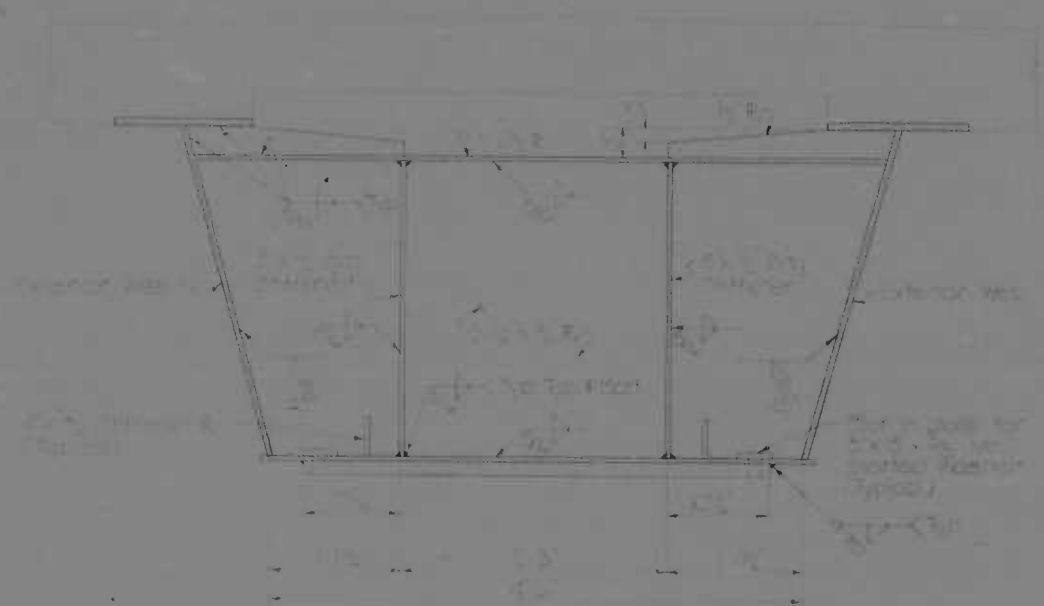
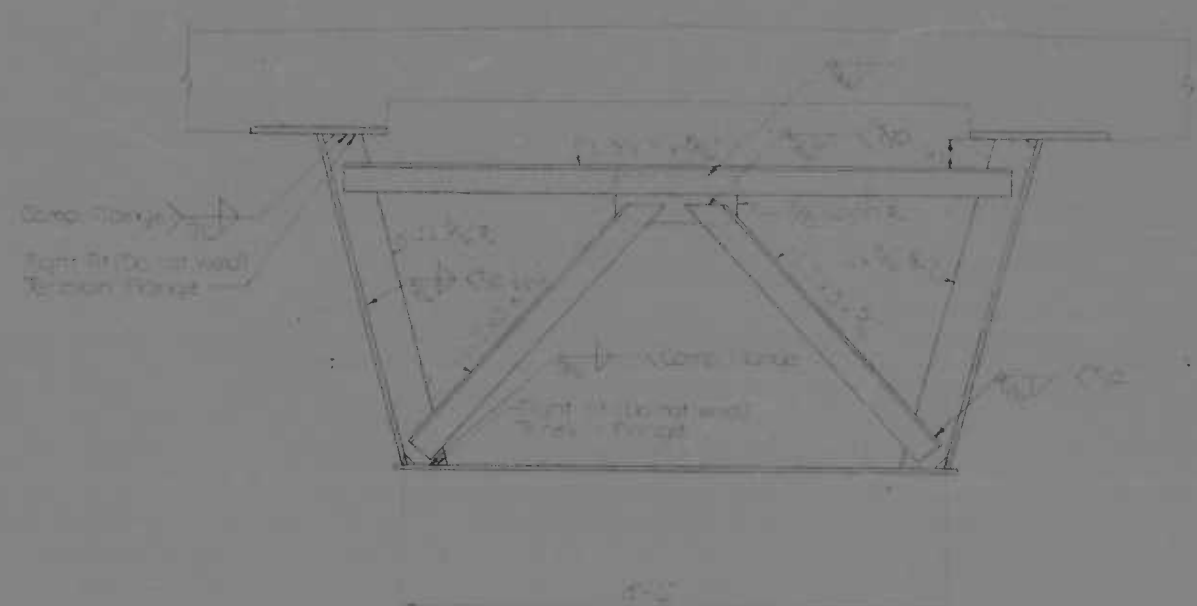


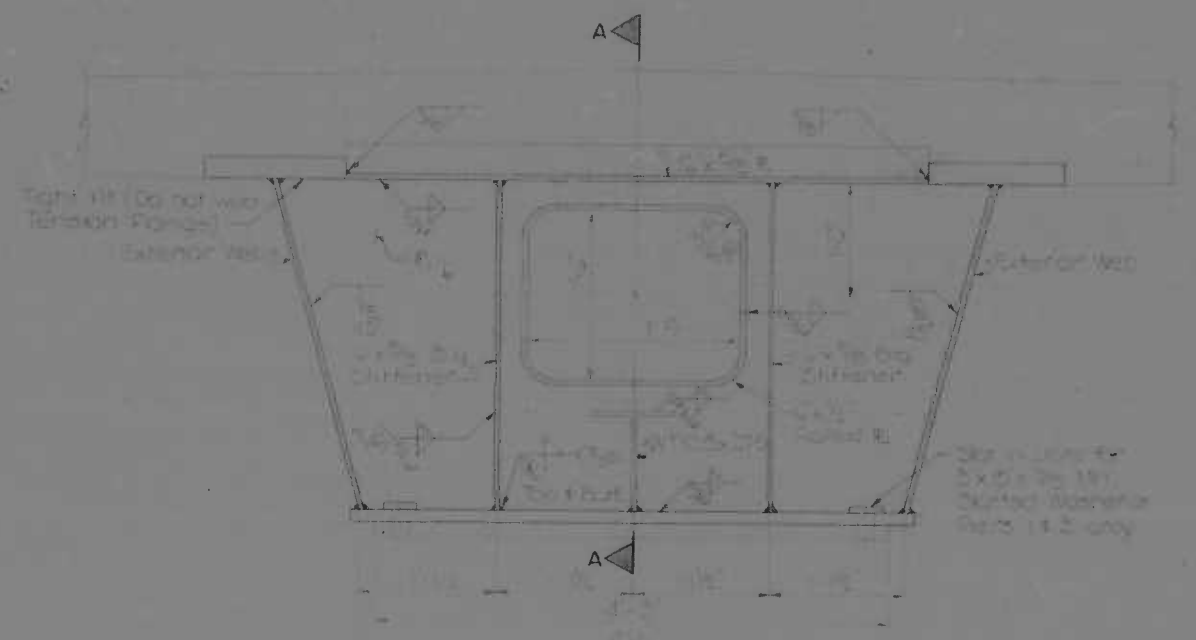
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(52)35	10	18



