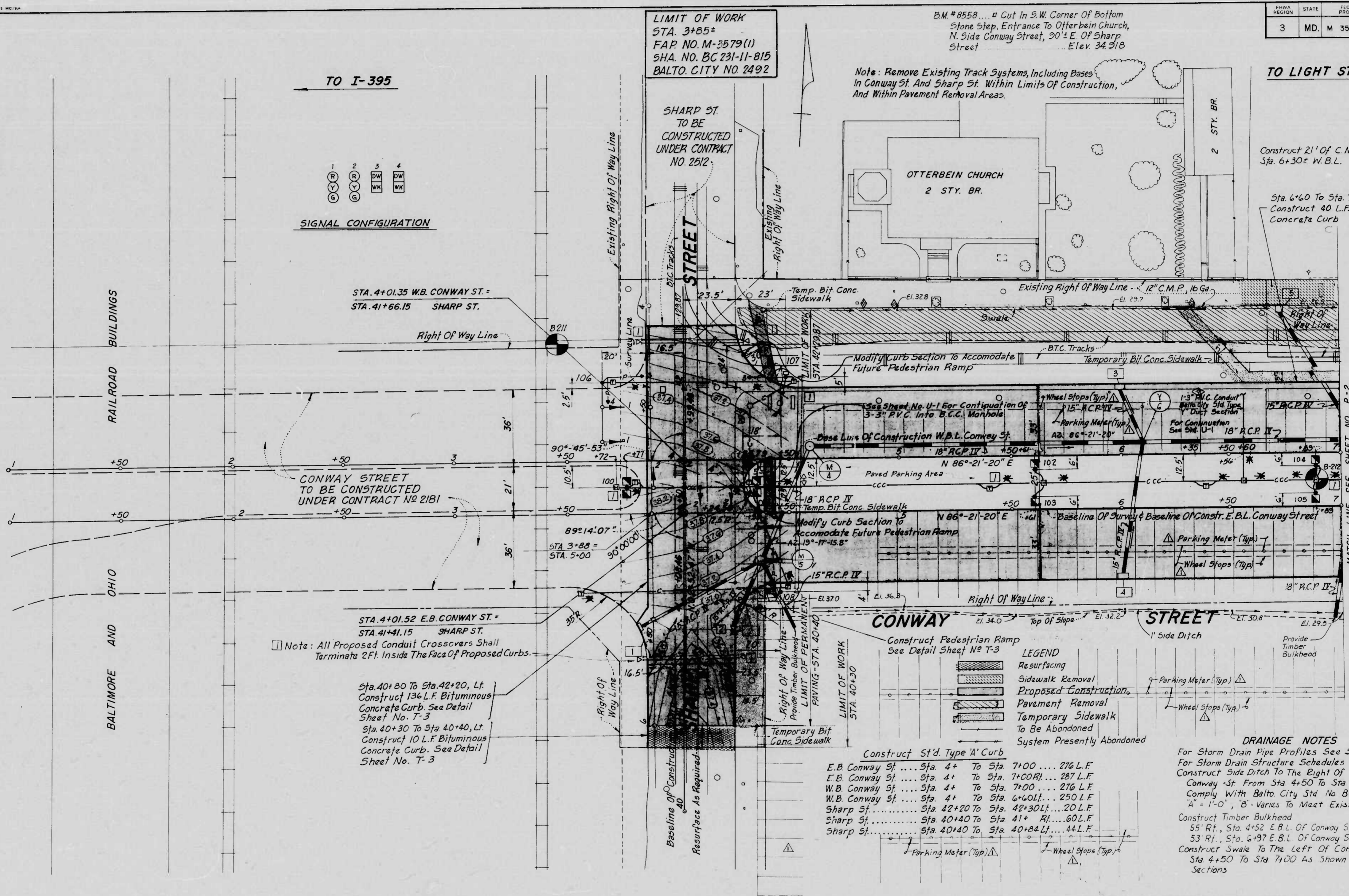


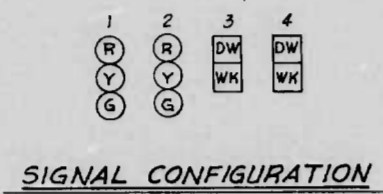
K&E BALTIMORE 11-848 PROJEK 11-78 10/17/78

| | | | | |
|-------------|-------|--------------------|-----------|--------------|
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M 3579 (1) | P-1 | (4) |



TO I-395

TO LIGHT ST.



LIMIT OF WORK
 STA. 3+85±
 FAP NO. M-3579(1)
 SHA. NO. BC 231-11-815
 BALTO. CITY NO. 2492

Note: Remove Existing Track Systems, Including Bases In Conway St. And Sharp St. Within Limits Of Construction, And Within Pavement Removal Areas.

Construct 21' Of C.M. Pipe 69" Lt. Sta. 6+30± W.B.L.

Sta. 6+60 To Sta. 7+00, Lt. Construct 40 L.F. Bituminous Concrete Curb

STA. 4+01.35 W.B. CONWAY ST. =
 STA. 41+66.15 SHARP ST.

CONWAY STREET TO BE CONSTRUCTED UNDER CONTRACT NO. 2181

STA. 4+01.52 E.B. CONWAY ST. =
 STA. 41+41.15 SHARP ST.

Note: All Proposed Conduit Crossovers Shall Terminate 2 Ft. Inside The Face Of Proposed Curbs.

Sta. 40+80 To Sta. 42+20, Lt. Construct 136 L.F. Bituminous Concrete Curb. See Detail Sheet No. T-3
 Sta. 40+30 To Sta. 40+40, Lt. Construct 10 L.F. Bituminous Concrete Curb. See Detail Sheet No. T-3

LEGEND

- Resurfacing
- Sidewalk Removal
- Proposed Construction
- Pavement Removal
- Temporary Sidewalk
- To Be Abandoned
- System Presently Abandoned

DRAINAGE NOTES
 For Storm Drain Pipe Profiles See Sht. No. P-8 thru P-11
 For Storm Drain Structure Schedules See Sheet No. P-13
 Construct Side Ditch To The Right Of Conway St. From Sta. 4+50 To Sta. 7+00 To Comply With Balto. City Std. No. BC 389.04
 "A" = 1'-0", "B" = varies To Meet Existing Grade
 Construct Timber Bulkhead
 55 Rt., Sta. 4+52 E.B.L. Of Conway St.
 53 Rt., Sta. 6+97 E.B.L. Of Conway St.
 Construct Swale To The Left Of Conway St. From Sta. 4+50 To Sta. 7+00 As Shown On The Typical Sections

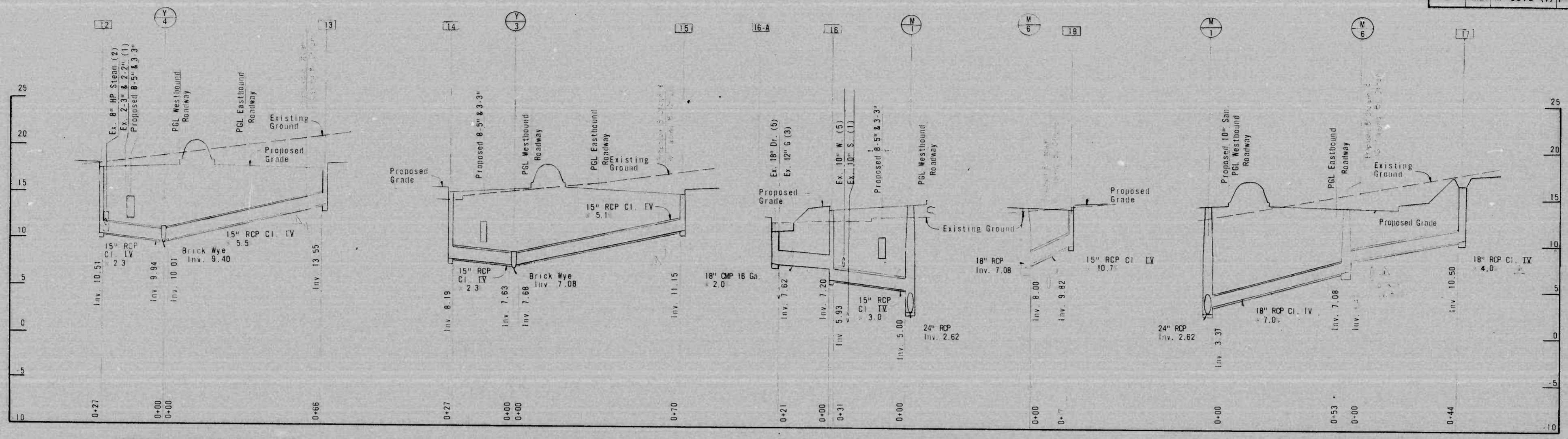
Construct Std. Type A' Curb

| | | |
|----------------------|----------------------------|--------------|
| E.B. Conway St. | Sta. 4+ ... To Sta. 7+00 | ... 276 L.F. |
| E.B. Conway St. | Sta. 4+ ... To Sta. 7+00 | ... 287 L.F. |
| W.B. Conway St. | Sta. 4+ ... To Sta. 7+00 | ... 276 L.F. |
| W.B. Conway St. | Sta. 4+ ... To Sta. 6+40 | ... 250 L.F. |
| Sharp St. | Sta. 42+20 To Sta. 42+30 | ... 20 L.F. |
| Sharp St. | Sta. 40+40 To Sta. 41+ ... | ... 60 L.F. |
| Sharp St. | Sta. 40+40 To Sta. 40+84 | ... 44 L.F. |

For Profile Conway Street, See Sheet No. P-4
 For Geometry, See Sheet No. T-6
 For Profile Sharp Street, See Sheet No. P-5

| | | | | |
|---|--|--|--|------------------------------------|
| REVISIONS Addendum No. 1 - 10/21/78 1 Revisions To Conduits & Duct Markers... 12/7/78 2 Storm Drain, Rt. Sta. 4+... 3/26/79 | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS CONWAY STREET SHARP STREET TO LIGHT STREET PLAN - Sta. 3+ To Sta. 7+00 | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | | | DRAWN BY: J.F. TRACED BY: F.L. | DES. BY: W.R.H. CHK. BY: L.M.H. |

| FED. REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-10 | P-17 (41) |

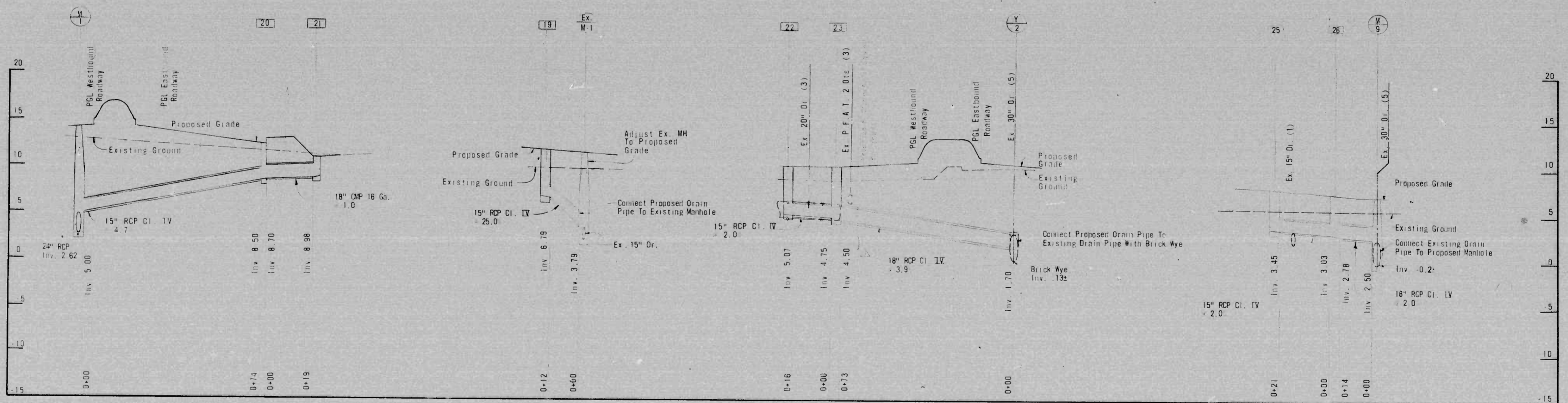


LEFT & RIGHT OF STA. 10+20±

LEFT & RIGHT OF STA. 11+00±

LEFT & RIGHT OF STA. 11+60±

RIGHT OF STA. 11+60±



RIGHT OF STA. 11+60±

LEFT OF STA. 12+35±

LEFT OF STA. 13+50±

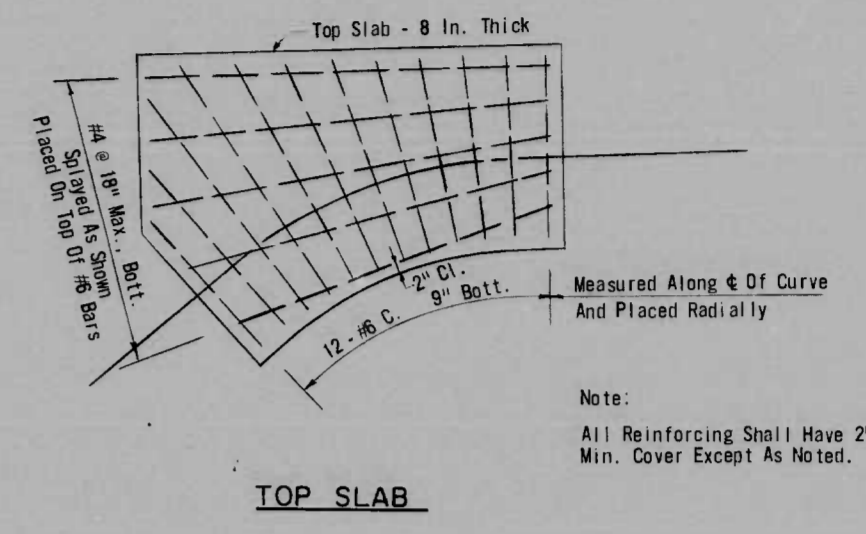
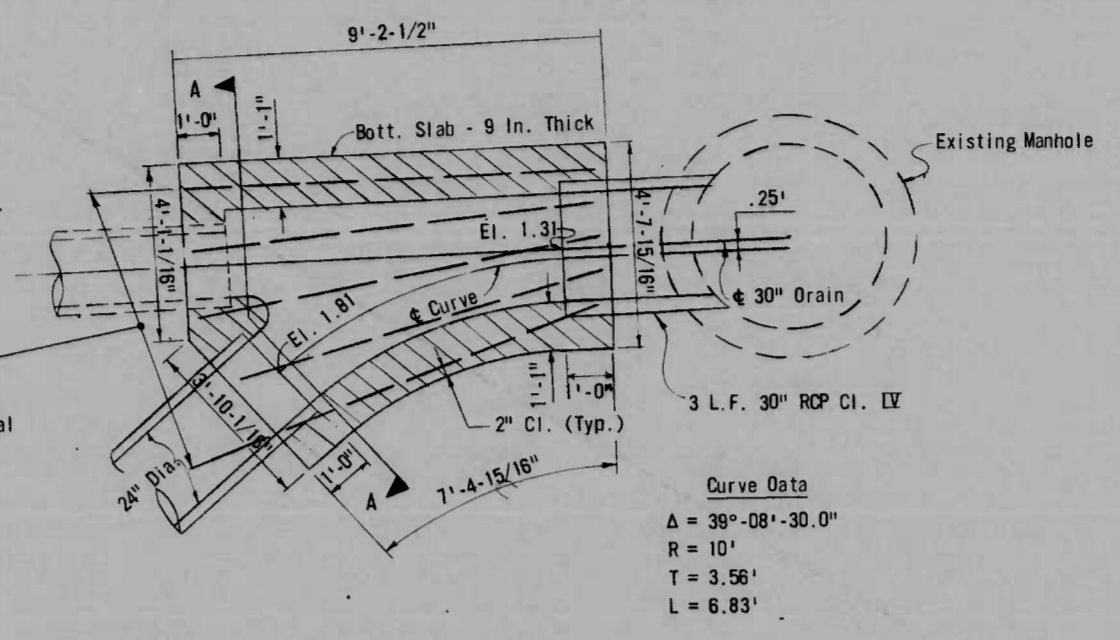
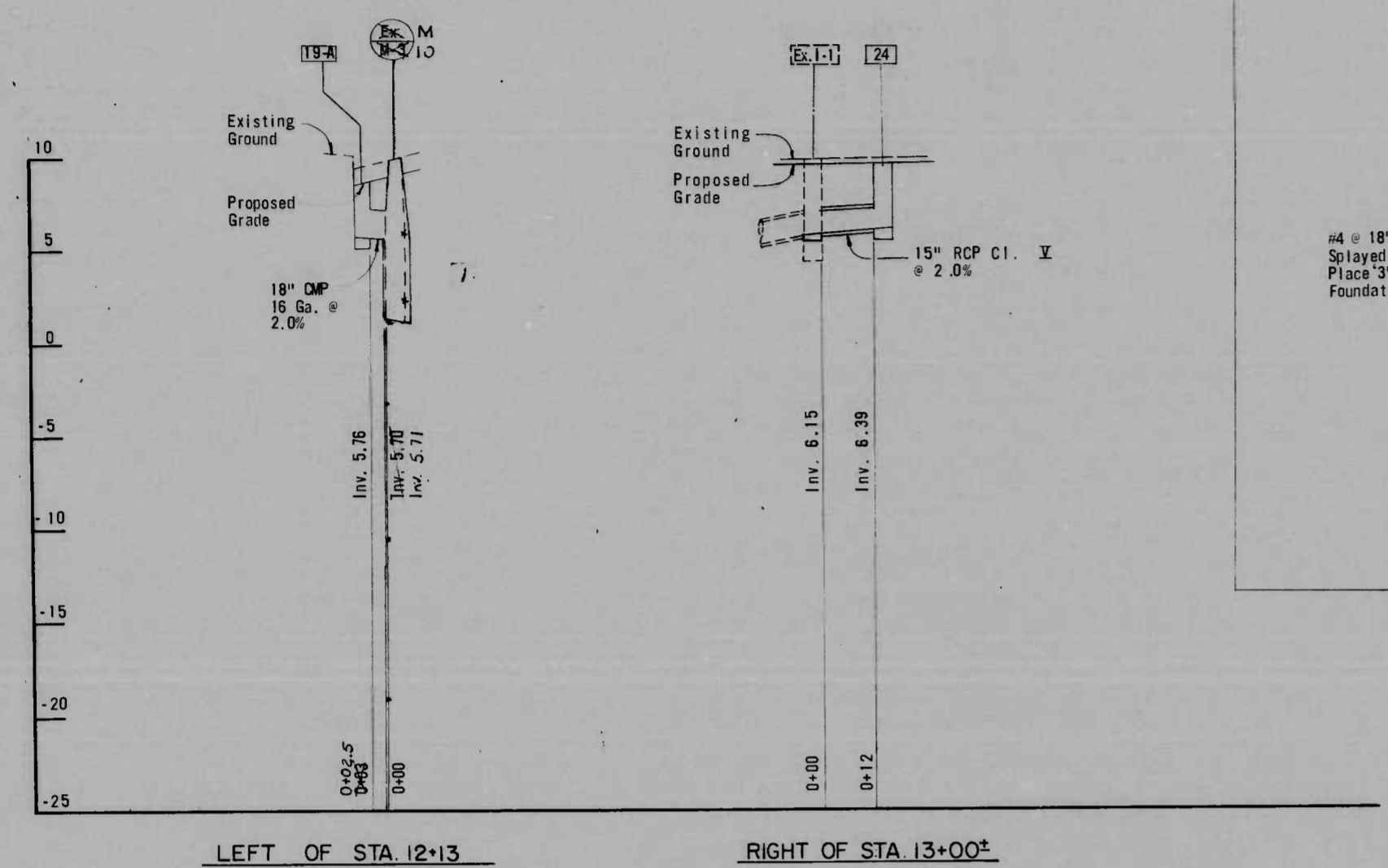
LEFT OF STA. 15+75±

- UTILITY DISPOSITION NOTATION
- (1) Existing Utility To Be Abandoned By Contractor
 - (2) Existing Utility To Be Abandoned By Others
 - (3) Existing Utility Presently Abandoned
 - (4) Existing Utility, Location And/Or Depth Unknown
 - (5) Existing Utility To Remain In Service
 - (6) Existing Utility To Be Relocated By Others

NOTE
Provide Gravel Grade Under All Storm Drain Systems Shown This Sheet. Unless Noted Otherwise See City Of Baltimore Standard No. BC-302.01 For Gravel Dimensions And Material

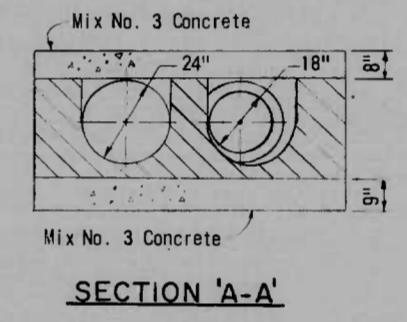
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|-----------|------------------------|--|------------------|
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET | |
| | CONSULTING ENGINEERS | SHARP STREET TO LIGHT STREET | |
| | BALTIMORE, MARYLAND | PROFILES-STORM WATER DRAIN | |
| | | SCALE: HORIZ. 1"=20' VERT. 1"=5' | DATE |
| | | DRAWN BY: MLK | DES. BY: MLK |
| | | TRACED BY: DDH | CHK BY: JAR, JR. |
| | | F.A.P. NO. M 3579 (1) | SHEET NO. |
| | | S.H.A. NO. BC 231-11-915 | P-10 OF P-17 |
| | | BALTO. CITY NO. 2492 | |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-11 | (41) |



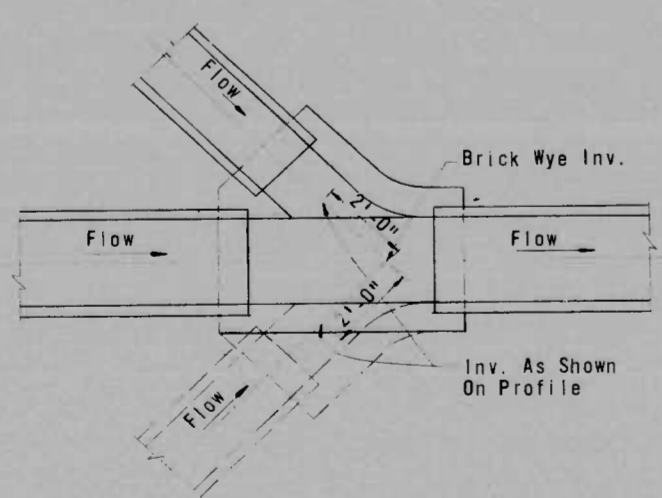
BELLMOUTH DETAIL

SCALE: 3/8" = 1'-0"
60' LT. STA. 12+30 W.B.L.

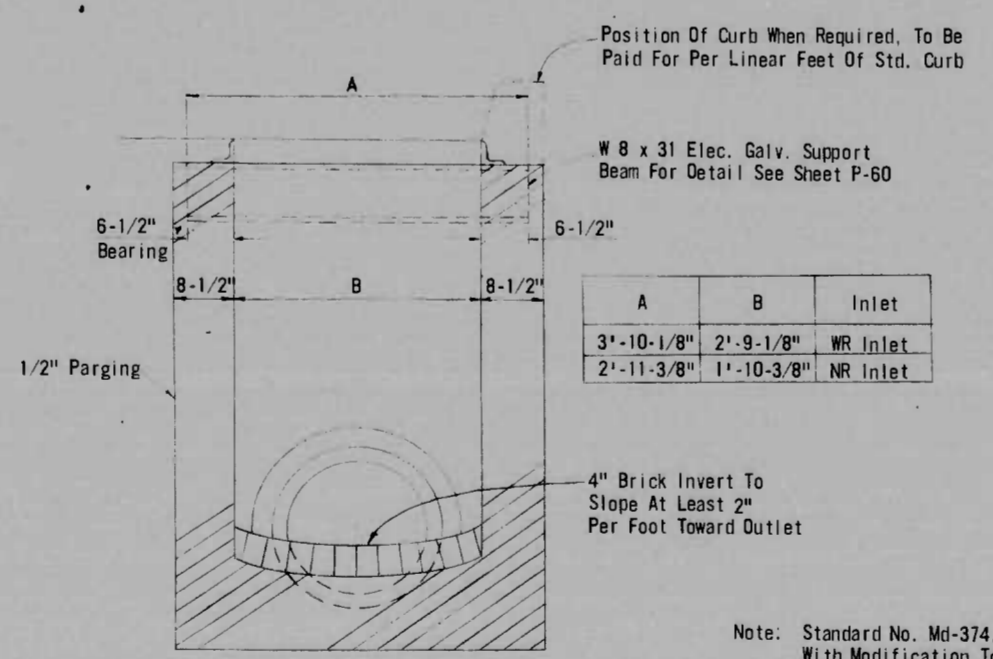


NOTE: Provide Gravel Cradle Under All Storm Drain Systems Shown This Sheet. Unless Noted Otherwise See City Of Baltimore Standard No. BC-302.01 For Cradle Dimensions And Material.

NOTE: For Details Of Brick 'Y' See Balto. City Std. No. BC-381.01

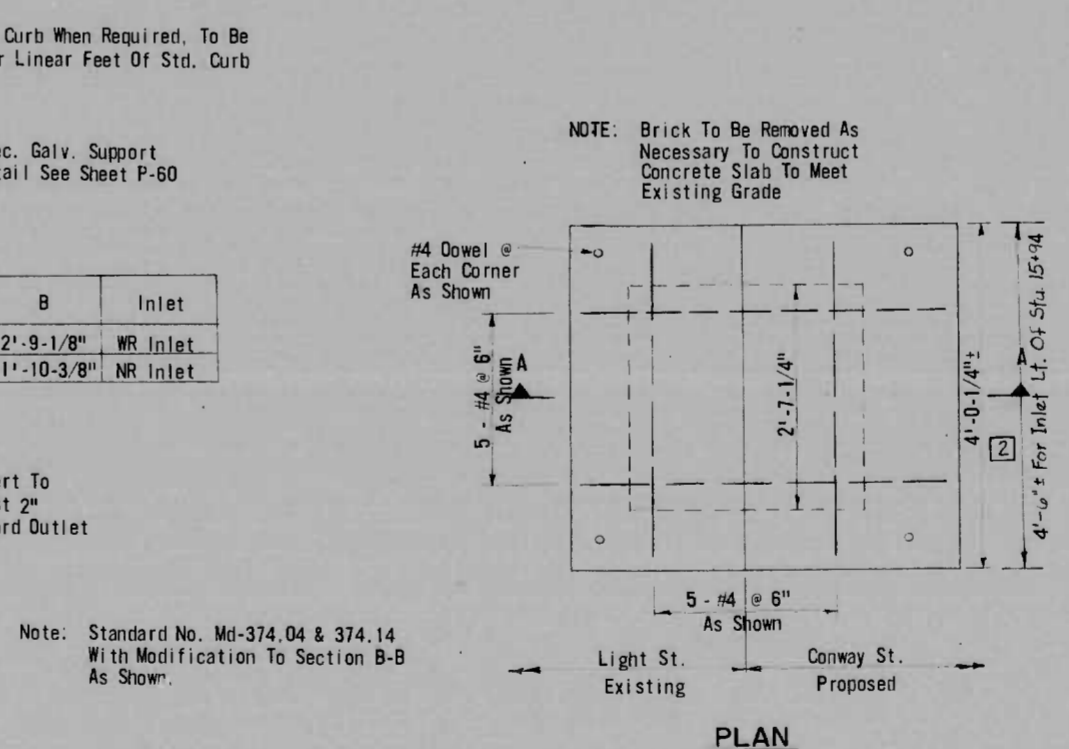


BRICK 'Y' LAYOUT SINGLE & DOUBLE



STD. NR INLET & SINGLE WR INLET MODIFIED TYPE I

SCALE: 3/4" = 1'-0"



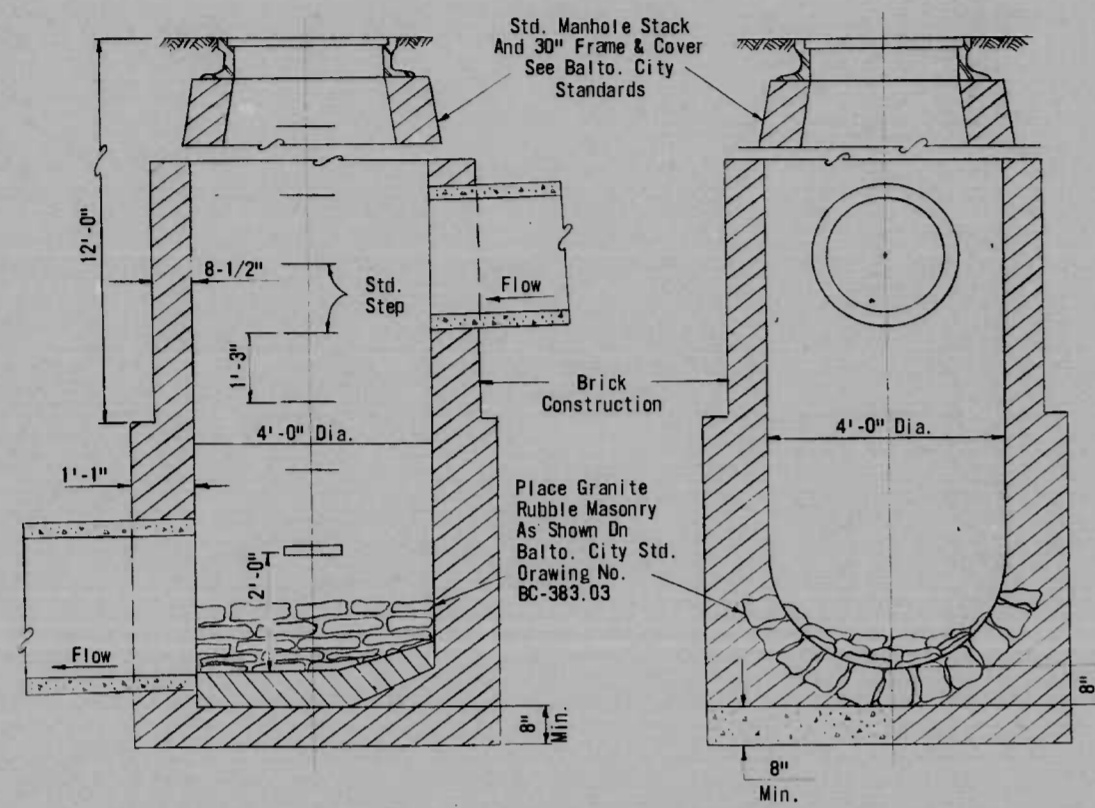
CONCRETE SLAB OVER EXISTING INLET 4' LT. STA. 16+07 W.B.L.

SCALE: 3/4" = 1'-0"

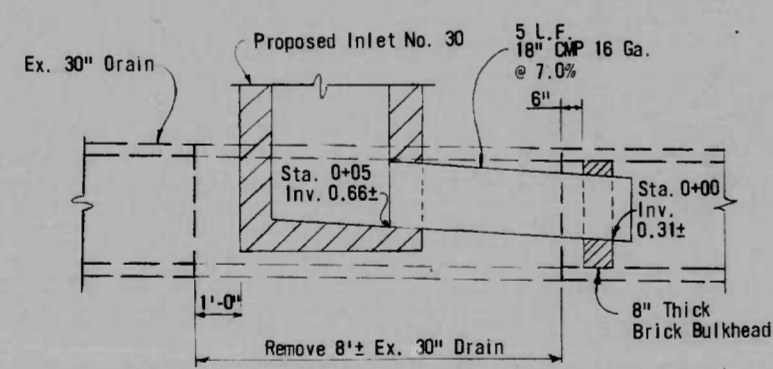
And Concrete Slab Over Existing Inlet 4' Lt. Sta 15+94 @ W.B.L.

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|--|--|--|---|
| 1. Revise Concrete Slab Detail ... 3/26/79 | RUMMEL ; KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILES & DETAILS-STORM WATER DRAINS SCALE: AS NOTED | DATE: _____ DRAWN BY: DDH TRACED BY: DDH DES. BY: MLK CHK. BY: WHB/JAR Jr. F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |

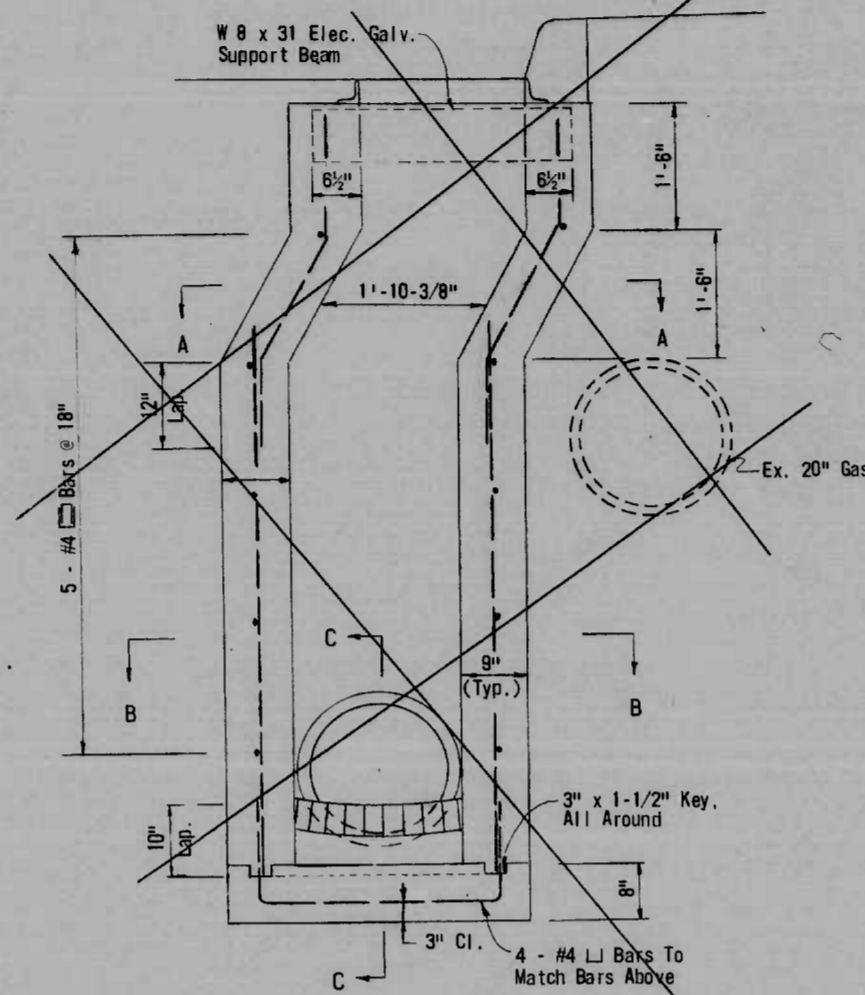
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-12 | (41) |



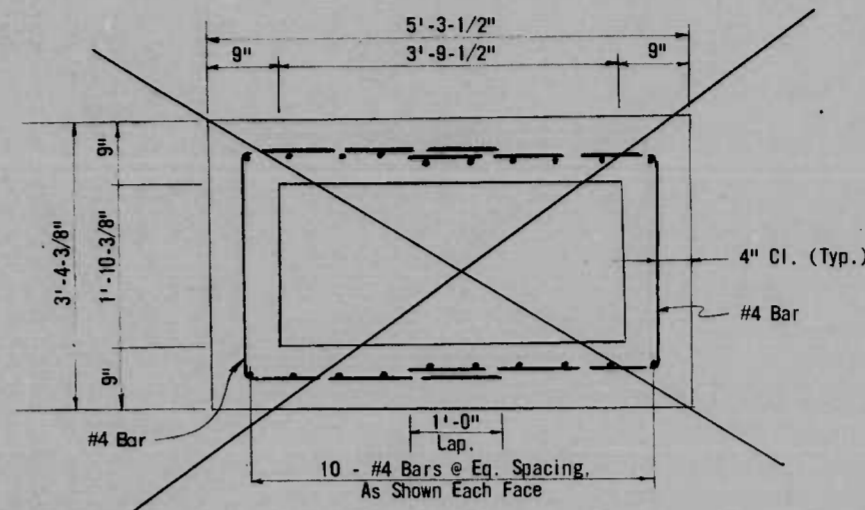
DETAIL - STD. S.W. MH (MOD.) TYPE 1
SCALE 1/2 = 1'-0"



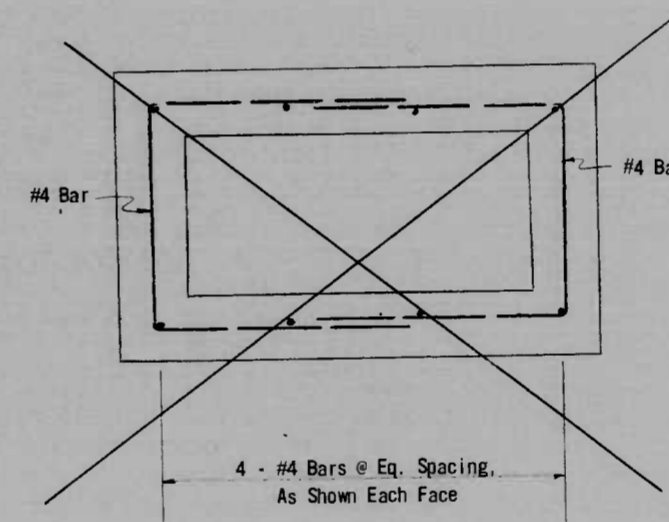
DRAIN CONNECTION, LEFT STA., I5+50
SCALE 3/8 = 1'-0"



SECTION C-C



SECTION A-A



SECTION B-B

NR INLET MODIFIED TYPE 2
SCALE 3/4 = 1'-0"

NOTES:
Concrete: Mix No. 3
All Reinforcing To Have 2" Min. Clear
Unless Otherwise Noted.

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|------------------------|------------|--|--|
| ☐ Delete Inlet 3/26/79 | | CONWAY STREET SHARP STREET TO LIGHT STREET DETAILS-STORM WATER DRAIN | |
| SCALE: AS NOTED | | DATE: | DRAWN BY MLK TRACED BY TJA F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| | | | DES. BY MLK CHK. BY JAR, JR. SHEET NO. P-12 of P-17 |

| | | | | |
|-------------|-------|--------------------|-----------|--------------|
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M 3579 (1) | P-13 | (41) |

DRAINAGE STRUCTURE SCHEDULE

| INLET SCHEDULE | | | | | |
|----------------|------------------------------------|---|--------------|--------|--|
| NO. | TYPE | LOCATION | TOP OF GRATE | INVERT | REMARKS |
| 1 | STD. NR INLET, MOD. TYPE 1 | 56.67' RT. STA. 3+81 BASE LINE, E.B.L. | 36.6 | 28.17 | SHA STD. NO. 374.14 |
| 2 | NR INLET, MOD. TYPE 1 | 33' RT. STA. 3+75 BASE LINE, E.B.L. | 36.6 | 37.37 | SHA STD. NO. 374.14 MOD. WIDTH TO FIT BETWEEN UTILITIES |
| 3 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 6+00 BASE LINE, W.B.L. | 32.25 | 25.81 | SHA STD. NO. 374.14 |
| 4 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 6+00 BASE LINE, E.B.L. | 32.25 | 28.21 | SHA STD. NO. 374.14 |
| 5 | STD. TYPES 'S' INLET, SINGLE GRATE | 68' LT. STA. 6+75 BASE LINE, W.B.L. | 27.3 | 23.06 | |
| 6 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 7+10 BASE LINE, W.B.L. | 28.12 | 20.90 | SHA STD. NO. 374.14 |
| 7 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 7+10 BASE LINE, E.B.L. | 28.12 | 24.20 | SHA STD. NO. 374.14 |
| 8 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 8+20 BASE LINE, W.B.L. | 24.00 | 17.08 | SHA STD. NO. 374.14 |
| 9 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 8+20 BASE LINE, E.B.L. | 24.00 | 20.35 | SHA STD. NO. 374.14 |
| 10 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 9+30 BASE LINE, W.B.L. | 19.90 | 13.08 | SHA STD. NO. 374.14 |
| 11 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 9+30 BASE LINE, E.B.L. | 19.90 | 16.38 | SHA STD. NO. 374.14 |
| 12 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 10+10 BASE LINE, W.B.L. | 17.28 | 10.51 | SHA STD. NO. 374.14 |
| 13 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 10+00 BASE LINE, E.B.L. | 17.58 | 13.55 | SHA STD. NO. 374.14 |
| 14 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 10+90 BASE LINE, W.B.L. | 15.09 | 8.19 | SHA STD. NO. 374.14 |
| 15 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 10+70 BASE LINE, E.B.L. | 15.60 | 11.15 | SHA STD. NO. 374.14 |
| 16 | STD. NR INLET, MOD. TYPE 1 | 42' LT. STA. 11+60 BASE LINE, W.B.L. | 13.2 | 5.93 | SHA STD. NO. 374.14 |
| 16A | STD. TYPE 'S' INLET, SINGLE GRATE | 63' LT. STA. 11+60 BASE LINE, W.B.L. | 11.1 | 7.62 | |
| 17 | STD. TYPE 'S' INLET, SINGLE GRATE | 54' RT. STA. 11+00 BASE LINE, E.B.L. | 16.5 | 10.50 | |
| 18 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 11+40 BASE LINE, E.B.L. | 13.89 | 9.82 | SHA STD. NO. 374.14 |
| 19 | STD. NR INLET, MOD. TYPE 1 | 34.74' LT. STA. 12+20 BASE LINE, W.B.L. | 12.1 | 6.79 | SHA STD. NO. 374.14 |
| 19A | STD. TYPE 'S' INLET, SINGLE GRATE | 63' LT. STA. 12+13 BASE LINE, W.B.L. | 8.6 | 5.76 | |
| 20 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 12+05 BASE LINE, E.B.L. | 12.33 | 8.50 | SHA STD. NO. 374.14 |
| 21 | STD. TYPE 'S' INLET, SINGLE GRATE | 50' RT. STA. 12+17 BASE LINE, E.B.L. | 11.0 | 8.98 | |
| 22 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 13+20 BASE LINE, W.B.L. | 10.11 | 5.07 | SEE DETAIL SHEET NO. P-12 |
| 23 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 13+40 BASE LINE, W.B.L. | 10.05 | 4.50 | SHA STD. NO. 374.14 |
| 24 | SINGLE WR INLET, MOD. TYPE 1 | 33' RT. STA. 13+02 BASE LINE, E.B.L. | 10.02 | 6.39 | SHA STD. NO. 374.06 |
| 25 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 15+50 BASE LINE, W.B.L. | 7.42 | 3.45 | SHA STD. NO. 374.14 |
| 26 | STD. NR INLET, MOD. TYPE 1 | 33' LT. STA. 15+75 BASE LINE, W.B.L. | 6.89 | 2.78 | SHA STD. NO. 374.14 |
| 27 | STD. NR INLET, MOD. TYPE 1 | 33' RT. STA. 15+20 BASE LINE, E.B.L. | 8.6 | 5.00 | SHA STD. NO. 374.14 |
| 28 | SINGLE WR INLET, MOD. TYPE 1 | 33' RT. STA. 16+09 BASE LINE, E.B.L. | 7.7 | 3.7 | SHA STD. NO. 374.06 |
| 29 | SINGLE WR INLET, MOD. TYPE 1 | 12.5' LT. STA. 16+06 BASE LINE, E.B.L. | 6.89 | 4.63 | SHA STD. NO. 374.06 |
| 30 | STD. TYPE 'S' INLET, SINGLE GRATE | 58' LT. STA. 15+55 BASE LINE, W.B.L. | 5.5 | 0.66 | SEE DETAIL SHEET NO. P-12 |
| EX. 1-1 | EXISTING INLET | 33' RT. STA. 12+88 BASE LINE, E.B.L. | | | |
| EX. 1-2 | EXISTING INLET | 4' LT. STA. 16+07 BASE LINE, W.B.L. | | | REMOVE GRATE AND REPLACE WITH CONCRETE SLAB SEE DETAIL SHEET NO. 11 |

| BRICK 'Y' SCHEDULE | | | | |
|--------------------|---|--------------------------------------|--------|---------------------------|
| NO. | TYPE | LOCATION | INVERT | REMARKS |
| 1 | 12" x 15" x 15" PIPE CONNECTION, DOUBLE BRICK 'Y' | 22' RT. STA. 15+47 BASE LINE, E.B.L. | -0.21 | |
| 2 | 36" x 18" PIPE CONNECTION, BRICK 'Y' | 13' RT. STA. 13+85 BASE LINE, E.B.L. | 0.13' | |
| 3 | 21" x 15" x 15" PIPE CONNECTION, DOUBLE BRICK 'Y' | 6' LT. STA. 11+00 BASE LINE, W.B.L. | 7.08' | |
| 4 | 21" x 15" x 15" PIPE CONNECTION, DOUBLE BRICK 'Y' | 6' LT. STA. 10+20 BASE LINE, W.B.L. | 9.40' | |
| 5 | 21" x 15" x 15" PIPE CONNECTION, DOUBLE BRICK 'Y' | 6' LT. STA. 8+30 BASE LINE, W.B.L. | 15.05' | |
| 6 | 18" x 15" x 15" PIPE CONNECTION, DOUBLE BRICK 'Y' | 6' LT. STA. 6+10 BASE LINE, W.B.L. | 23.11' | |
| | | 10' LT. STA. 12+30 BASE LINE, W.B.L. | | SEE DETAIL SHEET NO. P-11 |

| MANHOLE SCHEDULE | | | | | | | |
|------------------|--------------------------------|--|--------------|--------|-------------|-------------|---------------------------|
| NO. | TYPE | LOCATION | TOP OF GRATE | INVERT | COORDINATES | | REMARKS |
| | | | | | SOUTH | EAST / WEST | |
| 1 | STD. S.W. MANHOLE, MOD. TYPE 1 | 6' LT. STA. 11+60 BASE LINE, W.B.L. | 13.95 | 2.62 | 4837.3043 | 187.2543 E | SEE DETAIL SHEET NO. P-12 |
| 2 | STD. STORM WATER MANHOLE | 6' LT. STA. 9+30 BASE LINE, W.B.L. | 20.44 | 11.90 | 4851.9240 | 42.2805 W | |
| 3 | STD. S.W. MANHOLE, MOD. TYPE 1 | 6' LT. STA. 7+10 BASE LINE, W.B.L. | 28.66 | 18.00 | 4865.9081 | 261.8357 W | SEE DETAIL SHEET NO. P-12 |
| 4 | STD. STORM WATER MANHOLE | 6' LT. STA. 4+60 BASE LINE, W.B.L. | 37.08 | 25.33 | 4881.7991 | 511.3301 W | |
| 5 | STD. STORM WATER MANHOLE | 21' RT. STA. 4+38 BASE LINE, E.B.L. | 37.14 | 26.77 | 4935.0923 | 523.9803 W | |
| 6 | STD. STORM WATER MANHOLE | 16.5' RT. STA. 11+27 BASE LINE, E.B.L. | 14.53 | 7.08 | 4886.8059 | 157.3403 E | |
| 7 | STD. STORM WATER MANHOLE | 16.5' RT. STA. 9+11 BASE LINE, E.B.L. | 20.92 | 14.88 | 4900.5357 | 58.2229 W | |
| 8 | STD. STORM WATER MANHOLE | 16.5' RT. STA. 7+02 BASE LINE, E.B.L. | 28.75 | 22.20 | 4913.8206 | 266.8002 W | |
| 9 | STD. STORM WATER MANHOLE | 35' LT. STA. 15+92 BASE LINE, W.B.L. | 6.9 | -0.21 | 4780.9033 | 616.5374 E | |
| 10 | STD. STORM WATER MANHOLE | 60' LT. STA. 12+19 BASE LINE, W.B.L. | 11.0 | 1.60 | 4779.3937 | 243.6245 E | 5' Dia. @ Inv. |
| EX. M-1 | EXISTING MANHOLE | 36' LT. STA. 12+36 BASE LINE, W.B.L. | | | | | |
| EX. M-2 | EXISTING MANHOLE | 60' LT. STA. 12+36 BASE LINE, W.B.L. | | | | | |
| EX. M-3 | EXISTING MANHOLE | 30' LT. STA. 12+19 BASE LINE, W.B.L. | | | | | |

NOTES: TOP OF GRATE ELEVATION, STATION AND OFFSET ARE REFERENCED TO CENTERLINE OF INLET AT FACE OF CURB FOR INLETS CONSTRUCTED ADJACENT TO CURB OR CURB AND GUTTER.

