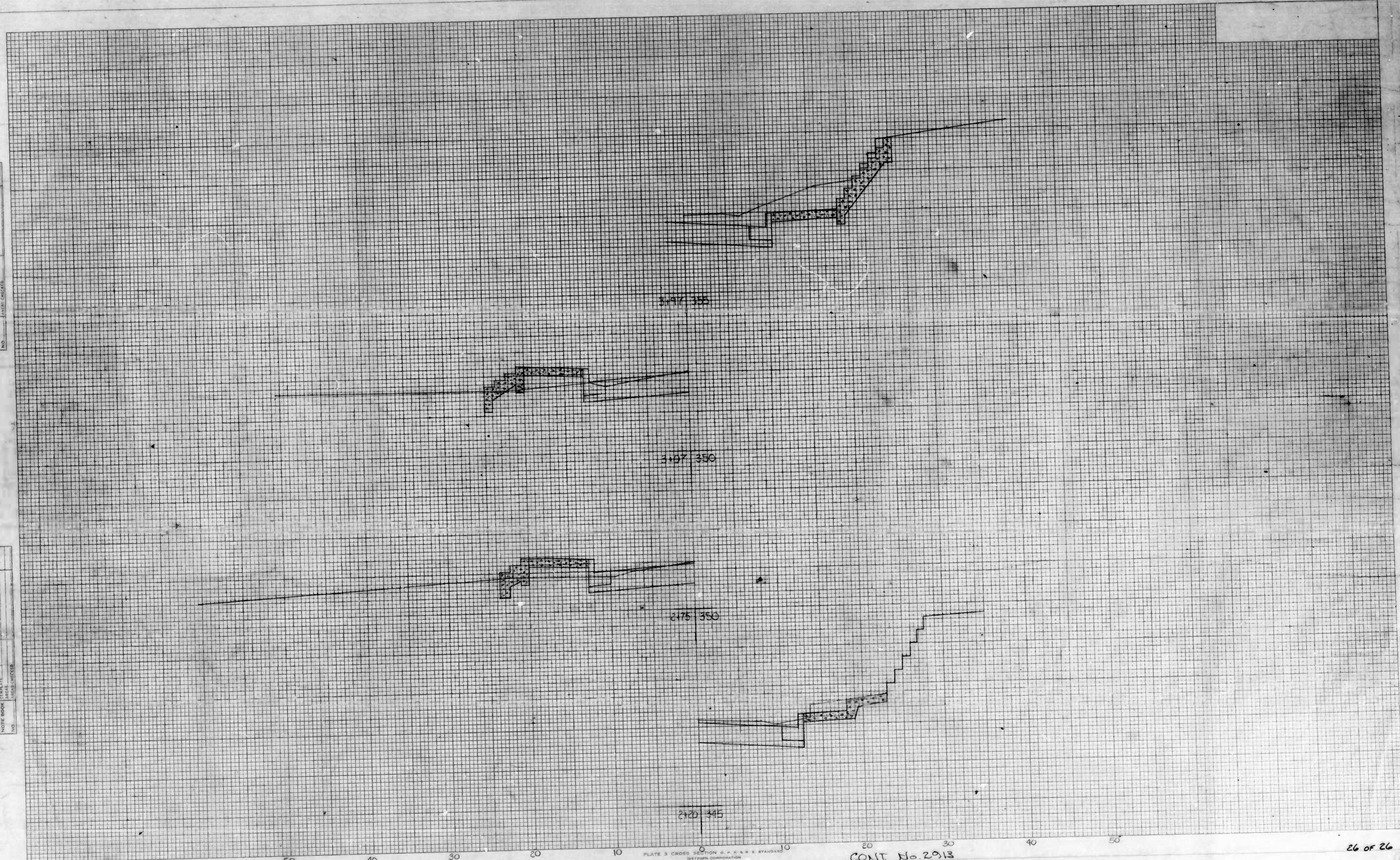
Lyndhurst Ave - Driveways

PLATE 3 CROSS SECTION S. P. A. & S. STANDARD  
DETROIT CORPORATION

CONT. No. 2913

DATE	
BY	
FINAL SURVEY	
FOR	
PROJECT	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
FOR	
PROJECT	
NO.	



Lyndhurst Ave - Steps

PLATE 3 CROSS SECTION G. P. & R. E. STANDARD  
DETROIT CORPORATION

CONT No. 2913

Lyndhurst Avenue - Index Of Cross Sections

Sht No	Description	Station To Station	Baltimore City
1	Lyndhurst Ave	0+50 To 0+85.5	
2		1+00 To 2+00	
3		2+50 To 3+50	
4		4+00 To 5+00	
5		5+290 To 6+00	
6		8+50 To 9+50	
7		10+00 To 11+00	
8		11+50 To 12+56.48	
9		13+50 To 15+50	
10		14+00 To 15+00	
11	Lyndhurst Ave	15+45.5 To 15+67	
12	Mondawmin Ave	0+00 To 1+00	
13	Mondawmin Ave	1+50 To 2+00	
14	Alto Rd	0+00 To 0+27.2	
15	Alto Rd	1+01.4 To 2+00	
16	Bateman Ave	0+00 To 1+00	
17	Bateman Ave	1+50 To 1+95	
18	Carlisle Ave	0+00 To 1+00	
19	Carlisle Ave	1+50 To 2+00	
20	Alley	0+00 To 0+64.3	
21	Duvall Ave	0+00 To 0+78.7	
22	Duvall Ave	0+86.3 To 0+95.1	
23	Duvall Ave	1+11 To 2+00	
24	Lyndhurst Ave Driveways		
25	Lyndhurst Ave Driveways		
26	Lyndhurst Ave Steps		

FIELD BOOK

No.	TITLE	DESCRIPTION
x-873	Cross Sections and Taps	Lyndhurst Ave, Duvall Ave to Bateman Ave / Alto Road to Mondawmin Ave.
x-842	Driveway Cross Sections	Lyndhurst Ave, Duvall Ave to Bateman Ave / Alto Road to Mondawmin Ave.

DATE  
BY  
FINAL SURVEY NOTE BOOK NO.

DATE  
BY  
ORIGINAL SURVEY NOTE BOOK NO.