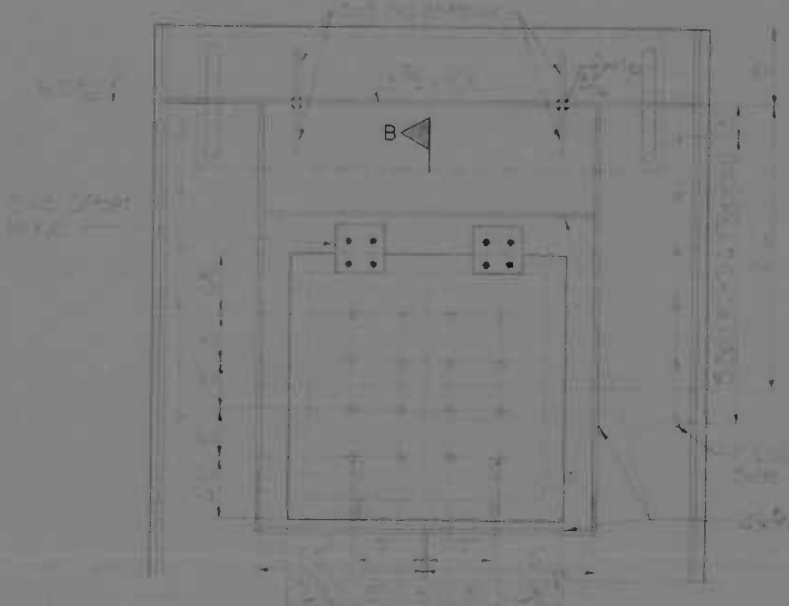
DIAPHRAGM D-4



DIAPHRAGM D-1



DIAPHRAGM D-3

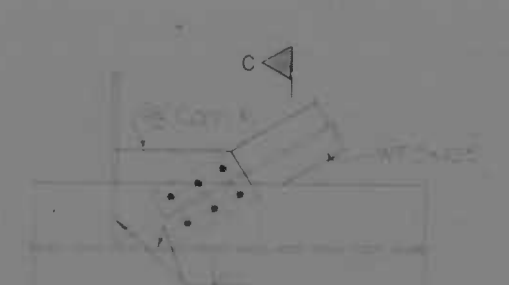


PLAN-DIAPHRAGM D-4

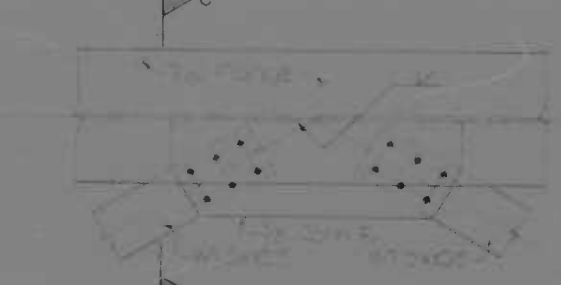


SECTION B-B

LADDER SUPPORT DETAIL



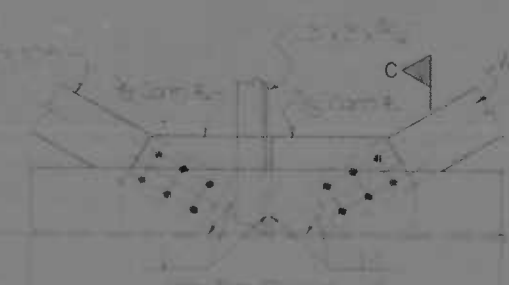
DETAIL 'X'



DETAIL 'Y'



SECTION A-A

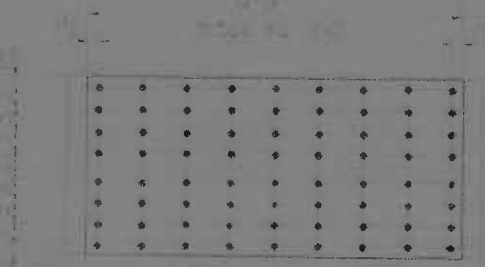


DETAIL 'Z'