ALL STD. TYPE 'S' INLETS, SINGLE GRATE, TO CONFORM TO CITY OF BALTO. STD. NO. B.C. 379.01.
 FOR BRICK WYE LAYOUT SEE SHEET NO. P-11
 ALL STD. STORM WATER MANHOLES TO CONFORM TO CITY OF BALTO. STD. NOS. B.C. 383.01 OR B.C. 383.02.
 FOR DETAIL OF STD. NR INLET, MOD. TYPE 1, AND SINGLE WR INLET, MOD. TYPE 1, SEE SHEET NO. P-11.

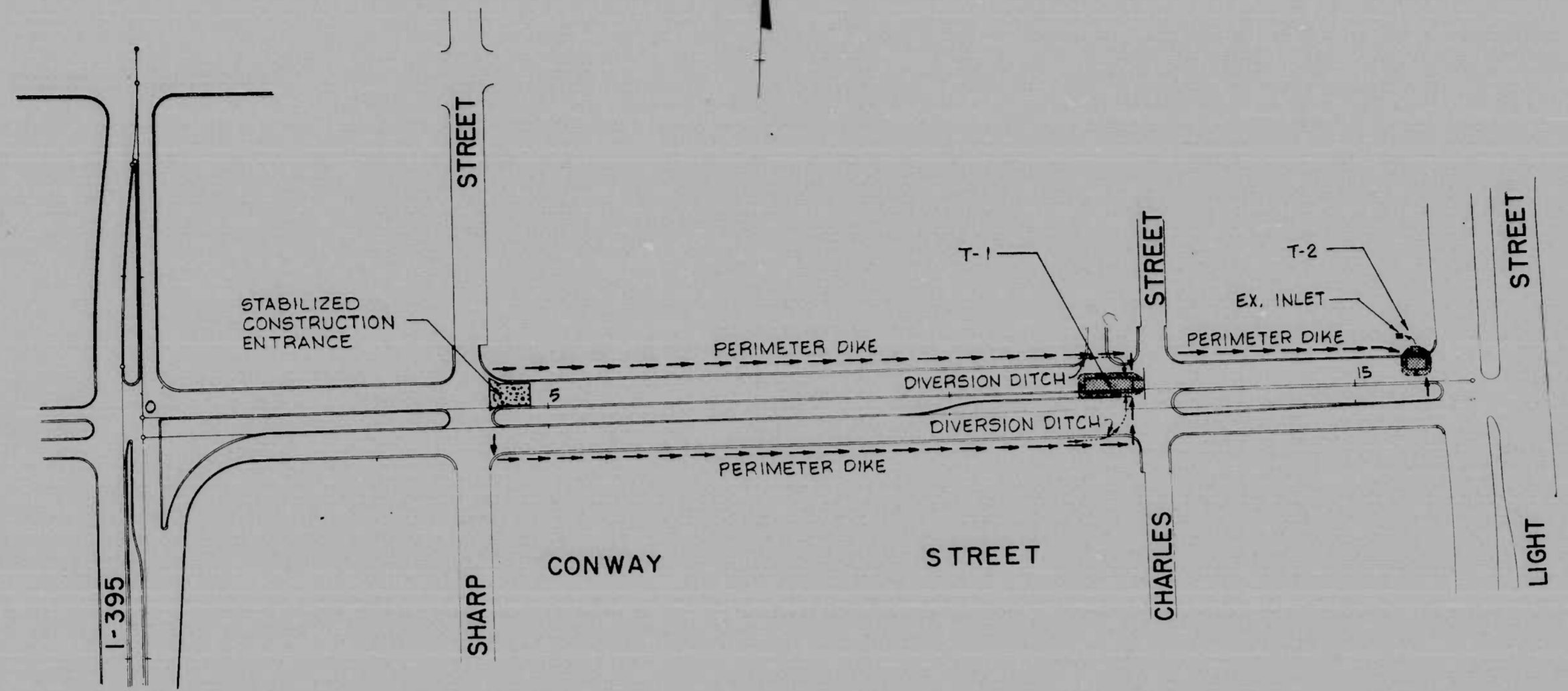
EXPLANATORY NOTES AND REFERENCES

ROOF DRAINS
 ANY EXISTING ROOF DRAINS ENCOUNTERED DURING CONSTRUCTION, WHICH HAVE NOT BEEN INDICATED ON THE PLANS, SHALL BE CONNECTED TO THE CURB LINE USING 4" P.V.C. PIPE WHERE DIRECTED BY THE ENGINEER.
 PIPE CONNECTIONS TO EXISTING MASONRY DRAINS
 PRIOR TO REMOVING ANY MASONRY FOR CONNECTING PROPOSED STORM DRAINAGE SYSTEMS INTO EXISTING CIRCULAR DRAINS, ALL OVERBURDEN SHALL BE REMOVED FROM THE EXISTING DRAIN FOR A LENGTH OF 2-1/2 FEET EACH SIDE OF THE CENTERLINE OF THE PROPOSED PIPE CONNECTION AND TRANSVERSELY TO VERTICAL PLANES WHICH PASS THROUGH THE INTERIOR LATERAL WIDTH OF THE EXISTING DRAIN. ALL COSTS IN REMOVING AND BACKFILLING OVERBURDEN SHALL BE INCIDENTAL TO THE PRICE BID FOR THE RESPECTIVE PIPE ITEMS IN THE PROPOSAL.

| BRICK CURVE SCHEDULE | | | |
|----------------------|-------------------|-------------|--------------------|
| TYPE | LOCATION | COORDINATES | |
| | | SOUTH | EAST |
| 24" BRICK CURVE | Plan Sheet N° P-3 | PC | 4784.5954 242.9526 |
| | | PT | 4790.1537 240.0234 |

| | | | |
|--|--|---|--|
| REVISIONS [1] Storm Drain, Lt. Sta. 12+130/79 Storm Drain, Rt. Sta. 15+651. 3/26/79 | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS CONWAY STREET SHARP STREET TO LIGHT STREET DRAINAGE STRUCTURE SCHEDULE | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: W.B. TRACED BY: W.B. F.A.P. NO. M. 3579 (1) SHA NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| SCALE: _____ DATE: _____ | | SHEET NO. P-13 of P-17 | |

| FED. PROJ. NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|----------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-14 | P-17 (41) |



- LEGEND & SYMBOLS**
- TOP OF CUT
 - BOTTOM OF CUT
 - TOE OF FILL
 - TOP OF FILL
 - SEDIMENT TRAPS
 - STABILIZED CONSTRUCTION ENTRANCES
 - DIVERSION DIKE OR PERIMETER DIKE
 - DIVERSION DITCHES

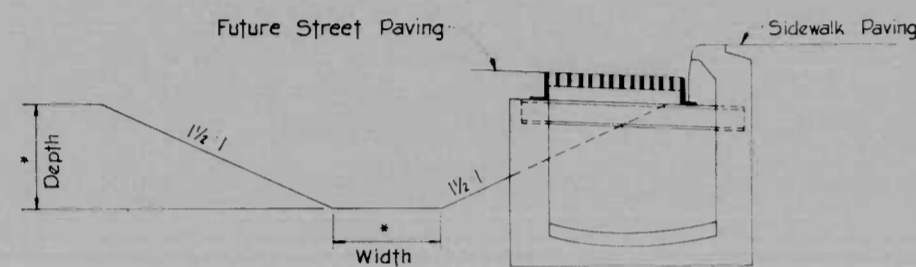
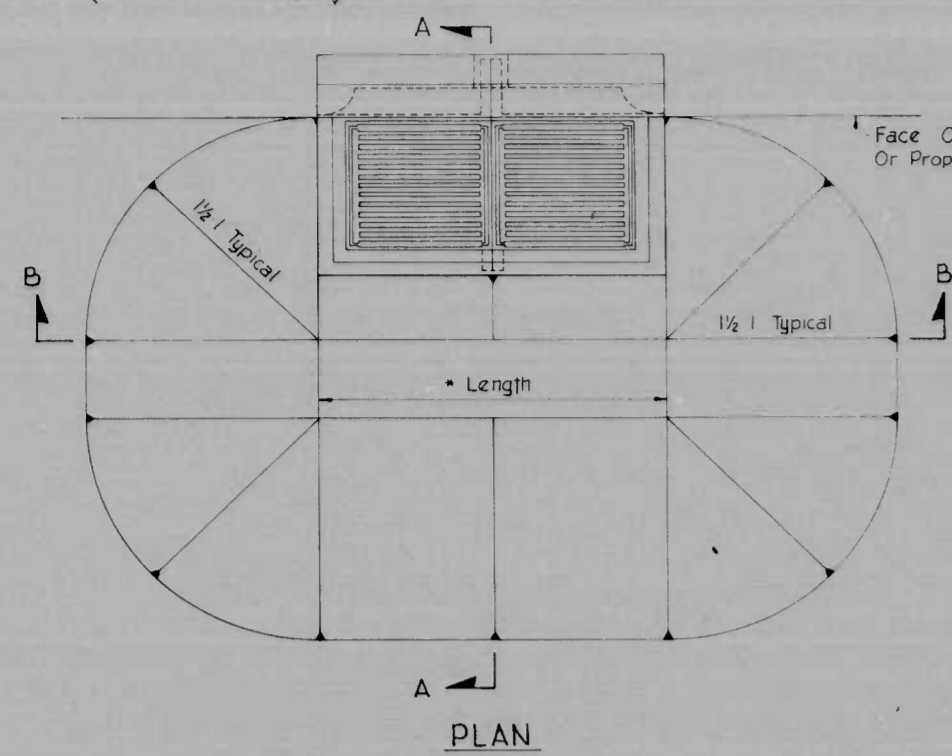
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|-----------|--|--|---------------------------|
| REVISIONS | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | | CONWAY STREET SHARP STREET TO LIGHT STREET SUGGESTED SEDIMENT CONTROL PLAN | |
| SCALE: | DATE: | DRAWN BY HMT TRACED BY HMT | DES. BY CHK. BY |
| | | F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | SHEET NO. P-14 OF P-17 |

ASB BALTIMORE 191181 HERCULENE MC 4774

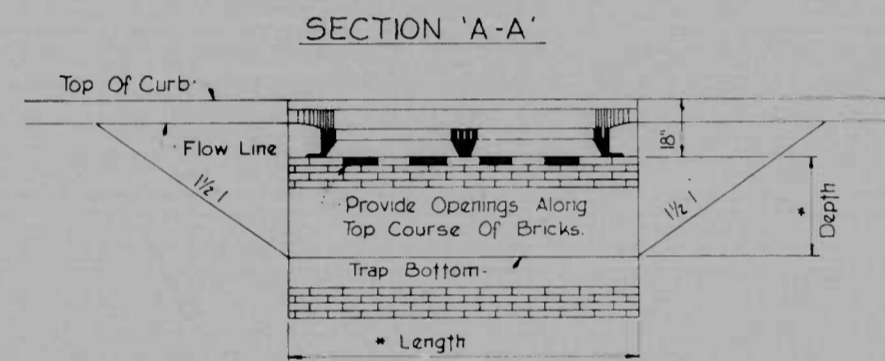
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-15 | (41) |

GENERAL NOTES

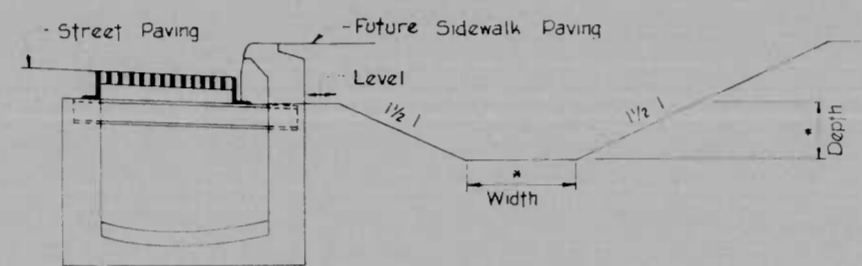
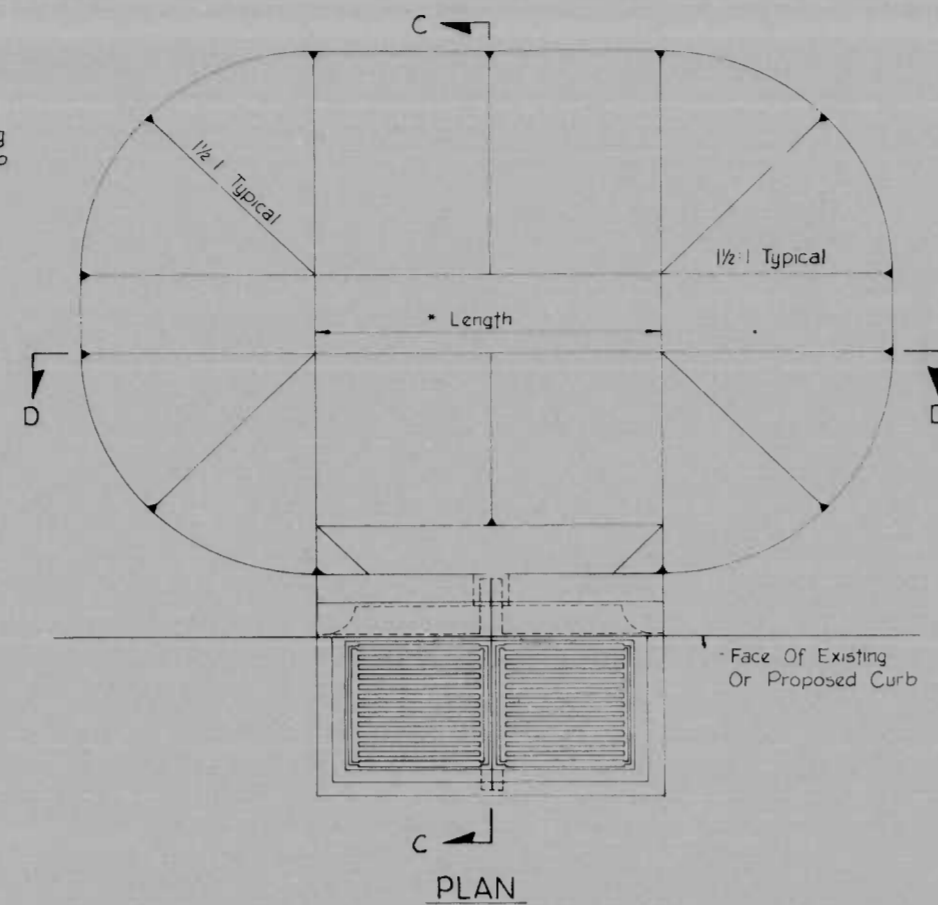
- SPECIAL ATTENTION IS DIRECTED TO THE FACT THAT NO CLEARING, DEMOLITION, GRUBBING OR GRADING MAY COMMENCE FOR THE PROJECT UNTIL THE CONTRACTOR HAS AN APPROVED SEDIMENT AND EROSION CONTROL PLAN AND THE LIMITS ARE STAKED IN THE FIELD AND APPROVED BY THE ENGINEER. NO CLEARING WILL BE ALLOWED BEYOND PROJECT LIMITS.
- UPON INITIATION OF ANY CONSTRUCTION PHASE APPROPRIATE DITCHES AND TEMPORARY SEDIMENT CONTROL DEVICES SHALL BE CONSTRUCTED IN ORDER TO RETAIN SEDIMENTATION ON SITE. SEE DETAILS SHEET NO. P-14 OF P-17. ALL TEMPORARY DITCHES AND DIKES SHALL BE STABILIZED BY SEEDING AND MULCHING UNLESS OTHERWISE NOTED.
- A TYPE NO. 1 AND OR TYPE NO. 2 SEDIMENT TRAP OF THE LENGTH, WIDTH, DEPTH, AND AT THE LOCATIONS AS DIRECTED BY THE ENGINEER, SHALL BE CONSTRUCTED AT ANY EXISTING OR PROPOSED INLET WHEN THE ENGINEER SO DIRECTS. THE EXACT LOCATION AND TYPE OF SEDIMENT CONTROL DEVICES WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER AND THE EROSION CONTROL OFFICER, I. D. B. C. THEY RESERVE THE RIGHT TO ORDER ADDITIONAL EROSION AND SEDIMENT DEVICES. SEE STANDARD NO. MD 388.01, .02, .11, .12 & .21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES DURING THESE WORK OPERATIONS. (SEE DETAILS THIS SHEET.)
- CONSTRUCT TYPE NO. 3 SEDIMENT TRAPS OF THE LENGTHS, WIDTHS, DEPTHS AND LOCATIONS AS SHOWN ON THE SEDIMENT TRAP SCHEDULE AND ON THE SEDIMENTATION CONTROL PLAN. THE EXACT LOCATION AND TYPE OF SEDIMENT CONTROL DEVICES WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER AND THE EROSION CONTROL OFFICER, I. D. B. C. THEY RESERVE THE RIGHT TO ORDER ADDITIONAL EROSION AND SEDIMENT DEVICES. SEE STANDARD NO. MD 388.01, .02, .11, .12 & .21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES DURING THESE WORK OPERATIONS. (SEE DETAILS ON SHEET NO. P-15 OF P-17.)
- THE CONTRACTOR SHALL STABILIZE THE PRIMARY INFLOW POINTS AND OUTLETS OF ALL SEDIMENT CONTROL DEVICES TO PREVENT EXCESSIVE EROSION TO THE SATISFACTION OF THE ENGINEER.
- CONTINUOUS INSPECTION AND MAINTENANCE OF ALL SEDIMENT CONTROL DEVICES AND APPURTENANCES SHALL BE PROVIDED TO INSURE THAT THE INTENDED PURPOSES ARE ACCOMPLISHED DURING THE LIFE OF THE CONSTRUCTION CONTRACT.
- ALL SEDIMENT TRAPS SHALL BE CLEANED OUT WHEN ACCUMULATED SILT REACHES 50% OF THE TRAP DEPTH. (SEE SEDIMENT TRAP DETAILS THIS SHEET AND THE SEDIMENT TRAP SCHEDULE ON SHEET NO. P-15 OF P-17.)
- THE CONTRACTOR SHALL CONSTRUCT AND MAINTAIN FENCES FOR THE DURATION OF THE CONTRACT AROUND ALL SEDIMENT TRAPS. (SEE SEDIMENT TRAP DETAIL AND SAFETY FENCING DETAIL THIS SHEET AND SEDIMENT TRAP SCHEDULE SHEET NO. P-15 OF P-17.)
- THE CONTRACTOR SHALL, WHEN DIRECTED BY THE ENGINEER, CONSTRUCT AND MAINTAIN DIVERSION DITCHES OR DIKES REQUIRED TO DIVERT SEDIMENT LADEN RUNOFF TO SEDIMENT TRAPS. COST OF SAID DIVERSIONS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE SEDIMENT TRAP AND NO ADDITIONAL COMPENSATION WILL BE MADE.
- STRAW BALE DIKES SHALL BE PLACED WHERE DIRECTED BY THE ENGINEER AND CHANGED WHEN CHOKED, AT HIS DISCRETION. (SEE STRAW BALE DIKE DETAIL THIS SHEET.)
- ALL STRAW BALES USED ON PROJECT SHALL BE OF THE SAME APPROXIMATE SIZE (14" X 18" X 36"), AND SHALL BE Baled WITH NON WEATHERING MATERIALS.
- MODIFICATIONS TO SEDIMENT CONTROL MEASURES SHOWN ON THESE PLANS MAY BE ORDERED BY THE ENGINEER TO SUIT FIELD CONDITIONS ENCOUNTERED THROUGHOUT THE LIFE OF THE CONSTRUCTION CONTRACT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PREVENT SEDIMENT, RESULTING FROM HIS CONSTRUCTION, FROM ENTERING ANY ADJACENT PROPERTY OR STORM DRAIN SYSTEM, AND INSURE THAT ALL RUNOFF WATER FROM THE CONSTRUCTION AREA SHALL PASS THROUGH SEDIMENT TRAPS.
- ALL SEDIMENTATION CONTROL DEVICES ARE TEMPORARY AND SHALL BE USED ONLY DURING THE LIFE OF THE CONSTRUCTION CONTRACT. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REMOVE ALL TEMPORARY SEDIMENTATION CONTROL DEVICES AND WORK THOSE AREAS OF PLACEMENT TO ORIGINALLY EXISTING OR PROPOSED CONDITIONS.



Note To Be Determined By The Engineer.



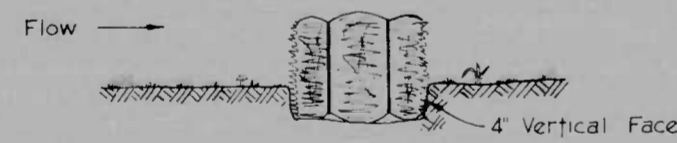
SECTION 'B-B'
SEDIMENT TRAP TYPE 1
Not To Scale



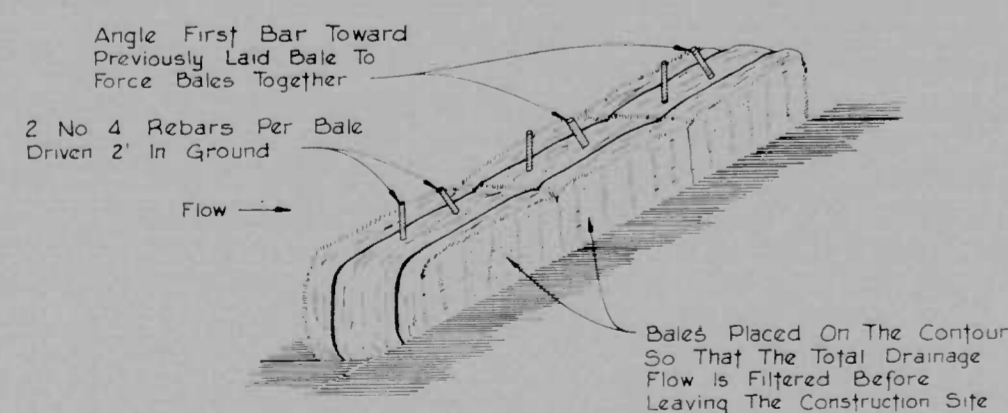
Note To Be Determined By The Engineer



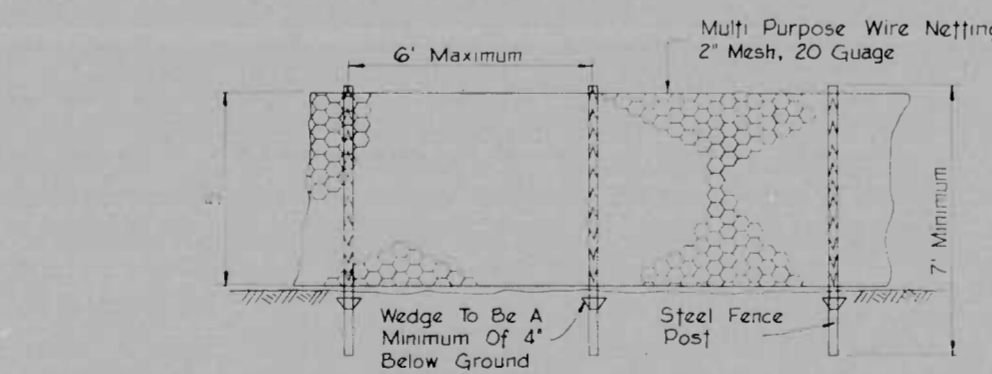
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SEDIMENT TRAP TYPE 2
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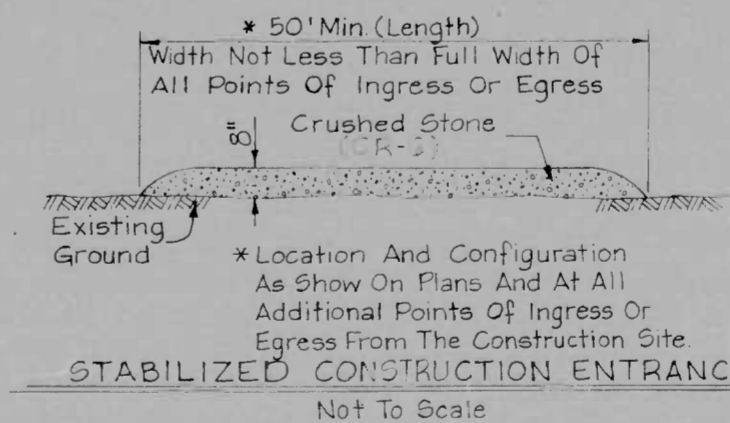
EMBEDDING DETAIL



ANCHORING DETAIL
STRAW BALE DIKE
Not To Scale



SAFETY FENCING FOR SEDIMENT DEVICES
SCALE 1" = 5'

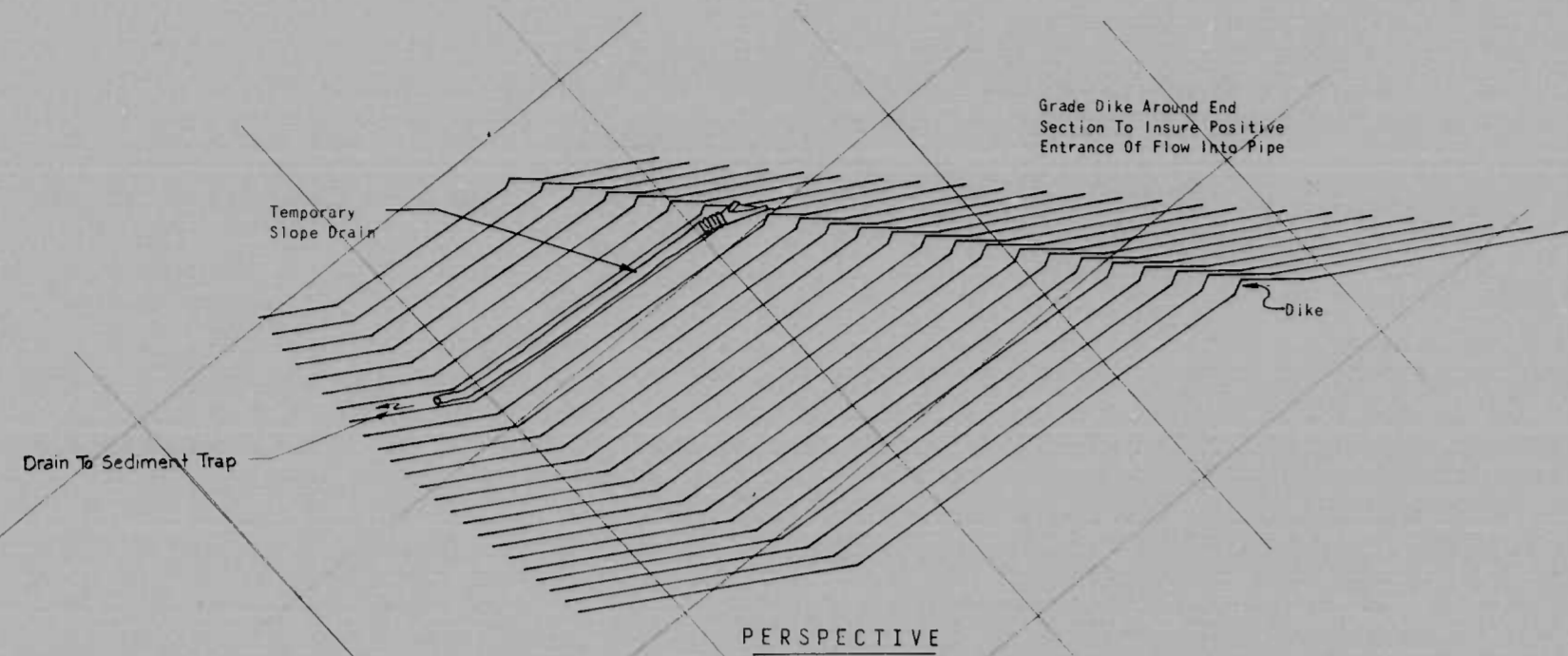


STABILIZED CONSTRUCTION ENTRANCE
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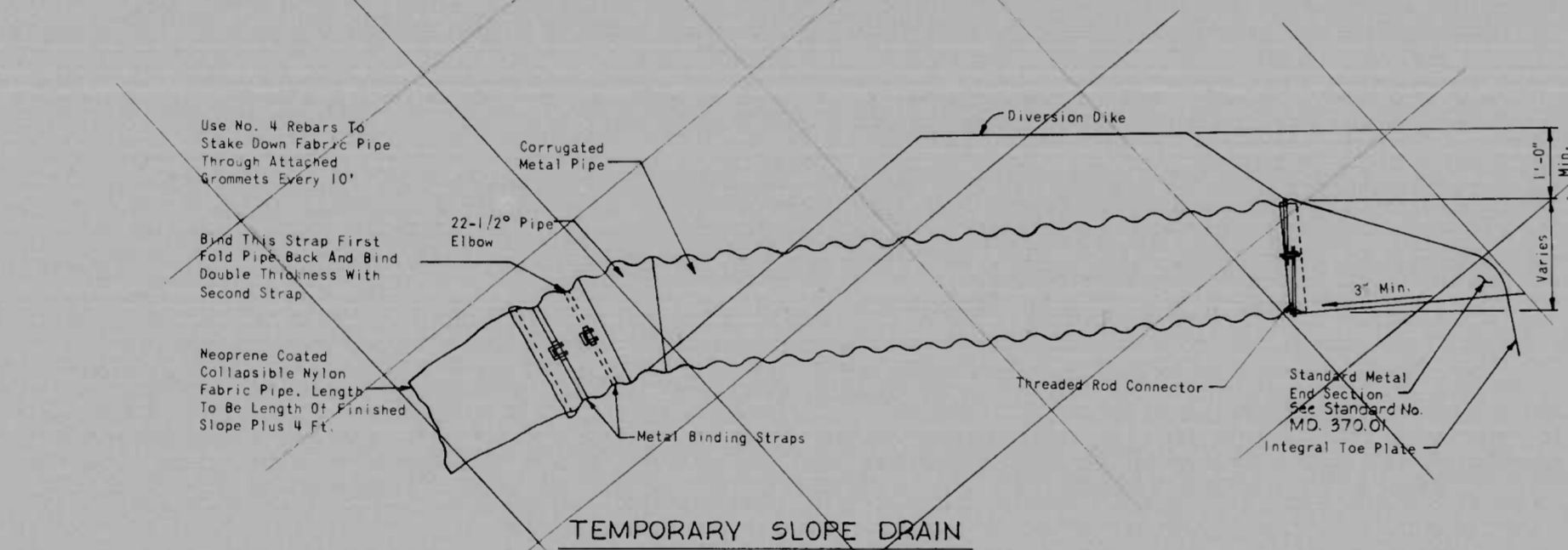
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|--|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET SUGGESTED SEDIMENT CONTROL DETAILS | DRAWN BY: [Signature] TRACED BY: [Signature] F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| | | SCALE: As Noted | DES. BY: JMT CHK. BY: JAR SHEET NO. P-15 OF P-17 |

NOT IN CONTRACT

| FHWA REGION | STATE | FED AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|-------------------|-----------|--------------|
| 3 | MD | M 3579 (1) | P-16 | (41) |

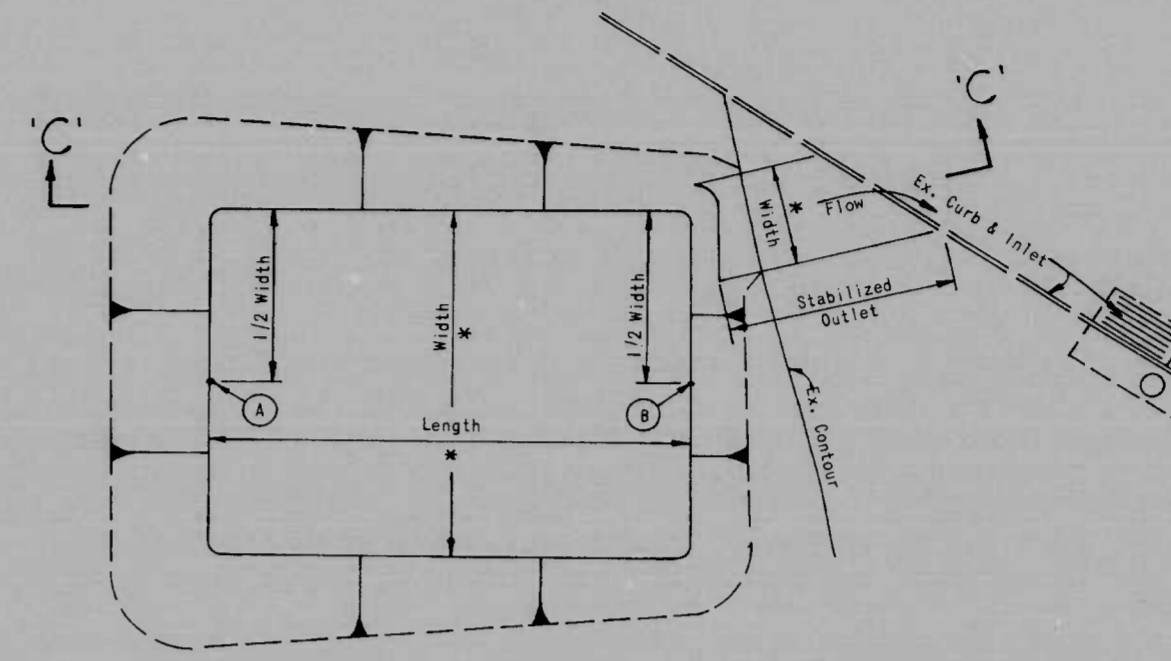


PERSPECTIVE



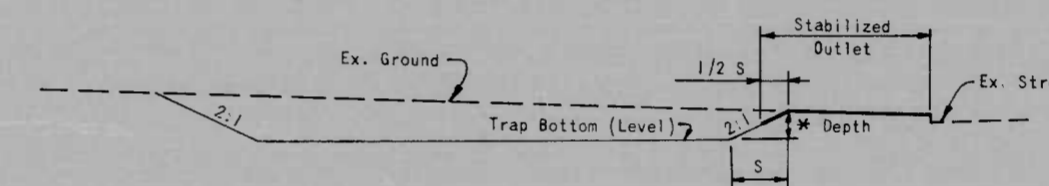
TEMPORARY SLOPE DRAIN

Not To Scale



PLAN

* See Sediment Trap Schedule This Sheet

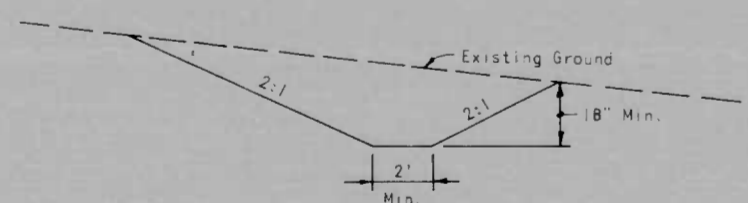


SECTION 'C-C'

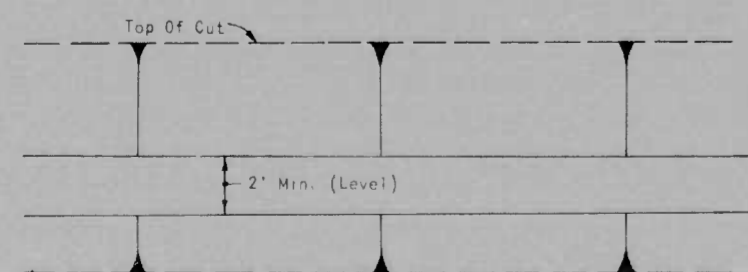
SEDIMENT TRAP TYPE 3

Not To Scale

| TRAP NO. | TYPE | LOCATION | | REMARKS |
|----------|------|----------|-----------|---|
| | | STATION | OFFSET | |
| 1 | 3 | 12+19 | 34.5' LT. | BOTTOM: (SIZE: L = 6' W = 4' D = 4') ELEV. = 6.5' MINIMUM OUTLET WIDTH = 2' |
| | | 2+25 | 34.5' LT. | |
| 2 | 3 | 15+56 | 50.0' LT. | BOTTOM: (SIZE: L = 4' W = 4' D = 4') ELEV. = 1.5' MINIMUM OUTLET WIDTH = 2'. STABILIZE OUTLET FROM TRAP 1-2 TO EX. INLET LT. OF STA. 15+. |
| | | 15+90 | 50.0' LT. | |



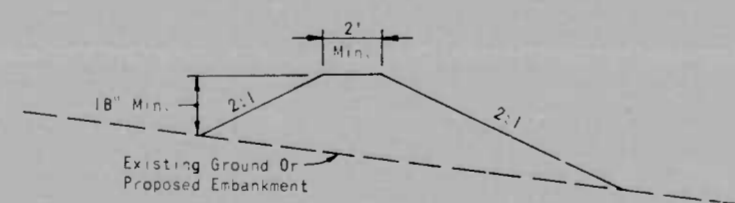
SECTION



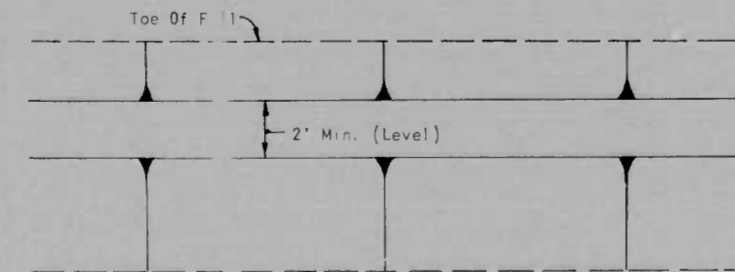
PLAN

DIVERSION DITCH

Not To Scale



SECTION



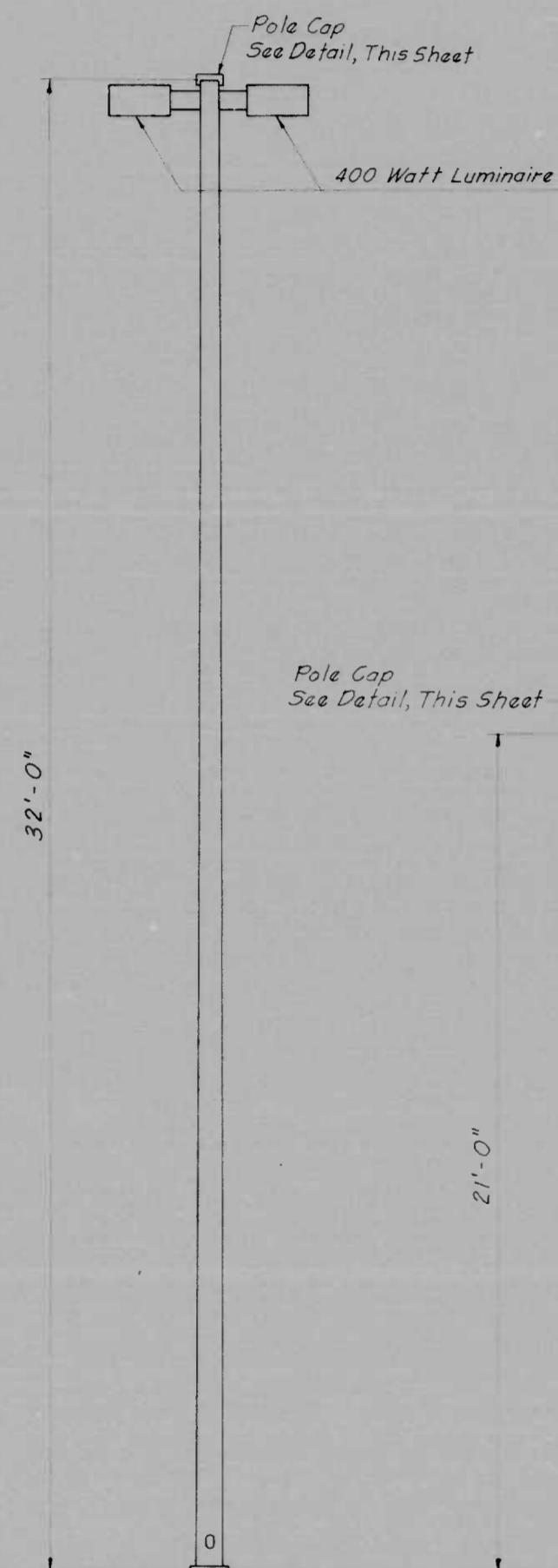
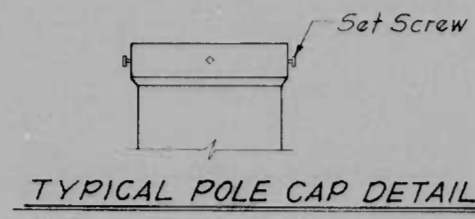
PLAN