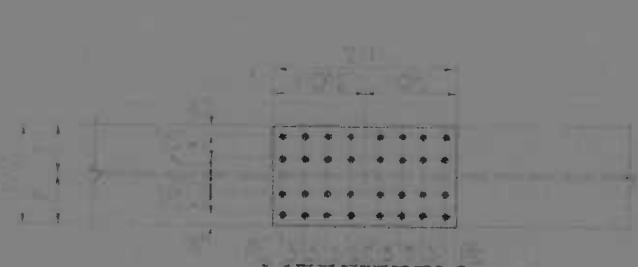


SECTION C-C

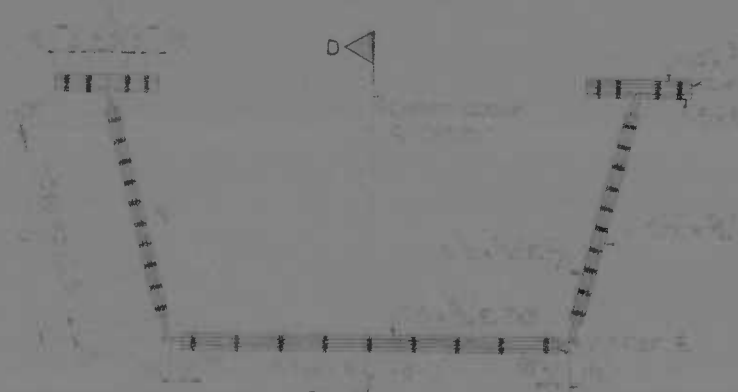
LATERAL BRACING DETAILS



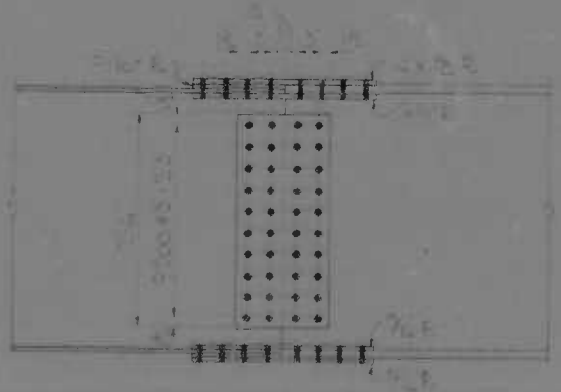
BOTTOM PLATE



TOP PLATE



TRANSVERSE SECTION THRU SPLICE



SECTION D-D

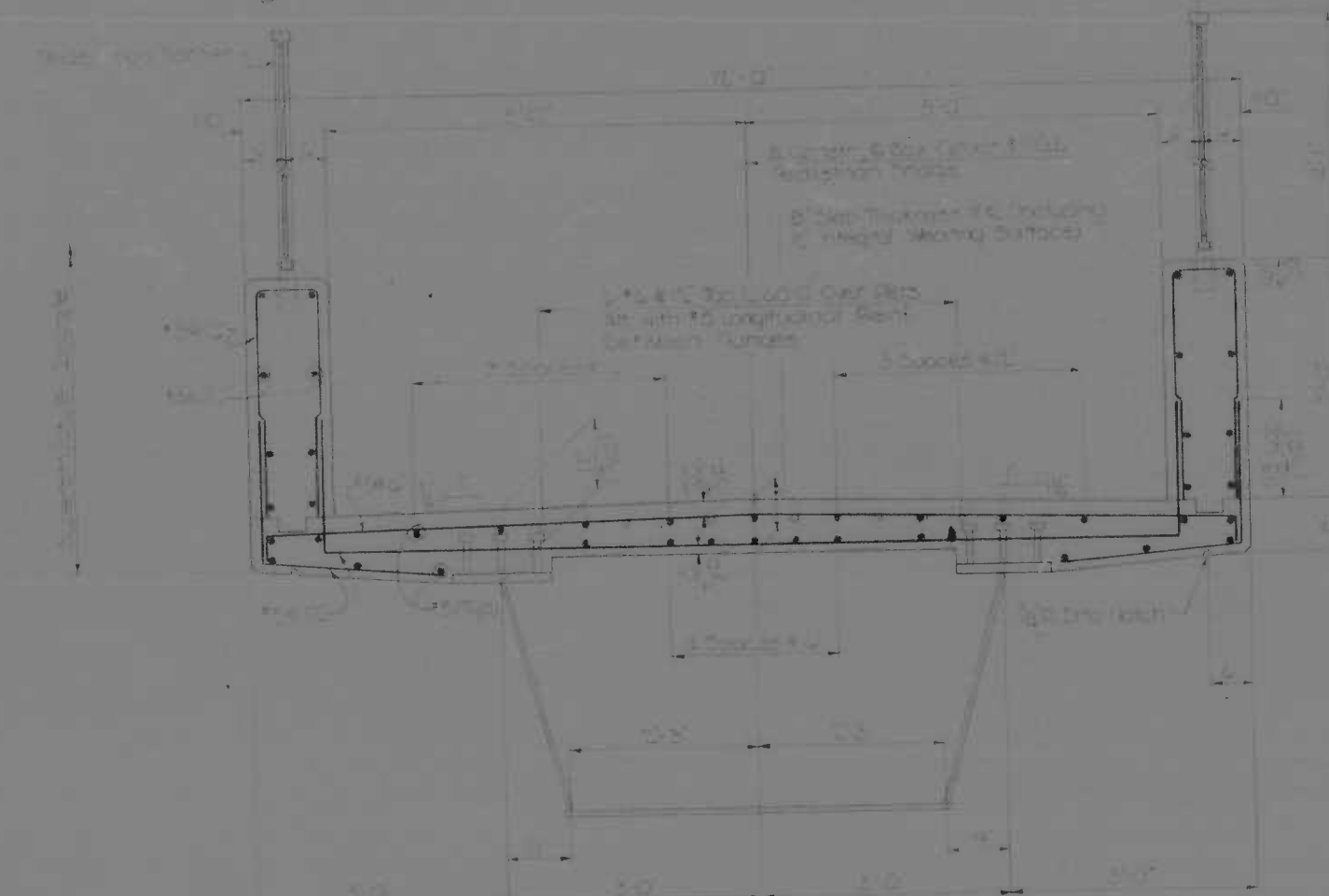
FIELD SPLICE DETAILS

NOTE: All connections to be made with 5/8" high strength bolts for additional info see Draw No. 55

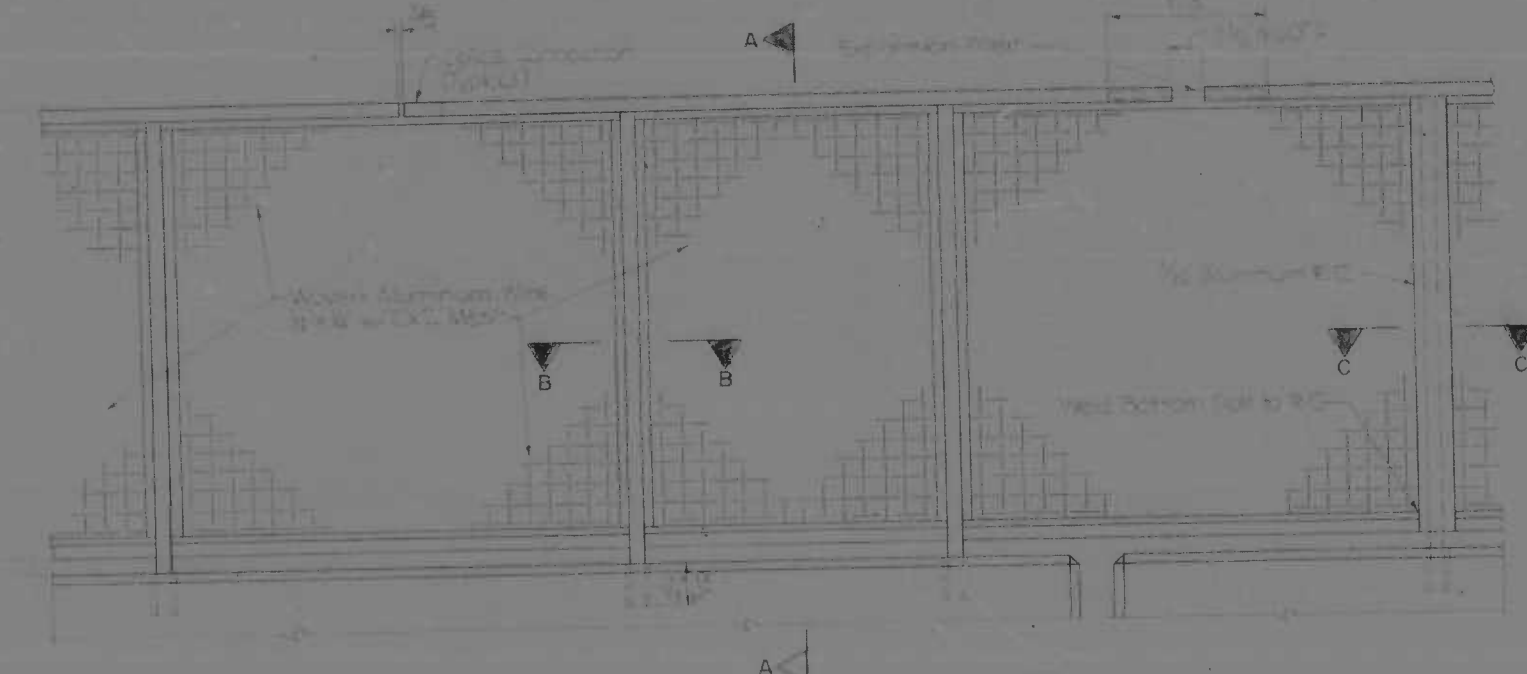
REVISIONS: 1. Change from 5/8" to 3/4" high strength bolts for additional info see Draw No. 55

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	&	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95		DIAPHRAGM AND LATERAL BRACING DETAILS
		SCALE: AS SHOWN	DATE: MAY 11, 1973	DRAWN BY: L.M.W. TRACED BY: L.M.W. F.P. NO.: I-95-4(52)35 S.P.C. NO.: BC 245-49-85 BALTO. CITY NO. 2140
				DES. BY: J.H. CHK. BY: D.M.P. SHEET NO. 10 of 18

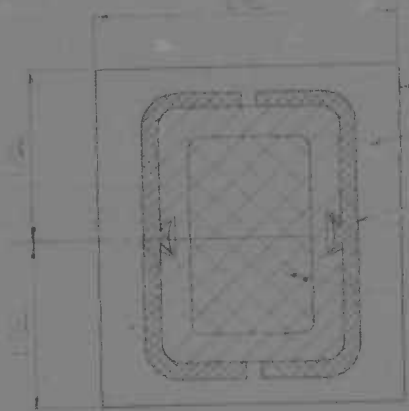
REV. NO.	DATE	BY	CHKD. BY
2	MD. I 95-4(52)35	11	18



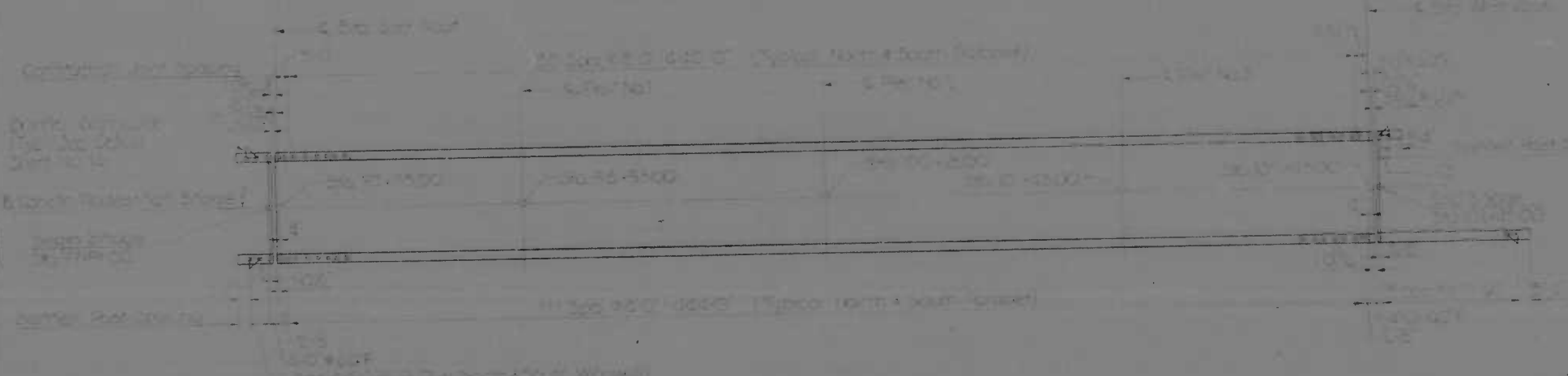
TYPICAL DECK SECTION



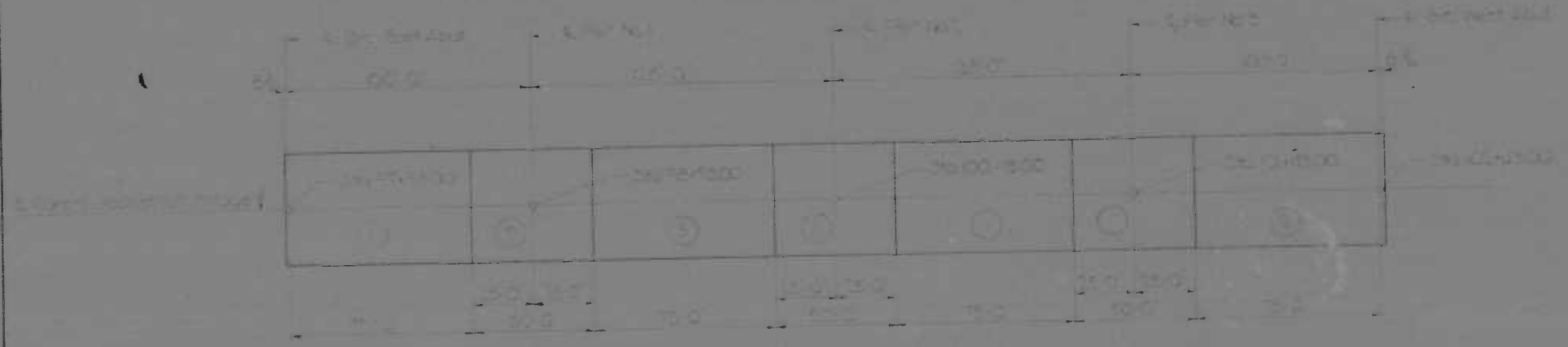
TYPICAL ELEVATION OF PEDESTRIAN BARRIER



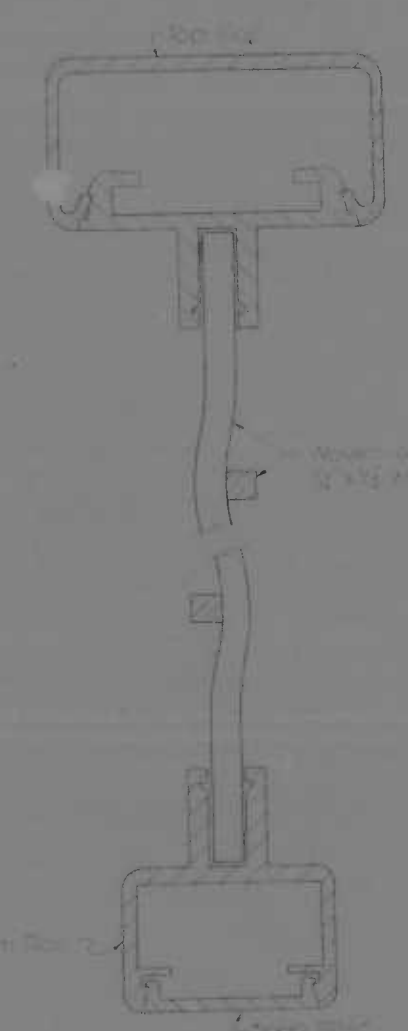
POST HOLE PLAN



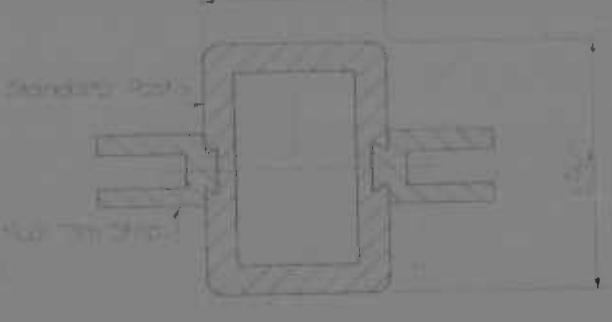
PARAPET CONTRACTION JOINT AND PEDESTRIAN BARRIER SPACING