PERIMETER DIKE

Not To Scale

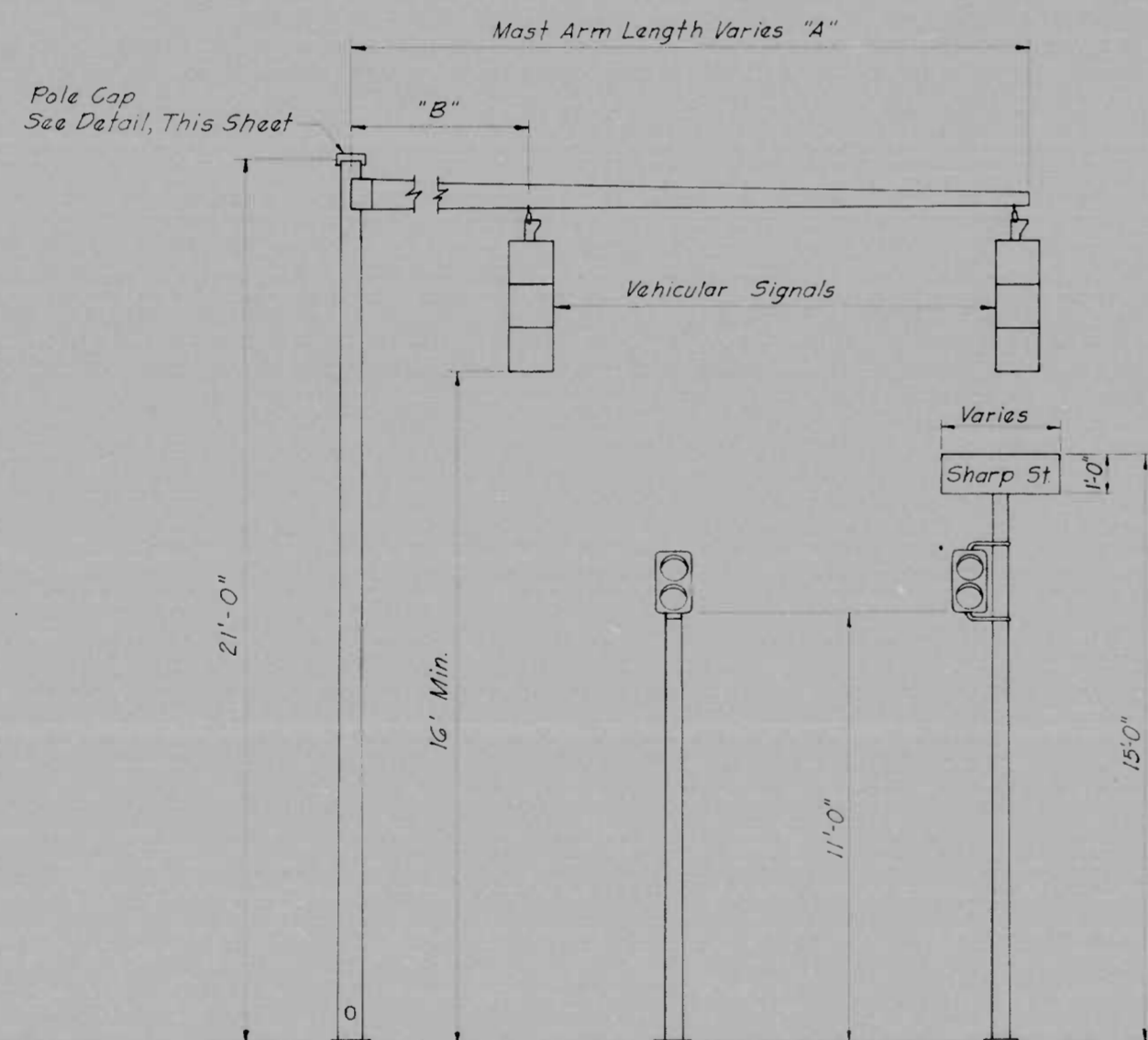
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|--|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET SUGGESTED SEDIMENT CONTROL DETAILS | DRAWN BY: [] TRACED BY: [] DES. BY: HMT CHK. BY: JAR |
| | | SCALE: As Noted | F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| | | | SHEET NO. P-16 of P-17 |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-17 | (41) |



ELEVATION-ROADWAY LIGHTING
STANDARD AND LUMINAIRE
TYPE G SHOWN

Note: Type F Consists Of 32'-0" Lighting Standard And A Single Luminaire
Pole Nos. 102 Thru 105, 201 Thru 204, 301, 302, 304 Thru 309



ELEVATION
TRAFFIC SIGNALS

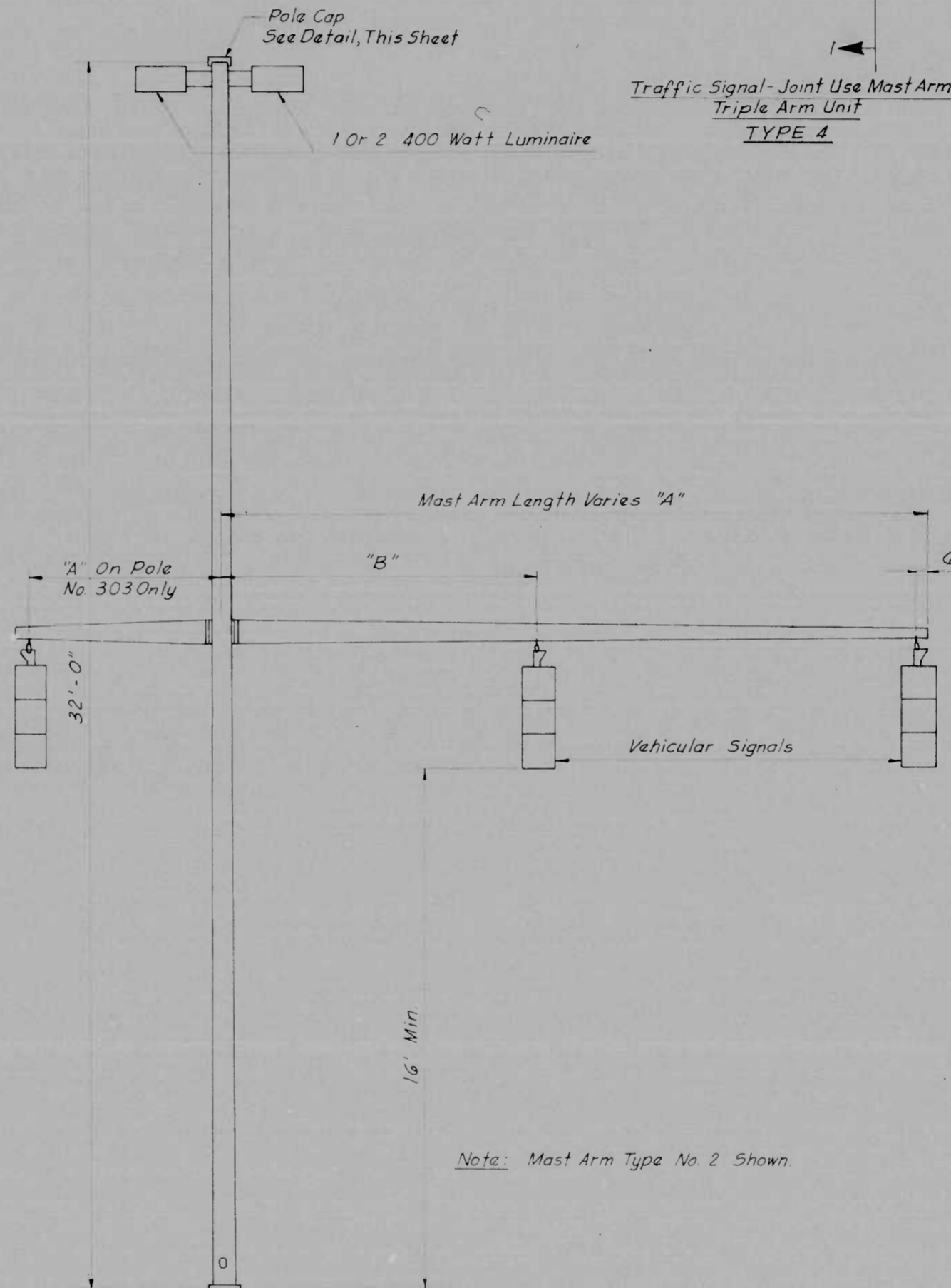
Pole Nos. 106, 108, 312, & 316

ELEVATION
PEDESTRIAN
SIGNAL POLE

Pole No. 317

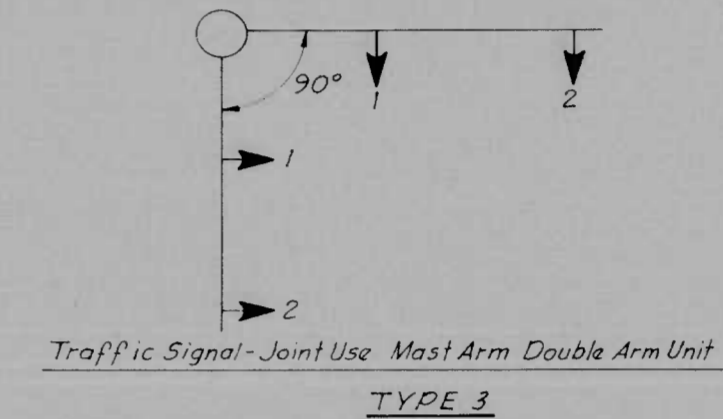
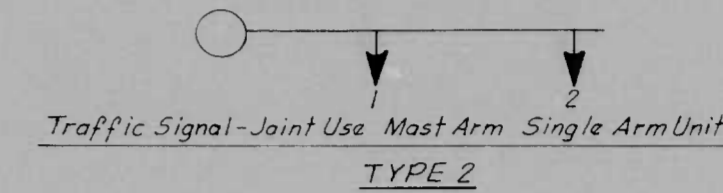
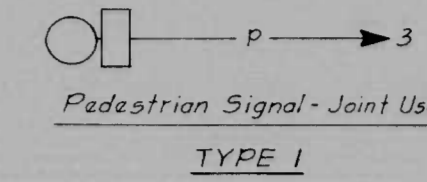
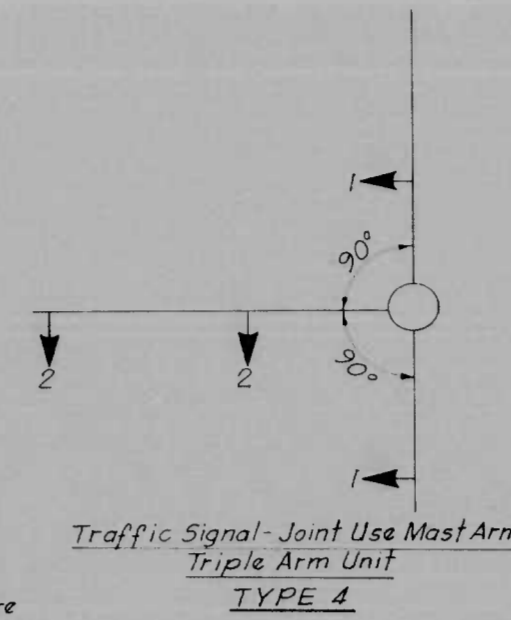
ELEVATION
PEDESTRIAN
SIGNAL POLE
WITH STREET
NAME SIGN

Pole Nos. 107, 313, 315 & 318



ELEVATION-JOINT USE STANDARD ROADWAY
LIGHTING AND TRAFFIC SIGNALS

Pole Nos. 100, 101, 303 & 318

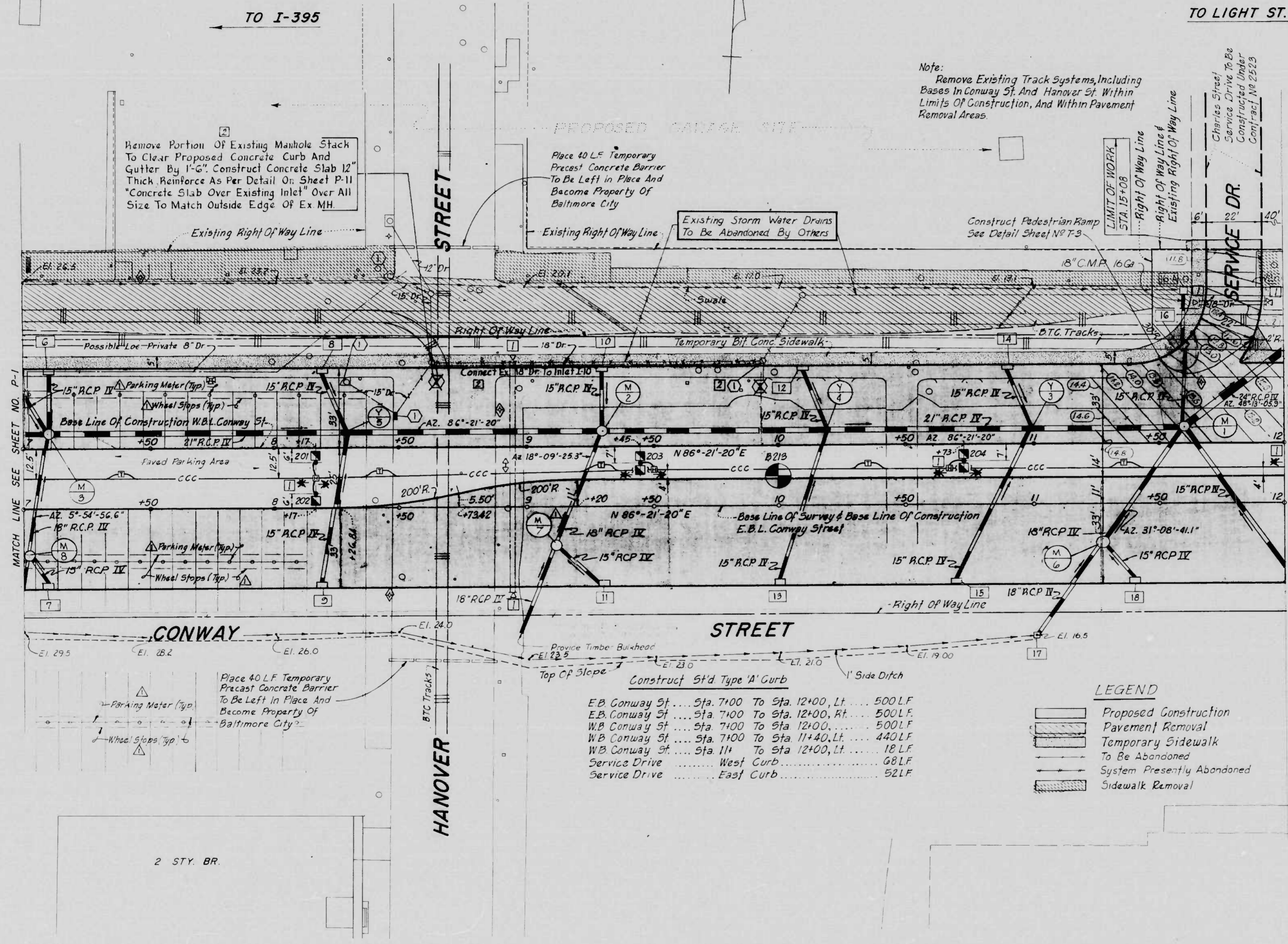


PLAN-JOINT USE STANDARDS AND
CONFIGURATION OF MAST ARMS

| JOINT USE POLES | | | | | | |
|-----------------|------------|---------|---------------------|---------------------|---------------|------------------------|
| POLE NUMBER | TYPE | | MAST ARM LENGTH "A" | MAST ARM LENGTH "B" | EQUIPMENT MTD | |
| | FOUNDATION | FIXTURE | | | | |
| 100 | F-3 | G | 2 | 26' | 12' | Traffic Signals |
| 101 | F-3 | G | 2 | 26' | 12' | Traffic Signals |
| 106 | F-3 | - | 1 & 2 | 28' | 12' | Traffic & Ped. Signals |
| 107 | F-2 | - | 1 | - | - | Pedestrian Signals |
| 108 | F-4 | - | 1 & 2 | 30' | 14' | Traffic & Ped. Signals |
| 312 | F-3 | - | 1 & 2 | 26' | 10' | Traffic & Ped. Signals |
| 313 | F-2 | - | 1 | - | - | Pedestrian Signals |
| 303 | F-4 | - | 4 | 30, 36' | 14' | Traffic Signals |
| 315 | F-2 | - | 1 | - | - | Pedestrian Signals |
| 316 | F-3 | - | 1 & 2 | 20' | 6' | Traffic & Ped. Signals |
| 317 | F-2 | - | 1 | - | - | Pedestrian Signals |
| 318 | F-4 | - | 2 | 38' | 18' | Traffic Signals |
| 319 | F-2 | - | 1 | - | - | Pedestrian Signals |

| | | | |
|-----------|---|---|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET POLE DETAILS | DRAWN BY RLJ TRACED BY RLJ F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-1-815 BALTO. CITY NO. 2492 |
| | CONSULTING ENGINEERS BALTIMORE, MARYLAND | SCALE NO SCALE | DATE |
| | | | DES. BY REE CHK. BY C.C.B. SHEET NO. P-17 |

| | | | | |
|-------------|-------|--------------------|-----------|--------------|
| FED. REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M 3579 (1) | P-2 | P-17 (41) |



Note:
Remove Existing Track Systems, Including Bases In Conway St. And Hanover St. Within Limits Of Construction, And Within Pavement Removal Areas.

Remove Portion Of Existing Manhole Stack To Clear Proposed Concrete Curb And Gutter By 1'-6\".

Place 40 LF Temporary Precast Concrete Barrier To Be Left In Place And Become Property Of Baltimore City

Existing Storm Water Drains To Be Abandoned By Others

Construct Pedestrian Ramp See Detail Sheet No. 7-B

Charles Street Service Drive To Be Constructed Under Contract No. 2523

| | |
|---|--------|
| E.B. Conway St. ... Sta. 7+00 To Sta. 12+00, Lt. | 500 LF |
| E.B. Conway St. ... Sta. 7+00 To Sta. 12+00, Rt. | 500 LF |
| W.B. Conway St. ... Sta. 7+00 To Sta. 12+00, Lt. | 500 LF |
| W.B. Conway St. ... Sta. 7+00 To Sta. 12+00, Rt. | 440 LF |
| W.B. Conway St. ... Sta. 11+ To Sta. 12+00, Lt. | 18 LF |
| Service Drive ... West Curb | 68 LF |
| Service Drive ... East Curb | 52 LF |

LEGEND

| | |
|----------|----------------------------|
| [Symbol] | Proposed Construction |
| [Symbol] | Pavement Removal |
| [Symbol] | Temporary Sidewalk |
| [Symbol] | To Be Abandoned |
| [Symbol] | System Presently Abandoned |
| [Symbol] | Sidewalk Removal |

DRAINAGE NOTES

Construct Swale To The Left Of Conway St. From Sta. 7+00 To Sta. 11+00 As Shown On Typical Sections For Storm Drain Pipe Profiles See Sht. No. P-8 thru P-11 For Storm Drain Structure Schedules See Sheet No. P-13

Construct Side Ditch To The Right Of Conway St. From Sta. 7+00 To Sta. 11+00 To Comply With Balto City Std. No. B.C. 389.04. A'-1'-0\", B'-varies To Meet Existing Grade

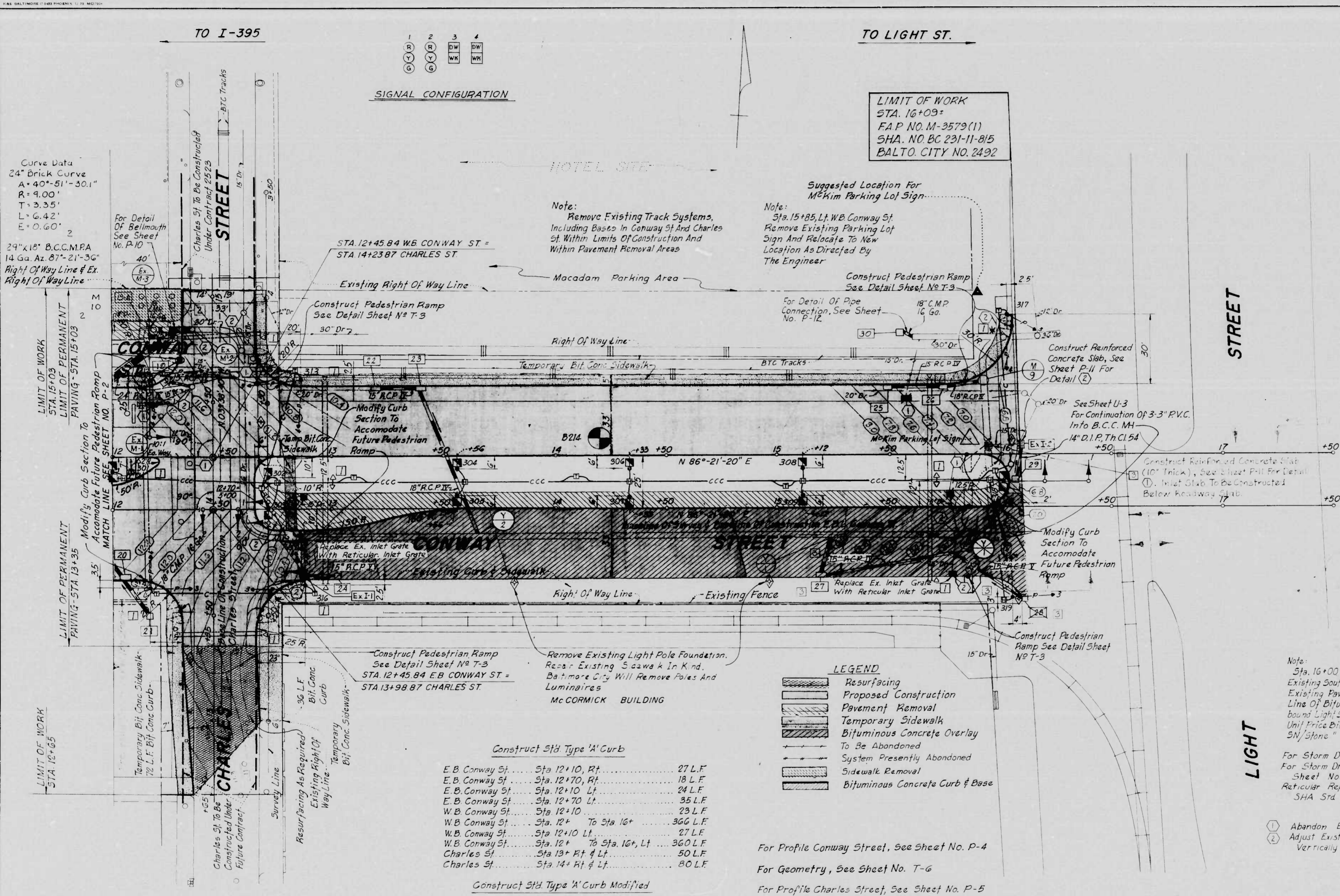
Construct Timber Bulkhead 53' R.R., Sta. 8+97 E.B.L. Of Conway St.

① Adjust Existing Inlet
② Adjust Existing Utility Surface Feature Vertically To Project Finished Grade

For Profile Conway Street, See Sheet No. P-4
For Geometry, See Sheet No. T-6
For Profile Service Drive, See Sheet No. P-5

| | | | |
|---|--|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE | STATE HIGHWAY ADMINISTRATION OF MARYLAND |
| Addendum No. 1 - 10/21/78 Revisions To Conduits & Duct Markers - 12/7/78 Revisions To Storm Dr. Lt. Sta. 8+ To Sta. 3/26/78 | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE MARYLAND | DEPARTMENT OF PUBLIC WORKS CONWAY STREET SHARP STREET TO LIGHT STREET PLAN - Sta. 7+00 To Sta. 12+00 | INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: CHL TRACED BY: JFL F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| DES. BY: WRH CHK. BY: HMH | | SCALE: 1"=20' DATE: | SHEET NO. P-2 of P-17 |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-3 | P-17 (41) |



Construct Std. Type 'A' Curb

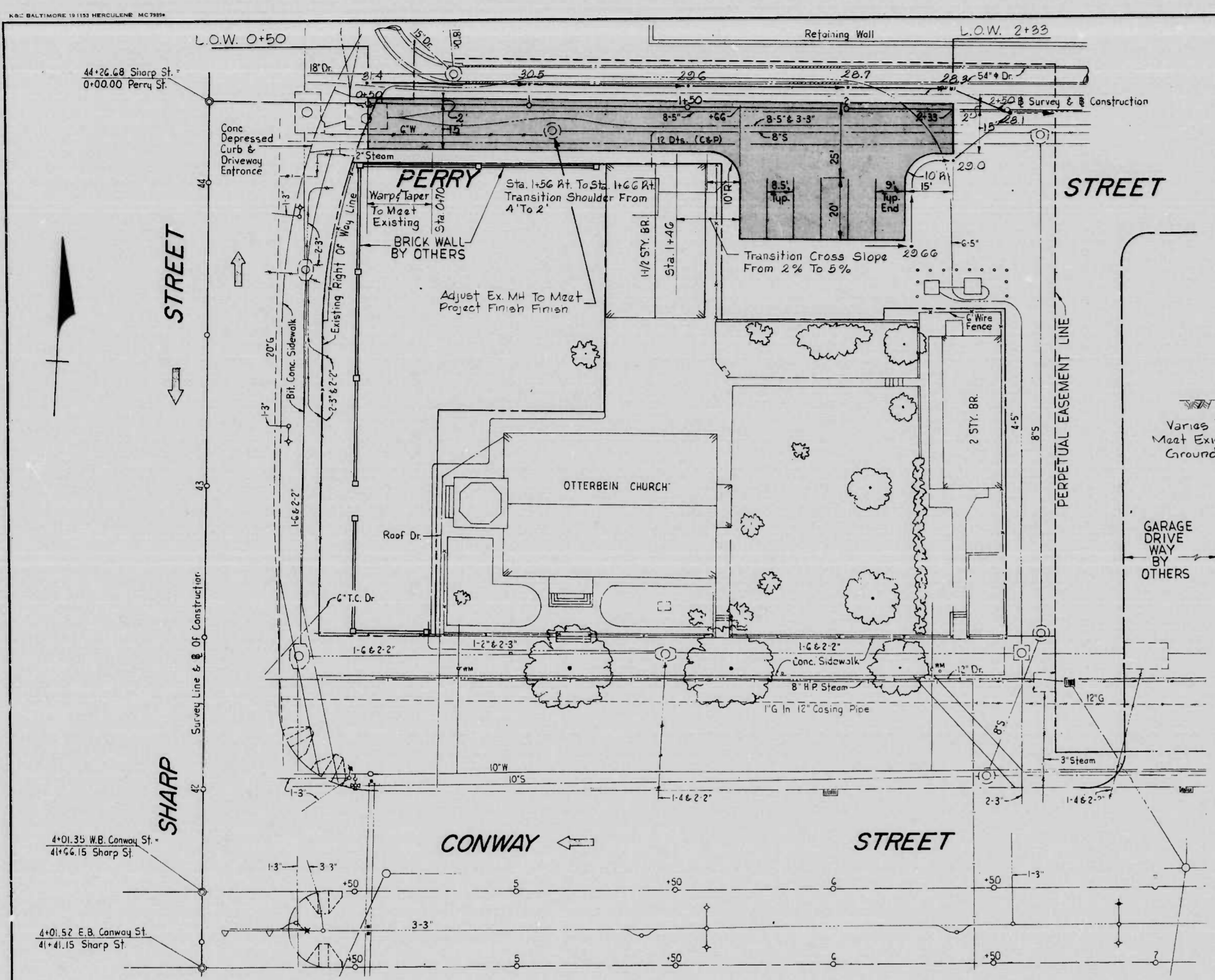
| | | |
|-----------------|---------------------------|----------|
| E. B. Conway St | Sta. 12+10, Rt. | 27 L.F. |
| E. B. Conway St | Sta. 12+70, Rt. | 18 L.F. |
| E. B. Conway St | Sta. 12+10, Lt. | 24 L.F. |
| E. B. Conway St | Sta. 12+70, Lt. | 35 L.F. |
| W. B. Conway St | Sta. 12+10 | 23 L.F. |
| W. B. Conway St | Sta. 12+ To Sta. 16+ | 366 L.F. |
| W. B. Conway St | Sta. 12+10, Lt. | 27 L.F. |
| W. B. Conway St | Sta. 12+ To Sta. 16+, Lt. | 360 L.F. |
| Charles St | Sta. 13+ Rt. & Lt. | 50 L.F. |
| Charles St | Sta. 14+ Rt. & Lt. | 80 L.F. |

Construct Std. Type 'W' Curb Modified

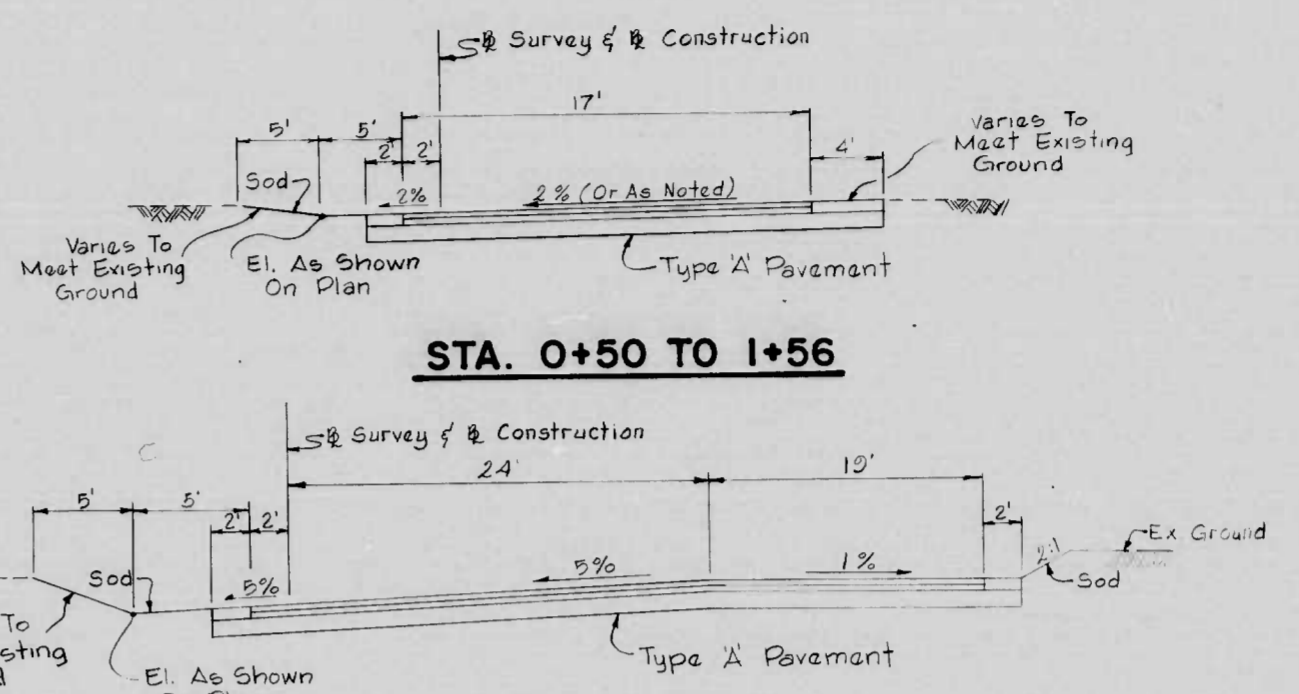
| | | |
|-----------------|---------------------------|----------|
| E. B. Conway St | Sta. 13+ To Sta. 16+, Lt. | 321 L.F. |
|-----------------|---------------------------|----------|

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|--|------------------------|--|--|
| 1) Revisions To Conduits & Duct Markers... 12/1/78 | RUMMEL, KLEPPER & KAHL | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| 2) Storm Drain, Lt. Sta. 12+... 1/30/79 | CONSULTING ENGINEERS | CONWAY STREET SHARP STREET TO LIGHT STREET PLAN - Sta. 12+00 To Sta. 16+ | DRAWN BY: JFL TRACED BY: JLLJ DES. BY: WRH CHK. BY: HMM |
| 3) Storm Drain, Lt. & Rt. Cr. Sta. 15+... 3/26/77 | BALTIMORE MARYLAND | SCALE: 1" = 20' | F.A.P. NO. - M 3579 (1) SHEET NO. P-3 OF P-17 SHA NO. BC 231-11-815 BALTO. CITY NO. 2492 |

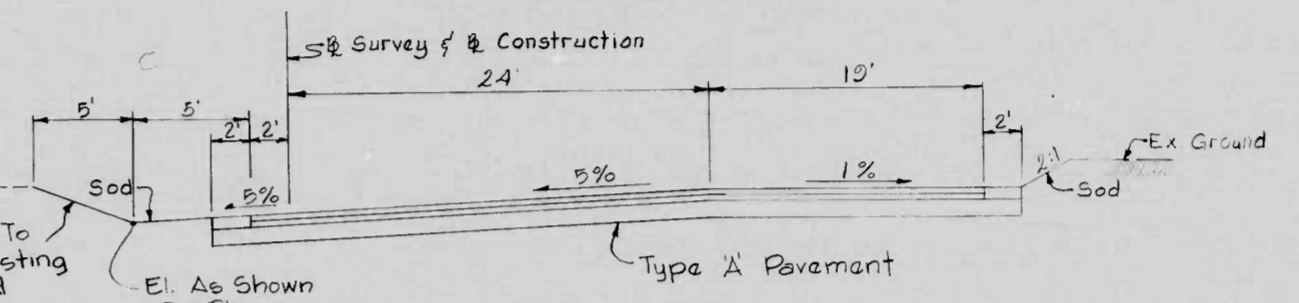
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M-3579 (1) | P-3A | P-17 (41) |



PLAN
Scale: 1"=20'



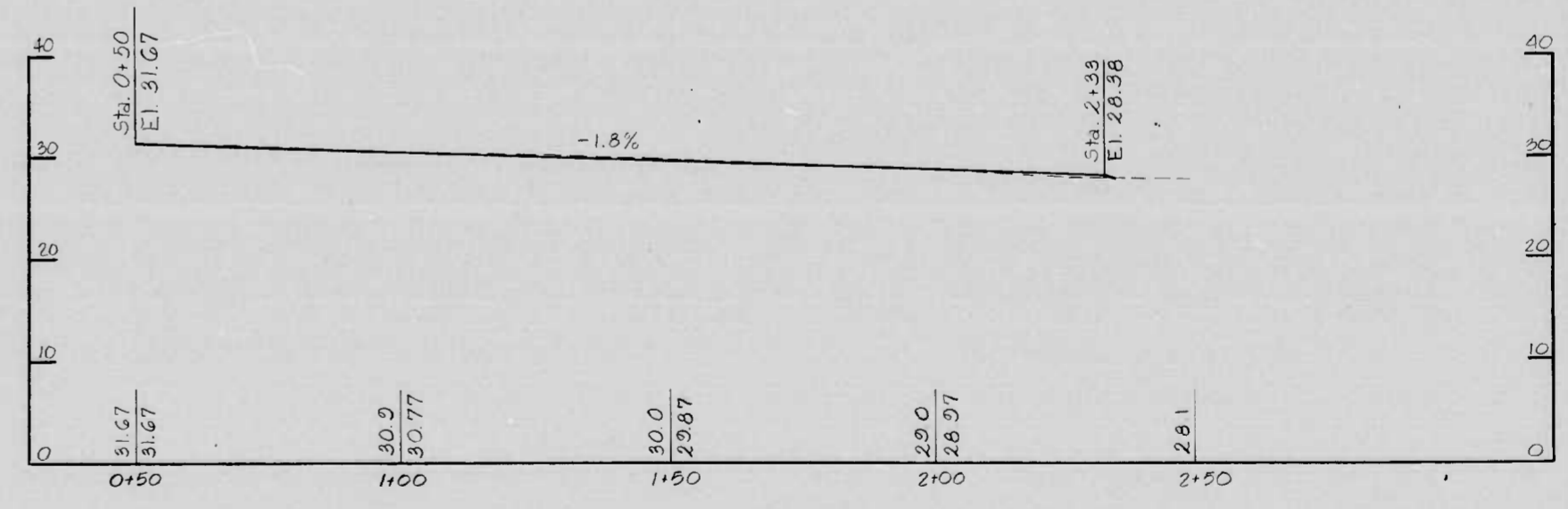
STA. 0+50 TO 1+56



STA. 1+66 TO 2+

- 1" Bituminous Concrete Surface Course Band SN/Stone
- 1" Bituminous Concrete Base Course Band BI
- 6" Subbase Using Crusher Run CR-6
- 3" Stabilized Shoulder, (3" Sub-base Using Crusher Run)

PAVEMENT TYPE 'A'
Not To Scale



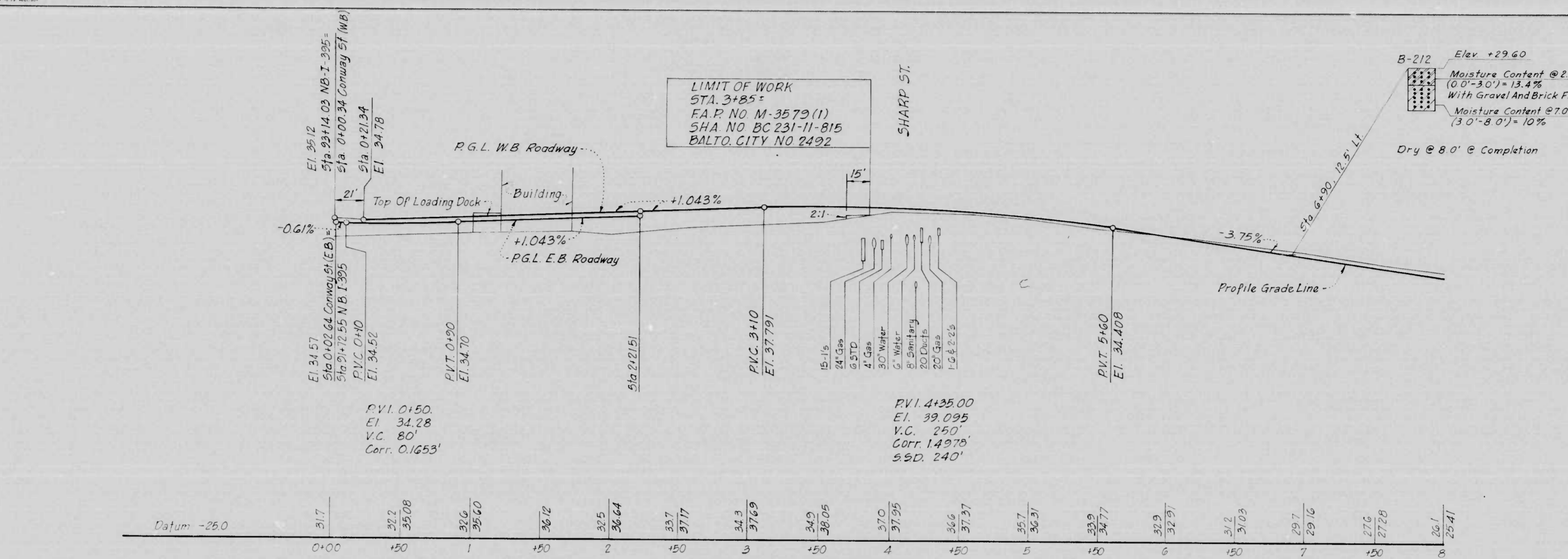
PROFILE
Scale: Horz 1"=20'
Vert 1"=10'

LEGEND

Proposed Paving

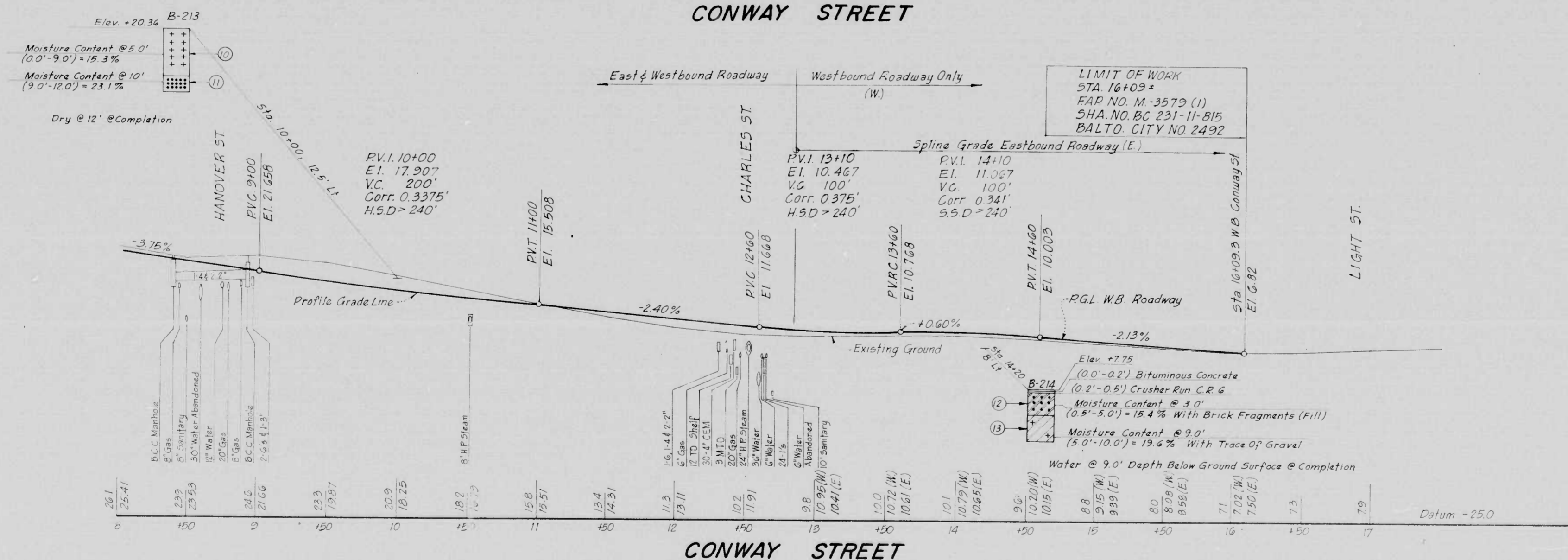
| | | | |
|--|--|--|---|
| REVISIONS [] Red Line Revision No. 4 6/15/79 | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | | CONWAY STREET SHARP STREET TO LIGHT STREET PERRY STREET ENTRANCE AND PARKING LOT SCALE: AS NOTED DATE: | |
| | | DRAWN BY: FWS TRACED BY: FWS F.A.P. NO. M-3579(1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | DES. BY: CHK. BY: SHEET NO. P-3A of P-17 |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-4 | P-7 (41) |