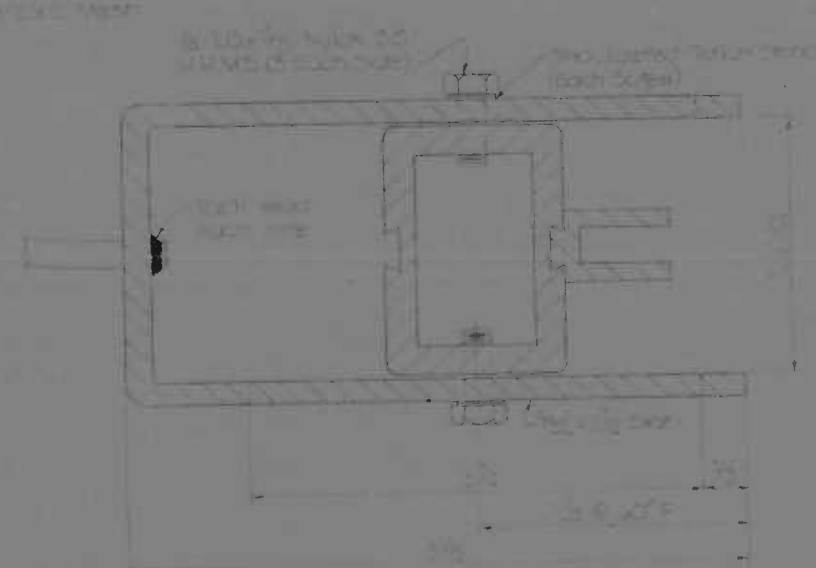
DECK POURING SEQUENCE DIAGRAM



SECTION A-A



SECTION B-B



SECTION C-C



TYPICAL END POST

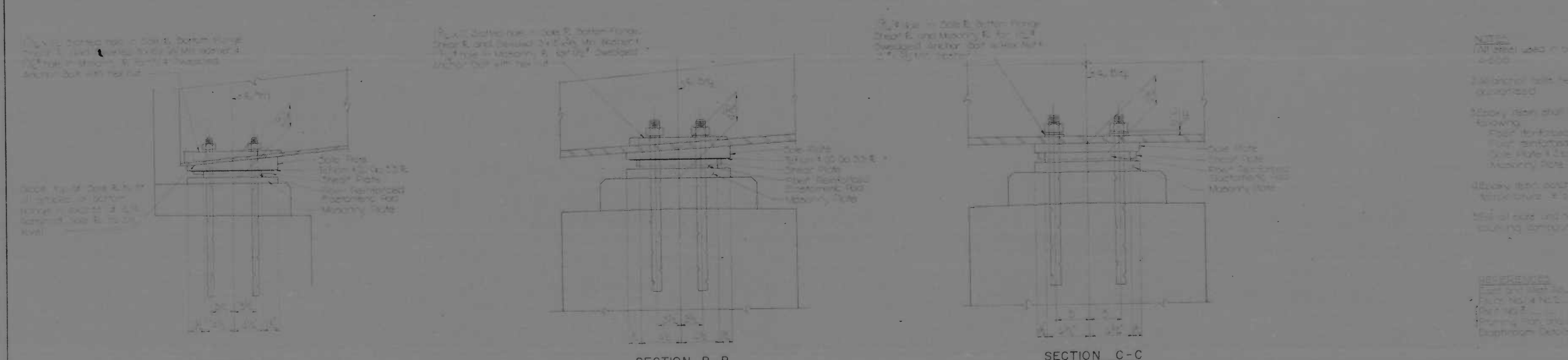
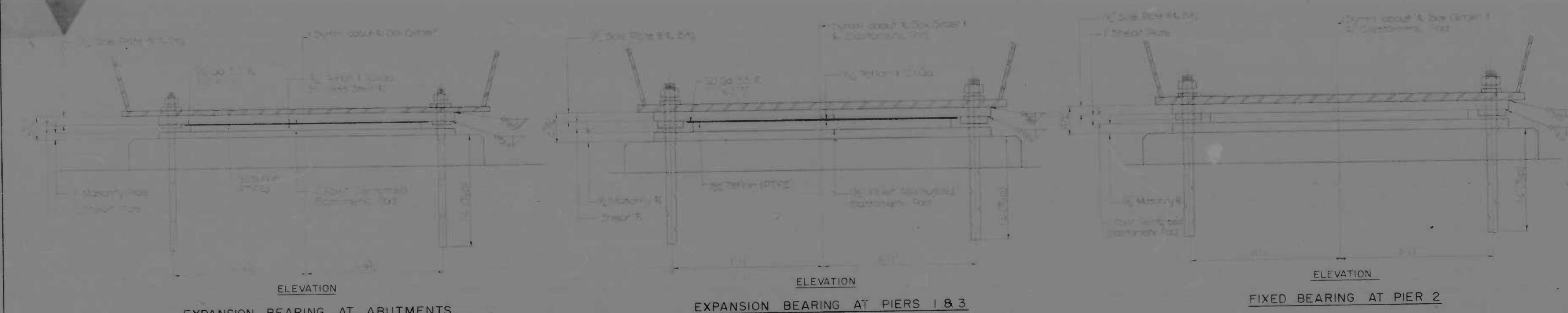
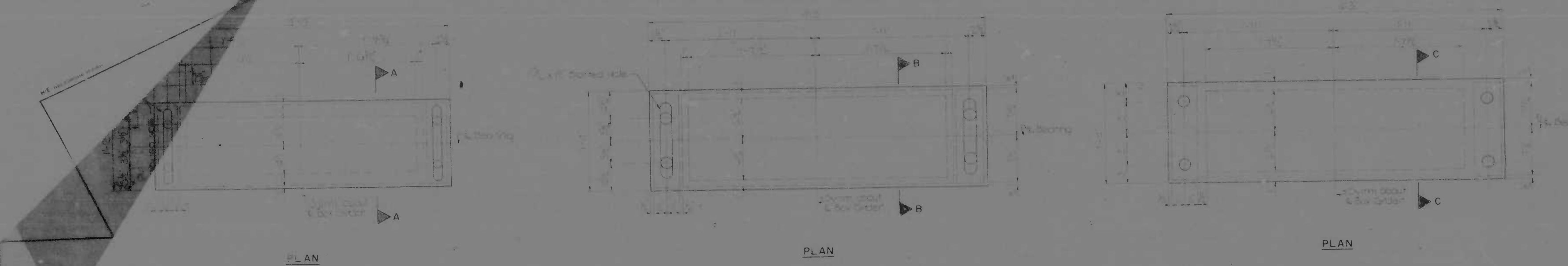
**Notes:**