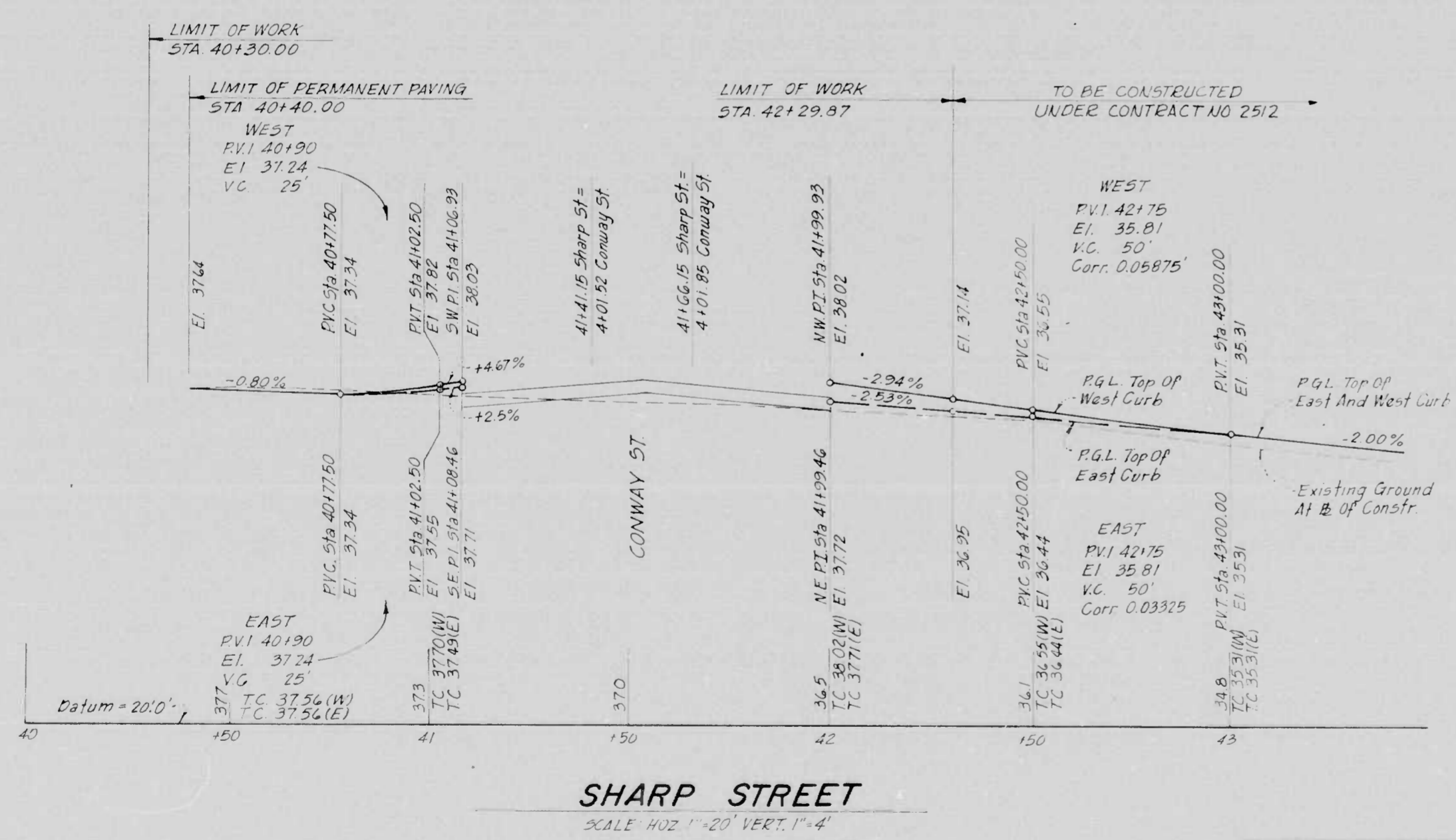
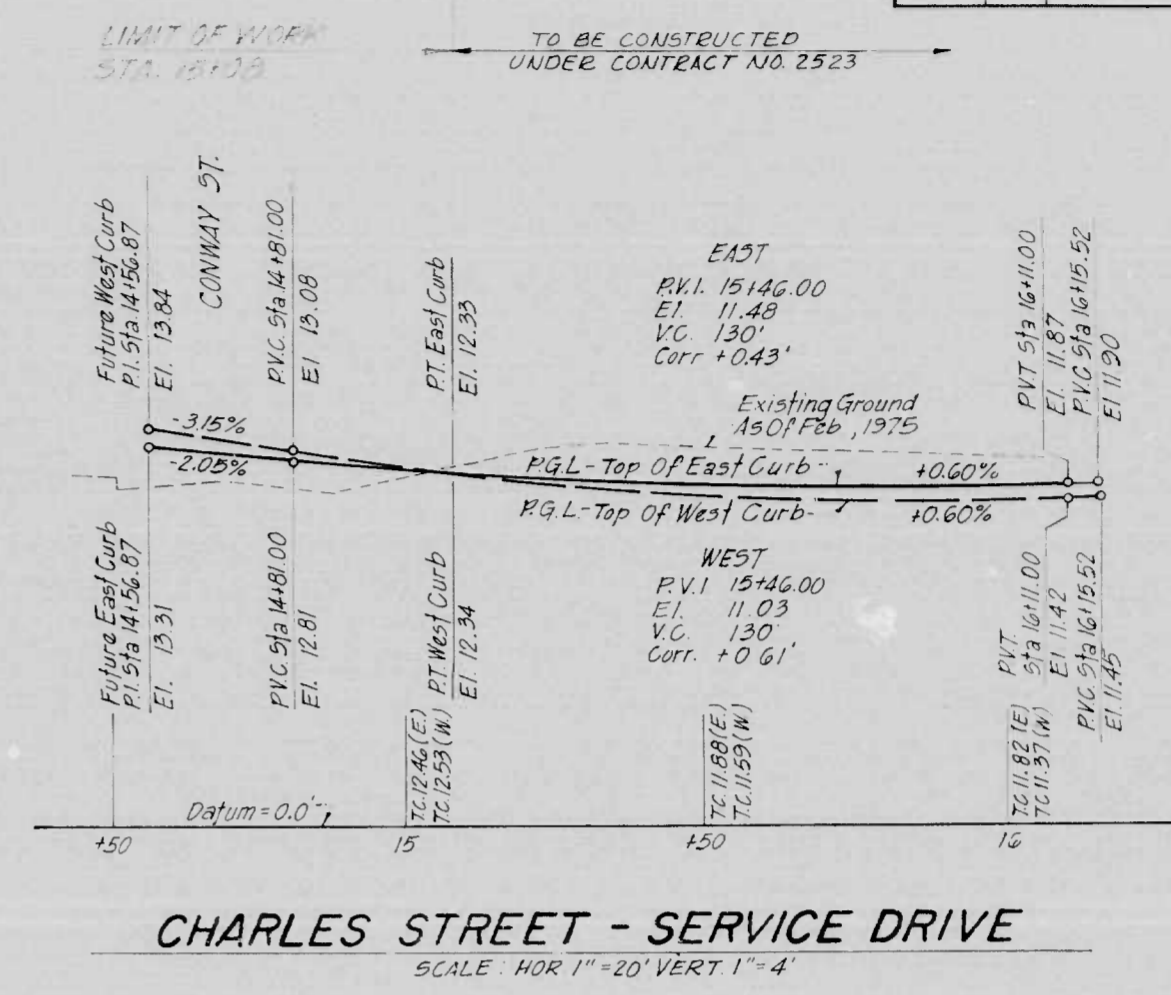
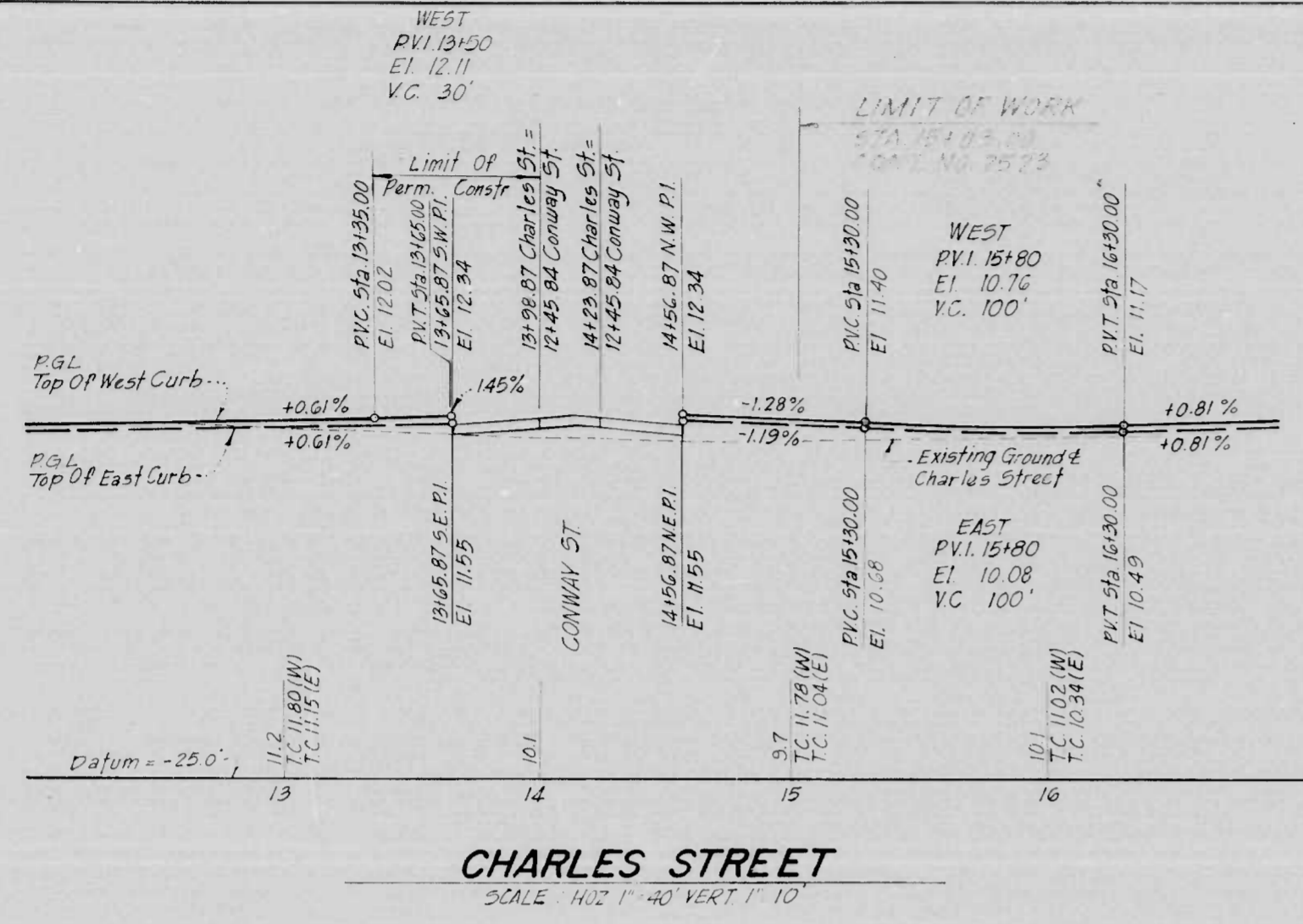
SOILS TEST DATA

| No. | L.L. | P.I. |
|-----|------|------|
| 10 | 41.5 | 20.8 |
| 11 | N.P. | N.P. |
| 12 | 24.2 | 7.4 |
| 13 | 22.6 | 8.5 |



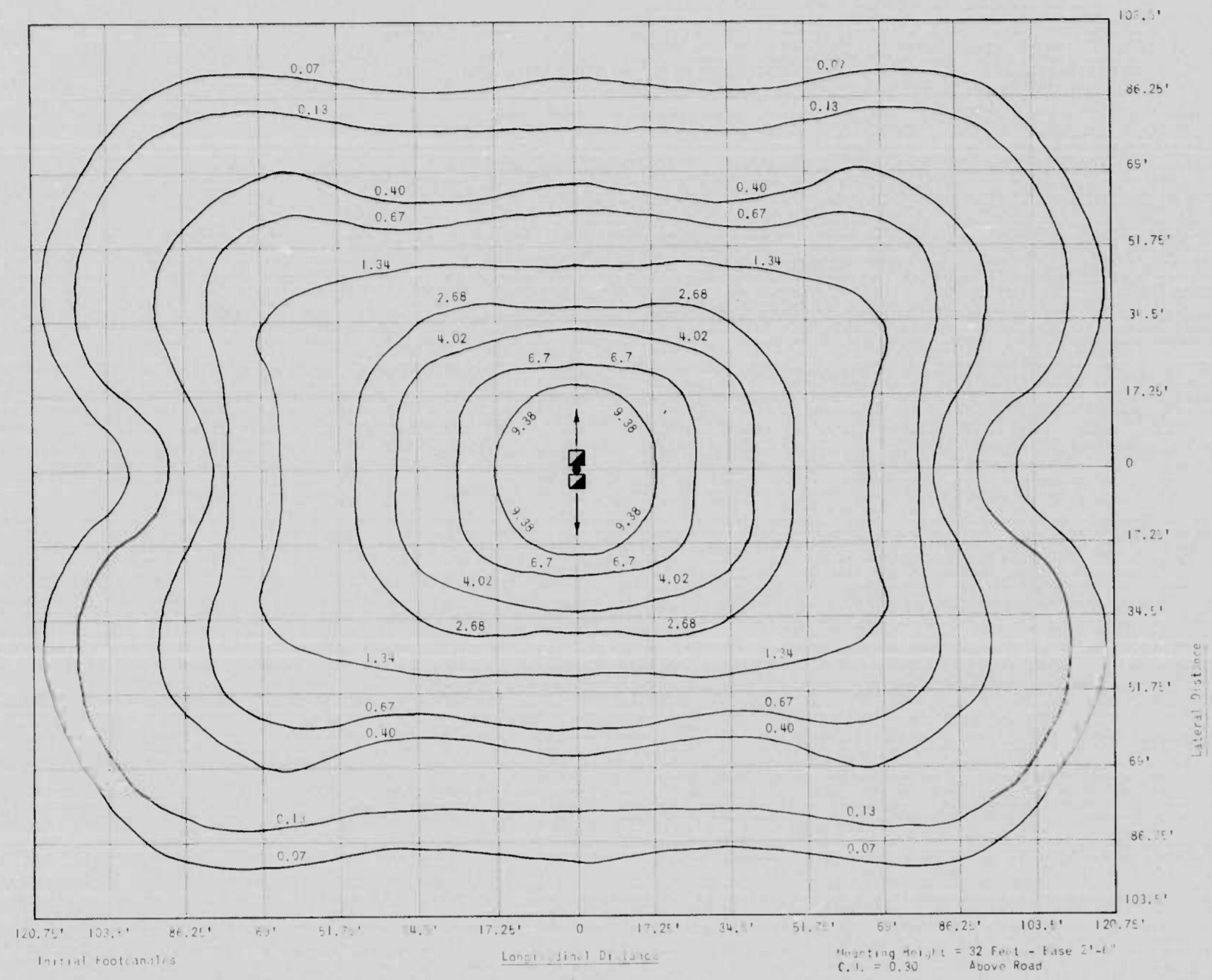
| | | | |
|-----------|--|--|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILE STA. 3+ TO STA. 16+ | |
| | CONSULTING ENGINEERS BALTIMORE MARYLAND | SCALE | DATE |
| | | DRAWN BY: R.L.J. TRACED BY: J.F.L. | DES. BY: WRH CHK. BY: H.M.H. |
| | | F.A.P. NO. M-3579 (1) SHA. NO. BC 231-11-815 BALTO. CITY NO. 2492 | SHEET NO. P.4 OF P-17 |

| | | | | |
|-------------|-------|----------------|-----------|-------------|
| FED. REGION | STATE | FED. PROJ. NO. | SHEET NO. | SHEET TOTAL |
| 3 | MD. | M 3579 (1) | P-5 | P-7 (41) |



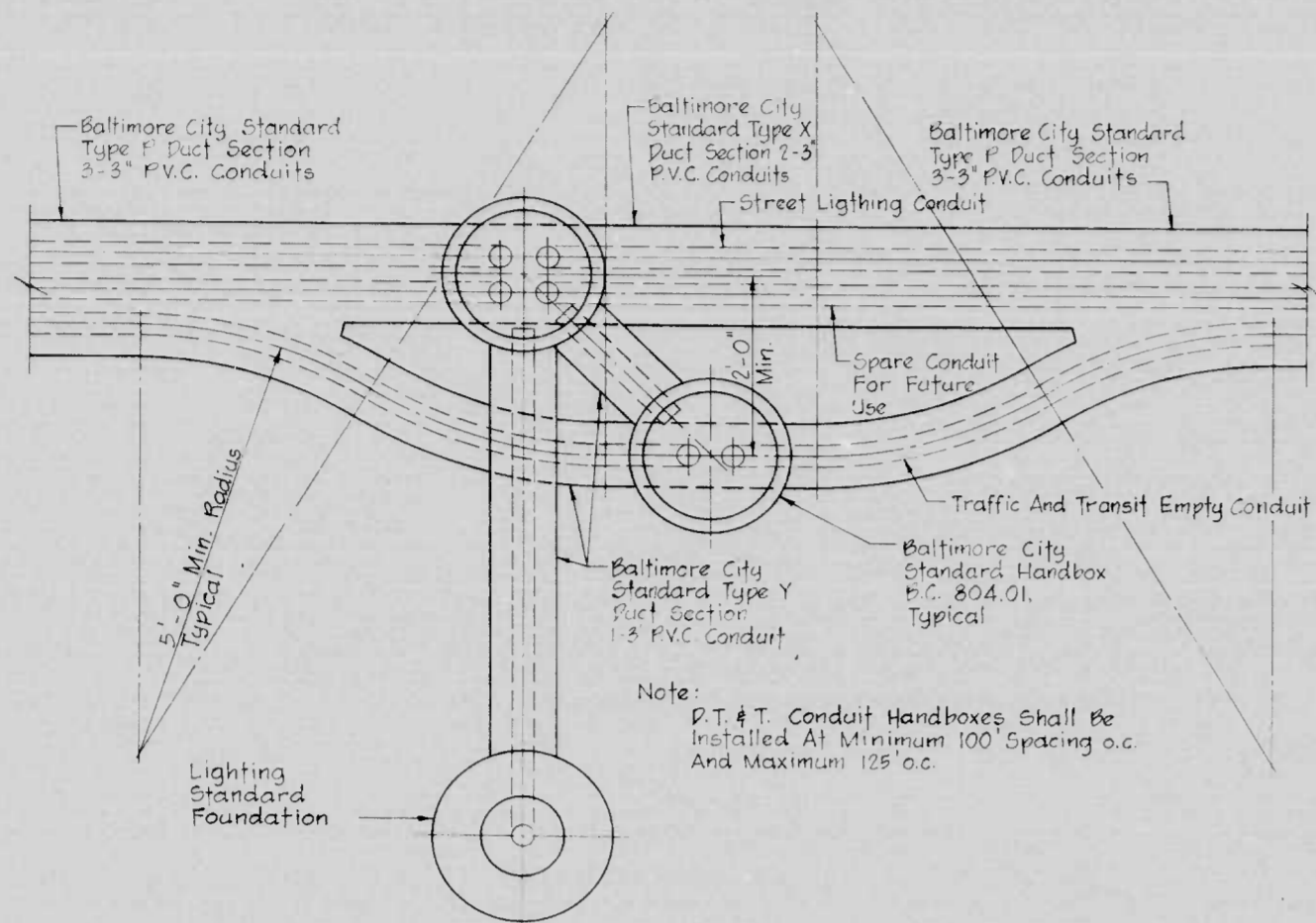
| | | | |
|-----------|------------------------|--|----------------------------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILES SHARP ST & CHARLES ST | DRAWN BY RLJ TRACED BY JFL |
| | CONSULTING ENGINEERS | | DES BY RMH CHK BY WRH |
| | BALTIMORE MARYLAND | SCALE AS NOTED | SHEET NO. P-5 OF 7 |

| FHWA REGION | STATE | FED AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|-------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-6 | (41) |

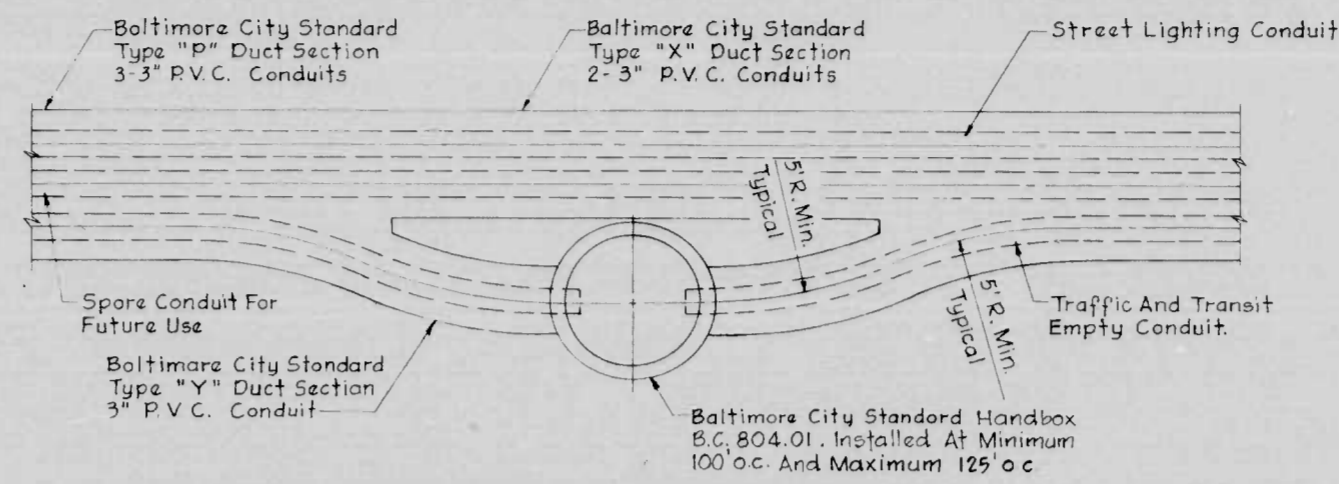


SCALE BALTIMORE 11/28/18 PROJ. NO. 11-815

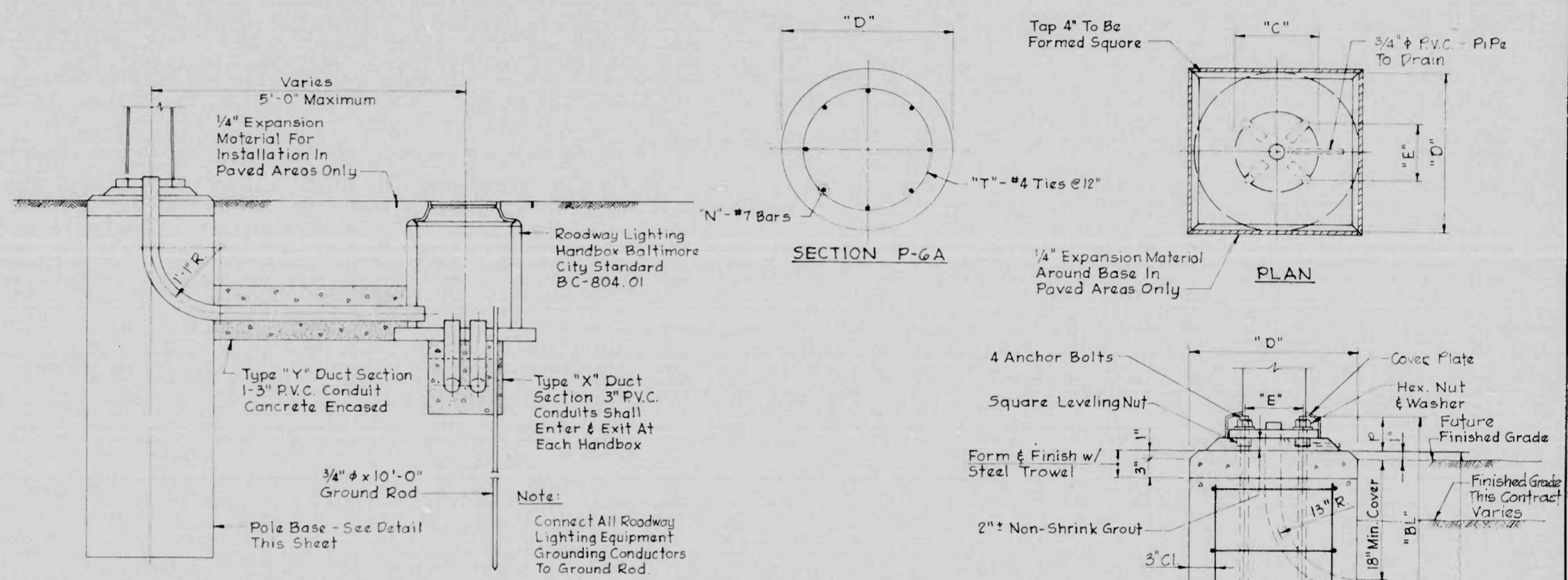
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-7 | P-17 (41) |



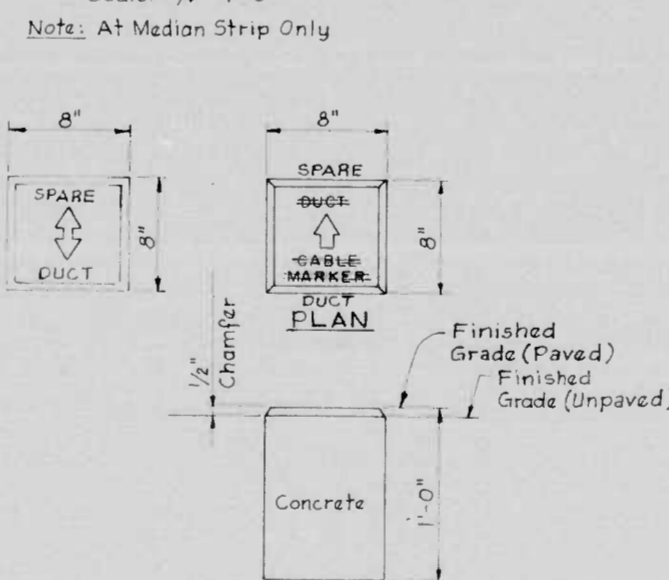
PLAN - CONDUITS AND HANDBOXES AT LIGHTING STANDARDS
Scale: 3/4" = 1'-0"



PLAN - CONDUITS AT DEPARTMENT OF TRAFFIC AND TRANSIT HANDBOX
Scale: 3/4" = 1'-0"



UNDERGROUND RACEWAYS AT POLE AND HANDBOXES
Scale: 3/4" = 1'-0"



ELEVATION SPARE DUCT AND CABLE MARKER
Scale: 1/2" = 1'-0"

| FNDN TYPE | FNDN DIA "D" | FNDN DEPTH "L" | No Vert REBARS "N" | No #4 TIES "T" | ANCHOR BOLTS | | | Projection "P" | |
|-----------|--------------|----------------|--------------------|----------------|--------------|------------|-----------|----------------|----|
| | | | | | SIZE "BL" | CIRCLE "C" | SPACE "E" | | |
| F-2 | 18" | 60" | 5 | 6 | 1" | 3 1/2" | 8" | 5 3/8" | 8" |
| F-1 | 24" | 72" | 8 | 6 | See Note A | | | | |
| F-3 | 24" | 96" | 8 | 8 | 1 3/4" | 5 1/2" | 15" | 10 3/8" | 8" |
| F-4 | 30" | 108" | 10 | 9 | 2" | 5 1/2" | 21" | 14 7/8" | 8" |

* Use F1 Foundation For Pole Nos 102 Thru 105, 201 Thru 204, 301, 302 & 304 Thru 309

Notes:

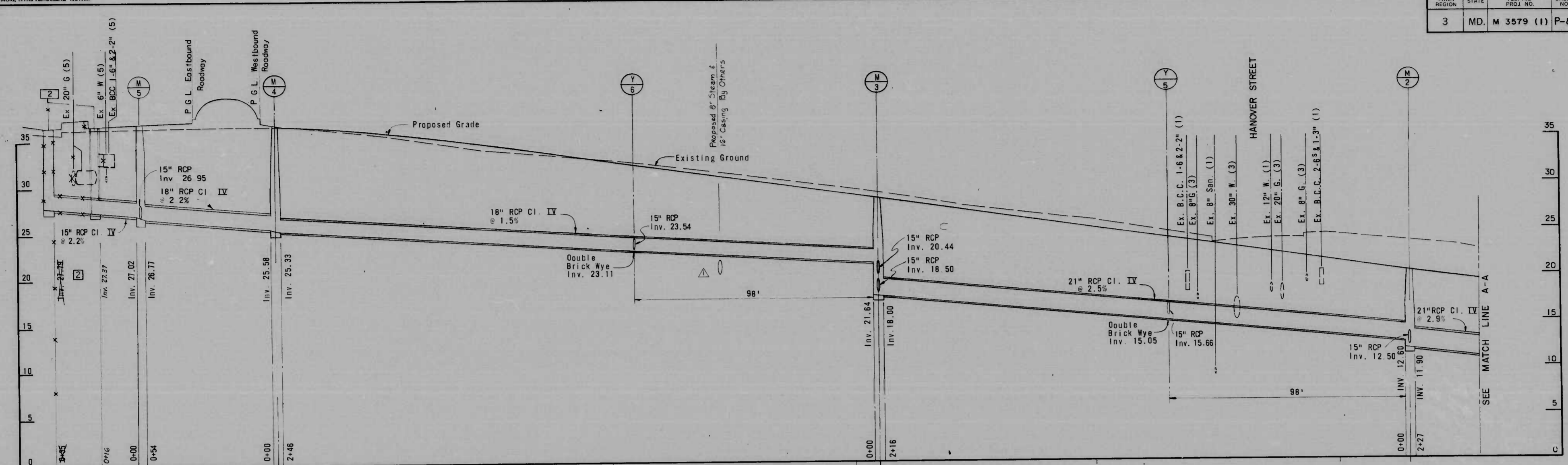
The Contractor Shall Exercise Additional Care In Setting Anchor Bolts For Joint Use Poles In Order That The Signal Arms Will Be Located At The Angles As Indicated On Sheets

Note A: Contractor Shall Determine Anchor Bolt Data For Poles To Be Furnished By The City.

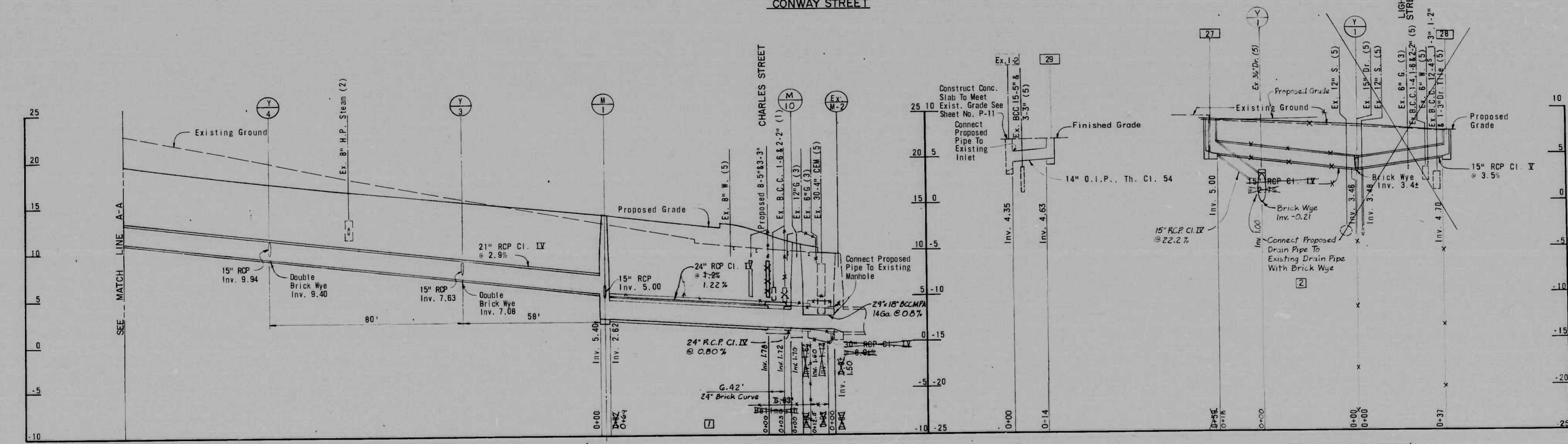
POLE FOUNDATION DETAILS
Not To Scale

| | | | | |
|---|--|--|--|--|
| REVISIONS 1 Revisions To Duct & Cable Markers.. 12/1/18 | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY CONWAY STREET SHARP STREET TO LIGHT STREET ELECTRICAL DETAILS | DRAWN BY GDB TRACED BY GDB F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | DES. BY REE CHK. BY DCB SHEET NO. P-7 of P-17 |
|---|--|--|--|--|

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD | M 3579 (1) | P-8 | P-17 (41) |



6' LEFT OF WESTBOUND ROADWAY STA. 4+25± TO STA. 12+30±
CONWAY STREET



6' LEFT OF WESTBOUND ROADWAY STA. 4+25± TO STA. 12+30±
CONWAY STREET

STA. 16+00

LEFT & RIGHT OF STA. 15+65±

- UTILITY DISPOSITION NOTATION
- (1) Existing Utility To Be Abandoned By Contractor
 - (2) Existing Utility To Be Abandoned By Others
 - (3) Existing Utility Presently Abandoned
 - (4) Existing Utility, Location And/Or Depth Unknown
 - (5) Existing Utility To Remain In Service
 - (6) Existing Utility To Be Relocated By Others

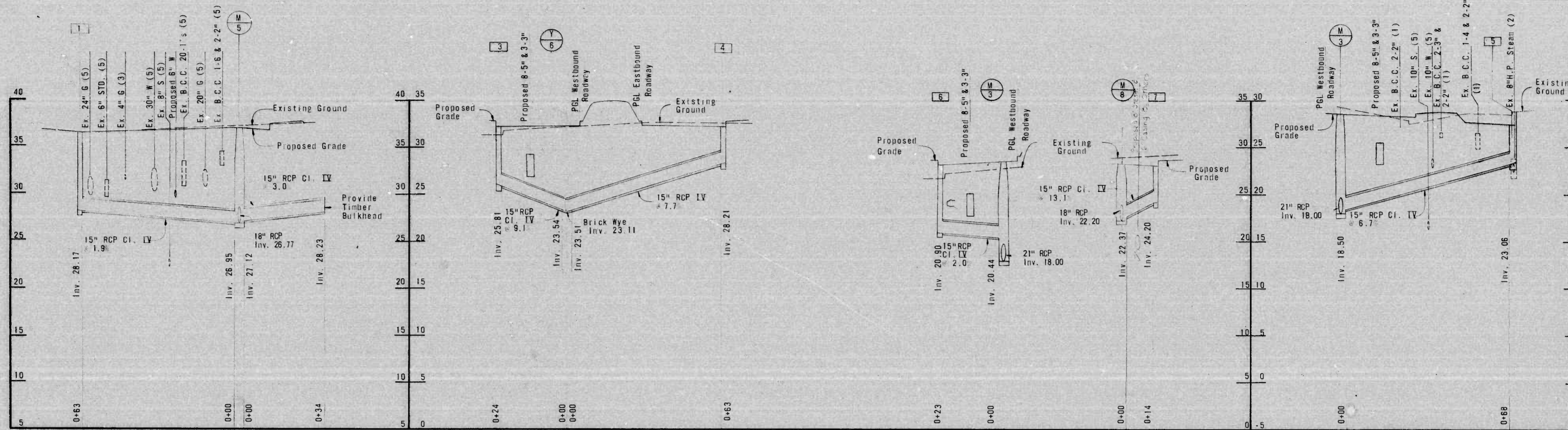
NOTE:
Provide Gravel Cradle Under All Storm Drain Systems Shown
This Sheet, Unless Noted Otherwise See City Of Baltimore
Standard No. BC-302.01 For Cradle Dimensions And Material.

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|--|---|---|--|--|---|
| <ul style="list-style-type: none"> 1 Addendum N^o 1 10-2-78 2 Storm Drain, Lt. Sta. 12+... 1/30/19 3 Storm Drain, Rt. Sta. 4+27± & Sta. 15+65±... 3/26/79 | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILES-STORM WATER DRAIN | | DRAWN BY MLK/FWS/DDH TRACED BY FWS/DDH F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | DES. BY MLK CHK. BY WHB/JAR JR. SHEET NO. P-8 of P-17 |

SCALE: HORIZ: 1"=20' VERT: 1"=5' DATE:

PAE BALTIMORE 12193 MERCURINE MC 497

| FEMA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | P-9 | 41 |

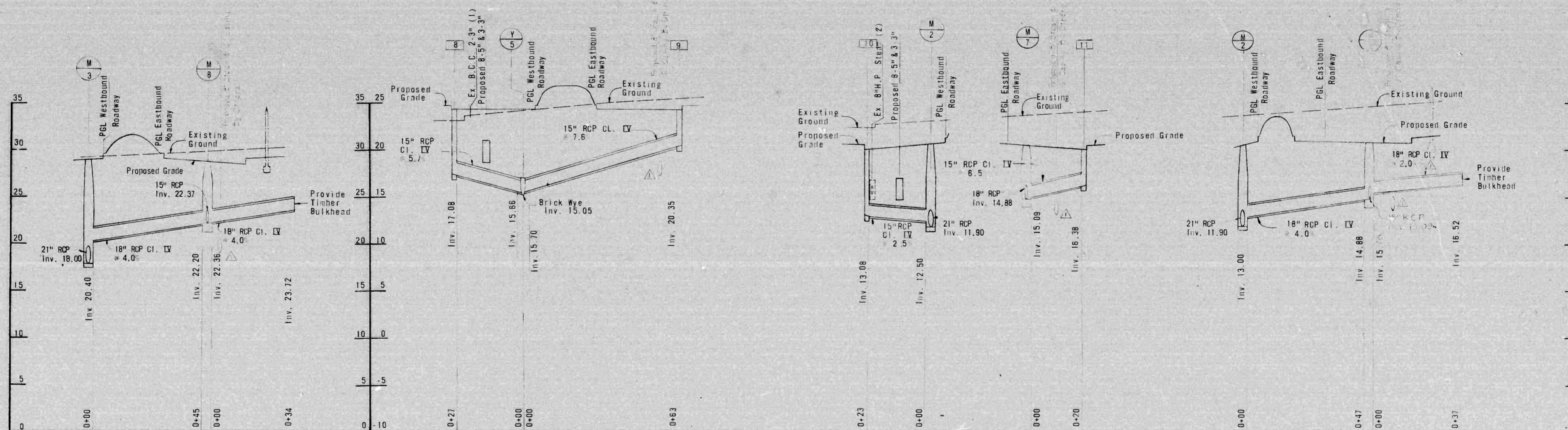


RIGHT OF STA. 4+50

LEFT & RIGHT OF STA. 6+10±

LEFT & RIGHT OF STA. 7+10±

LEFT OF STA. 7+10±



RIGHT OF STA. 7+10±

LEFT & RIGHT OF STA. 8+30±

LEFT & RIGHT OF STA. 9+30±

RIGHT OF STA. 9+30±

- UTILITY DISPOSITION NOTATION
- (1) Existing Utility To Be Abandoned By Contractor
 - (2) Existing Utility To Be Abandoned By Others
 - (3) Existing Utility Presently Abandoned
 - (4) Existing Utility, Location And/Or Depth Unknown
 - (5) Existing Utility To Remain In Service
 - (6) Existing Utility To Be Relocated By Others

NOTE: Provide Gravel Cradle Under All Storm Drain Systems Shown This Sheet. Unless Noted Otherwise See City Of Baltimore Station No. BC-302.01 For Cradle Dimensions And Material.

| | | | | | |
|-----------|---------------------------|---|--|--|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET | | DRAWN BY MLK TRACED BY DDH | |
| | CONSULTING ENGINEERS | PROFILES-STORM WATER DRAIN | | DES. BY MLK CHK BY JAR JR. | |
| | BALTIMORE, MARYLAND | SCALE: HORIZ 1"=20' VERT 1"=5' | | DATE | |
| | | F.A.P. NO. M 3579 (1) | | SHEET NO. | |
| | | SHA NO. BC 231-11-815 | | P-9 of P-17 | |
| | | BALTO. CITY NO. 2492 | | | |

K&E BALTIMORE 18 188 HERRICKS 12 75 M2124

TABULATION OF QUANTITIES

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | Q-1 | Q-4 (4) |

| LOCATION | R.C. PIPE CLASS 4 | | | | R.C. PIPE CLASS 5 | | C.M. PIPE 16 GAUGE | | PIPE CONN. BRICK Y | | PIPE CONNECTION DOUBLE BRICK Y | | | | DRAINAGE | | | | INLETS | | | | | | | | MANHOLES | | | | REMARKS |
|---|-------------------|--------|--------|--------|-------------------|--------|--------------------|-----------------|--------------------------|--------------------------|--------------------------------|------------------------------------|--|-------------------------------|------------------------------------|--------------------------------------|--|--|---|--|--|---|--|---|-----------------------|--|---|--|---|-------------------------|---------|
| | 15 IN. | 18 IN. | 21 IN. | 24 IN. | 15 IN. | 12 IN. | 18 IN. | 36 IN. X 18 IN. | 15 IN. X 15 IN. X 15 IN. | 18 IN. X 15 IN. X 15 IN. | 21 IN. X 15 IN. X 15 IN. | BELLMOUTH 30 IN. X 24 IN. X 18 IN. | REMOVAL OF OLD PIPE COLLECTORS - 12 IN. AND OVER | BULKHEAD EXISTING STORM DRAIN | ABANDON EXISTING STORM DRAIN INLET | ABANDON EXISTING STORM DRAIN MANHOLE | STANDARD TYPE S INLET - SINGLE GRADE (MINIMUM DEPTH) | STANDARD TYPE S INLET - MODIFIED TYPE 1 (VERTICAL DEPTH) | SINGLE NR INLET MODIFIED TYPE 1 (MINIMUM DEPTH) | SINGLE NR INLET MODIFIED TYPE 2 (VERTICAL DEPTH) | STANDARD TYPE NR INLET - MODIFIED TYPE 1 (MINIMUM DEPTH) | STANDARD TYPE NR INLET - MODIFIED TYPE 2 (VERTICAL DEPTH) | NR INLET MODIFIED TYPE 2 (MINIMUM DEPTH) | NR INLET MODIFIED TYPE 2 (VERTICAL DEPTH) | ADJUST EXISTING INLET | STANDARD STORM WATER MANHOLE (MINIMUM DEPTH) | STANDARD STORM WATER MANHOLE (VERTICAL DEPTH) | STANDARD STORM WATER MANHOLE MODIFIED TYPE 1 (MINIMUM DEPTH) | STANDARD STORM WATER MANHOLE MODIFIED TYPE 1 (VERTICAL DEPTH) | ADJUST EXISTING MANHOLE | |
| ITEM NO. | 304 | 305 | 306 | 307 | 308 | 309 | 310 | 314 | 315 | 316 | 317 | 318 | 319 | 320 | 321 | 322 | 325 | 326 | 327 | 328 | 329 | 330 | 331 | 332 | 333 | 334 | 335 | 336 | 337 | 338 | |
| UNIT | L.F. | L.F. | L.F. | L.F. | L.F. | L.F. | L.F. | EA | EA | EA | EA | EA | L.F. | C.Y. | EA | EA | EA | L.F. | EA | L.F. | EA | L.F. | EA | EA | EA | L.F. | L.F. | EA | L.F. | EA | |
| SHEET NO. P-1 STA. 3+50 TO STA. 7+00 | 284 | 331 | - | - | - | 21 | - | - | - | 1 | - | - | - | - | - | - | 1 | 0.2 | - | - | 3 | 7.2 | 1 | 5.0 | - | 2 | 13.5 | - | - | - | |
| SHEET NO. P-2 STA. 7+00 TO STA. 12+00 | 464 | 226 | 434 | 82 | - | - | 21 | - | - | 3 | - | - | 0.1 | 3 | - | - | 2 | 1.9 | - | - | 12 | 20.1 | - | - | - | 4 | 11.5 | 2 | 13.5 | - | |
| SHEET NO. P-3 STA. 12+00 TO STA. 16+50 | 105 | 84 | - | - | 46 | - | 27 | 1 | 1 | - | - | 1 | 35 | 0.3 | 7 | - | 3 | 0.8 | 3 | 0.0 | 7 | 4.8 | - | - | 2 | 1 | 2.9 | - | - | 3 | |
| TOTALS (QUANTITIES) | 853 | 641 | 434 | 82 | 46 | 21 | 48 | 1 | 1 | 1 | 3 | 1 | 35 | 0.4 | 10 | 0 | 6 | 2.9 | 3 | 0.0 | 22 | 32.1 | 1 | 5.0 | 2 | 1 | 28.0 | 2 | 13.5 | 3 | |
| TOTALS (PROPOSAL QUANTITIES) | 860 | 650 | 440 | 85 | 50 | 25 | 50 | 1 | 1 | 1 | 3 | 1 | 50 | 2.0 | 10 | 1 | 6 | 3.0 | 3 | 1 | 22 | 33 | 1 | 5.0 | 5 | 7 | 28.0 | 2 | 14.0 | 5 | |

| | | | |
|-----------|---|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET TABULATION OF QUANTITIES | DRAWN BY W.B. TRACED BY W.B. F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| | | DES. BY MLK CHK. BY JAR | SHEET NO. Q-1 OF 24 |
| | | SCALE | DATE |

TABULATION OF QUANTITIES

| | | | | |
|--------------|-------|--------------------|-----------|--------------|
| FHWY. REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M-3579 (1) | Q-2 | Q-4 (41) |

| LOCATION | PAVING | | | | | | | | SHOULDERS | | | LANDSCAPING | | | | | REMARKS |
|------------------------------|----------------------------------|----------------------------------|---|------------------------------------|---|---|------------------------------------|---|--------------------------|--|-----------------------------------|-------------------|-------------------------------------|-------------|--------------------------|------------------------|---------|
| | 3 IN. SUB-BASE USING CRUSHER RUN | 6 IN. SUB-BASE USING CRUSHER RUN | VARIABLE DEPTH SUB-BASE USING CRUSHER RUN | BITUMINOUS MATERIAL FOR PRIME COAT | BITUMINOUS CONCRETE FOR WEAR AND/OR LEVELING COURSE | BITUMINOUS CONCRETE USING SAND ON STONE | BITUMINOUS CONCRETE USING SAND BI. | 9 IN. REINFORCED CEMENT CONCRETE PAVEMENT (18 IN. X 18 IN.) | BITUMINOUS CONCRETE CURB | STANDARD TYPE "A" CURB (18 IN. X 18 IN.) | STANDARD TYPE "A" CURB (MODIFIED) | TEMPORARY SEEDING | SAFETY FENCING FOR SEDIMENT DEVICES | STRAW BALES | SEDIMENT TRAP EXCAVATION | SEDIMENT TRAP CLEANOUT | |
| ITEM NO. | 501 | 502 | 503 | 505 | 506 | 507 | 508 | 510 | 601 | 602 | 603 | 701 | 703 | 704 | 705 | 706 | |
| UNIT | S.Y. | S.Y. | TON | GAL. | TON | TON | TON | S.Y. | L.F. | L.F. | L.F. | S.Y. | L.F. | EA. | C.Y. | C.Y. | |
| SHEET NO. P-1 | | | | | | | | | | | | | | | | | |
| E. B. CONWAY ST. | | 1218 | | | | 130.8 | | 1178 | | 563 | | 816 | | | | | |
| W. B. CONWAY ST. | 47 | 1218 | | 23 | | 138.4 | 1.5 | 1178 | 40 | 540 | | 1,344 | | | | | |
| SHARP ST. | 94 | 590 | | 47 | | 76.0 | 5.6 | 547 | 146 | 124 | | 289 | | | | | |
| SHEET NO. P-2 | | | | | | | | | | | | | | | | | |
| E. B. CONWAY ST. | | 2306 | | | | 283.4 | | 2233 | | 1000 | | 1,807 | | | | | |
| W. B. CONWAY ST. | 221 | 1906 | | 111 | | 193.0 | | 1833 | | 958 | | 2415 | | | | | |
| SERVICE DRIVE | 38 | 151 | | 19 | | 20.0 | | 142 | | 120 | | | | | | | |
| SHEET NO. P-3 | | | | | | | | | | | | | | | | | |
| E. B. CONWAY ST. | | 275 | | | 92.8 | 135.8 | | 267 | | 111 | 321 | 234 | | | | | |
| W. B. CONWAY ST. | 213 | 1497 | | 107 | | 182.8 | | 1439 | | 783 | | 925 | | | | | |
| CHARLES ST. | 135 | 861 | | 68 | 8.9 | 96.3 | 7.1 | 620 | 108 | 130 | | 78 | | | | | |
| SHEET NO. P-14 | | | | | | | | | | | | | | | | | |
| | | | 21 | | | | | | | | | | 180 | 100 | 50 | 25 | |
| TOTALS (QUANTITIES) | 748 | 9822 | 21 | 375 | 101.7 | 1256.5 | 14.2 | 9437 | 294 | 4329 | 321 | 7908 | 180 | 100 | 50 | 25 | |
| TOTALS (PROPOSAL QUANTITIES) | 775 | 9850 | 150 | 400 | 105 | 1270 | 20 | 9490 | 300 | 4340 | 325 | 8700 | 180 | 100 | 50 | 25 | |

| | | | |
|-----------|------------------------|--|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET | |
| | CONSULTING ENGINEERS | TABULATION OF QUANTITIES | |
| | BALTIMORE, MARYLAND | DRAWN BY: RLJ TRACED BY: TEA F.A.P. NO. M-3579 (1) S.H.A. NO. BC 231-II 815 BALTO. CITY NO. 2492 | DES. BY: RLJ CHK. BY: HMM SHEET NO. Q-2 of Q-4 |
| | | SCALE: _____ | DATE: _____ |

SUMMARY OF QUANTITIES

| FINA REGION | STATE | FED AID PROJ NO | SHEET NO | TOTAL SHEETS |
|-------------|-------|-----------------|----------|--------------|
| 3 | MD. | M 3579 (1) | Q4 | Q-4 (21) |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|--------------------------|--|------|------------|-------|-----------|----------|---------------------|-------------------|
| PRELIMINARY ITEMS | | | | | | | | |
| 101 | CLEARING AND GRUBBING | L.S. | | L.S. | | L.S. | | LUMP SUM |
| 102 | ENGINEERS FACILITIES | L.S. | | L.S. | | L.S. | | LUMP SUM |
| 103 | MAINTENANCE OF TRAFFIC | L.S. | | L.S. | | L.S. | | LUMP SUM |
| 104 | TEMPORARY PRECAST CONCRETE BARRIER | L.F. | | 1 180 | | 1 180 | | 1 180 |
| 105 | TEMPORARY PRECAST CONCRETE BARRIER LEFT IN PLACE | L.F. | | 80 | | 80 | | 80 |
| 106 | TEMPORARY PAVEMENT MARKING TAPE | L.F. | | 1 680 | | 1 700 | | 1 700 |
| 107 | SHEET ALUMINUM FOR SIGNING | S.F. | | 400 | | 400 | | 400 |
| 108 | CONSTRUCTION STAKEOUT | L.S. | | L.S. | | L.S. | | LUMP SUM |
| 109 | MOBILIZATION | L.S. | | L.S. | | L.S. | | LUMP SUM |
| 110 | ON THE JOB TRAINING | HR | | 1 000 | | 1 000 | | 1 000 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|----------------------|---|------|------------|-------|-----------|----------|---------------------|-------------------|
| GRADING ITEMS | | | | | | | | |
| 201 | CLASS 1 EXCAVATION | C.Y. | | 6 860 | | 6 860 | | 6 860 |
| 202 | CLASS 1-A EXCAVATION | C.Y. | | 1 366 | | 1 366 | | 1 400 |
| 203 | CLASS 2 EXCAVATION | C.Y. | | | | 58 | | 60 |
| 204 | BORROW EXCAVATION | C.Y. | | 1 188 | | 1 188 | | 2 000 |
| 205 | CONTINGENT BORROW EXCAVATION | C.Y. | | | | 500 | | 500 |
| 206 | SELECT BORROW EXCAVATION | C.Y. | | 1 571 | | 1 571 | | 600 |
| 207 | CONTINGENT SELECT BORROW EXCAVATION | C.Y. | | 500 | | 500 | | 500 |
| 208 | TEST PIT EXCAVATION | C.Y. | | 300 | | 300 | | 300 |
| 209 | REMOVAL OF EXISTING CURB, CURB AND GUTTER | L.F. | | 844 | | 844 | | 850 |
| 210 | REMOVAL OF EXISTING PAVEMENT | S.Y. | | 1 896 | | 1 896 | | 1 900 |
| 211 | REMOVAL OF EXISTING SIDEWALK | S.Y. | | 567 | | 567 | | 575 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|-----------------------|---|------|------------|-------|-----------|----------|---------------------|-------------------|
| DRAINAGE ITEMS | | | | | | | | |
| 301 | STANDARD STORM WATER MANHOLE MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 50 | | 50 |
| 302 | ADJUST EXISTING MANHOLE | EA | | | | 100 | | 100 |
| 303 | CONTINGENT MANHOLE | EA | | | | 100 | | 100 |
| 304 | 16 IN. R.C. PIPE CLASS 4 | L.F. | | | | 853 | | 860 |
| 305 | 18 IN. R.C. PIPE CLASS 4 | L.F. | | | | 641 | | 650 |
| 306 | 21 IN. R.C. PIPE CLASS 4 | L.F. | | | | 434 | | 440 |
| 307 | 24 IN. R.C. PIPE CLASS 4 | L.F. | | | | 82 | | 85 |
| 308 | 16 IN. R.C. PIPE CLASS 5 | L.F. | | | | 46 | | 50 |
| 309 | 12 C.M. PIPE 16 GAUGE | L.F. | | | | 21 | | 25 |
| 310 | 18 C.M. PIPE 16 GAUGE | L.F. | | | | 48 | | 50 |
| 311 | STANDARD CLEAR WATER MANHOLE MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 100 | | 100 |
| 312 | ADJUST EXISTING CLEAR WATER MANHOLE MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 100 | | 100 |
| 313 | CONTINGENT CLEAR WATER MANHOLE MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 100 | | 100 |
| 314 | 36 IN. X 18 IN. PIPE CONNECTION BRICK Y | EA | | | | 1 | | 1 |
| 315 | 18 IN. X 18 IN. X 18 IN. PIPE CONNECTION BRICK Y | EA | | | | 1 | | 1 |
| 316 | 18 IN. X 18 IN. X 18 IN. PIPE CONNECTION BRICK Y | EA | | | | 1 | | 1 |
| 317 | 18 IN. X 18 IN. X 18 IN. PIPE CONNECTION BRICK Y | EA | | | | 3 | | 3 |
| 318 | 30 IN. X 24 IN. X 18 IN. BELLMOUTH | EA | | | | 1 | | 1 |
| 319 | REMOVAL OF OLD PIPE COVERTS 12 IN. AND OVER | L.F. | | | | 35 | | 50 |
| 320 | BULKHEAD EXISTING STORM DRAIN | C.Y. | | | | 0.4 | | 0.4 |
| 321 | ABANDON EXISTING STORM DRAIN INLET | EA | | | | 10 | | 10 |
| 322 | ABANDON EXISTING STORM DRAIN MANHOLE | EA | | | | 0 | | 1 |
| 323 | NOT USED | | | | | | | |
| 324 | NOT USED | | | | | | | |
| 325 | STANDARD TYPE S INLET SINGLE GRATE MINIMUM DEPTH | EA | | | | 6 | | 6 |
| 326 | STANDARD TYPE S INLET SINGLE GRATE VERTICAL DEPTH | EA | | | | 2.9 | | 3 |
| 327 | SINGLE WR INLET MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 3 | | 3 |
| 328 | SINGLE WR INLET MOD. TYPE 1 VERTICAL DEPTH | EA | | | | 0 | | 0 |
| 329 | STANDARD NR INLET MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 22 | | 22 |
| 330 | STANDARD NR INLET MOD. TYPE 1 VERTICAL DEPTH | EA | | | | 32.1 | | 33 |
| 331 | STANDARD NR INLET MOD. TYPE 2 MINIMUM DEPTH | EA | | | | 1 | | 1 |
| 332 | STANDARD NR INLET MOD. TYPE 2 VERTICAL DEPTH | EA | | | | 5 | | 5 |
| 333 | ADJUST EXISTING INLET | EA | | | | 2 | | 2 |
| 334 | STANDARD STORM WATER MANHOLE MINIMUM DEPTH | EA | | | | 7 | | 7 |
| 335 | STANDARD STORM WATER MANHOLE VERTICAL DEPTH | EA | | | | 28 | | 29 |
| 336 | STANDARD STORM WATER MANHOLE MOD. TYPE 1 MINIMUM DEPTH | EA | | | | 2 | | 2 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|-------------------------------|--|------|------------|-------|-----------|----------|---------------------|-------------------|
| DRAINAGE ITEMS (CON'T) | | | | | | | | |
| 337 | STANDARD STORM WATER MANHOLE MOD. TYPE 1 VERTICAL DEPTH | L.F. | | | | 13.5 | | 14 |
| 338 | ADJUST EXISTING MANHOLE | EA | | | | 3 | | 3 |
| 339 | CONTINGENT M.I.X. NO. 1 CONC. FOR MISC. STRUCTURES | C.Y. | | | | 5 | | 5 |
| 340 | CONTINGENT M.I.X. NO. 3 CONC. FOR MISC. STRUCTURES | C.Y. | | | | 5 | | 5 |
| 341 | CONTINGENT BRICK MASONRY FOR M.I.X. NO. 1 CONC. STRUCTURES | C.Y. | | | | 5 | | 5 |
| 342 | CONTINGENT SEWER BRICK MASONRY | C.Y. | | | | 5 | | 5 |
| 343 | CONTINGENT UTILITY TRENCH UNDERDRAIN | L.F. | | | | 200 | | 200 |
| 344 | CONTINGENT PERFORATED CIRCULAR PIPE UNDERDRAIN | L.F. | | | | 200 | | 200 |
| 345 | CONTINGENT PERFORATED CIRCULAR PIPE UNDERDRAIN | L.F. | | | | 200 | | 200 |
| 346 | CONTINGENT PERFORATED CIRCULAR PIPE UNDERDRAIN | L.F. | | | | 100 | | 100 |
| 347 | CONTINGENT AGGREGATE BACKFILL FOR UNDERDRAIN | C.Y. | | | | 30 | | 30 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|---------------------|--|------|------------|-------|-----------|----------|---------------------|-------------------|
| PAVING ITEMS | | | | | | | | |
| 501 | 3 IN. SUB-BASE USING CRUSHER RUN | S.Y. | | | | 748 | | 775 |
| 502 | 6 IN. SUB-BASE USING CRUSHER RUN | S.Y. | | | | 9 822 | | 9 850 |
| 503 | VARIABLE DEPTH SUB-BASE USING CRUSHER RUN | TON | | | | 21 | | 129 |
| 504 | CRUSHER RUN AGGREGATE FOR MAINTENANCE OF TRAFFIC | TON | | | | 300 | | 300 |
| 505 | BITUMINOUS MATERIAL FOR PRIME COAT | TON | | | | 375 | | 400 |
| 506 | BITUMINOUS CONCRETE FOR WEDGE AND OR LEVELING COURSE | TON | | | | 101.7 | | 105 |
| 507 | BITUMINOUS CONCRETE USING BAND ON STONE | TON | | | | 1 256.5 | | 1 270 |
| 508 | BITUMINOUS CONCRETE USING BAND ON | TON | | | | 14.2 | | 20 |
| 509 | BITUMINOUS CONCRETE FOR MAINTENANCE OF TRAFFIC | TON | | | | 300 | | 300 |
| 510 | PORTLAND CEMENT CONCRETE PAVEMENT M.I.X. NO. 1 | S.Y. | | | | 9 437 | | 9 450 |
| 511 | CALCIUM CHLORIDE | TON | | | | 10 | | 10 |
| 512 | 7 IN. PORTLAND CEMENT CONCRETE PAVEMENT M.I.X. NO. 1 | S.Y. | | | | 20 | | 20 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|-----------------------|---------------------------------------|------|------------|-------|-----------|----------|---------------------|-------------------|
| SHOULDER ITEMS | | | | | | | | |
| 601 | BITUMINOUS CONCRETE 1 1/2 IN. | L.F. | | | | 294 | | 300 |
| 602 | STANDARD TYPE 'A' CURB 8 IN. X 18 IN. | L.F. | | | | 4 329 | | 4 340 |
| 603 | STANDARD TYPE 'A' CURB MODIFIED | L.F. | | | | 321 | | 325 |

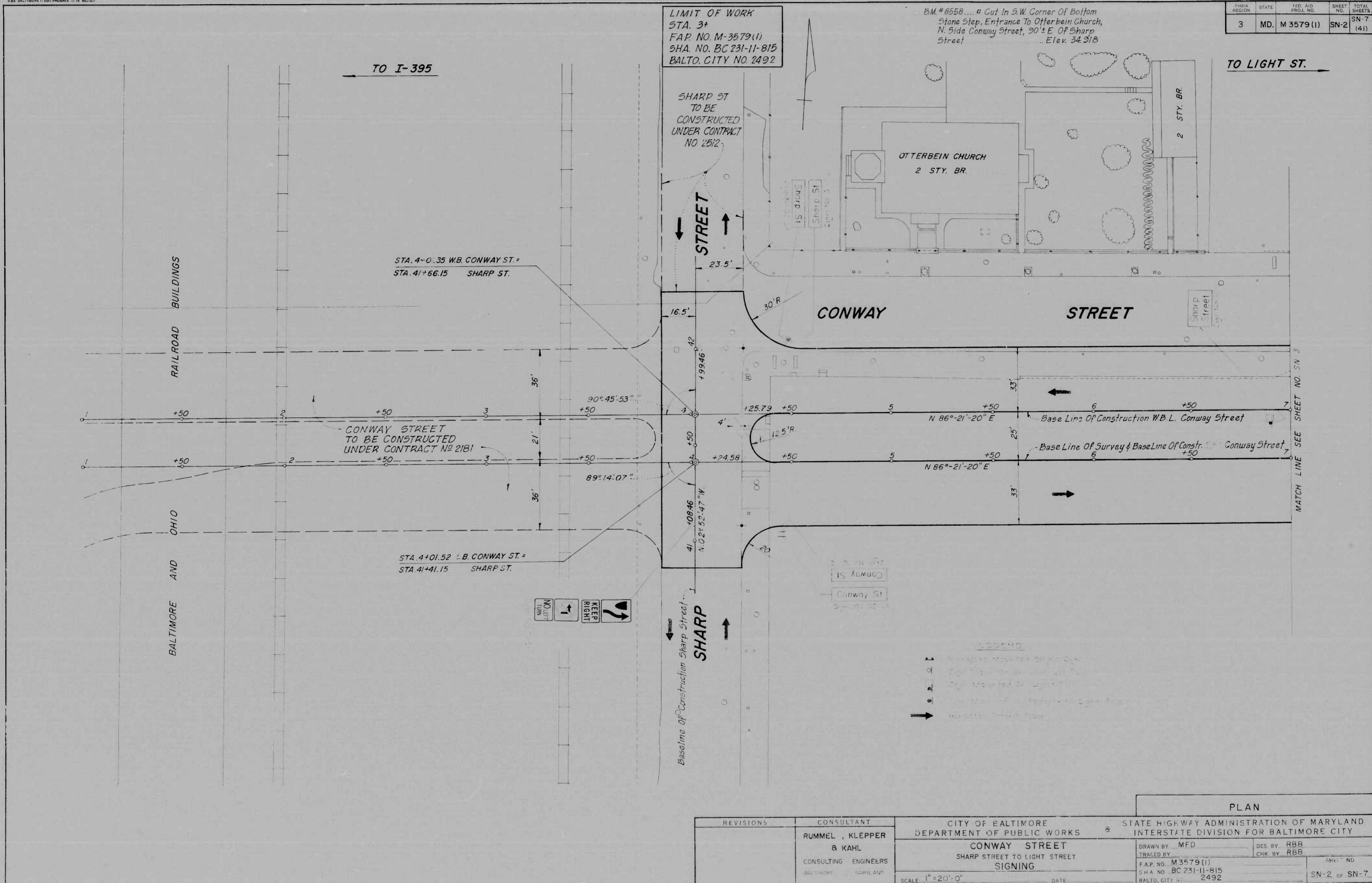
| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|--------------------------|-------------------------------------|------|------------|-------|-----------|----------|---------------------|-------------------|
| LANDSCAPING ITEMS | | | | | | | | |
| 701 | TEMPORARY SEEDING | S.Y. | | | | 7 908 | | 8 100 |
| 702 | SOODING | S.Y. | | | | | | 30 |
| 703 | SAFETY FENCING FOR SEDIMENT DEVICES | L.F. | | | | 180 | | 180 |
| 704 | STRAW BALES | EA | | | | 100 | | 100 |
| 705 | SEDIMENT TRAP EXCAVATION | C.Y. | | | | 50 | | 50 |
| 706 | SEDIMENT TRAP CLEANOUT | C.Y. | | | | 25 | | 25 |
| 707 | RELOCATE M.I.X. PARKING LOT SIGN | L.S. | | | | L.S. | | LUMP SUM |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|----------------------|--|------|------------|-------|-----------|----------|---------------------|-------------------|
| UTILITY ITEMS | | | | | | | | |
| 801 | MANHOLE FOR TRAFFIC DEVICES | EA | | | | 23 | | 23 |
| 802 | HANDHOX CONDUIT | EA | | | | 17 | | 20 |
| 803 | 10 AWG TYPE THHN THIN STRANDED COPPER WIRE | L.F. | | | | 1 520 | | 2 300 |
| 804 | 6 AWG BARE STRANDED COPPER WIRE | L.F. | | | | 210 | | 300 |
| 805 | FUSIBLE DISCONNECT POLES | EA | | | | 20 | | 20 |
| 806 | ROUND HOOD 3/4 IN. ROUND 10 FT. SECTION | EA | | | | 14 | | 18 |
| 807 | DUCT MARKERS | EA | | | | 27 | | 30 |
| 808 | TYPE W DUCT SECTION 6.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 149 | | 230 |
| 809 | TYPE D DUCT SECTION 8.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 134 | | 140 |
| 810 | TYPE P DUCT SECTION 3.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 1 181 | | 1 200 |
| 811 | TYPE X DUCT SECTION 2.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 54 | | 57 |
| 812 | TYPE Y DUCT SECTION 1.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 231 | | 238 |
| 813 | TYPE W DUCT SECTION 6.5 IN. I.D. DUCT 28 X 28 | L.F. | | | | 111 | | 116 |
| 814 | MECHANICAL ELECTRICAL MANHOLE NO. 1 | EA | | | | 1 | | 1 |
| 815 | MECHANICAL ELECTRICAL MANHOLE NO. 2 | EA | | | | 1 | | 1 |
| 816 | MECHANICAL ELECTRICAL MANHOLE NO. 3 | EA | | | | 1 | | 1 |
| 817 | MECHANICAL ELECTRICAL MANHOLE NO. 4 | EA | | | | 1 | | 1 |
| 818 | MODIFY MECHANICAL ELECTRICAL MANHOLE | EA | | | | 5 | | 5 |
| 819 | ABANDON MANHOLE MECHANICAL ELECTRICAL & SANITARY | EA | | | | 11 | | 11 |

| NO. | ITEM | UNIT | INTERSTATE | METRO | NON-PART. | QUANTITY | CONTINGENT QUANTITY | PROPOSAL QUANTITY |
|------------------------------|--|------|------------|-------|-----------|----------|---------------------|-------------------|
| UTILITY ITEMS (CON'T) | | | | | | | | |
| 820 | INSTALL FIRE ALARM BOX | EA | | | | 2 | | 2 |
| 821 | 6 IN. FIRE HYDRANT AND VALVE | EA | | | | 6 | | 6 |
| 822 | REMOVE FIRE HYDRANT AND VALVE | EA | | | | 4 | | 4 |
| 823 | ABANDON WATER SERVICE ISOLATION VALVES | EA | | | | 3 | | 3 |
| 824 | 8 IN. CUTTING IN VALVE AND SLEEVE | EA | | | | 1 | | 1 |
| 825 | 10 IN. CUTTING IN VALVE AND SLEEVE | EA | | | | 1 | | 1 |
| 826 | INSTALL PLUG CLAMP CAP OR PLUG AND BUTTRESS | EA | | | | 3 | | 3 |
| 827 | SANITARY OFFSET MANHOLE | EA | | | | 1 | | 1 |
| 828 | MODIFY SANITARY MANHOLE | EA | | | | 1 | | 1 |
| 829 | ADJUST EXISTING UTILITY SERVICE FEATURE TO PROTECT FROM DAMAGE | EA | | | | 30 | | 32 |
| 830 | BULKHEAD EXISTING UTILITIES | C.Y. | | | | 15 | | 20 |
| 831 | JOINT USE POLES NO. 100 AND 101 | EA | | | | 2 | | 2 |
| 832 | JOINT USE POLE NO. 106 | EA | | | | 1 | | 1 |
| 833 | JOINT USE POLES NO. 107 311 315 & 319 | EA | | | | 4 | | 4 |
| 834 | JOINT USE POLE NO. 108 | EA | | | | 1 | | 1 |
| 835 | JOINT USE POLE NO. 312 | EA | | | | 1 | | 1 |
| 836 | JOINT USE POLE NO. 363 | EA | | | | 1 | | 1 |
| 837 | JOINT USE POLE NO. 316 | EA | | | | 1 | | 1 |
| 838 | JOINT USE POLE NO. 317 | EA | | | | 1 | | 1 |
| 839 | JOINT USE POLE NO. 318 | EA | | | | 1 | | 1 |
| 840 | TYPE F LUMINAIRE AND JOINT USE POLE INSTALL & TEST | EA | | | | 13 | | 13 |
| 841 | TYPE F LUMINAIRE AND JOINT USE POLE INSTALL & TEST | EA | | | | 3 | | 3 |
| 842 | TYPE F LUMINAIRE AND JOINT USE POLE INSTALL & TEST | EA | | | | 3 | | 3 |
| 843 | LIGHTING AND JOINT USE POLE FOUNDATION TYPE F-1 | EA | | | | 16 | | 16 |
| 844 | LIGHTING AND JOINT USE POLE FOUNDATION TYPE F-2 | EA | | | | 5 | | 5 |
| 845 | LIGHTING AND JOINT USE POLE FOUNDATION TYPE F-3 | EA | | | | 5 | | 5 |
| 846 | LIGHTING AND JOINT USE POLE FOUNDATION TYPE F-4 | EA | | | | 3 | | 3 |
| 847 | CONTINGENT M.I.X. NO. 3 CONCRETE FOR FOUNDATIONS | C.Y. | | | | 5 | | 5 |

DATE BALTIMORE 11/24/53 PROJECT NO. 2492

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | SN-2 | SN-7 (4) |



LIMIT OF WORK
 STA. 3+
 F.A.P. NO. M-3579(1)
 SHA. NO. BC 231-11-815
 BALTO. CITY NO. 2492

B.M. # 8558... a Cut In S.W. Corner Of Boftom
 Stone Step, Entrance To Otterbein Church,
 N. Side Conway Street, 90° E Of Sharp
 Street Elev. 34.218

TO I-395

TO LIGHT ST.

RAILROAD BUILDINGS

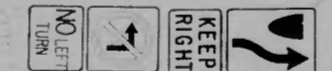
BALTIMORE AND OHIO

STA. 4+0.35 W.B. CONWAY ST. =
 STA. 41+66.15 SHARP ST.

CONWAY STREET
 TO BE CONSTRUCTED
 UNDER CONTRACT NO. 2181

STA. 4+01.52 = B. CONWAY ST. =
 STA. 41+41.15 SHARP ST.

Base Line Of Construction Sharp Street



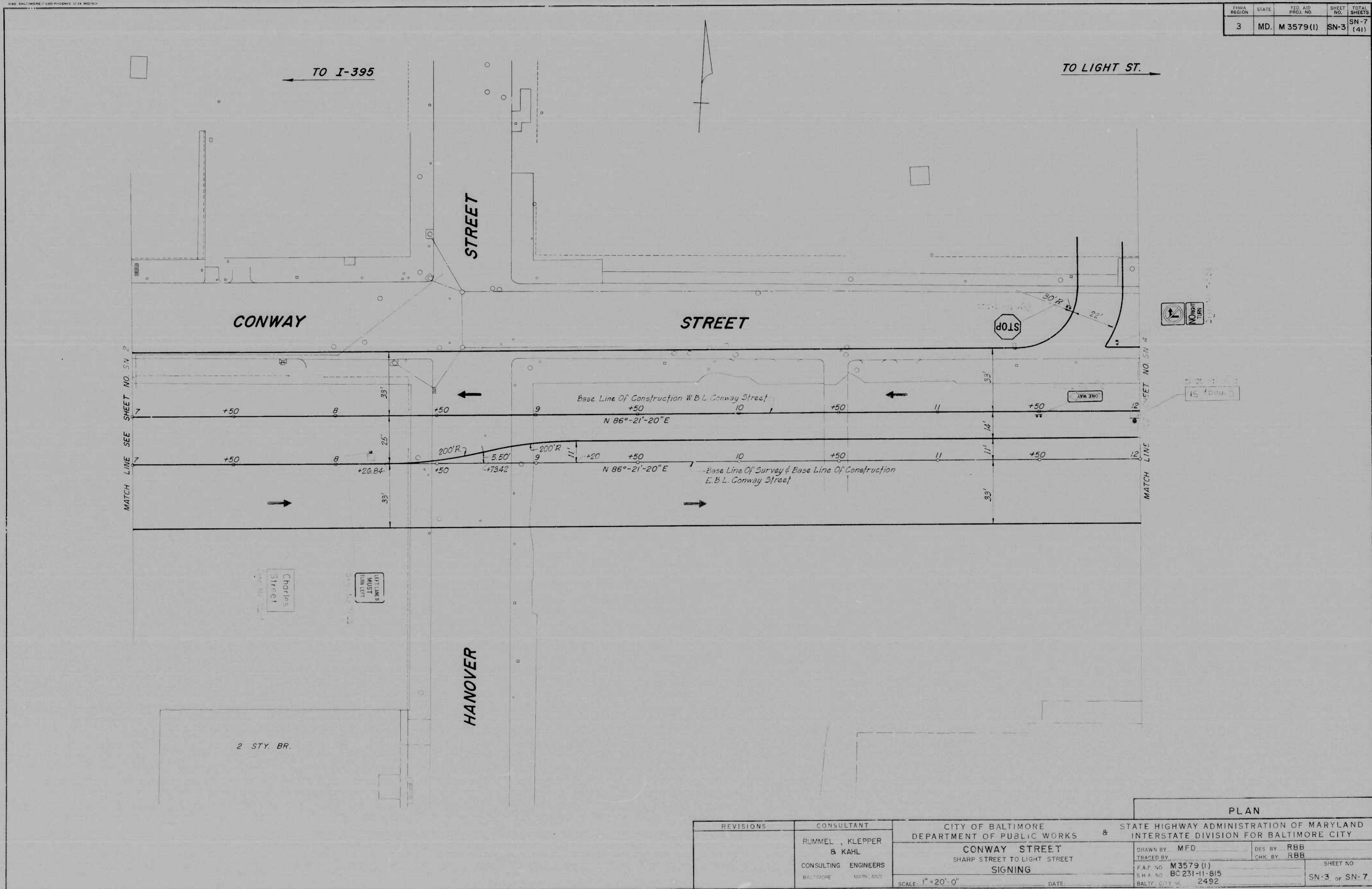
- LEGEND**
- 1 Proposed Work
 - 2 Existing Conditions
 - 3 Utility Lines
 - 4 Proposed Work
 - 5 Proposed Work
 - 6 Proposed Work
 - 7 Proposed Work

MATCH LINE SEE SHEET NO. SN-3

PLAN

| | | | |
|-----------|------------------------|------------------------------|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE | STATE HIGHWAY ADMINISTRATION OF MARYLAND |
| | RUMMEL, KLEPPER & KAHL | DEPARTMENT OF PUBLIC WORKS | INTERSTATE DIVISION FOR BALTIMORE CITY |
| | CONSULTING ENGINEERS | CONWAY STREET | |
| | BALTIMORE | SHARP STREET TO LIGHT STREET | |
| | | SIGNING | |
| | | SCALE: 1" = 20'-0" | DATE: |
| | | DRAWN BY: MFD | DES. BY: RBB |
| | | TRACED BY: | CHK BY: RBB |
| | | F.A.P. NO. M 3579 (1) | SHEET NO. |
| | | SHA. NO. BC 231-11-815 | SN-2 OF SN-7 |
| | | BALTO. CITY NO. 2492 | |

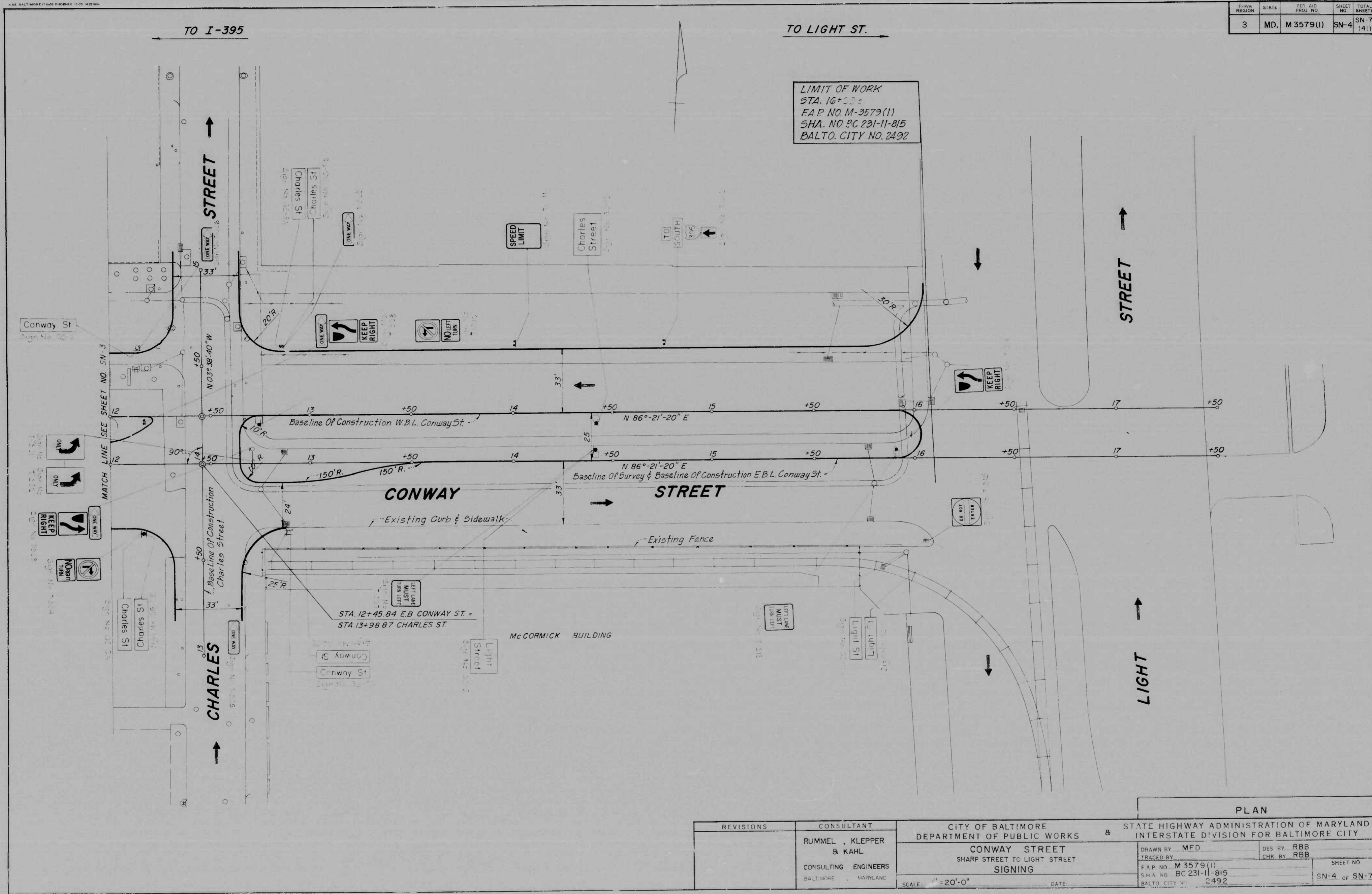
| FED. AID REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-----------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (I) | SN-3 | SN-7 (41) |



PLAN

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | | | | | | | | | | |
|--------------------------|---|--|--|---------------|-------------|------------|-------------|----------------------|-----------|--------------------------|--------------|----------------------|--|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET SIGNING | <table border="1"> <tr> <td>DRAWN BY: MFD</td> <td>DES BY: RBB</td> </tr> <tr> <td>TRACED BY:</td> <td>CHK BY: RBB</td> </tr> <tr> <td>F.A.P. NO. M3579 (I)</td> <td>SHEET NO.</td> </tr> <tr> <td>S.H.A. NO. BC 231-11-815</td> <td>SN-3 of SN-7</td> </tr> <tr> <td>BALTO. CITY NO. 2492</td> <td></td> </tr> </table> | DRAWN BY: MFD | DES BY: RBB | TRACED BY: | CHK BY: RBB | F.A.P. NO. M3579 (I) | SHEET NO. | S.H.A. NO. BC 231-11-815 | SN-3 of SN-7 | BALTO. CITY NO. 2492 | |
| DRAWN BY: MFD | DES BY: RBB | | | | | | | | | | | | |
| TRACED BY: | CHK BY: RBB | | | | | | | | | | | | |
| F.A.P. NO. M3579 (I) | SHEET NO. | | | | | | | | | | | | |
| S.H.A. NO. BC 231-11-815 | SN-3 of SN-7 | | | | | | | | | | | | |
| BALTO. CITY NO. 2492 | | | | | | | | | | | | | |
| SCALE: 1" = 20'-0" | | DATE: | | | | | | | | | | | |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579(1) | SN-4 | SN-7 (4) |

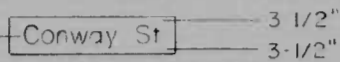
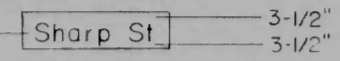
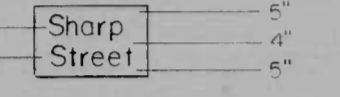
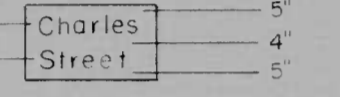
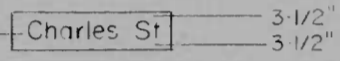
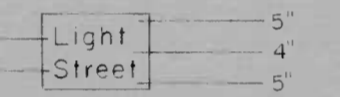
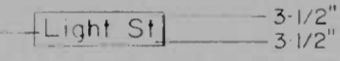


PLAN

| | | | |
|----------------------|--|--|----------------------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET SIGNING | |
| DRAWN BY MFD | | DES. BY RBB | SHEET NO. |
| TRACED BY | | CHK BY RBB | SN-4 OF SN-7 |
| F.A.P. NO. M 3579(1) | | SHA. NO. BC 231-11-815 | BALTO. CITY NO. 2492 |
| SCALE: 1" = 20'-0" | | DATE: | |

MD M3579(1) SN-5 (41) SN-7 (41)

GUIDE SIGN MESSAGES, DETAILS AND DIMENSIONS

| PANEL DESIGNATION | SHEET NO. | QUANTITY | LEGEND | SIZE | | COLOR | | BORDER | | | SHIELD | REMARKS |
|-------------------|-----------|----------|--|-------|--------|--------|-----------|--------|--------|-------|--------|--|
| | | | | WIDTH | HEIGHT | LEGEND | BACK GR'D | WIDTH | RADIUS | ARROW | | |
| GS-1A & GS-1B | SN-2 | 1 | 5' x 3-3/4"  3-1/2" | 3'-9" | 1'-0" | W | G | 11/8" | 3" | | | GS-1A & GS-1B TO BE MOUNTED BACK TO BACK |
| GS-2A & GS-2B | SN-2 | 1 | 5' x 3-3/4"  3-1/2" | 3'-3" | 1'-0" | W | G | 11/8" | 3" | | | GS-2A & GS-2B TO BE MOUNTED BACK TO BACK |
| GS-3 | SN-2 | 1 | 8' x 6"  5" | 4'-6" | 3'-0" | W | G | 2" | 5" | | | |
| GS-4 | SN-3 | 1 | 8' x 6"  5" | 5'-3" | 3'-0" | W | G | 2" | 5" | | | |
| GS-5A & GS-5B | SN-4 | 1 | 5' x 3-3/4"  3-1/2" | 3'-9" | 1'-0" | W | G | 11/8" | 3" | | | GS-5A & GS-5B TO BE MOUNTED BACK TO BACK |
| GS-6 | SN-4 | 1 | SAME AS GS-1A & B | | | W | G | | | | | |
| GS-7A & GS-7B | SN-4 | 1 | SAME AS GS-1A & B | | | W | G | | | | | GS-7A & GS-7B TO BE MOUNTED BACK TO BACK |
| GS-8A & GS-8B | SN-4 | 1 | SAME AS GS-5A & B | | | W | G | | | | | GS-8A & GS-8B TO BE MOUNTED BACK TO BACK |
| GS-9 | SN-4 | 1 | 8' x 6"  5" | 4'-6" | 3'-0" | W | G | 2" | 5" | | | |
| GS-10 | SN-4 | 1 | SAME AS GS-4 | | | W | G | | | | | |
| GS-11A & GS-11B | SN-4 | 1 | 5' x 3-3/4"  3-1/2" | 3'-3" | 1'-0" | W | G | 11/8" | 3" | | | GS-11A & GS-11B TO BE MOUNTED BACK TO BACK |
| GS-12 | SN-3 | 1 | SAME AS GS-1A & B | | | W | G | | | | | |

NOTE
L. COLOR LEGEND W-WHITE, G-GREEN, Y-YELLOW, B-BLACK

GUIDE SIGN DETAILS

| | | | |
|-----------|------------------------|--|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET SIGNING | DESIGNED BY: MFD CHECKED BY: RBB |
| | CONSULTING ENGINEERS | SCALE: 1" = 20'-0" | DATE: _____ |
| | BALTIMORE, MARYLAND | PROJECT NO: M 3579(1) BC 231-11-B15 | SHEET NO: SN-5 OF SN-7 |
| | | DATE: _____ | DATE: _____ |

MS&E BALTIMORE 11183 HERGULENE MC1994

| | | | | |
|-------------|-------|--------------------|-----------|--------------|
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD | M 3579(1) | SN-6 | SN-7 (41) |

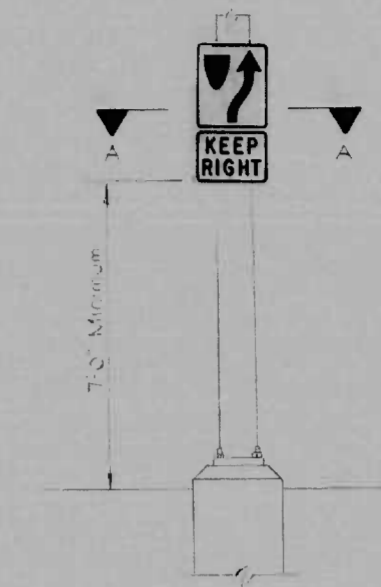
STANDARD SIGNS

| SIGN NO. | STANDARD SIGN ASSEMBLY | SUPPORT TYPE | LOCATION & QUANTITIES PER SHEET | | | | | | | | | | | | TOTAL NO. | SIGN SIZE | REMARKS | |
|----------|---------------------------------|--------------|---------------------------------|---|---|---|---|---|---|---|---|----|----|----|-----------|--|--|-----------------------------------|
| | | | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | | | | |
| T-101 | R4-7 R3-2 | S-1 | | 1 | | | | | | | | | | | | 24" x 30" 24" x 18" 24" x 24" 24" x 18" | MOUNTED ON JOINT USE POLE #101 | |
| T-201 | R3-7L(M) | S-5 | | | 1 | | | | | | | | | | | 30" x 30" | MODIFY TO READ "LANES" M.O.L.P. #202 | |
| T-202 | R1-1 | S-3 | | | 1 | | | | | | | | | | | 24" x 24" | MOUNTED AT STA. 11+6 WBR CONWAY ST. | |
| T-203 | R6-1(R) | S-3 | | | 1 | | | | | | | | | | | 36" x 12" | MOUNTED AT STA. 11+50 WBR CONWAY ST. | |
| T-204 | R3-1 | S-3 | | | 1 | | | | | | | | | | | 24" x 24" 24" x 18" | MOUNTED AT STA. 11+90 WBR CONWAY ST. | |
| T-301 | R3-5 | S-2 | | | | 1 | | | | | | | | | | 30" x 36" | T-301 & T-302 ARE MOUNTED ON JOINT USE POLE #314 ON WAST ARM | |
| T-302 | R3-5 | | | | | 1 | | | | | | | | | | 30" x 36" | | |
| T-303 | R6-1(L) R4-7" | S-1 | | | | 1 | | | | | | | | | | 36" x 12" 24" x 30" 24" x 18" | MOUNTED ON JOINT USE POLE #314 | |
| T-304 | R3-2 | S-1 | | | | 1 | | | | | | | | | | 24" x 24" 24" x 18" | MOUNTED ON PEDESTRIAN SIGNAL POLE #315 | |
| T-305 | R6-1(L) | S-1 | | | | 1 | | | | | | | | | | 36" x 12" | MOUNTED ON JOINT USE POLE #316 | |
| T-306 | R6-1(R) | S-1 | | | | 1 | | | | | | | | | | 36" x 12" | MOUNTED ON JOINT USE POLE #312 | |
| T-307 | R3-7L | S-5 | | | | 1 | | | | | | | | | | 30" x 30" | M.O.L.P. #304 | |
| T-308 | R6-1(R) R4-7" | S-3 | | | | 1 | | | | | | | | | | 36" x 12" 24" x 30" 24" x 18" | MOUNTED AT STA. 12+17 WBR CONWAY ST. | |
| T-309 | R6-1(R) | S-1 | | | | 1 | | | | | | | | | | 36" x 12" | MOUNTED ON PEDESTRIAN SIGNAL POLE #313 | |
| T-310 | R3-2 | S-1 | | | | 1 | | | | | | | | | | 24" x 24" 24" x 18" | MOUNTED ON LIGHT POLE #302 | |
| T-311 | R2-1 | S-3 | | | | 1 | | | | | | | | | | 18" x 24" | M. A. S. 14+00 WBR CONWAY ST. | |
| T-312 | M4-5a M3-3a M1-1 M6-3a | S-3 | | | | 1 | | | | | | | | | | 24" x 12" 24" x 12" 30" x 24" 21" x 15" | MOUNTED AT STA. 14+75 WBR CONWAY ST. | |
| T-313 | R3-7L | | S-1 | | | | 1 | | | | | | | | | | 30" x 30" | MOUNTED ON LIGHT POLE #311 |
| T-314 | R4-7" | | S-1 | | | | 1 | | | | | | | | | | 24" x 30" 24" x 18" | MOUNTED ON JOINT USE POLE #318 |
| T-315 | R5-1 | | S-5 | | | | 1 | | | | | | | | | | 30" x 30" | MOUNTED ON POLE #319 |

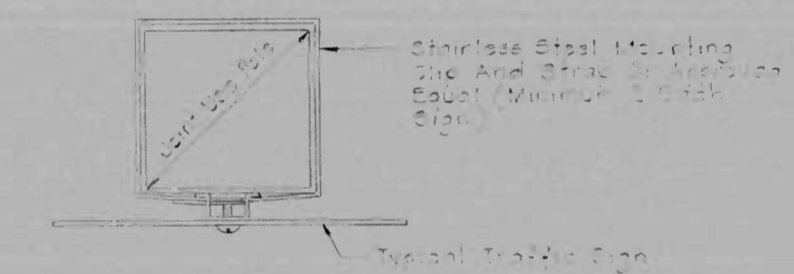
M. A. S. IS ABBREVIATION FOR "MOUNTED AT STATION"
M. O. L. P. IS ABBREVIATION FOR "MOUNTED ON LIGHT POLE"
R4-7 SHALL INCLUDE "KEEP RIGHT" AUXILIARY PANEL AS
SHOWN IN FHWA STANDARD HIGHWAY SIGNS MANUAL DATED 1977