- The pouring sequence for bridge deck shall be in the following order: (1) concrete deck, (2) concrete barrier base, (3) concrete barrier post, (4) concrete barrier top rail, (5) concrete barrier base, (6) concrete barrier post, (7) concrete barrier top rail.
- The contractor shall be responsible for the proper placement of the concrete deck in the construction responsibility to which the contractor is required to perform the work of the contractor. The contractor shall be responsible for the proper placement of the concrete deck in the construction responsibility to which the contractor is required to perform the work of the contractor.
- The contractor shall be responsible for the proper placement of the concrete deck in the construction responsibility to which the contractor is required to perform the work of the contractor.
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- The contractor shall be responsible for the proper placement of the concrete deck in the construction responsibility to which the contractor is required to perform the work of the contractor.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KHOERLE, BENDER, STONE & ASSOC., INC. AND MA'Z, CHUNG & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	MOORE'S RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 TYPICAL DECK SECTION & PED BARRIER DET.	DRAWN BY: L.M.W. DES. BY: J.H. CHECKED BY: L.M.W. CHN. BY: O.M.P. P.A.P. NO. 1-95-4(52)35 S.R.C. NO. BC 246-49-815 BALTO. CITY NO. 2140
		SCALE AS SHOWN	DATE: MAY 11, 1973



DES. YEAR	STATE	FED. AID DIST. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(52)35	12	18



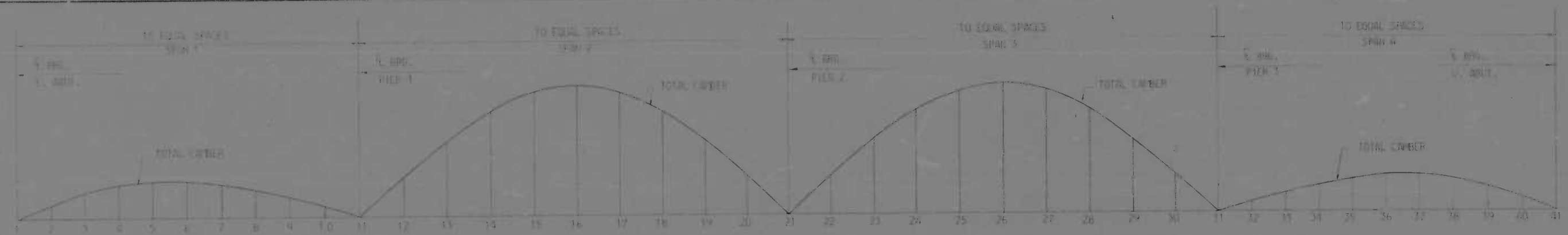
Notes:  
 (1) All steel used in bearing shall conform to ASTM Specification A-500.  
 (2) All steel used in bearing shall be hot rolled and quenched.  
 (3) Masonry shall be placed on full contact with the following:  
 - Steel Reinforced Concrete Deck & Masonry Deck  
 - Steel Reinforced Concrete Deck & Teflon Plate  
 - Steel Plate & Masonry Deck Plate  
 - Masonry Deck & Concrete Deck  
 (4) Every steel bearing component shall be used in accordance with design.  
 (5) All steel used shall be galvanized or painted with a rust-inhibiting compound or zinc rich primer.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 BEARING DETAILS	DRAWN BY: M.W. DES. BY: J.H. CHECKED BY: M.W. CHK. BY: D.M.P. S&C NO.: I-95-4(52)35 S&C NO.: BC 246-49-35 BALTO. CITY NO. 2140

SCALE: 1/2" = 1'-0" DATE: MAY 31, 1972

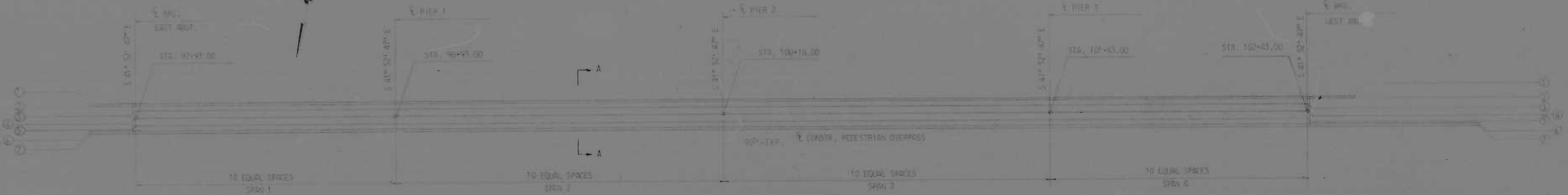
SHEET NO. 12 OF 18

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(52)35	13	18

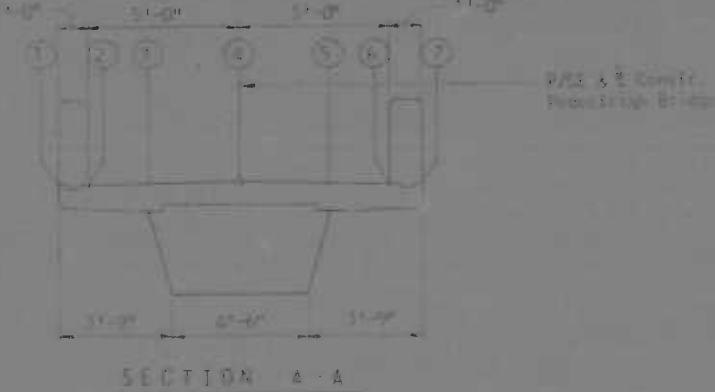


CAMBER DIAGRAM  
SCALE: HORZ. 1" = 20'-0"  
VERT. 1/4" = 1'-0"

DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	
Δ STEEL	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Δ CONCRETE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Δ S. G. L.	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Δ GEOMETRIC	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
TOTAL CAMBER	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00



DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41
STATION	32+00.00	32+05.00	32+10.00	32+15.00	32+20.00	32+25.00	32+30.00	32+35.00	32+40.00	32+45.00	32+50.00	32+55.00	32+60.00	32+65.00	32+70.00	32+75.00	32+80.00	32+85.00	32+90.00	32+95.00	33+00.00	33+05.00	33+10.00	33+15.00	33+20.00	33+25.00	33+30.00	33+35.00	33+40.00	33+45.00	33+50.00	33+55.00	33+60.00	33+65.00	33+70.00	33+75.00	33+80.00	33+85.00	33+90.00	33+95.00	34+00.00
1 SOUTH KEY LINE	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
2 SOUTH KEY LINE	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
3 FLOOR SLAB	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
4 PAV. & CURB	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
5 TOP OF CURB	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
6 NORTH KEY LINE	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40
7 SOUTH KEY LINE	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40	41.40

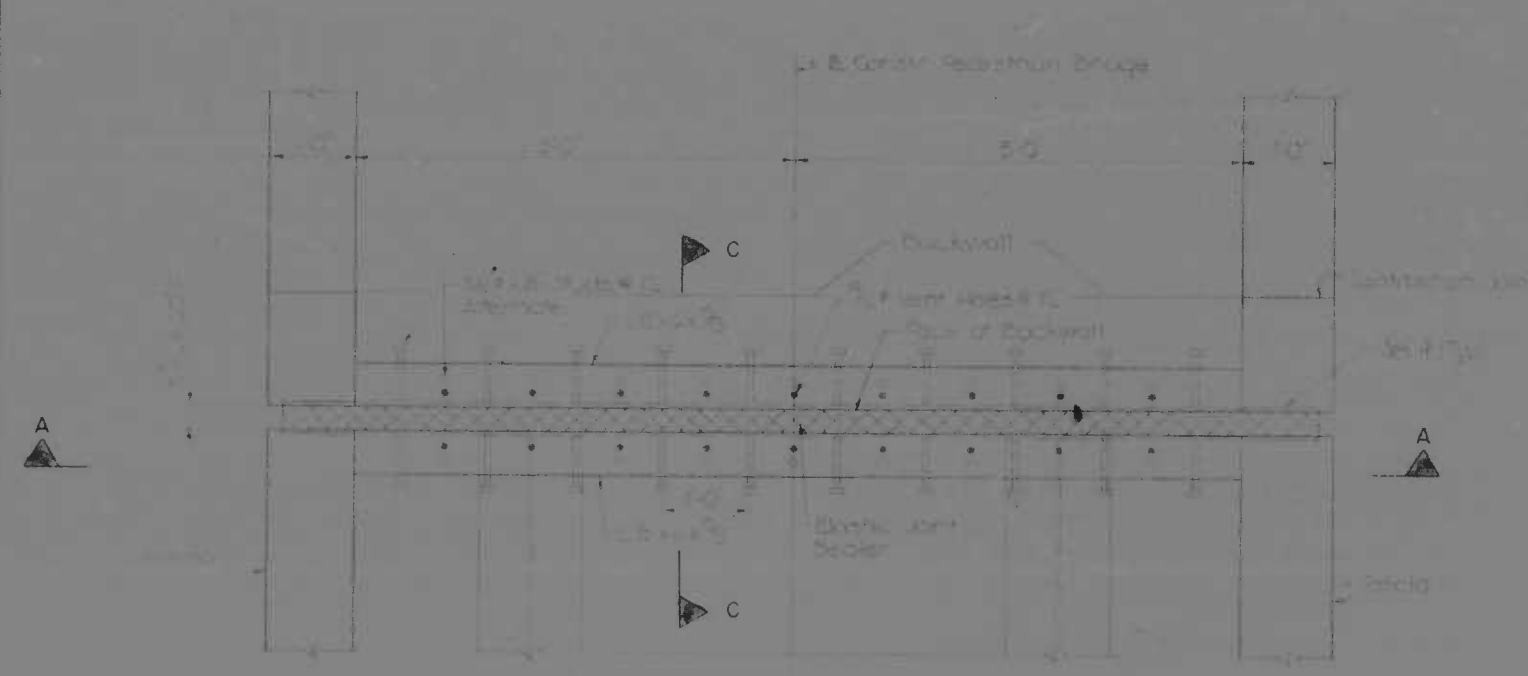


REFERENCES  
 General Plan and Elevation  
 Existing Plan  
 Vertical Scale Stationing

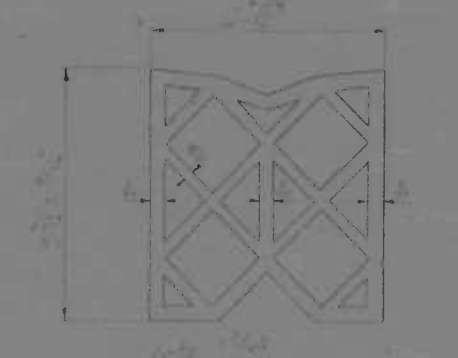
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS MOOREC RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 CAMBER DIAGRAM & SUPERSTRUCTURE ELEVATIONS SCALE AS SHOWN	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: [ ] DEC BY: [ ] TRACED BY: [ ] F.A.P. NO. I-95-4(52)35 S.R.C. NO. BC 246-49-815 BALTO. CITY NO. 2140
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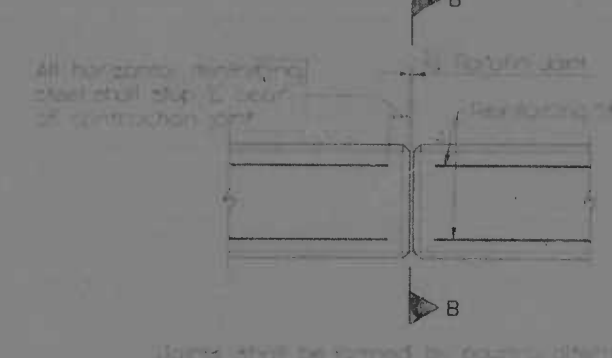
REV. NO.	DATE	BY	CHKD. BY	APP. BY
2	MD. T-95-4(52)35			14 18