GUIDE SIGNS

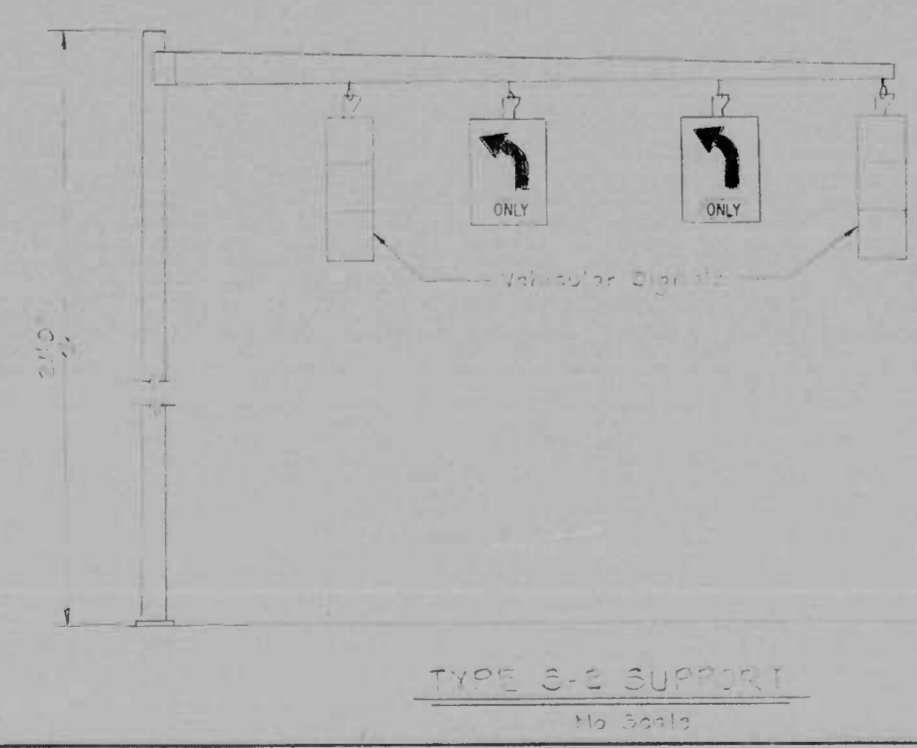
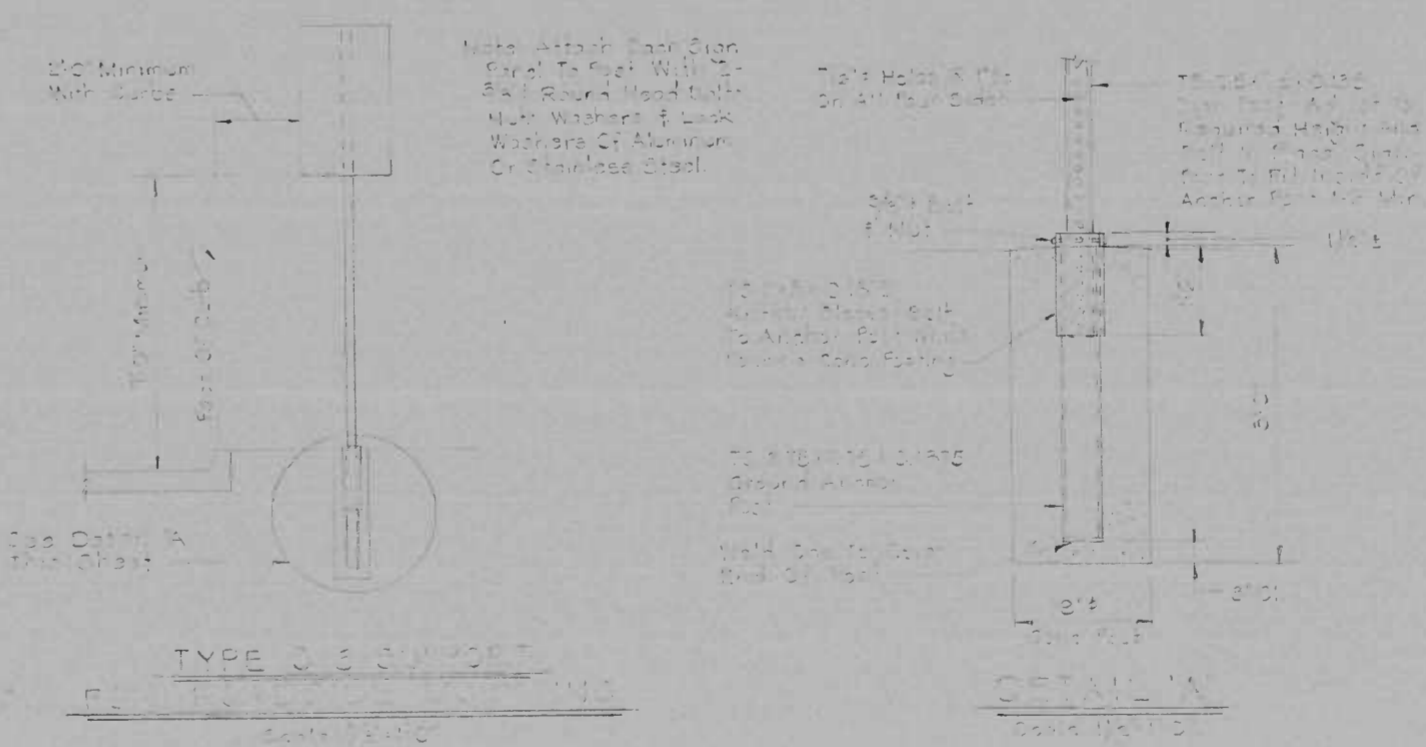
| SIGN NO. | SHEET NO. | PANEL A | PANEL B | PANEL C | PANEL D | TOTAL AREA SQ. FT. | STRUCT. TYPE | GUARD FENCE LIN. FT. | | REMARKS |
|-----------------|-----------|---------|---------|---------|---------|--------------------|--------------|----------------------|---------|--|
| | | | | | | | | TYPE I | TYPE II | |
| GS-1A & GS-1B | SN-2 | 3.75 | 3.75 | | | 7.50 | S-7 | | | TO BE MOUNTED BACK TO BACK ON JOINT USE POLE #10B |
| GS-2A & GS-2B | SN-2 | 3.25 | 3.25 | | | 6.50 | S-6 | | | TO BE MOUNTED BACK TO BACK ON PEDESTRIAN SIGNAL POLE #107 |
| GS-3 | SN-2 | | | | | 13.50 | S-4 | | | MOUNTED ON LIGHT POLE #104 |
| GS-4 | SN-3 | | | | | 15.75 | S-5 | | | MOUNTED ON LIGHT POLE #202 |
| GS-5A & GS-5B | SN-4 | 3.75 | 3.75 | | | 7.50 | S-6 | | | TO BE MOUNTED BACK TO BACK ON PEDESTRIAN SIGNAL POLE #315 |
| GS-6 | SN-4 | 3.75 | | | | 3.75 | S-7 | | | MOUNTED ON JOINT USE POLE #312 |
| GS-7A & GS-7B | SN-4 | 3.75 | 3.75 | | | 7.50 | S-7 | | | TO BE MOUNTED BACK TO BACK ON JOINT USE POLE #316 |
| GS-8A & GS-8B | SN-4 | 3.75 | 3.75 | | | 7.50 | S-6 | | | TO BE MOUNTED BACK TO BACK ON PEDESTRIAN SIGNAL POLE #313 |
| GS-9 | SN-4 | | | | | 13.50 | S-5 | | | MOUNTED ON LIGHT POLE #304 |
| GS-10 | SN-4 | | | | | 15.75 | S-4 | | | MOUNTED ON LIGHT POLE #303 |
| GS-11A & GS-11B | SN-4 | 3.75 | 3.75 | | | 7.50 | S-6 | | | TO BE MOUNTED BACK TO BACK ON PEDESTRIAN SIGNAL POLE #319 |
| GS-12 | SN-3 | 3.75 | | | | 3.75 | | | | MOUNTED ON LIGHT POLE #206 |



TYPE 3-1 SIGN
FOR USE ON JOINT MOUNTING
Scale: 1/4" = 1'-0"



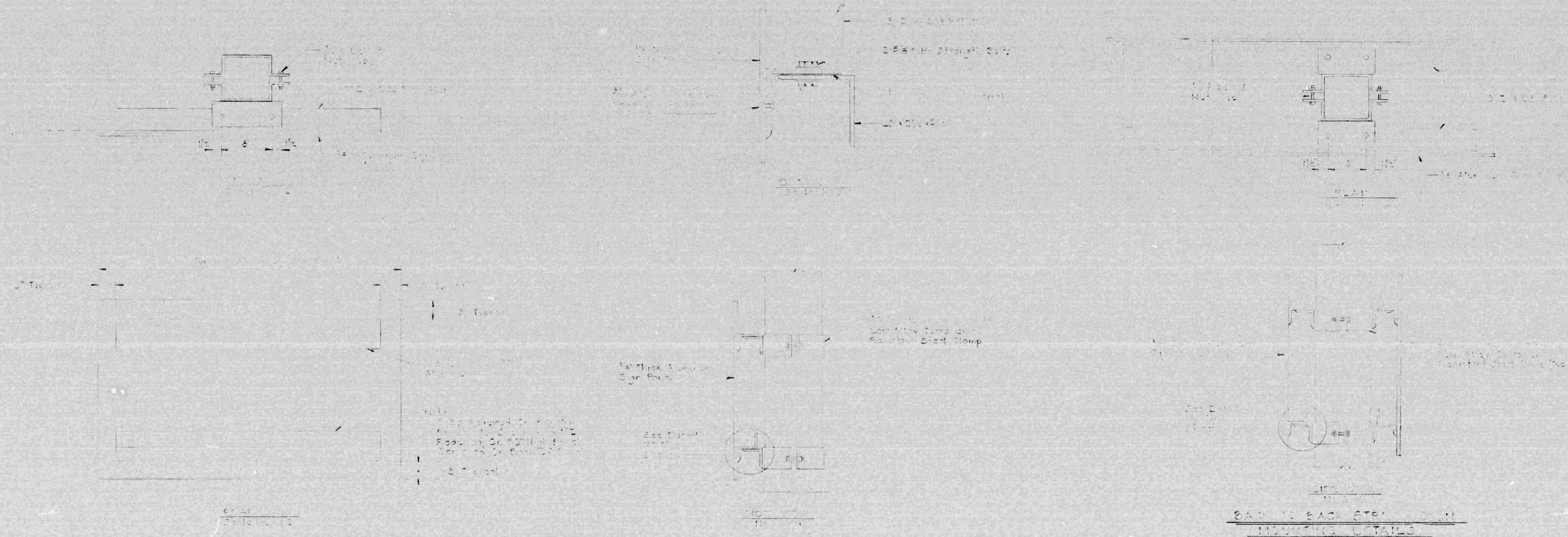
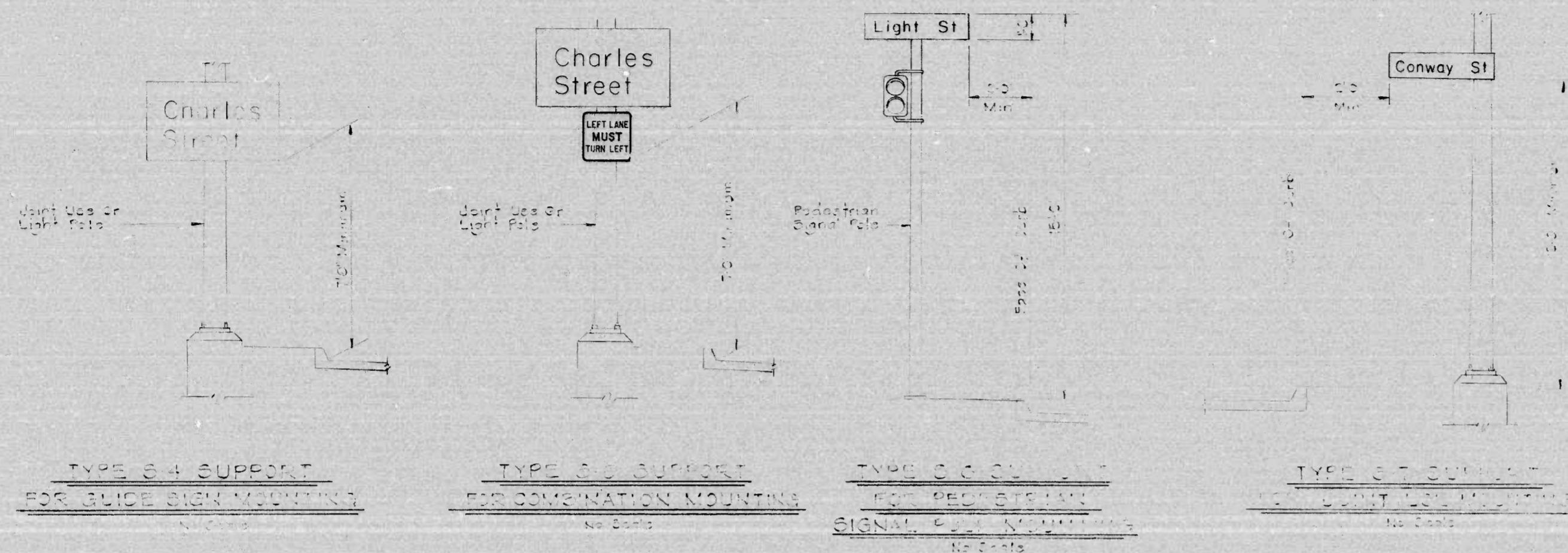
SECTION 4-4
No. 304



SIGN TABULATIONS AND DETAILS

| | | | |
|-----------|------------------------|--|--------------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET SIGNING | |
| | CONSULTING ENGINEERS | DRAWN BY: MFD | DES. BY: RBB |
| | BALTIMORE, MARYLAND | TRACED BY: | CHK. BY: RBB |
| | | F.A.P. NO. M3579(1) | SHEET NO. |
| | | SHA NO. BC 231-11-8)5 | SN-6 of SN-7 |
| | | BALTO. CITY NO. 2492 | |
| | | SCALE: AS SHOWN | DATE: |

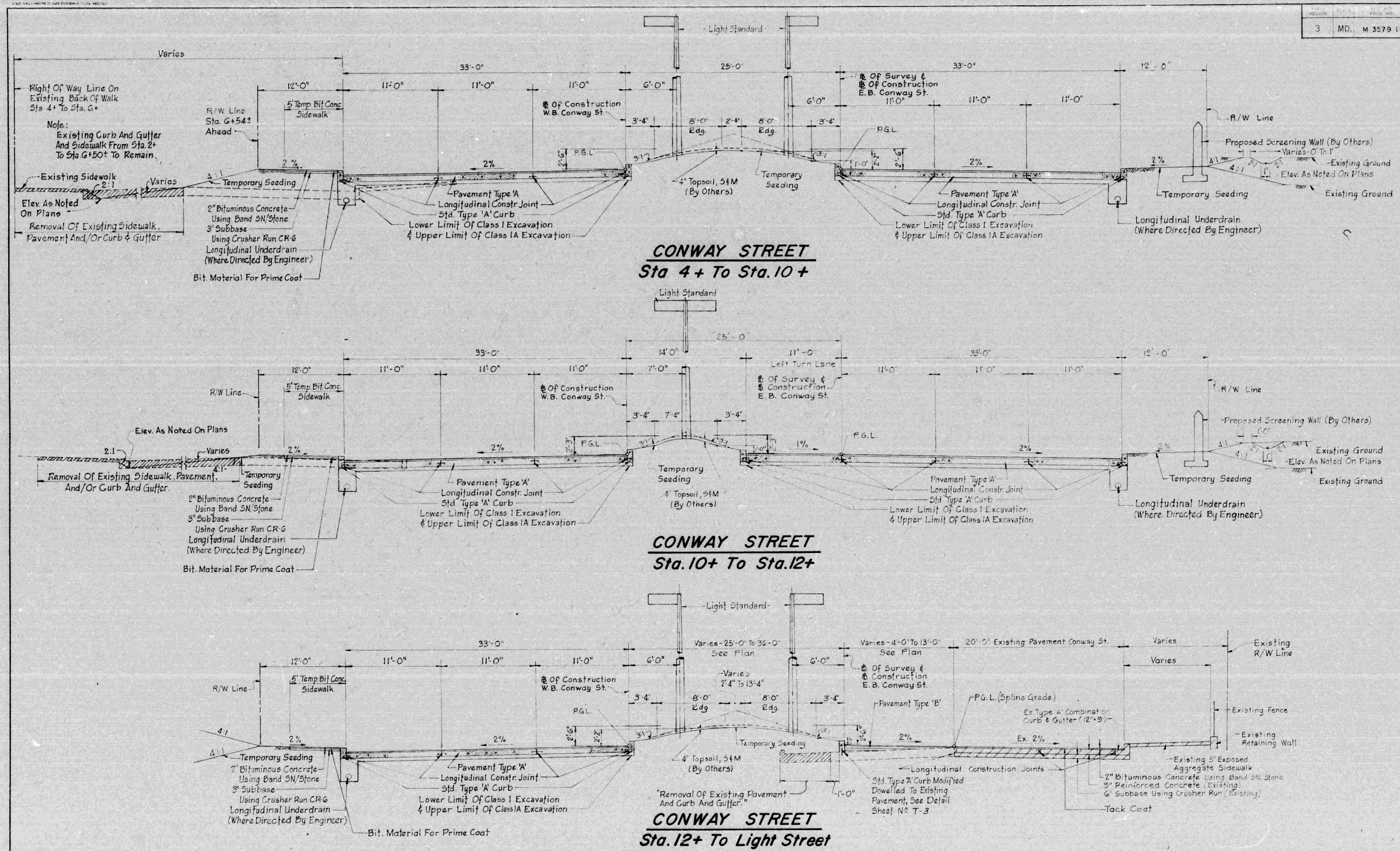
| FED. REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (I) | SN-7 | (4) |



| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|-----------|---|--|---|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET SIGNING | |
| | | SCALE: AS SHOWN | DATE: |
| | | DRAWN BY: MFD TRACED BY: F.A.P. NO. M3579 (I) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | DES. BY: RBB CHK. BY: RBB SHEET NO. SN-7 of SN-7 |

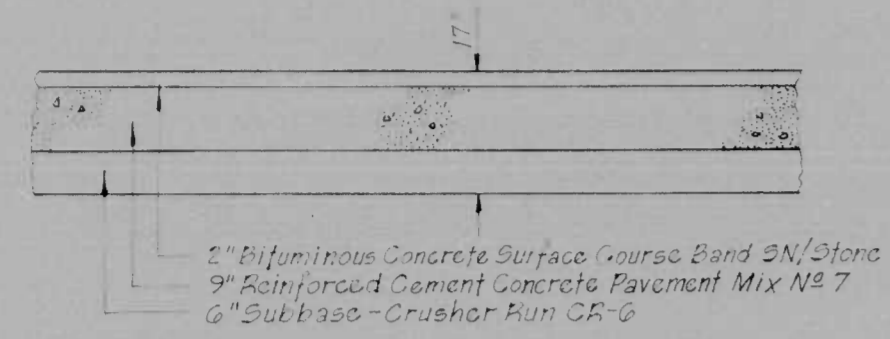
GUIDE SIGN MOUNTING DETAILS

| | | | | |
|--------|-------|------------|-----------|--------------|
| REGION | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M 3579 (1) | T-2 | (41) |

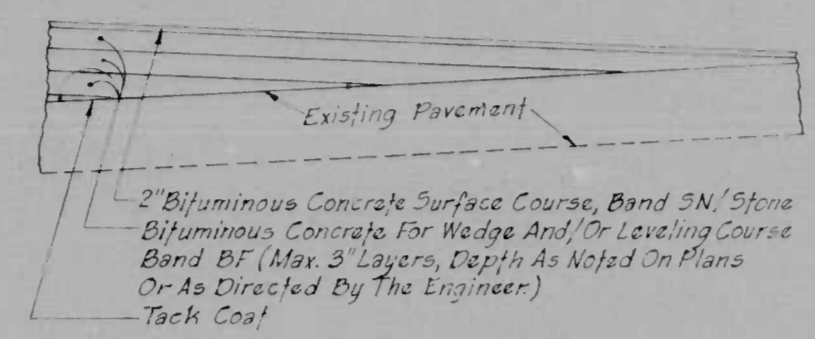


| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET TYPICAL SECTIONS | DRAWN BY: CHL TRACED BY: CHL F.A.P. NO. M. 3579 (1) S.H.A. NO. BC 231-II-815 BALTO. CITY NO. 2492 |
| | | DES. BY: HMH CHK. BY: ERS | SHEET NO. T-2 OF T |
| | | SCALE: 1" = 5' | DATE: |

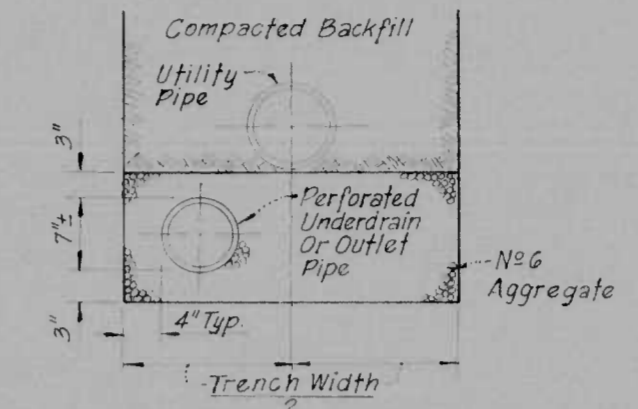
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|---|-----|------------|-----|------|
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|---|-----|------------|-----|------|



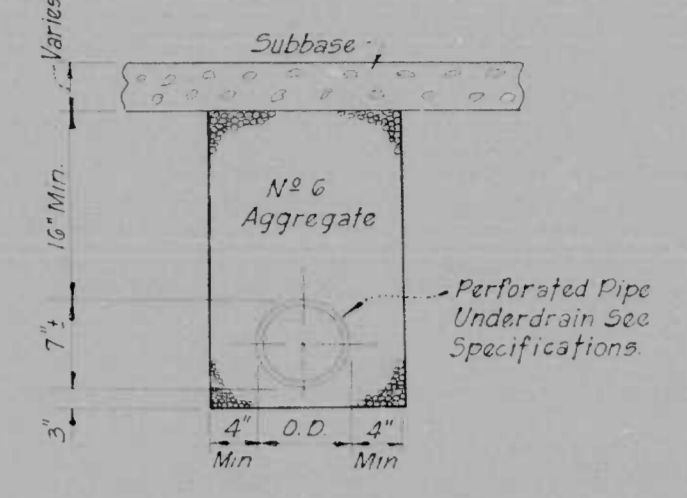
PAVEMENT TYPE 'A'
Not To Scale



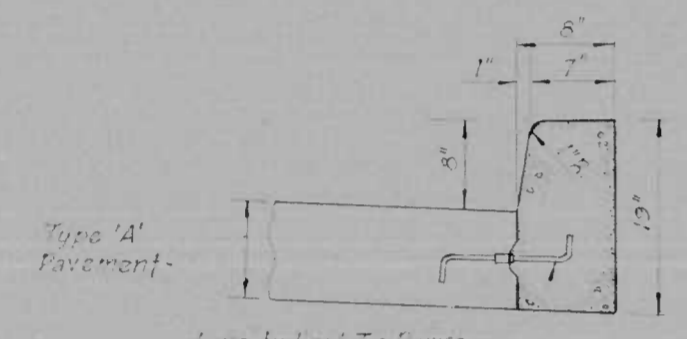
PAVEMENT TYPE 'B'
Not To Scale



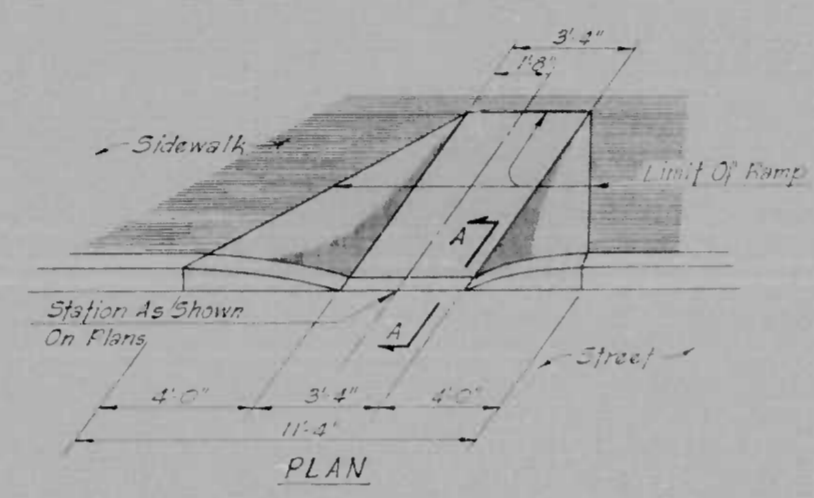
UTILITY TRENCH UNDERDRAIN



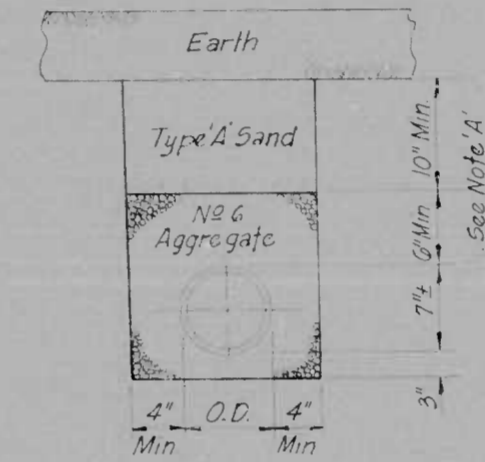
LONGITUDINAL UNDERDRAIN C



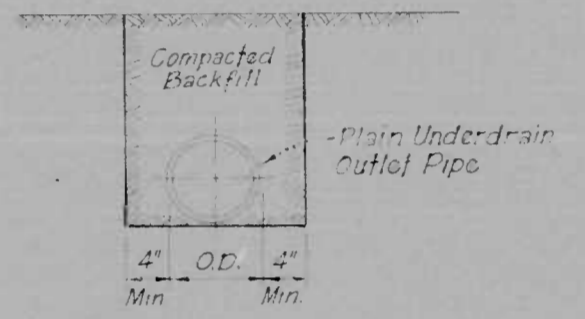
STD. TYPE 'A' CURB
Scale: 1" = 1'



PEDESTRIAN RAMP
Not To Scale

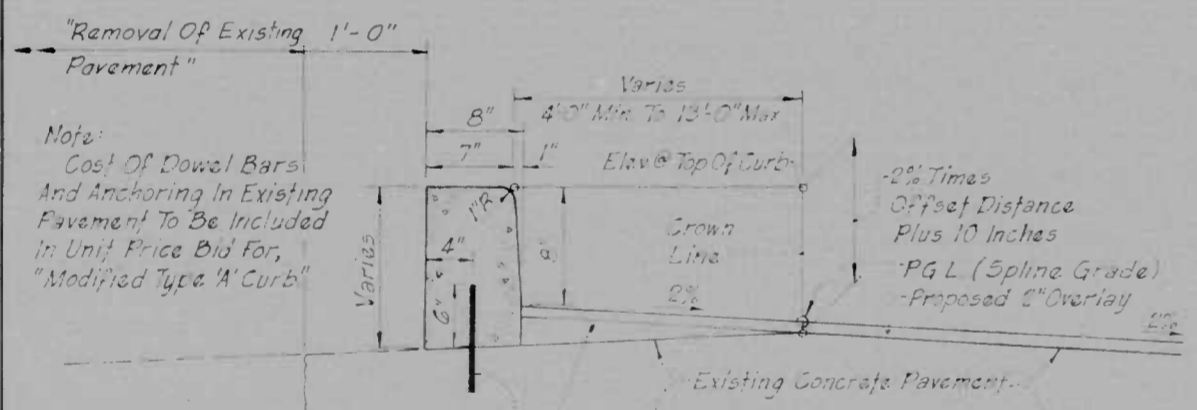


SUB-SURFACE UNDERDRAIN

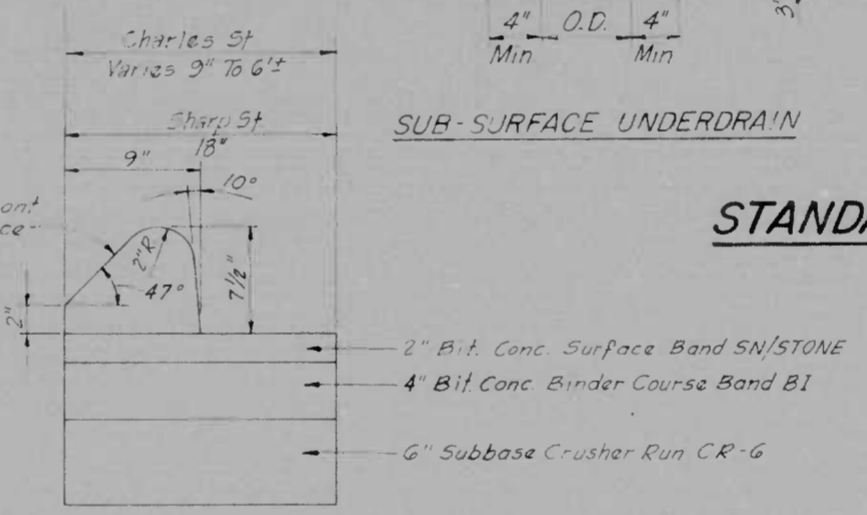


UNDERDRAIN OUTLET

*Note 'A'
Where Underdrains Outlet To An Inlet Or Where Unsuitable Trench Bottoms Are Encountered, Or Any Other Unusual Conditions Preval, These Dimensions May Be Varied By The Engineer. See Specifications For Pipe Joining.

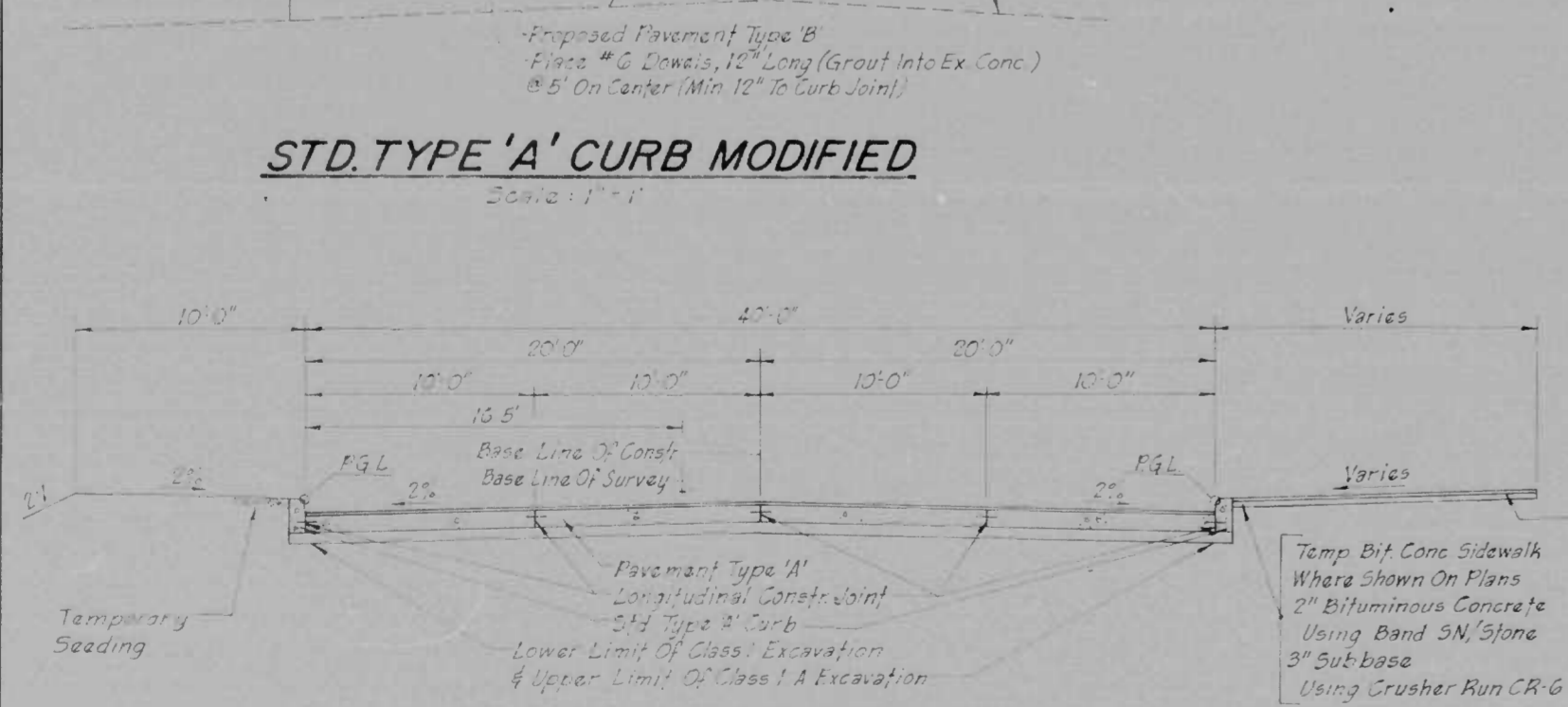


STD. TYPE 'A' CURB MODIFIED
Scale: 1" = 1'

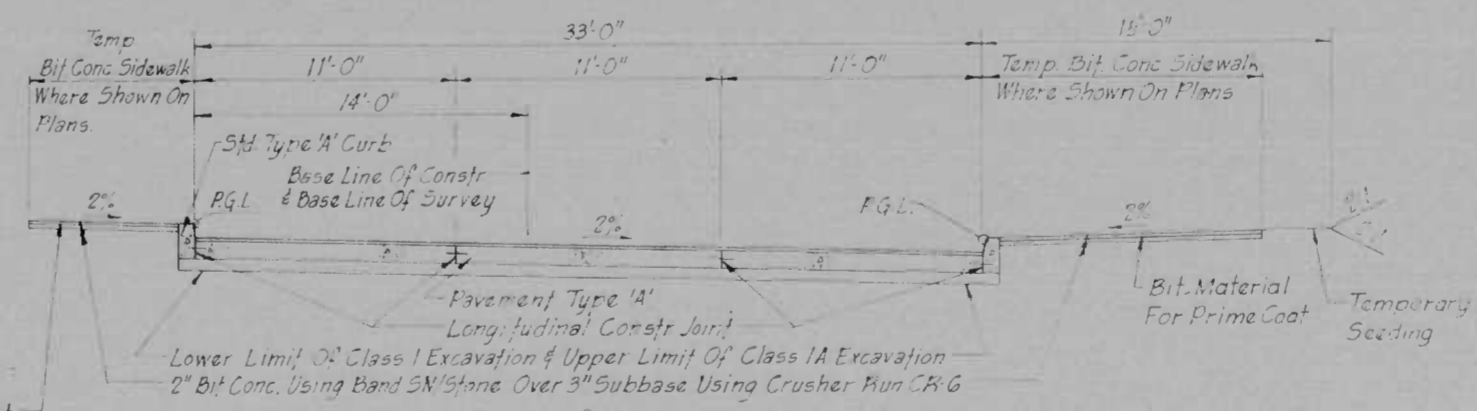


BITUMINOUS CONCRETE CURB AND BASE
Not To Scale

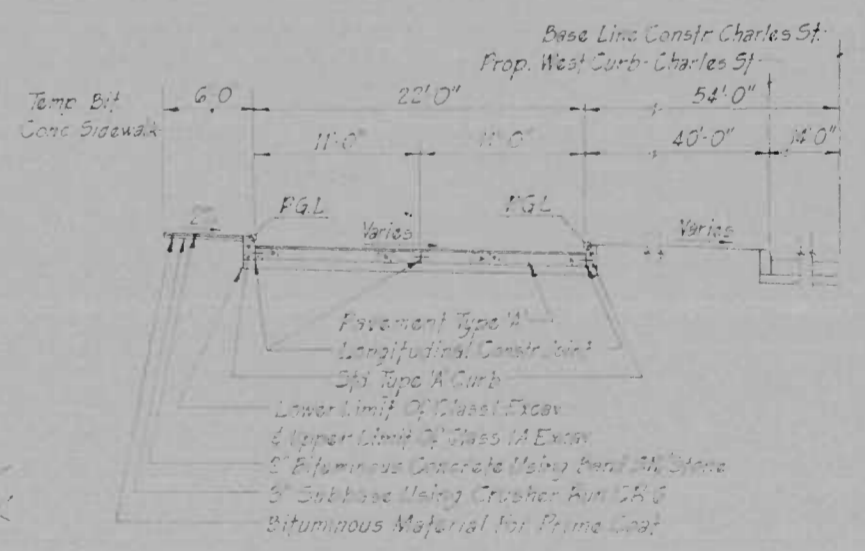
STANDARD UNDERDRAINS
Not To Scale



TYPICAL SECTION SHARP ST.
Scale: 1" = 5'



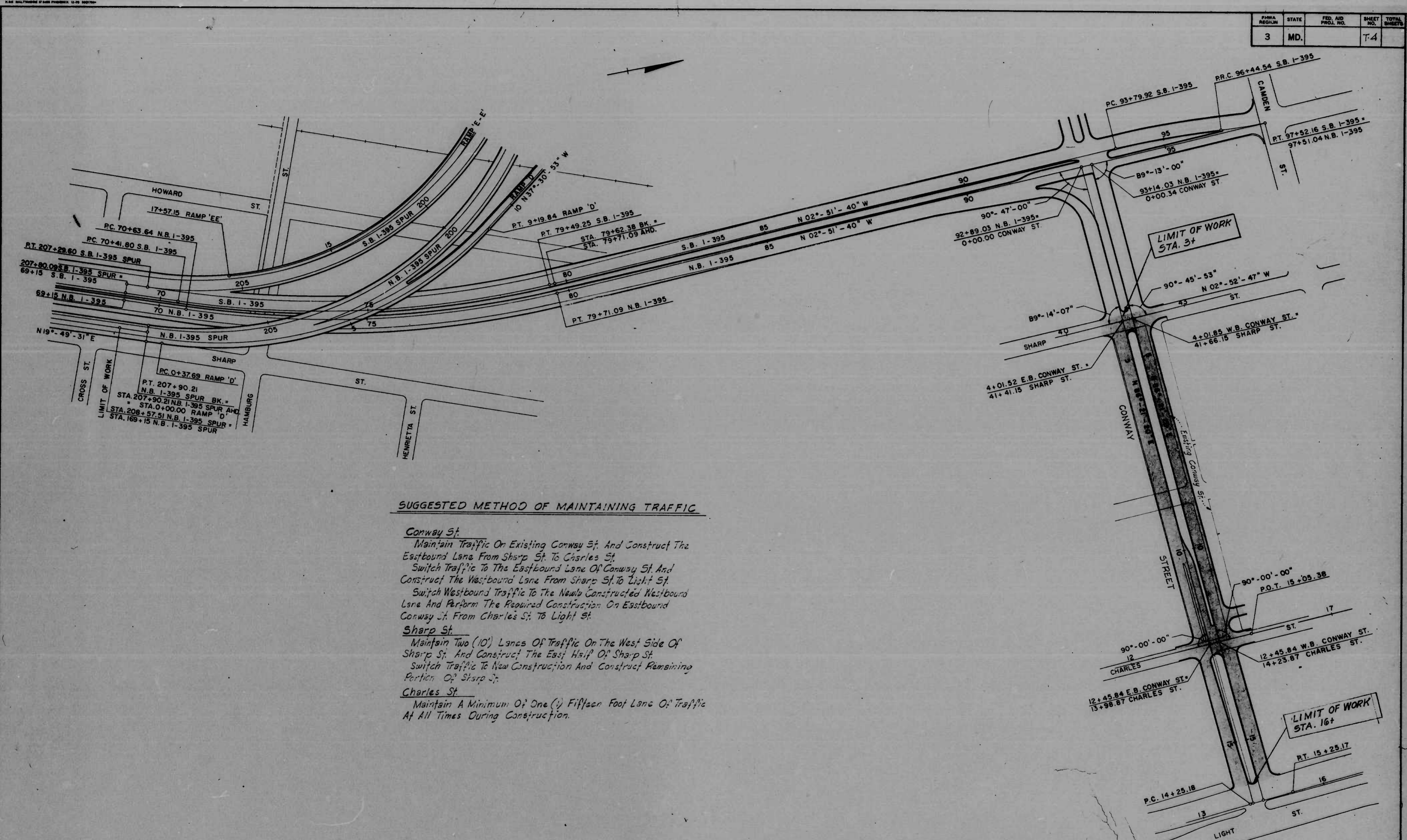
TYPICAL SECTION CHARLES ST.
Scale: 1" = 5'



TYPICAL SECTION SERVICE DRIVE
Scale: 1" = 5'

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|--|---|
| | RUMMEL, KLEPPER & KAYL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET MISCELLANEOUS DETAILS | DRAWN BY: R.L.J. CHECKED BY: R.L.J. F.A.P. NO. M 3579 (1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | | T-4 | |



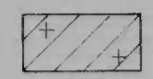
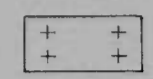
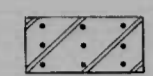
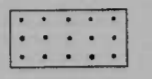
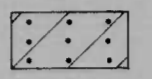
SUGGESTED METHOD OF MAINTAINING TRAFFIC

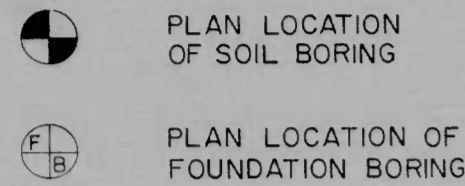
- Conway St.
 Maintain Traffic On Existing Conway St. And Construct The Eastbound Lane From Sharp St. To Charles St.
 Switch Traffic To The Eastbound Lane Of Conway St. And Construct The Westbound Lane From Sharp St. To Light St.
 Switch Westbound Traffic To The Newly Constructed Westbound Lane And Perform The Required Construction On Eastbound Conway St. From Charles St. To Light St.
- Sharp St.
 Maintain Two (2) Lanes Of Traffic On The West Side Of Sharp St. And Construct The East Half Of Sharp St.
 Switch Traffic To New Construction And Construct Remaining Portion Of Sharp St.
- Charles St.
 Maintain A Minimum Of One (1) Fifteen Foot Lane Of Traffic At All Times During Construction.

| | | | | | |
|-----------|---|---|-------|--|-----------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET MAINTENANCE OF TRAFFIC | | DRAWN BY | DES. BY |
| | | SCALE: 1"=100' | DATE: | F.A.P. NO. | CHK. BY |
| | | | | S.H.A. NO. | SHEET NO. |
| | | | | BALTO. CITY NO. | T-4 OF |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | T-5 | (41) |

SOILS LEGEND

-  A-4-7, CLAYEY SILT
-  A-7, CLAY
-  A-4-2, SANDY SILT
-  A-3, SAND, NON-PLASTIC
-  A-2-4, SILTY SAND



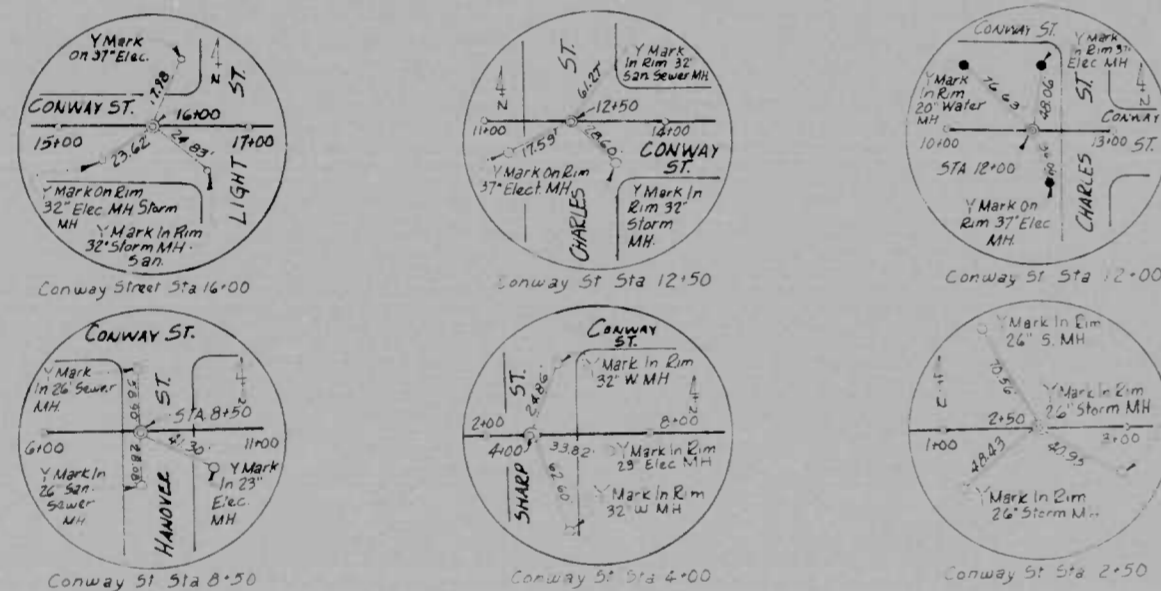
PROFILE VERTICAL SCALE: 1"=10'

WL WATER LEVEL (x) HOURS AFTER COMPLETION OF THE BORING

ABBREVIATIONS

- L.L. = LIQUID LIMIT
- P.I. = PLASTICITY INDEX
- N.P. = NON-PLASTIC
- P.C.F. = POUNDS PER CUBIC FOOT
- M.D.D. = MAXIMUM DRY DENSITY
- O.M.C. = OPTIMUM MOISTURE CONTENT

REFERENCES



GENERAL ABBREVIATIONS

- S.H.A. STATE HIGHWAY ADMINISTRATION
- I.D.B.C. INTERSTATE DIVISION FOR BALTIMORE CITY
- M.T.A. METROPOLITAN TRANSIT AUTHORITY
- D.P.W. DEPARTMENT OF PUBLIC WORKS
- N.B.R. NORTHBOLD ROADWAY
- S.D.R. SOUTHOLD ROADWAY
- P.C. POINT OF CURVATURE
- P.T. POINT OF TANGENCY
- P.O.C. POINT ON CURVE
- P.O.T. POINT ON TANGENT
- P.R.C. POINT OF REVERSING CURVATURE
- P.C.C. POINT OF COMPOUND CURVATURE
- STA. STATION
- Δ DEFLECTION ANGLE
- Q DEGREE OF CURVE
- R RADIUS
- T TANGENT LENGTH
- L LENGTH OF CURVE
- E EXTERNAL
- L.C. LENGTH OF CHORD
- P.V.C. POINT OF VERTICAL CURVATURE
- P.V.I. POINT OF VERTICAL INTERSECTION
- P.V.T. POINT OF VERTICAL TANGENCY
- V.C. VERTICAL CURVE LENGTH
- COFF. CORRECTION
- S.S.D. STOPPING SIGHT DISTANCE
- H.S.D. HEADLIGHT SIGHT DISTANCE
- S.F. SUPERELEVATION
- CL CENTER LINE
- DL DASE LINE
- R.W. RIGHT OF WAY
- P.G.L. PROFILE GRADE LINE
- P.G.L. PROFILE GROUND LINE
- P.C. POINT OF CROWN
- P.R. POINT OF ROTATION
- P.G.E. PROFILE GRADE ELEVATION
- Elev. Ft. ELEVATION
- B.M. BENCH MARK
- St. BC. BALTIMORE CITY STANDARD
- R.C.P. REINFORCED CONCRETE PIPE
- S.P.P.A. STRUCTURAL PLATE PIPE ARCH
- S & M. SEED AND MULCH
- S.F. SQUARE FEET
- L.F. LINEAR FEET

EXPLANATORY NOTES AND REFERENCES

SPECIFICATIONS
THE SPECIFICATIONS FOR THIS CONTRACT WILL BE THOSE OF THE MARYLAND STATE HIGHWAY ADMINISTRATION ENTITLED, "SPECIFICATIONS FOR MATERIALS, HIGHWAYS, BRIDGES AND INCIDENTAL STRUCTURES", DATED MARCH 1968, AND INTERSTATE DIVISION FOR BALTIMORE CITY SUPPLEMENT TO SPECIFICATIONS DATED MAR. 1978 AND ANY AND ALL REVISIONS OR ADDITIONS THERETO AND THE SPECIAL PROVISIONS.

HORIZONTAL AND VERTICAL CONTROL
THE HORIZONTAL AND VERTICAL CONTROL FOR THIS PROJECT IS REFERENCED TO THE CITY OF BALTIMORE SURVEY CONTROL SYSTEM.

MAINTENANCE OF TRAFFIC AND UTILITY SERVICE
SPECIAL ATTENTION IS DIRECTED TO THE REQUIREMENTS FOR MAINTENANCE OF TRAFFIC AND UTILITY SERVICE DURING THE CONSTRUCTION OF THIS PROJECT. SEE SPECIAL PROVISIONS AND PLANS FOR CONSTRUCTION PHASES REQUIRED.

EXISTING UTILITIES
EXISTING UTILITY LOCATIONS SHOWN ON PLANS ARE APPROXIMATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXACT LOCATIONS BY TEST PITS PRIOR TO COMMENCING WORK. THE NECESSARY RELOCATION AND OR ADJUSTMENT OF EXISTING UTILITIES WILL BE PERFORMED BY OTHERS UNLESS OTHERWISE PROVIDED FOR IN THE PROPOSAL OR SPECIAL PROVISIONS.

SIGHT DISTANCE
STOPPING SIGHT DISTANCE FOR CREST VERTICAL CURVES IS BASED ON A HEIGHT OF OBJECT = 0.33' AND A HEIGHT OF EYE = 3.0'. HEADLIGHT SIGHT DISTANCE FOR SAG VERTICAL CURVES IS BASED ON A HEIGHT OF HEADLIGHTS = 2.0' AND AN UPWARD DIVERGENCE OF BEAM = 1'.

MATERIALS SALVAGED IN CONSTRUCTION
ALL SALVAGEABLE GRATES AND FRAMES AND COVERS FROM STORM WATER INLETS, FRAMES AND COVERS FROM STORM WATER MANHOLES, SANITARY SEWER MANHOLES AND WATER VALVE VAULTS SHALL BE SALVAGED AS BALTIMORE CITY PROPERTY AND SHALL BE STORED WITHIN A FENCED-IN AREA ON THE PROJECT SITE, AS DIRECTED BY THE ENGINEER AND PICKED UP BY THE CITY.

CLEARING AND GRUBBING
SPECIAL ATTENTION IS DIRECTED TO THE FACT THAT NO CLEARING, DEMOLITION AND GRUBBING OR CRAVING MAY COMMENCE FOR THIS PROJECT UNTIL THE CONTRACTOR HAS AN APPROVED SEDIMENT AND EROSION CONTROL PLAN, AND THE LIMITS ARE STAKED IN THE FIELD AND APPROVED BY THE ENGINEER. NO CLEARING WILL BE ALLOWED BEYOND THE PROJECT LIMITS.

BORINGS
THE INTERSTATE DIVISION FOR BALTIMORE CITY DOES NOT REPRESENT, WARRANT OR GUARANTEE THAT THE CONDITIONS AND/OR MATERIALS ACTUALLY ENCOUNTERED IN THE PROSECUTING OF THE WORK OR ANY PART THEREOF WILL BE THE SAME AS THAT SHOWN BY THE BORINGS, THE RECORDINGS, OR ANY PART OF THEM, AND IF THE CONTRACTOR RELIES FOR ANY PURPOSE, UPON THE SAID INFORMATION, HE DOES SO AT HIS OWN RISK.

EXISTING SIDEWALKS
EXISTING SIDEWALKS THAT MUST BE REMOVED TO MAKE ISOLATED UTILITY CONNECTIONS OR ADJUSTMENTS TO INSTALL CONDUITS TO CONSTRUCT LIGHT POLE BASES, OR TO INSTALL TREE GRATES SHALL BE REPAVED USING 5" CONCRETE SIDEWALK COST FOR REPAVING SHALL BE AT THE UNIT PRICE BID PER SQUARE FOOT FOR "FIVE (5) INCH CONCRETE SIDEWALK".

EROSION AND SEDIMENT CONTROL
THE EXACT LOCATION AND TYPE OF SEDIMENT CONTROL DEVICES WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER AND THE EROSION AND SEDIMENT CONTROL OFFICER, I.D.D.C. THEY RESERVE THE RIGHT TO ORDER ADDITIONAL EROSION AND SEDIMENT DEVICES. SEE STD. NO. MD 388.01 02, 11, 12 & 21.

INVERT ELEVATIONS
IF APPROVED BY THE ENGINEER, PROPOSED INVERT ELEVATIONS MAY BE MODIFIED TO MEET CONDITIONS ENCOUNTERED DURING INSTALLATION OF UNDERGROUND FACILITIES.

REMOVAL OF EXISTING PAVEMENT
EXISTING PAVEMENT, CURBS, SIDEWALKS, INCLUDING STREETCAR AND RAILROAD TRACK SYSTEMS LOCATED WITHIN THE CONSTRUCTION LIMITS AND UNDER PROPOSED EMBANKMENTS SHALL BE REMOVED AS CLASS 1 EXCAVATION.

MATERIAL NOT SUITABLE FOR FILLS SUCH AS TRACK SYSTEMS SHALL BE DISPOSED OF IN ACCORDANCE WITH SECTION 31.02-1 OF THE SPECIFICATIONS.

CONSTRUCTION LEGEND

- BASE OR SURVEY LINE
- GROUND ELEVATION
- GRADE ELEVATION
- PROPOSED CURVE
- PROPOSED MANHOLE
- PROPOSED INLET
- DITCH - SEED & MULCH
- DITCH - SEEDING
- RIGHT OF WAY ABBREVIATIONS
- FEET SIMILE
- LEAVE AROUND
- REVERSIONARY INTEREST
- MAYOR AND CITY COUNCIL OF BALTIMORE
- UTILITY LEGEND - EXISTING
- MANHOLES (ALL TYPES)
- WATER METER
- VALVES (GAS, WATER)
- ELECTRIC BOX
- POWER POLES (ALL TYPES)
- STREET LIGHT
- FIRE HYDRANT
- GLY POLE
- GRATE
- INLET WITH CURB OPENING
- POLICE OR FIRE CALL BOX
- GAS VENT
- TRAFFIC SIGNAL POLE
- GAS LINE
- WATER LINE
- STORM DRAIN OR SANITARY LINE (AS NOTED)
- TELEPHONE LINE
- POLICE FIRE ALARM TELEPHONE LINE
- M & E - MECHANICAL AND ELECTRICAL LINE
- STEAM LINE
- LARGE UTILITY (SIZE & TYPE AS NOTED)

TOPOGRAPHY LEGEND

- SIGN
- FENCE
- RETAINING WALL
- FENCE ON RETAINING WALL
- TREE
- ENCLOSED TREE
- TREELINE
- HEDGE
- STEPS
- SIDEWALK VAULT
- BILLCARD
- NO CURB (CURB LABELED)
- RAILROAD TRACKS
- STREETCAR TRACKS

TOPOGRAPHY ABBREVIATIONS

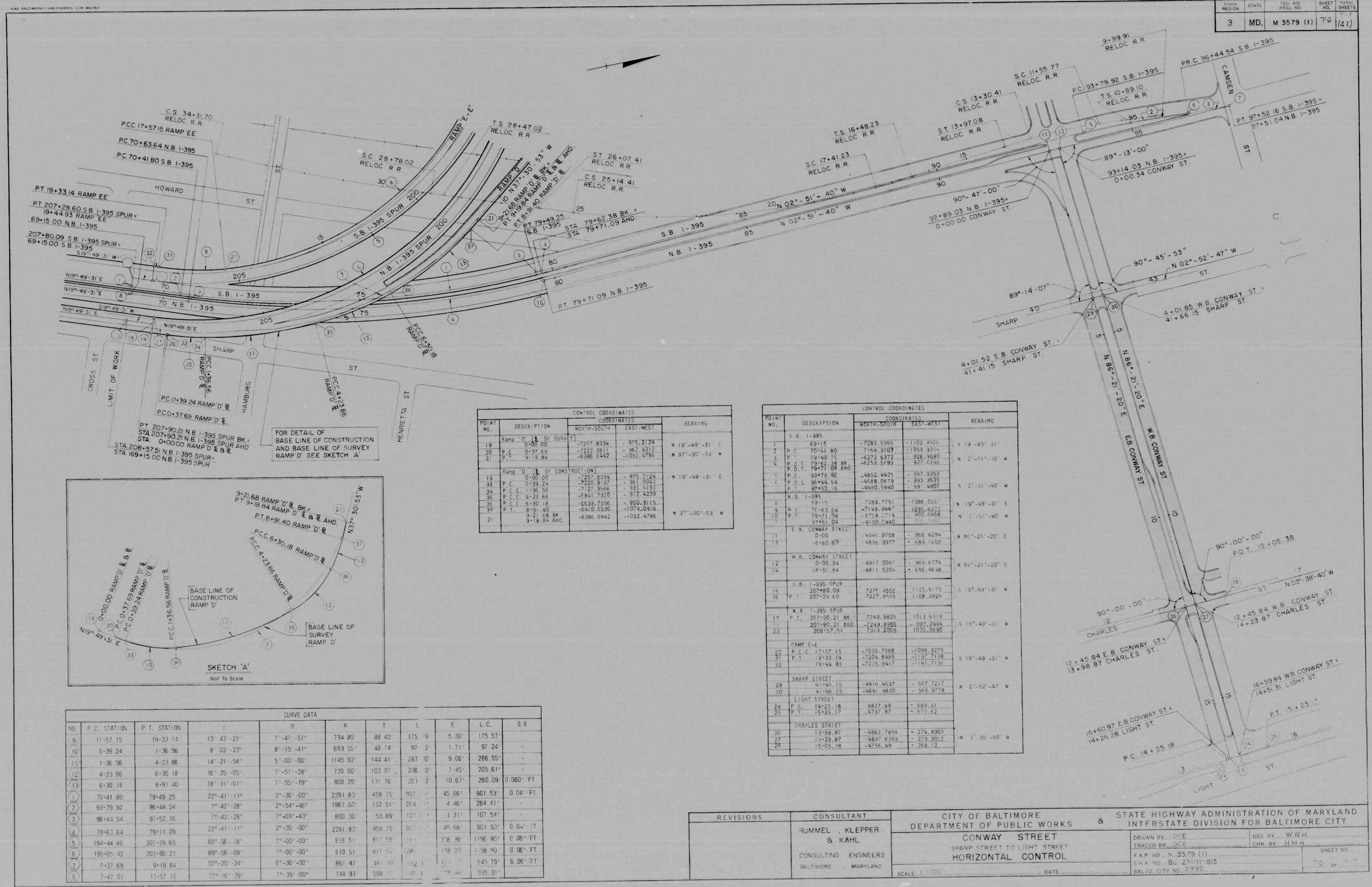
- CONC. CONCRETE
- R.W. RET. WALL
- STY. STORY
- DWG. DWELLING
- RET. ST. RETAIL STORE
- WHSE. WAREHOUSE
- CHUR. CHURCH
- GAR. GARAGE
- BR. BRICK
- BLK. BLOCK
- MTL. METAL
- FR. FRAME
- MAS. MASONRY
- STD. STONE
- ORAINAGE LEGEND - PROPOSED
- INLET NUMBERS
- MANHOLE NUMBERS
- BRICK # & NUMBERS

LEGEND - PROPOSED UTILITIES

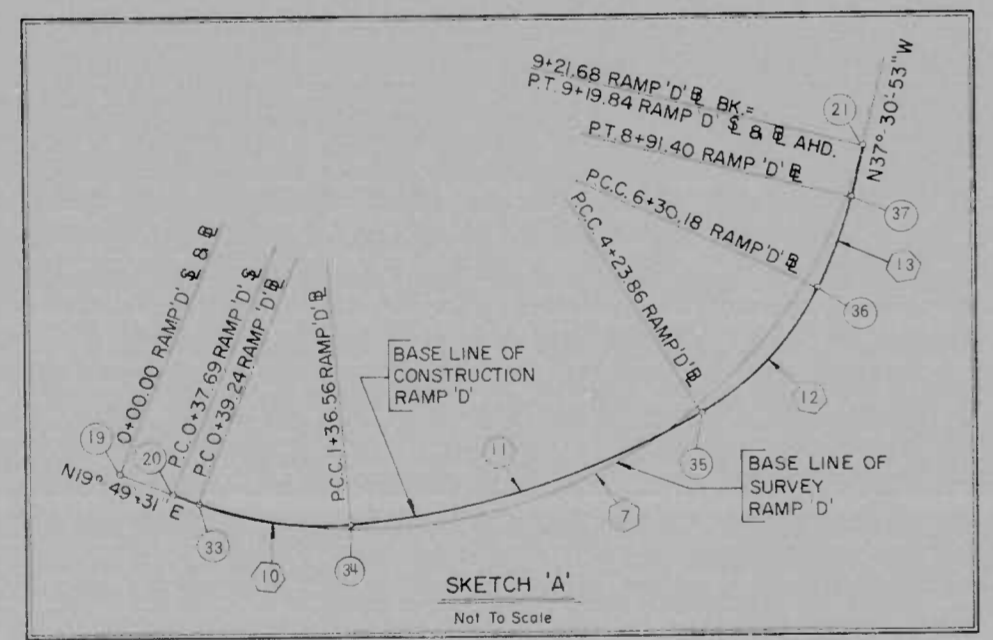
- PROPOSED WATER LINE
 - PROPOSED SANITARY LINE
 - PROPOSED POLICE FIRE ALARM TELEPHONE LINE
 - PROPOSED M. & E. - MECHANICAL AND ELECTRICAL LINE
 - PROPOSED FIRE HYDRANT
 - PROPOSED SANITARY MANHOLE
 - PROPOSED WATER VALVE
 - PROPOSED VAULTS
 - EXISTING CONDUIT
 - SPECIFIES NUMBER OF CONDUITS
- SEE SHEET NO. P-6 FOR ADDITIONAL TRANSIT AND TRAFFIC AND ELECTRICAL SYMBOLS

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|------------------------|---|---|
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET | DES. BY: H.M.H. CHK. BY: H.M.H. |
| | CONSULTING ENGINEERS | LEGEND AND GENERAL NOTES | DRAWN BY: J.F.L. TRACED BY: T.E.A. |
| | BALTIMORE, MARYLAND | SCALE: _____ DATE: _____ | F.A.P. NO. M-3579 (1) M & E NO. BC-231-II-B15 BALTO. CITY NO. 4375 |
| | | | SHEET NO. 7-5 OF 17 |

| | | | | |
|-------------|-------|--------------------|-----------|--------------|
| FED. REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD. | M 3579 (1) | T-6 | (41) |



FOR DETAIL OF
BASE LINE OF CONSTRUCTION
AND BASE LINE OF SURVEY
RAMP D' SEE SKETCH 'A'



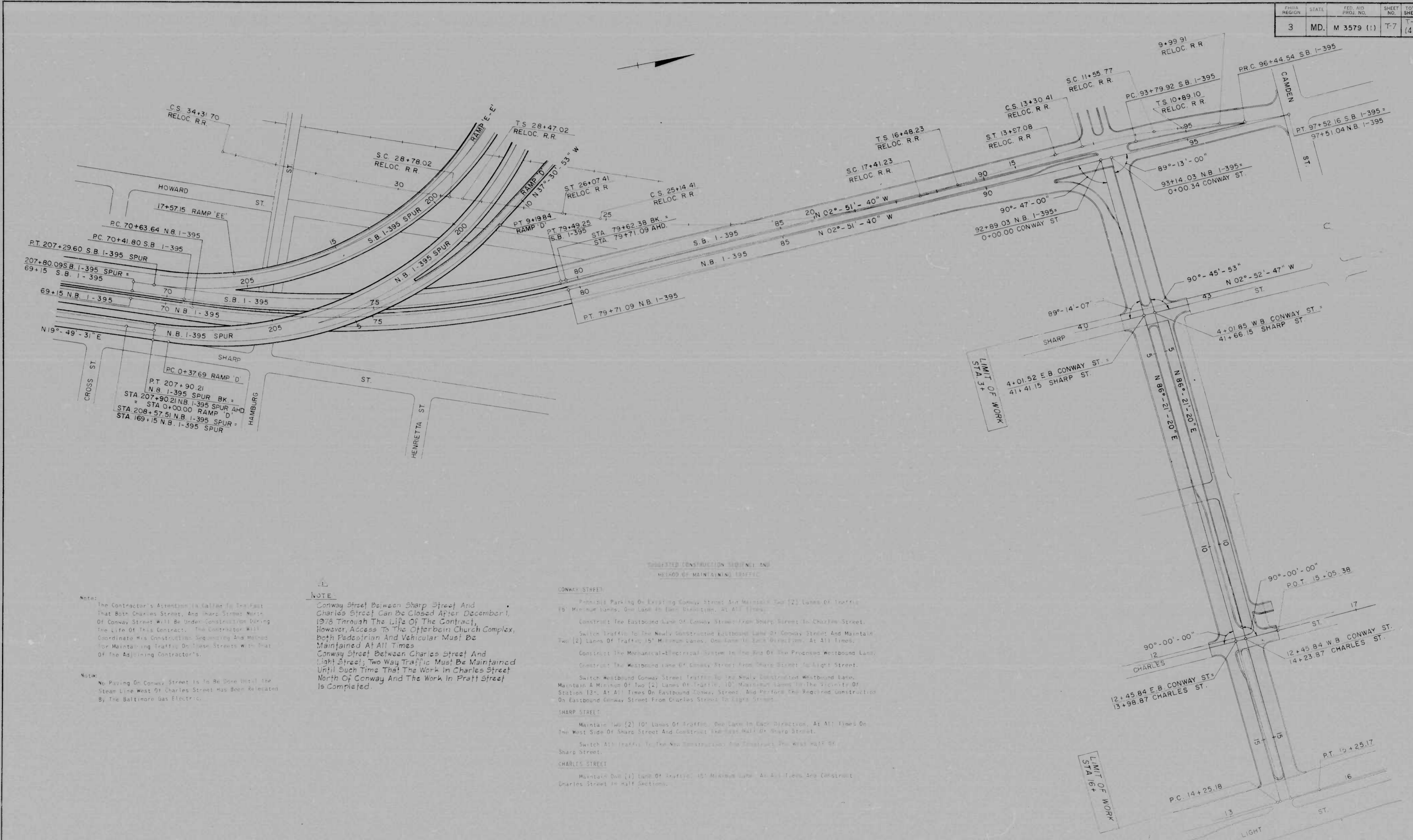
| POINT NO. | DESCRIPTION | CONTROL COORDINATES | | BEARING |
|-----------|-----------------------------|---------------------|------------|-----------------|
| | | NORTH-SOUTH | EAST-WEST | |
| 19 | 0+00.00 | -7257.8334 | -975.2124 | N 19°-49'-31" E |
| 20 | P.C. 0+37.69 | -7272.3811 | -965.4312 | N 37°-30'-53" W |
| 21 | P.T. 9+19.84 | -6386.6442 | -1092.4786 | N 37°-30'-53" W |
| 19 | Ramp D' (E) OF CONSTRUCTION | -7257.8334 | -975.2124 | N 19°-49'-31" E |
| 33 | P.C. 0+39.24 | -7220.9197 | -961.8043 | N 19°-49'-31" E |
| 34 | P.C.C. 1+36.56 | -7127.3566 | -935.4193 | |
| 35 | P.C.C. 4+23.86 | -6681.7320 | -912.4299 | |
| 36 | P.C.C. 5+30.18 | -6633.7556 | -950.3115 | |
| 37 | P.T. 8+91.40 | -6410.6590 | -1074.0416 | N 37°-30'-53" W |
| 21 | 9+19.84 BK. AHD. | -6386.6442 | -1092.4786 | |

| POINT NO. | DESCRIPTION | CONTROL COORDINATES | | BEARING | |
|-----------|----------------------|---------------------|------------|-----------------|-----------------|
| | | NORTH-SOUTH | EAST-WEST | | |
| 1 | S.B. 1-395 | -7283.5955 | -1102.4100 | N 19°-49'-31" E | |
| 2 | P.C. 70+41.80 | -7144.3103 | -1059.3554 | | |
| 3 | P.T. 79+49.75 | -6272.6372 | -926.9590 | N 2°-51'-10" W | |
| 4 | P.O.T. 79+52.34 BK. | -6259.5193 | -927.6156 | | |
| 5 | P.O.T. 79+71.09 AHD. | | | | |
| 6 | P.C. 83+79.92 | -4852.4425 | -997.9353 | | |
| 7 | P.R.C. 96+44.54 | -4588.0679 | -993.8535 | N 2°-51'-40" W | |
| 8 | P.T. 97+52.16 | -4480.5400 | -991.4901 | | |
| 8 | N.B. 1-395 | 7289.7751 | -1086.0337 | N 19°-49'-31" E | |
| 9 | P.C. 70+63.64 | -7149.3447 | -1035.6222 | | |
| 10 | P.T. 79+71.09 | -6708.7714 | -902.6458 | N 2°-51'-40" W | |
| 11 | P.T. 79+71.09 AHD. | -6708.7714 | -902.6458 | | |
| 11 | E.S. CONWAY STREET | 0+00.00 | -4341.9758 | -968.4294 | N 86°-21'-20" E |
| 13 | P.T. 16+60.97 | -4836.3977 | -689.1658 | | |
| 12 | W.B. CONWAY STREET | 0+00.34 | -4917.0047 | -369.6774 | N 86°-21'-20" E |
| 14 | P.T. 16+60.97 | -4811.5204 | -656.4648 | | |
| 15 | S.B. 1-395 SPUR | 207+80.09 | 7275.4558 | 1125.5175 | S 19°-49'-31" W |
| 16 | P.T. 207+29.60 | 7227.9545 | 1108.3924 | | |
| 17 | N.B. 1-395 SPUR | 207+90.21 BK. | 7248.9825 | 1013.6319 | S 19°-49'-31" W |
| 18 | P.T. 207+90.21 AHD. | -7249.8305 | -892.2434 | | |
| 23 | 208+57.51 | -7315.2005 | -1020.0690 | | |
| 22 | RAMP E-E | 17+57.15 | -7033.7568 | -1098.3275 | S 19°-49'-31" W |
| 31 | P.T. 19+33.14 | -7204.8485 | -1137.7138 | | |
| 32 | P.T. 19+44.93 | -7215.9417 | -1141.7131 | S 19°-49'-31" W | |
| 29 | SHARP STREET | 41+61.15 | -4916.4537 | -567.7217 | N 2°-52'-47" W |
| 30 | P.T. 41+66.15 | -4891.4830 | -568.9778 | | |
| 24 | LIGHT STREET | 14+25.18 | 4837.49 | +893.31 | |
| 25 | P.T. 15+25.17 | 4737.97 | +873.62 | | |
| 26 | CHARLES STREET | 13+96.87 | -4869.7854 | +274.8903 | N 3°-38'-40" W |
| 27 | P.T. 14+23.87 | -4877.8359 | +273.3012 | | |
| 28 | P.T. 15+05.38 | -4756.49 | +268.12 | | |

| NO. | P.C. STATION | P.T. STATION | CURVE DATA | | | | | L.C. | S.E. |
|-----|--------------|--------------|-------------|------------|----------|---------|---------|---------|----------|
| | | | Δ | D | R | T | L | | |
| 9 | 17+57.15 | 19+33.14 | 13°-43'-21" | 7°-47'-51" | 734.80' | 88.42' | 175.9' | 5.30' | 175.57' |
| 10 | 0+39.24 | 1+36.56 | 8°-02'-23" | 8°-15'-41" | 693.55' | 48.14' | 97.2' | 1.71' | 97.24' |
| 11 | 1+36.56 | 4+23.86 | 14°-21'-54" | 5°-00'-00" | 1145.92' | 144.41' | 287.0' | 9.06' | 286.55' |
| 12 | 4+23.86 | 6+30.18 | 16°-25'-05" | 7°-57'-28" | 720.00' | 103.87' | 206.2' | 7.45' | 205.61' |
| 13 | 6+30.18 | 8+91.40 | 18°-31'-01" | 7°-05'-19" | 808.29' | 131.76' | 261.2' | 10.67' | 260.09' |
| 1 | 70+41.80 | 79+49.25 | 22°-41'-11" | 2°-30'-00" | 2291.83' | 459.75' | 907.1' | 45.66' | 901.53' |
| 2 | 93+79.92 | 96+44.54 | 7°-42'-28" | 2°-54'-46" | 1967.65' | 132.51' | 264.1' | 4.46' | 264.41' |
| 3 | 96+44.54 | 97+52.16 | 7°-42'-28" | 7°-09'-43" | 800.30' | 53.89' | 107.1' | 1.31' | 107.54' |
| 4 | 70+63.64 | 79+11.09 | 22°-41'-11" | 2°-30'-00" | 2291.83' | 459.75' | 907.1' | 45.66' | 901.53' |
| 5 | 194+44.80 | 207+29.60 | 89°-56'-08" | 7°-00'-00" | 818.51' | 917.59' | 1281.1' | 378.30' | 1156.90' |
| 6 | 195+05.42 | 207+90.21 | 89°-56'-08" | 7°-00'-00" | 818.51' | 917.59' | 1281.1' | 378.30' | 1156.90' |
| 7 | 0+37.69 | 9+19.84 | 57°-20'-24" | 8°-30'-00" | 861.47' | 481.99' | 121.1' | 545.79' | 0.06' |
| 8 | 7+47.01 | 17+57.15 | 17°-16'-29" | 7°-39'-00" | 748.93' | 598.70' | 111.1' | 29.86' | 935.31' |

| | | | |
|-----------|---|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET HORIZONTAL CONTROL | DRAWN BY: DCE TRACED BY: DCE DES. BY: WRH CHK. BY: HMM |
| | CONSULTING ENGINEERS BALTIMORE, MARYLAND | SCALE: 1"=100' | F.A.P. NO. M. 3579 (1) S.H.A. NO. BU 231-11-815 BALTO. CITY NO. 2492 |

| SHA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M 3579 (1) | T-7 | (41) |



NOTE:
The Contractor's Attention is Called to the Fact that Both Charles Street and Sharp Street North of Conway Street will be Under Construction During the Life of This Contract. The Contractor will Coordinate His Construction Scheduling and Method for Maintaining Traffic on These Streets with that of the Adjoining Contractors.

NOTE:
No Paving on Conway Street is to be Done Until the Steam Line West of Charles Street has been Relocated by the Baltimore Gas Electric.

NOTE:
Conway Street Between Sharp Street and Charles Street Can be Closed After December 1, 1978 Through the Life of the Contract. However, Access to the Offenbain Church Complex, both Pedestrian and Vehicular Must be Maintained at All Times.
Conway Street Between Charles Street and Light Street, Two Way Traffic Must be Maintained until such time that the work in Charles Street North of Conway and the work in Pratt Street is completed.

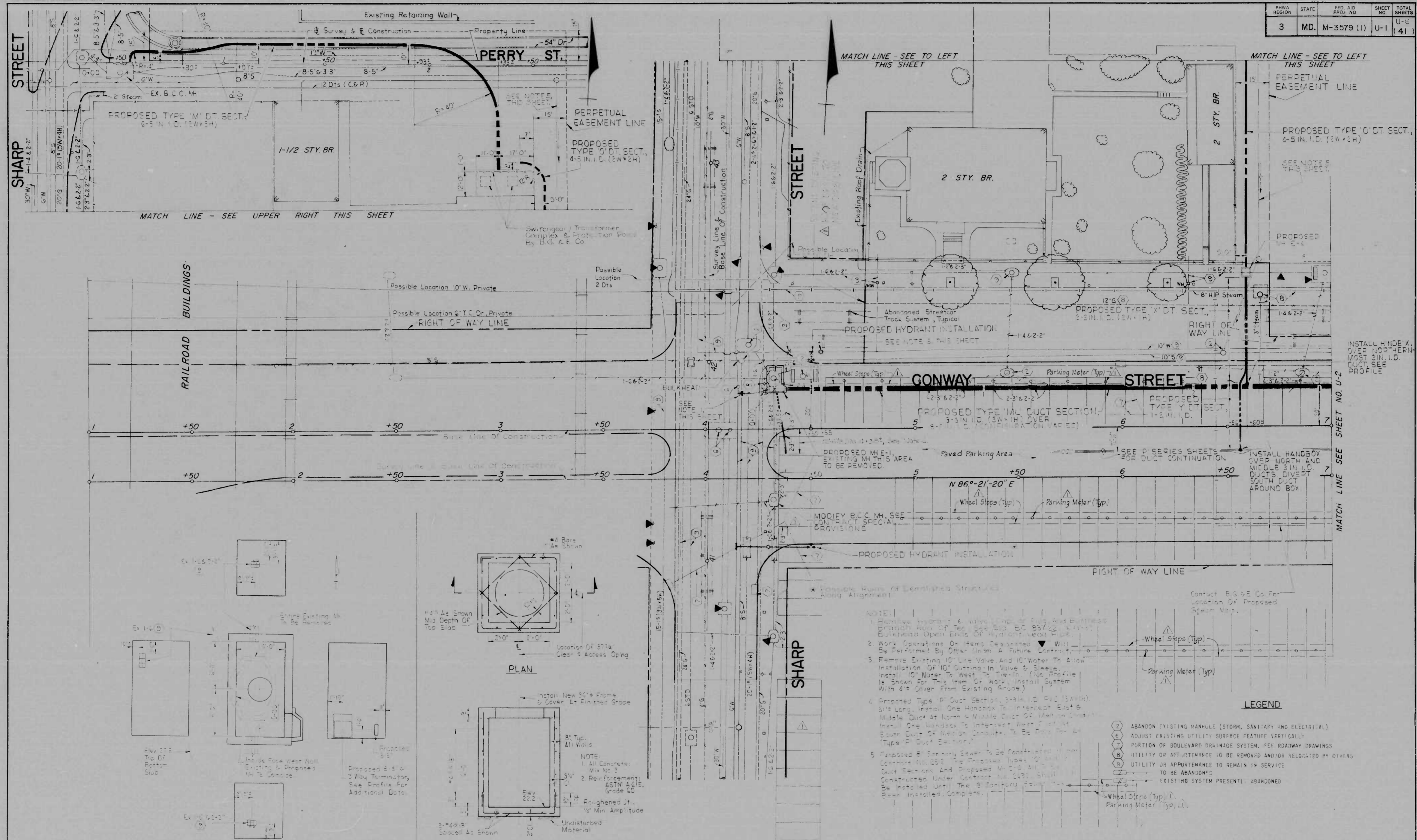
SUGGESTED CONSTRUCTION SEQUENCE AND METHOD OF MAINTAINING TRAFFIC:

CONWAY STREET
 - Provide Parking on Existing Conway Street and Maintain Two (2) Lanes of Traffic in Minimum Lanes, One Lane in Each Direction, at All Times.
 - Construct the Eastbound Lane of Conway Street from Sharp Street to Charles Street.
 - Switch Traffic to the Newly Constructed Eastbound Lane of Conway Street and Maintain Two (2) Lanes of Traffic, 15' Minimum Lanes, One Lane in Each Direction, at All Times.
 - Construct the Mechanical-Electrical System in the Mid of the Proposed Westbound Lane.
 - Construct the Westbound Lane of Conway Street from Sharp Street to Light Street.
 - Switch Westbound Conway Street Traffic to the Newly Constructed Westbound Lane, Maintain a Minimum of Two (2) Lanes of Traffic, 10' Minimum Lanes in the Vicinity of Station 13+. At All Times on Eastbound Conway Street, and Perform the Required Construction on Eastbound Conway Street from Charles Street to Light Street.

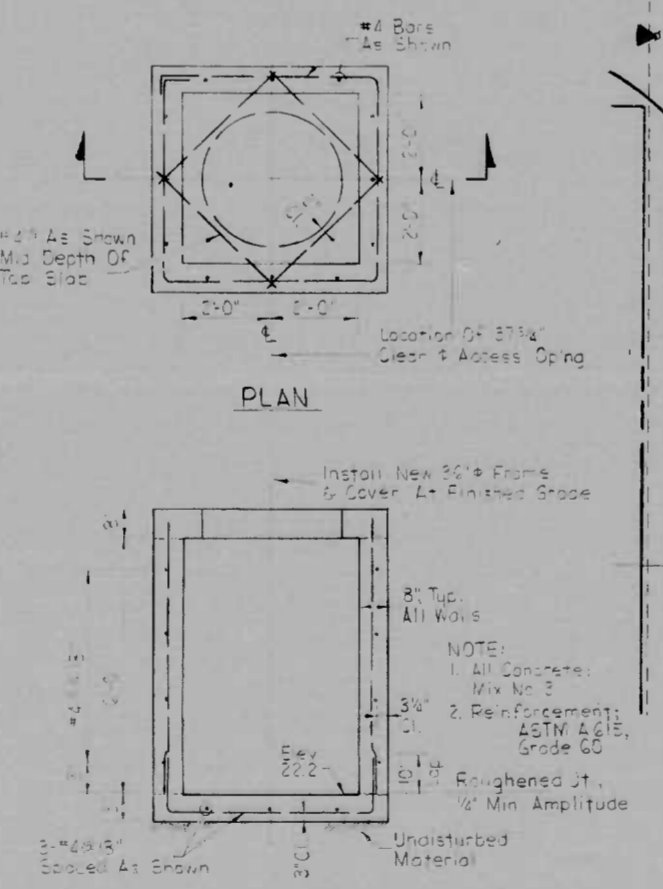
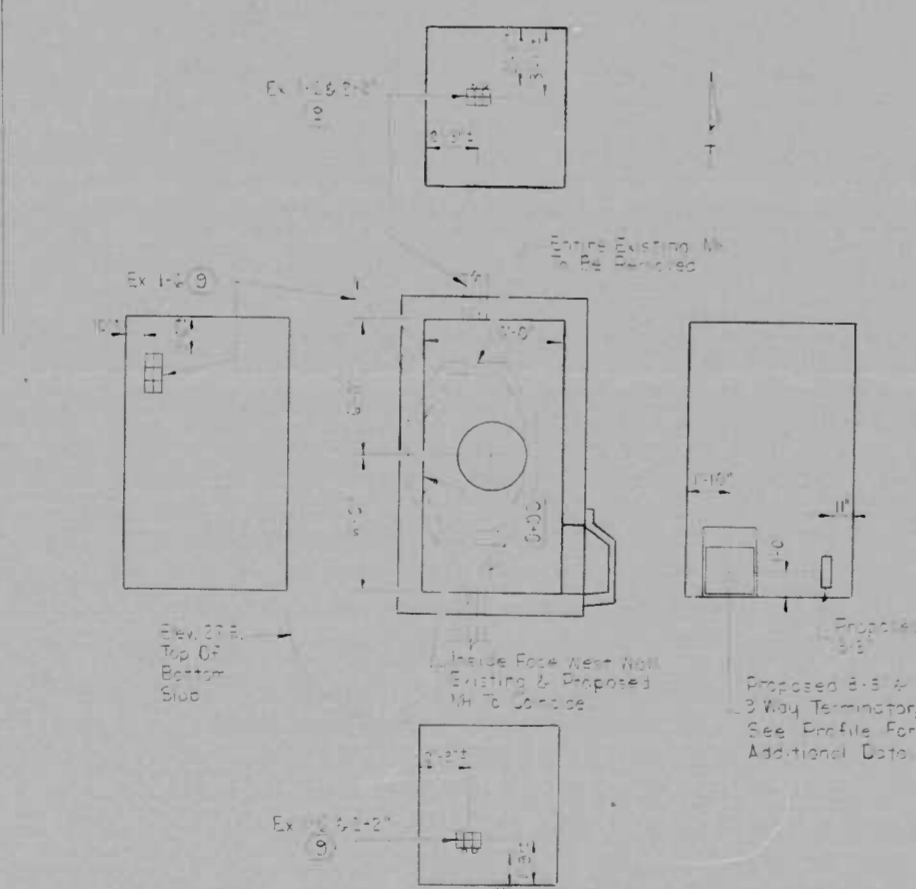
SHARP STREET
 - Maintain Two (2) 10' Lanes of Traffic, One Lane in Each Direction, at All Times on the West Side of Sharp Street and Construct the East Half of Sharp Street.
 - Switch All Traffic to the New Construction and Close the West Half of Sharp Street.

CHARLES STREET
 - Maintain One (1) Lane of Traffic, 15' Minimum Lane, at All Times and Construct Charles Street in Half Sections.

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|------------------------|--|---|
| | RUMMEL, KLEPPER & KAHL | CONWAY & REET | DRAWN BY DCE DES. BY HMM |
| | CONSULTING ENGINEERS | SHARP STREET TO LIGHT STREET | TRACED BY DCE CHK. BY HMM |
| | BALTIMORE, MARYLAND | MAINTENANCE OF TRAFFIC | F.A.P. NO. M 3579 (1) S.H.A. NO. BC 23-11-815 BALTO. CITY NO. 6492 |
| | | SCALE: 1"=100' | SHEET NO. 7 OF 7 |



| FHWA REGION | STATE | FED AID PROJ NO | SHEET NO. | TOTAL SHEETS |
|-------------|-------|-----------------|-----------|--------------|
| 3 | MD. | M-3579 (1) | U-1 | U-6 (41) |



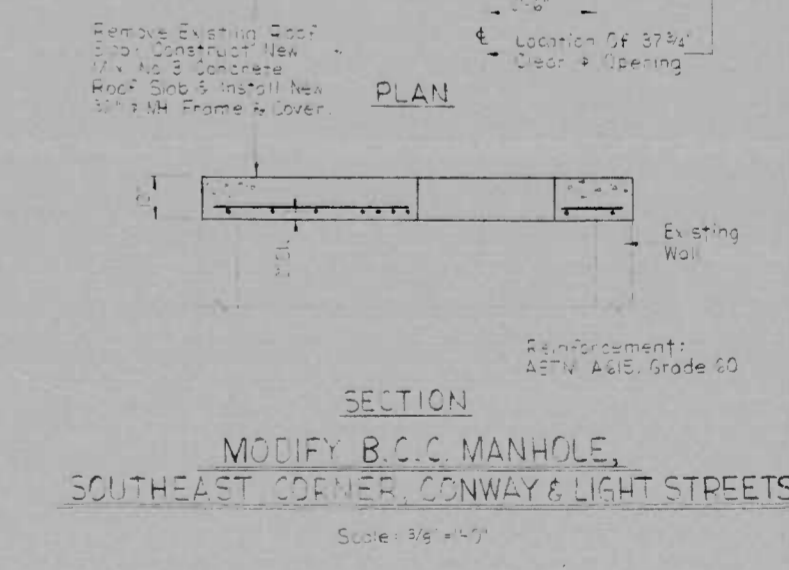
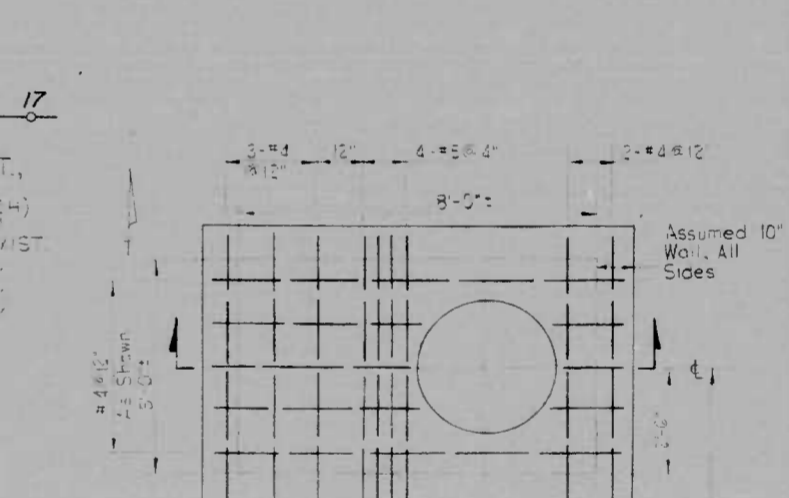
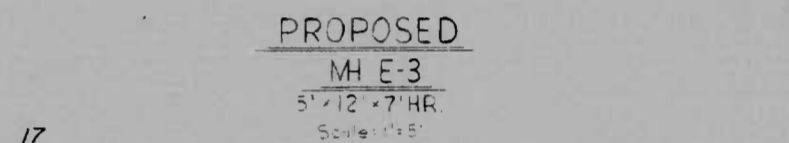
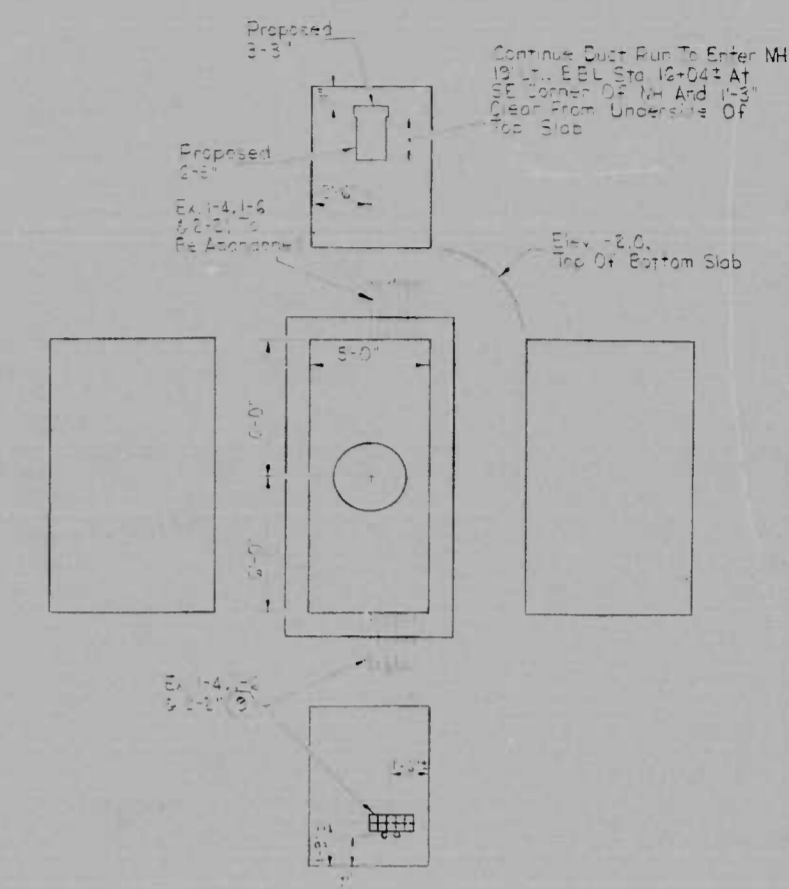
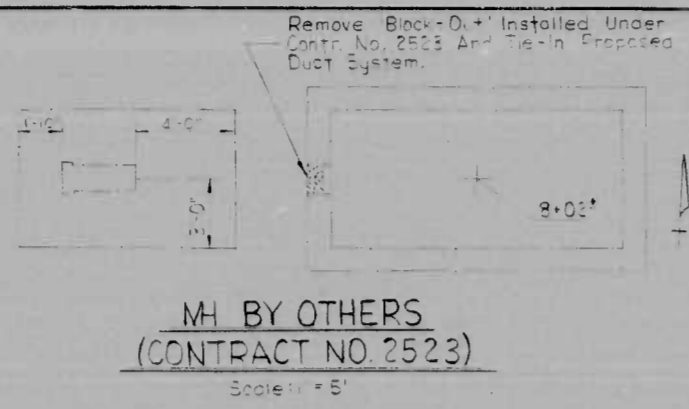
| | | | |
|--|--|--|---|
| REVISIONS Addendum No. 1 10/2/78 | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS CONWAY STREET SHARP STREET TO LIGHT STREET UTILITY RELOCATIONS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| SCALE: 1" = 20' | DATE | DRAWN BY TRACED BY | DES. BY CHK. BY |
| | | F.A.P. NO. M-3579(1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | SHEET NO. U-1 OF U-6 |

- NOTE**
- Remove hydrant & valve, cap, or plug and purchase appropriate size of tee. See S.D. E.C. 25722, 241-101, 241-102, 241-103, 241-104, 241-105, 241-106, 241-107, 241-108, 241-109, 241-110, 241-111, 241-112, 241-113, 241-114, 241-115, 241-116, 241-117, 241-118, 241-119, 241-120, 241-121, 241-122, 241-123, 241-124, 241-125, 241-126, 241-127, 241-128, 241-129, 241-130, 241-131, 241-132, 241-133, 241-134, 241-135, 241-136, 241-137, 241-138, 241-139, 241-140, 241-141, 241-142, 241-143, 241-144, 241-145, 241-146, 241-147, 241-148, 241-149, 241-150, 241-151, 241-152, 241-153, 241-154, 241-155, 241-156, 241-157, 241-158, 241-159, 241-160, 241-161, 241-162, 241-163, 241-164, 241-165, 241-166, 241-167, 241-168, 241-169, 241-170, 241-171, 241-172, 241-173, 241-174, 241-175, 241-176, 241-177, 241-178, 241-179, 241-180, 241-181, 241-182, 241-183, 241-184, 241-185, 241-186, 241-187, 241-188, 241-189, 241-190, 241-191, 241-192, 241-193, 241-194, 241-195, 241-196, 241-197, 241-198, 241-199, 241-200.
 - Work operation on items described will be performed by other under a future contract.
 - Remove existing 10" line valve and 10" water to allow installation of 10" cutting-in valve & sleeve. Install 10" water to west to tie-in. (No profile is shown for this item or work. Install system with 4" cover from existing grade.)
 - Proposed type 'P' duct section, 3'-6" x 15" D.C. (3' x 15") 5' long, install one handbox to intercept east & middle duct at north & middle duct of main duct. Install one handbox to intercept west duct at south duct of main duct. To be removed after type 'P' duct section.
 - Proposed 8" sanitary sewer to be installed under proposed 10" duct. See proposed MHE-1, MHE-4, MHE-5, MHE-6, MHE-7, MHE-8, MHE-9, MHE-10, MHE-11, MHE-12, MHE-13, MHE-14, MHE-15, MHE-16, MHE-17, MHE-18, MHE-19, MHE-20, MHE-21, MHE-22, MHE-23, MHE-24, MHE-25, MHE-26, MHE-27, MHE-28, MHE-29, MHE-30, MHE-31, MHE-32, MHE-33, MHE-34, MHE-35, MHE-36, MHE-37, MHE-38, MHE-39, MHE-40, MHE-41, MHE-42, MHE-43, MHE-44, MHE-45, MHE-46, MHE-47, MHE-48, MHE-49, MHE-50, MHE-51, MHE-52, MHE-53, MHE-54, MHE-55, MHE-56, MHE-57, MHE-58, MHE-59, MHE-60, MHE-61, MHE-62, MHE-63, MHE-64, MHE-65, MHE-66, MHE-67, MHE-68, MHE-69, MHE-70, MHE-71, MHE-72, MHE-73, MHE-74, MHE-75, MHE-76, MHE-77, MHE-78, MHE-79, MHE-80, MHE-81, MHE-82, MHE-83, MHE-84, MHE-85, MHE-86, MHE-87, MHE-88, MHE-89, MHE-90, MHE-91, MHE-92, MHE-93, MHE-94, MHE-95, MHE-96, MHE-97, MHE-98, MHE-99, MHE-100.
- LEGEND**
- ABANDON EXISTING MANHOLE (STORM, SANITARY AND ELECTRICAL)
 - ADJUST EXISTING UTILITY SURFACE FEATURE VERTICALLY
 - PORTION OF BOULEVARD DRAINAGE SYSTEM. SEE ROADWAY DRAWINGS
 - UTILITY OR APPURTENANCE TO BE REMOVED AND/OR RELOCATED BY OTHERS TO BE ABANDONED
 - EXISTING SYSTEM PRESENTLY ABANDONED

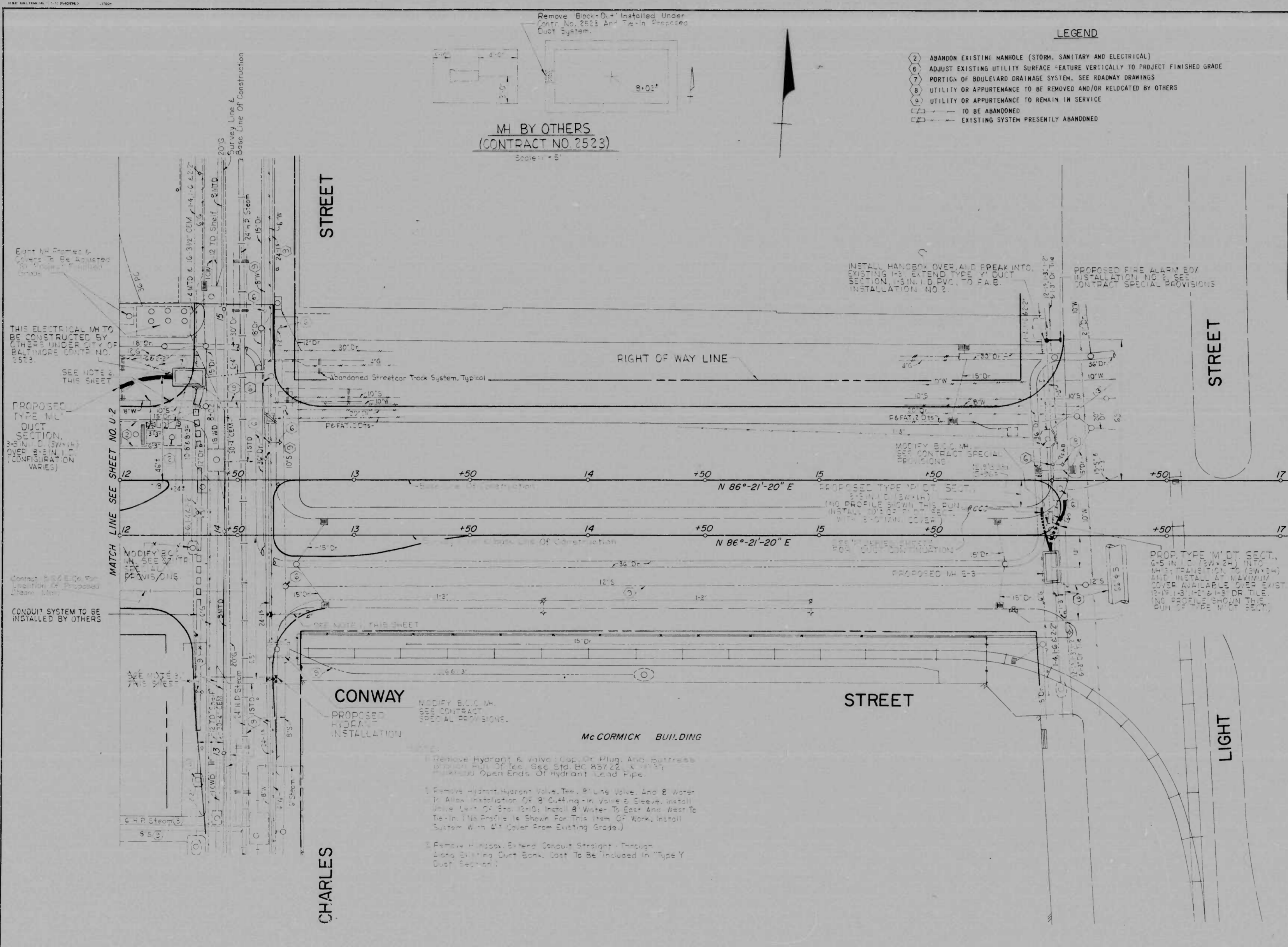
| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M-3579(1) | U-3 | U-6 (41) |

LEGEND

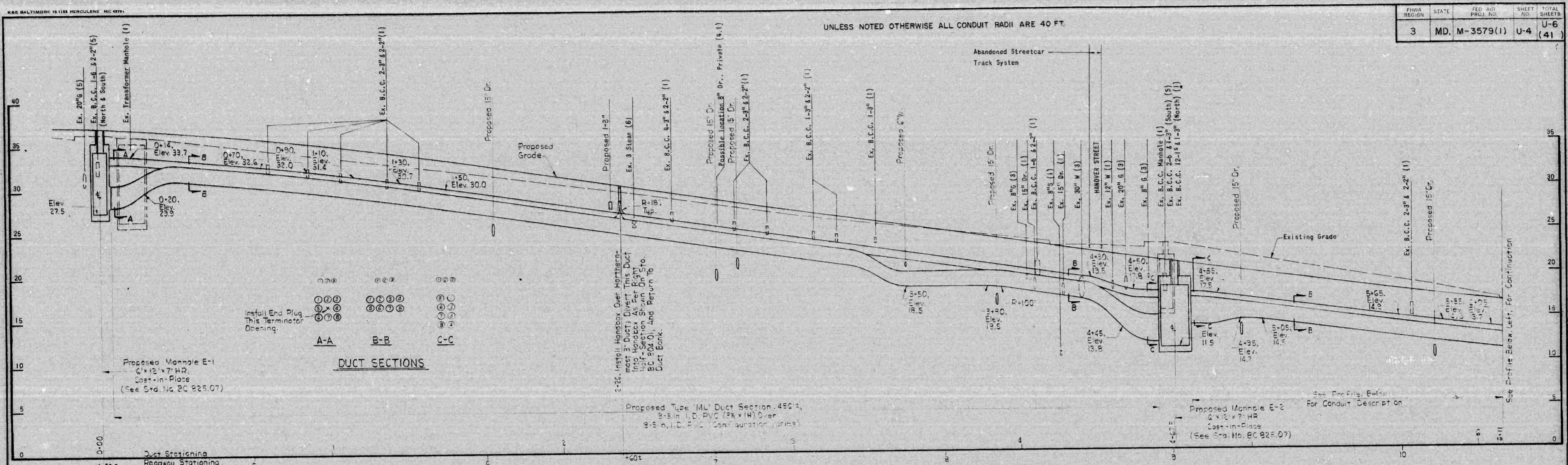
- ② ABANDON EXISTING MANHOLE (STORM, SANITARY AND ELECTRICAL)
- ⑥ ADJUST EXISTING UTILITY SURFACE FEATURE VERTICALLY TO PROJECT FINISHED GRADE
- ⑦ PORTION OF BOULEVARD DRAINAGE SYSTEM, SEE ROADWAY DRAWINGS
- ⑧ UTILITY OR APPURTENANCE TO BE REMOVED AND/OR RELOCATED BY OTHERS
- ⑨ UTILITY OR APPURTENANCE TO REMAIN IN SERVICE
- TO BE ABANDONED
- EXISTING SYSTEM PRESENTLY ABANDONED



SECTION
 MODIFY B.C.C. MANHOLE,
 SOUTHEAST CORNER, CONWAY & LIGHT STREETS
 Scale: 3/8" = 1'-0"

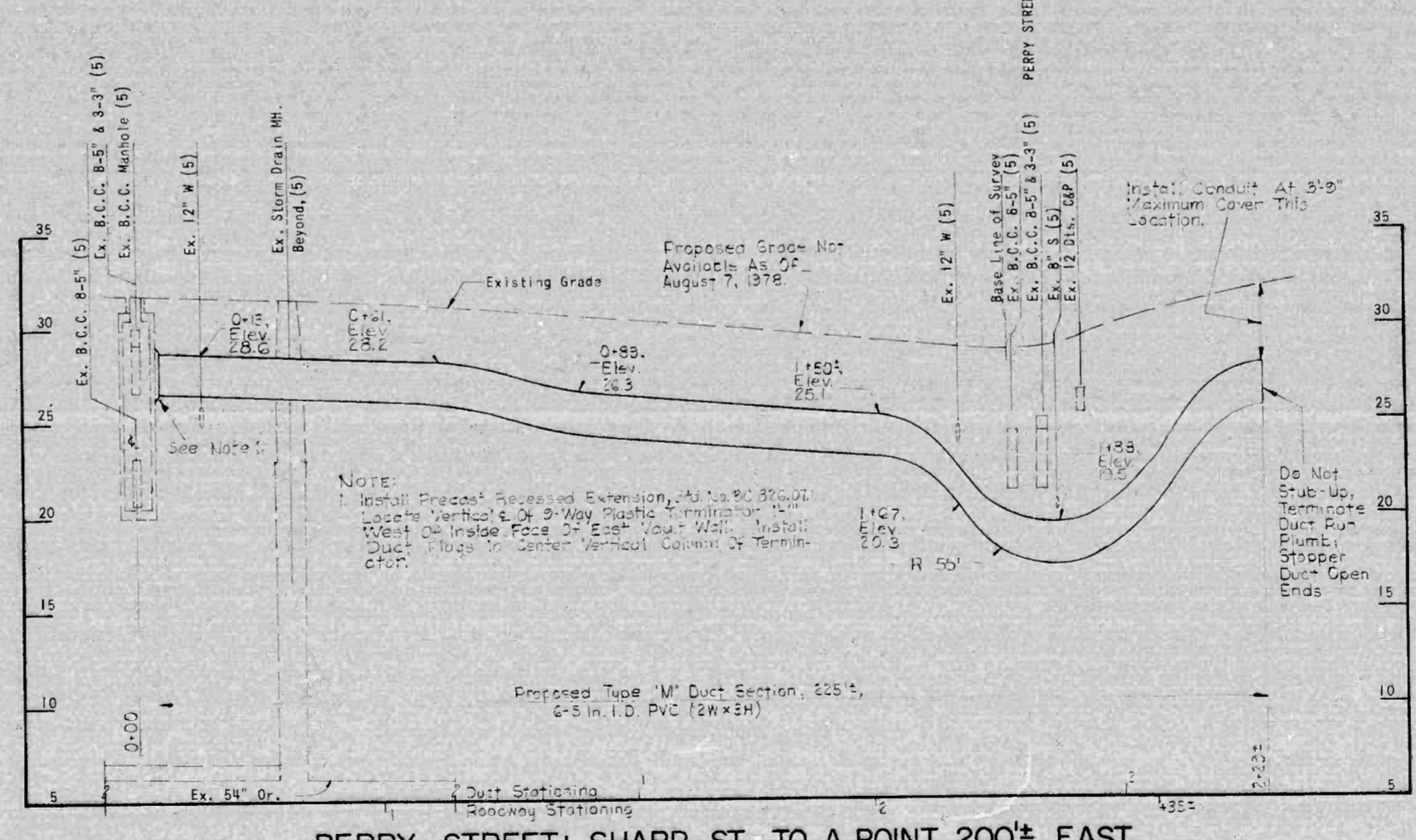
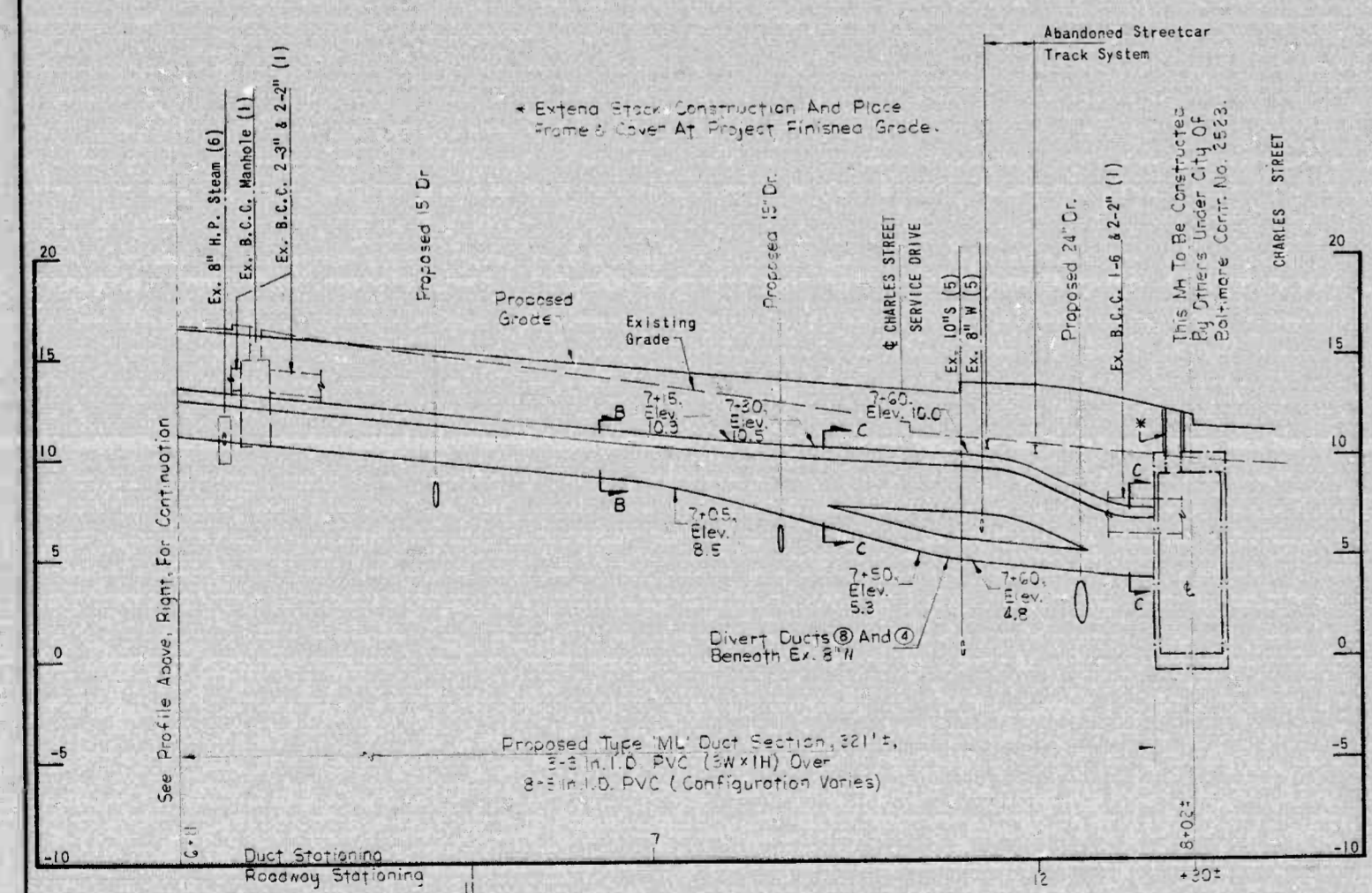


| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SH. RP STREET TO LIGHT STREET UTILITY RELOCATIONS | DRAWN BY DES B TRACED BY CHK. B1 |
| | CONSULTING ENGINEERS BALTIMORE MARYLAND | SCALE: 1" = 20' | F.A.P. NO. M-3579(1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 |
| | | | SHEET NO. U-3 of U-6 |

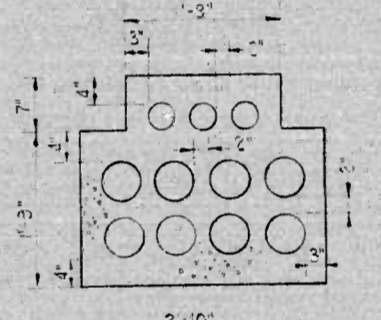


| | | | | |
|-------------|-------|-----------|-----------|--------------|
| FIRM REGION | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 3 | MD | M-3579(1) | U-4 | U-6 (41) |

CONWAY STREET: SHARP STREET TO CHARLES STREET



PERRY STREET: SHARP ST. TO A POINT 200± EAST



TYPE 'M' SECTION
(3-3" I.D. PVC (3x14) Over 3-3" I.D. PVC, Configuration Varies, See PLAN AND PROFILE)

| TYPE SECTION | DESCRIPTION | CONSTRUCT AS PER STD. NO. |
|--------------|-------------|---------------------------|
| 'M' | 3-3" 2WxH | 824.03 |
| 'D' | 4-6" 2WxH | 824.05 |
| 'P' | 3-3" 3WxH | 824.06 |
| 'X' | 2-2" 2WxH | 824.04 |
| 'Y' | 1-3" | 824.04 |

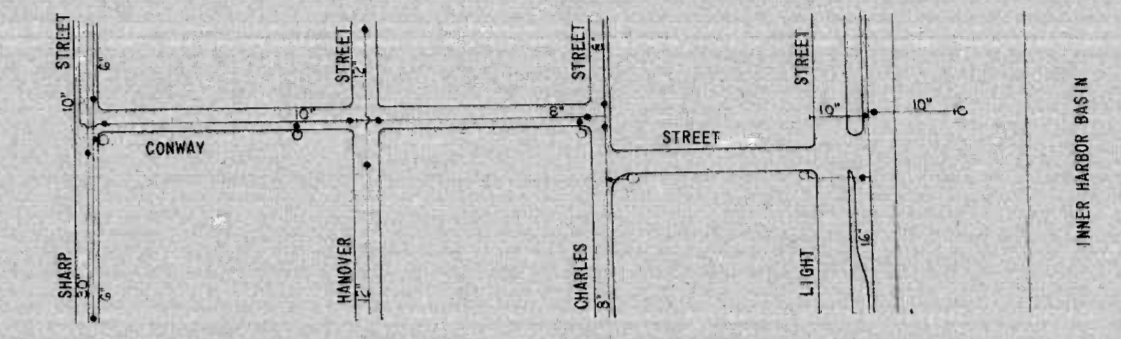
ENCASEMENT FOR ALL DUCT SECTIONS, MIX NO. 1 CONCRETE

- UTILITY DISPOSITION NOTATION
- (1) Existing Utility To Be Abandoned By Contractor
 - (2) Existing Utility To Be Abandoned By Others
 - (3) Existing Utility Presently Abandoned
 - (4) Existing Utility, Location And/Or Depth Unknown
 - (5) Existing Utility To Remain In Service
 - (6) Existing Utility To Be Relocated By Others

| | | | | | |
|-----------|------------------------|--|--|--|----------------------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | | STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | RUMMEL, KLEPPER & KAHL | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILES - ELECTRICAL DUCTS | | DRAWN BY: FWS | DES. BY: FWS |
| | CONSULTING ENGINEERS | | | TRACED BY: DCH-FWS | CHK. BY: WFL |
| | BALTIMORE, MARYLAND | SCALE: HORIZ. 1"=20' VERT. 1"=5' | | F.A.P. NO. M-3579(1) | SHEET NO. U-6 OF U-6 |
| | | DATE | | S.H.A. NO. BC 231-11-815 | |
| | | | | BALTO. CITY NO. 2492 | |

K&E BALTIMORE 18 1153 HERCULENE MC 677

| FHWA REGION | STATE | FED AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|-------------------|-----------|--------------|
| 3 | MD. | M-3579(1) | U-5 | U-6 (41) |



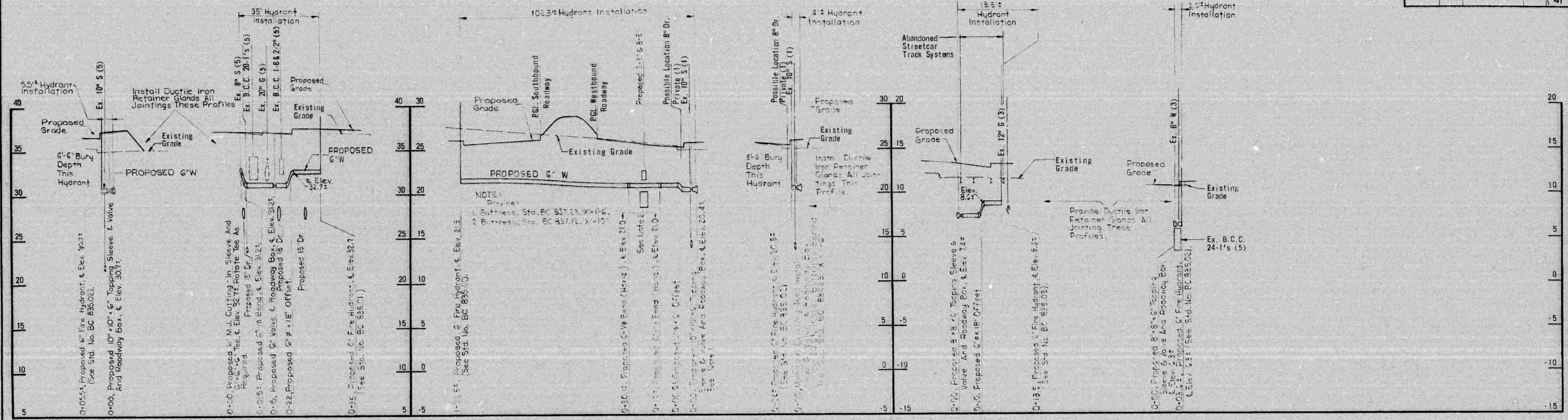
VALVE SHUT-OFF DIAGRAM
NO SCALE

GENERAL NOTES - UTILITIES

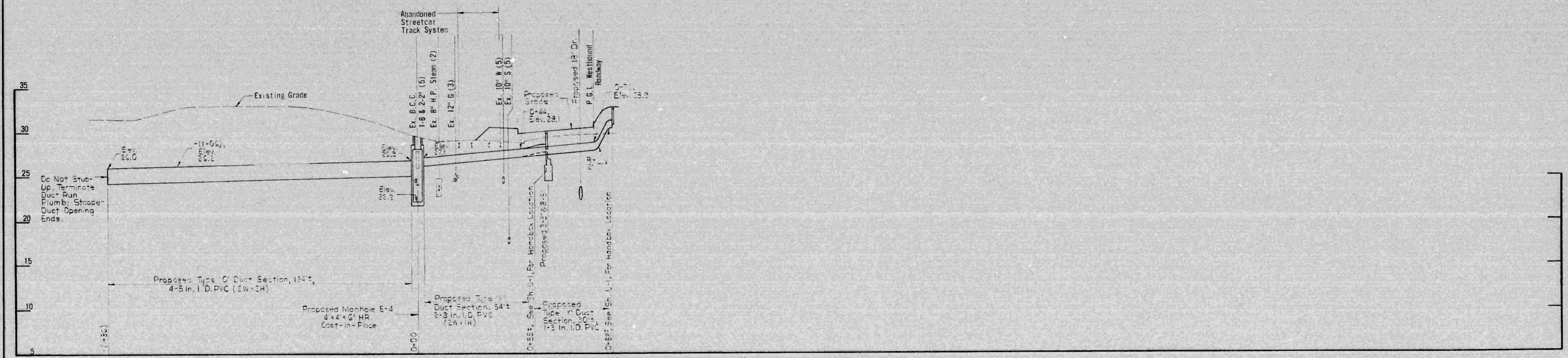
1. Existing Utility Locations Shown On The Contract Drawings Are Approximate Only. It Shall Be The Contractor's Responsibility To Verify Exact Locations By Test Pits Prior To Commencing Work.
2. Attention Is Directed To The Requirements Of The Special Provisions For Maintenance Of Utility Services During The Construction Of The Project.
3. Public Utility Appurtenances To Be Salvaged Shall Become The Property Of The Contractor And Shall Be Removed From The Project Site Unless Specified Otherwise.
4. The Locations And Elevations Of Bench Marks Are Shown On The Roadway Contract Drawings. All Elevations Shown Are Based On Baltimore City Datum.
5. Stations Shown Below Profile Datum Lines Are Project Survey Line And Base Line Of Construction Stationings.
6. Existing Potable Waterlines Have Been Installed With An Approximate 4'-0" Cover.
7. Except As Noted Otherwise, All Potable Waterlines To Be Installed Under The Project Shall Have A Minimum 4'-0" Cover, And Shall Be Of Ductile Iron Pipe, Thickness Class 54.
8. Test Pressures, Potable Water Systems This Contract:
 Pressure Test: 150 psi For 30 Minute Duration
 Leakage Test: 90 psi For 24 Hour Duration
 (Approximate Water Working Pressure: 70 To 80 psi)
9. The Abandonment, Or Removal, And Relaying Of Street Lighting Cable Systems, And The Removal And Installation Of Temporary Street Lighting Poles And Appurtenances (Excluding Concrete Pedestals) Will Be Performed By Others. Removal Of Existing Street Lighting Concrete Pedestals Shall Be As Directed By The Engineer. See Contract Special Provisions For Additional Information.
10. Prior To Performing Any Waterline Operations This Contract It Is Mandatory That Test Pits Be Dug To Establish Exact Points Of Tie-In, And Outside Diameters Of Existing Waterlines To Be Connected To.
11. Unless Shown Or Noted Otherwise The Following Utility Clearances Shall Be Maintained During Construction Operations:
 - A. Between Existing Utilities To Remain In Services And Proposed Utility Systems.
 Vertical Clearance: 6" Minimum
 Horizontal Clearance: 1'-6" Minimum
 - B. Between Proposed Utility Systems.
 As Per A., Above, Except As Shown Otherwise On The Contract Plans And/Or Profiles
12. Where The Contract Plans Show Existing Conduit Systems To Be Tied Into Proposed Structures, And No Proposed Duct Configuration Is Shown, Such Systems Shall Be Broken Back And Adjusted To Meet Location Shown In Proposed Structure. Utilize 3-1/2" Diameter "Split" Duct And Fittings To Extend Systems. Encase In Mix No. 1 Concrete. Services Shall Be Maintained During Construction.

| | | | |
|-----------|---|--|--|
| REVISIONS | CONSULTANT RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | | CONWAY STREET SHARP STREET TO LIGHT STREET GENERAL NOTES & VALVE SHUT-OFF DIAGRAM | |
| | | DRAWN BY: FWS TRACED BY: _____ F.A.P. NO. M-3579(1) SHA NO. BC 231-1-815 BALTO. CITY NO. 2492 | DES. BY: _____ CHK. BY: WFE SHEET NO. U-5 of U-6 |
| | | SCALE: AS SHOWN | DATE: _____ |

| FHWA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|--------------------|-----------|--------------|
| 3 | MD. | M-3579 (1) | U-6 | U-6 (41) |



LEFT OF CONWAY ST. STA. 4+50 RIGHT OF CONWAY ST. STA. 4+50 RIGHT OF CONWAY ST. STA. 7+75 LEFT OF CONWAY ST. STA. 7+75 LEFT OF CONWAY ST. STA. 11+68.5' RIGHT OF CONWAY ST. STA. 12+66'



- UTILITY DISPOSITION NOTATION
- (1) Existing Utility To Be Abandoned By Contractor
 - (2) Existing Utility To Be Abandoned By Others
 - (3) Existing Utility Presently Abandoned
 - (4) Existing Utility Location And/Or Depth Unknown
 - (5) Existing Utility To Remain In Service
 - (6) Existing Utility To Be Relocated By Others

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE HIGHWAY ADMINISTRATION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|-----------|---|--|---|
| | RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND | CONWAY STREET SHARP STREET TO LIGHT STREET PROFILES - WATERMAINS & ELECTRICAL DUCTS | |
| | | DRAWN BY: <i>RWS-00</i> CHECKED BY: <i>WEX</i> | DESIGNED BY: <i>RWS-00H</i> CHECKED BY: <i>WEX</i> |
| | | EXP. NO. M-3579(1) S.H.A. NO. BC 231-11-815 BALTO. CITY NO. 2492 | SHEET NO. U-6 OF U-6 |
| | | SCALE: HORIZ. 1"=20' VERT. 1"=5' | DATE |

| FHWIA REGION | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|--------------|-------|--------------------|-----------|--------------|
| 3 | MD. | | T-1 | |

- INDEX OF DRAWINGS -

- T-1 TITLE SHEET
- T-2 TYPICAL SECTIONS
- T-3 MISCELLANEOUS DETAILS
- T-4 MAINTENANCE OF TRAFFIC
- T-5 HORIZONTAL CONTROL

- P-1 - PLAN - STA. 3+ TO STA. 7+00
- P-2 - PLAN - STA. 7+00 TO STA. 12+00
- P-3 - PLAN - STA. 12+00 TO STA. 16+
- P-4 - PROFILES - STA. 3+ TO STA. 12+
- P-5 - ELECTRICAL PLAN

- U-1 - UTILITY RELOCATIONS
- U-2 - UTILITY RELOCATIONS
- U-3 - UTILITY RELOCATIONS
- U-4 - GENERAL NOTES & VALVE SHUT-OFF DIAGRAM
- U-5 - PROFILES - WATER MAINS

- SN-1 - GUIDE SIGN LAYOUT
- SN-2 - SIGNING
- SN-3 - SIGNING
- SN-4 - SIGNING

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
AND
STATE HIGHWAY ADMINISTRATION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

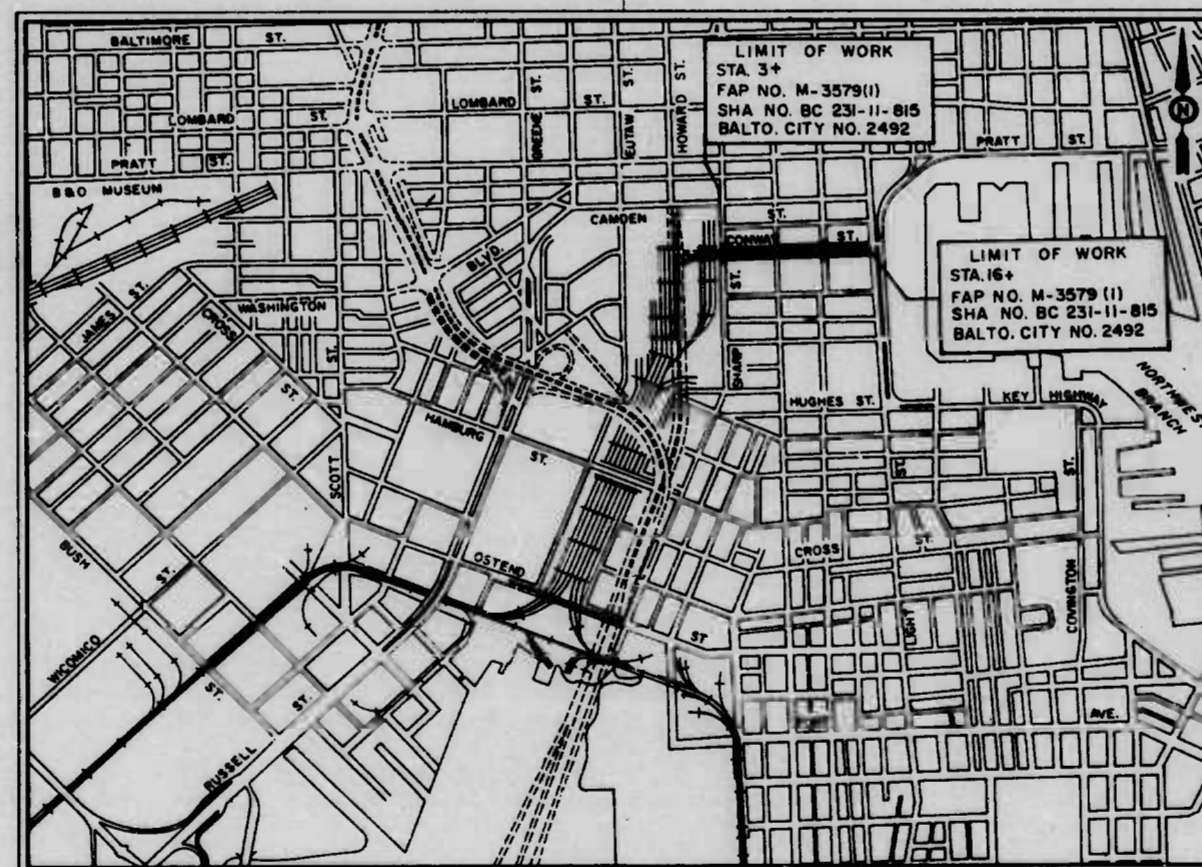
FEDERAL AID PROJECT NO. M-3579(I)
STATE HIGHWAY ADMINISTRATION PROJECT NO. B.C.231-II-815
CITY OF BALTIMORE BUREAU OF ENGINEERING,
HIGHWAY ENGINEERING DIVISION CONTRACT NO. 2492

CONWAY STREET

SHARP STREET TO LIGHT STREET

P.I. PLANS

BOOK NUMBERS



LOCATION PLAN
SCALE: 1" = 1000'

DESIGN SPEED - 35 M.P.H.
LENGTH OF PROJECT = 0.232 MILES

PRELIMINARY
DATE MAY 3 1978

THE DESIGN OF THIS PROJECT HAS INCORPORATED FACILITIES FOR THE ELDERLY AND HANDICAPPED IN COMPLIANCE WITH STATE AND FEDERAL LEGISLATION.

RIGHT OF WAY LINES SHOWN ON THESE PLANS DO NOT INCLUDE EASEMENT. THEY ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THESE LINES DO NOT REPRESENT THE OFFICIAL PROPERTY ACQUISITION LINES. FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION, SEE THE APPROPRIATE RIGHT OF WAY PLAT OR PLATS.

NOTE:
EFFECTIVE JULY 1, 1971 IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 526 OF THE ACTS OF THE 1970 GENERAL ASSEMBLY, WHEREVER THE TITLE "STATE ROADS COMMISSION" AND/OR TERM "COMMISSION" IS USED, IT SHALL BE CONSTRUED TO BE "THE STATE HIGHWAY ADMINISTRATION".

RIGHT OF WAY PLATS
I-395-007
I-395-008

| | | | | | |
|---|------------------------------|--|--|---|---|
| CHECKED BY: BUREAU OF ENGINEERING WATER DIVISION _____ WASTE WATER DIVISION _____ SEDIMENTATION & EROSION CONTROL _____ HIGHWAY ENGINEERING DIVISION _____ SURVEY AND RECORDS DIVISION _____ BUREAU OF UTILITY OPERATIONS LIGHTING SECTION - HIGHWAY MAINTENANCE DIV. _____ CONDUIT SECTION - HIGHWAY MAINTENANCE DIV. _____ | INITIALS _____ DATE _____ | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS APPROVAL RECOMMENDED HEAD, BUREAU OF ENGINEERING APPROVED DIRECTOR OF PUBLIC WORKS _____ | PREPARED BY RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND _____ DATE _____ | STATE HIGHWAY ADMINISTRATION OF MARYLAND REVIEWED AND APPROVAL RECOMMENDED CHIEF, BUREAU OF DESIGN _____ APPROVAL RECOMMENDED _____ APPROVED _____ CHIEF, INTERSTATE DIVISION FOR BALTIMORE CITY _____ CHIEF ENGINEER _____ | U. S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION APPROVED _____ DIVISION ADMINISTRATOR _____ DATE _____ |
|---|------------------------------|--|--|---|---|

| FHWY REGION | STATE | FED AID PROJ NO. | SHEET NO. | TOTAL SHEETS |
|-------------|-------|------------------|-----------|--------------|
| 3 | MD. | M-3579(1) | T-1 | T-7 (41) |

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
AND
STATE HIGHWAY ADMINISTRATION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

FEDERAL AID PROJECT NO. M-3579(1)
STATE HIGHWAY ADMINISTRATION PROJECT NO. B.C.231-II-815
CITY OF BALTIMORE BUREAU OF ENGINEERING,
HIGHWAY ENGINEERING DIVISION CONTRACT NO. 2492

CONWAY STREET
SHARP STREET TO LIGHT STREET

INDEX OF SHEETS

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- T-5 LEGEND AND GENERAL NOTES
- T-6 HORIZONTAL CONTROL
- T-7 MAINTENANCE OF TRAFFIC

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- P-2 PLAN - STA. 7+00 TO STA. 12+00
- P-3 PLAN - STA. 12+00 TO STA. 16+
- P-4 PROFILE - STA. 3+ TO STA. 16+
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- P-6 ELECTRICAL DETAILS
- P-7 ELECTRICAL DETAILS
- P-8 PROFILES - STORM WATER DRAINS
- P-9 PROFILES - STORM WATER DRAINS
- P-10 PROFILES - STORM WATER DRAINS
- P-11 PROFILES AND DETAILS - STORM WATER DRAINS
- P-12 DETAILS - STORM WATER DRAINS
- P-13 STRUCTURE SCHEDULE
- P-14 SUGGESTED SEDIMENT CONTROL PLAN
- P-15 SUGGESTED SEDIMENT CONTROL DETAILS
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- U-2 UTILITY RELOCATIONS AND DETAILS
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- SN-5 GUIDE SIGN DETAILS
- SN-6 SIGN TABULATIONS AND DETAILS
- SN-7 GUIDE SIGN MOUNTING DETAILS

- Q-1 TABULATION OF QUANTITIES
- Q-2 TABULATION OF QUANTITIES
- Q-3 TABULATION OF QUANTITIES
- Q-4 SUMMARY OF QUANTITIES

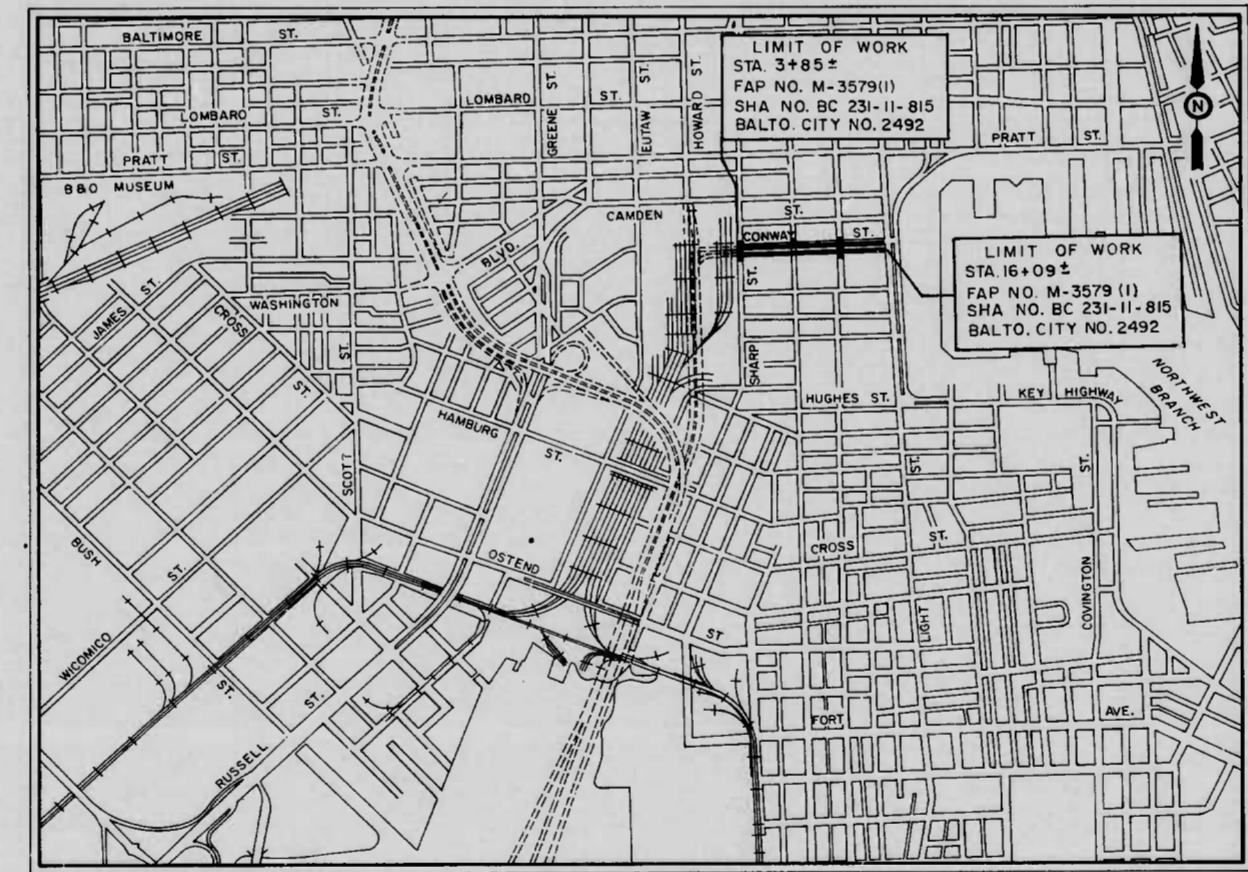
P-3A PERRY ST.
 ENTRANCE AND
 PARKING LOT

BOOK NUMBERS

BLVD. - 040 --- CRSSS SECTIONS
 BLVD. - 041 --- CRSSS SECTIONS
 BLVD. - 042 --- TRAVERSE & REFERENCES

TRAFFIC DATA

| CONWAY STREET | EBR. | WBR. |
|-------------------------------|------|------------|
| ADT | 1995 | 20,813 |
| DHV | " | 10% OF ADT |
| HEAVY TRUCKS | " | 4% OF ADT |
| MEDIUM TRUCKS | " | 3% OF ADT |
| PASSENGER CARS & LIGHT TRUCKS | " | 93% OF ADT |



LOCATION PLAN
 SCALE: 1" = 1000'
DESIGN SPEED - 35 M.P.H.
LENGTH OF PROJECT - 0.232 MILES

THE DESIGN OF THIS PROJECT HAS INCORPORATED FACILITIES FOR THE ELDERLY AND HANDICAPPED IN COMPLIANCE WITH STATE AND FEDERAL LEGISLATION.

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RIGHT OF WAY PLATS
 I-395-007
 I-395-008

INTERSTATE DIVISION FOR BALTIMORE CITY
 APPROVED: *[Signature]*
 SEDIMENTATION AND EROSION CONTROL OFFICER: *[Signature]*

| | | | | | | |
|---|--|---|---|--|---|--|
| CHECKED BY: BUREAU OF ENGINEERING WATER DIVISION WASTE WATER DIVISION HIGHWAY ENGINEERING DIVISION SURVEY AND RECORDS DIVISION BUREAU OF UTILITY OPERATIONS LIGHTING SECTION - HIGHWAY MAINTENANCE DIV. CONDUIT SECTION - HIGHWAY MAINTENANCE DIV. BUREAU OF UTILITIES IDBC DEPARTMENT OF TRANSIT AND TRAFFIC | INITIALS DATE 2JK 9/6/78 BAK 9/6/78 W.F.C. 9/6/78 J.F.M. 9/1/78 9/2/78 9/6/78 INITIALS DATE | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS APPROVAL RECOMMENDED <i>[Signature]</i> HEAD, BUREAU OF ENGINEERING APPROVED <i>[Signature]</i> FRANCIS W. KUERTZ DIRECTOR OF PUBLIC WORKS | PREPARED BY RUMMEL, KLEPPER & KAHL CONSULTING ENGINEERS BALTIMORE, MARYLAND <i>[Signature]</i> DATE 9/6/78 | Addendum No. 1 10/2/78 Revised Sheets T-1, T-7, P-1, P-2, P-8, P-9, P-10, U-1 & U-2 Red Line Revision No. 4 6/15/79 | STATE HIGHWAY ADMINISTRATION OF MARYLAND REVIEWED AND APPROVAL RECOMMENDED <i>[Signature]</i> CHIEF, BUREAU OF DESIGN APPROVAL RECOMMENDED <i>[Signature]</i> CHIEF, INTERSTATE DIVISION FOR BALTIMORE CITY APPROVED <i>[Signature]</i> CHIEF ENGINEER | U. S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION APPROVED DIVISION ADMINISTRATOR DATE |
|---|--|---|---|--|---|--|