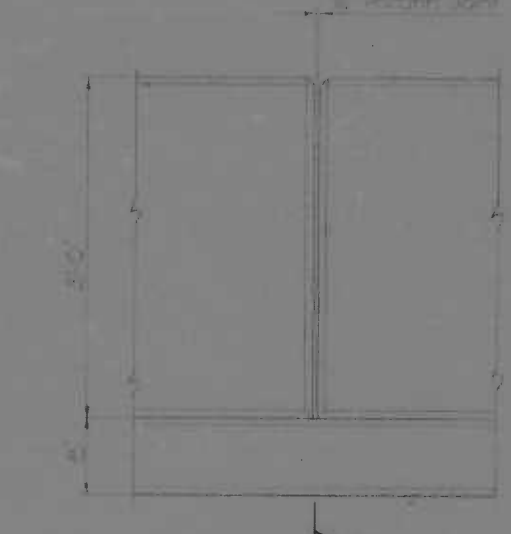
PLAN



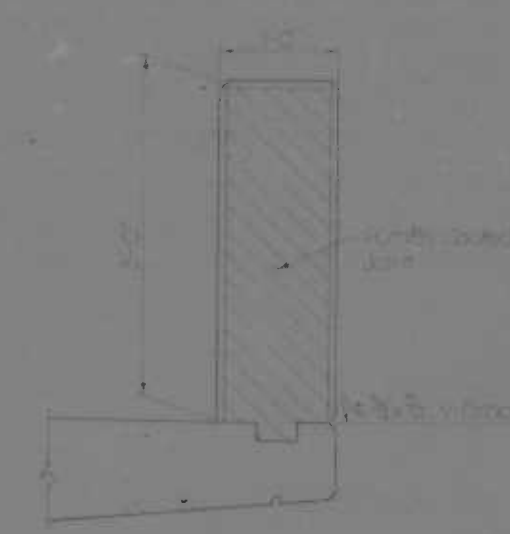
ELASTIC JOINT SEALER



PLAN

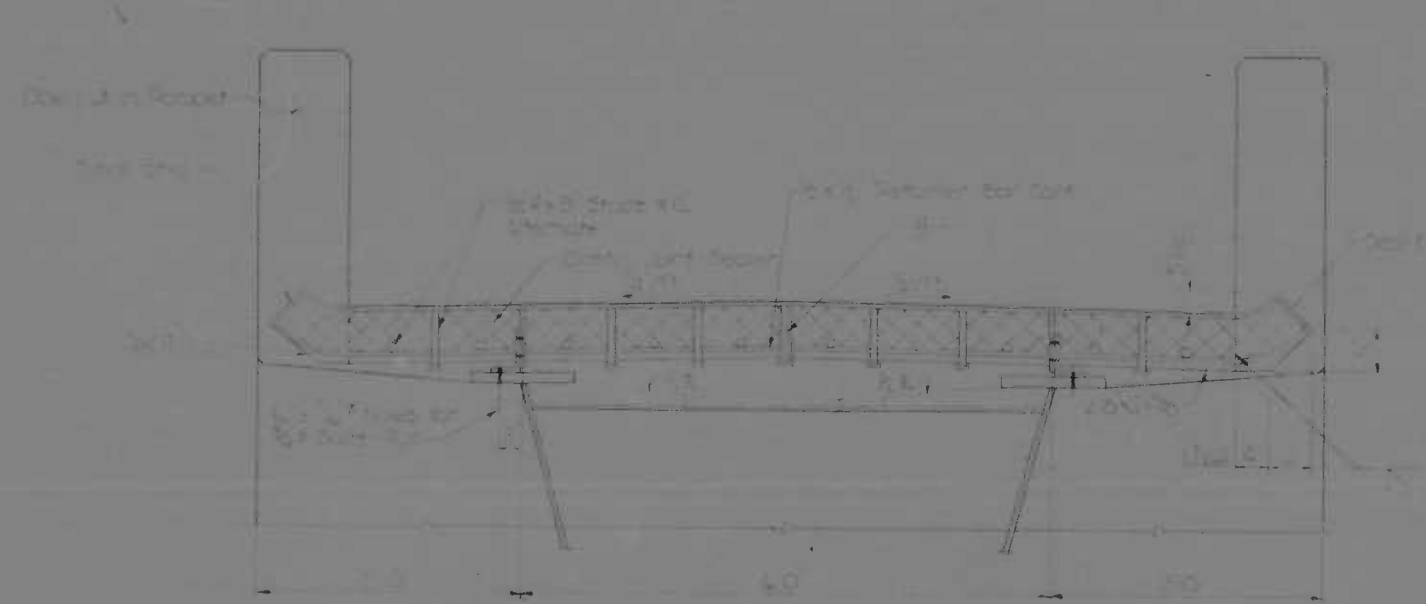


ELEVATION



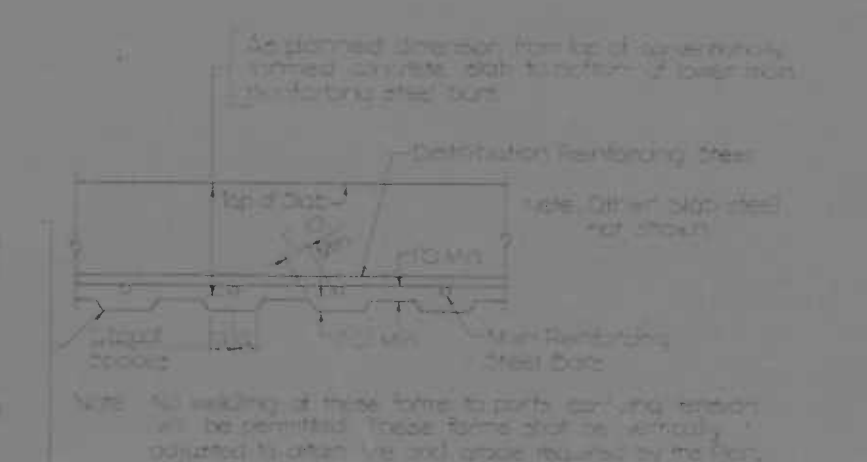
SECTION B-B

PARAPET CONTRACTION JOINT DETAILS

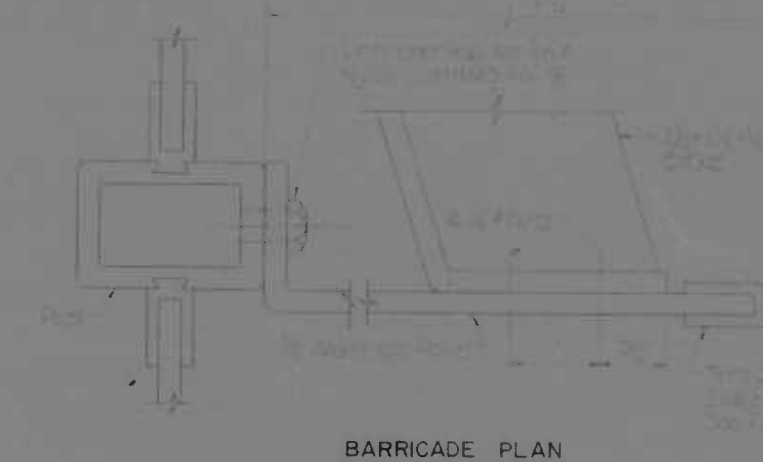


SECTION A-A

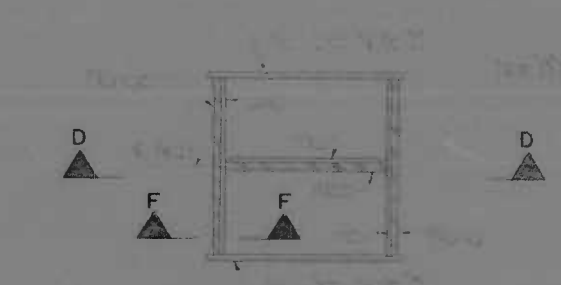
Steel forms which remain in place shall be removed within 24 hours of casting. The top edge of the concrete shall be finished with a float and trowel. The surface shall be kept moist and covered with a curing compound until the concrete has attained a minimum of 70% strength.



STEEL FORMS TO REMAIN IN PLACE



BARRICADE PLAN



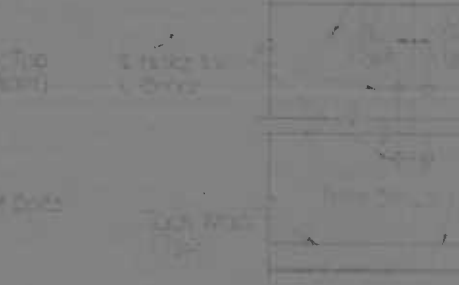
PLAN



SECTION F-F

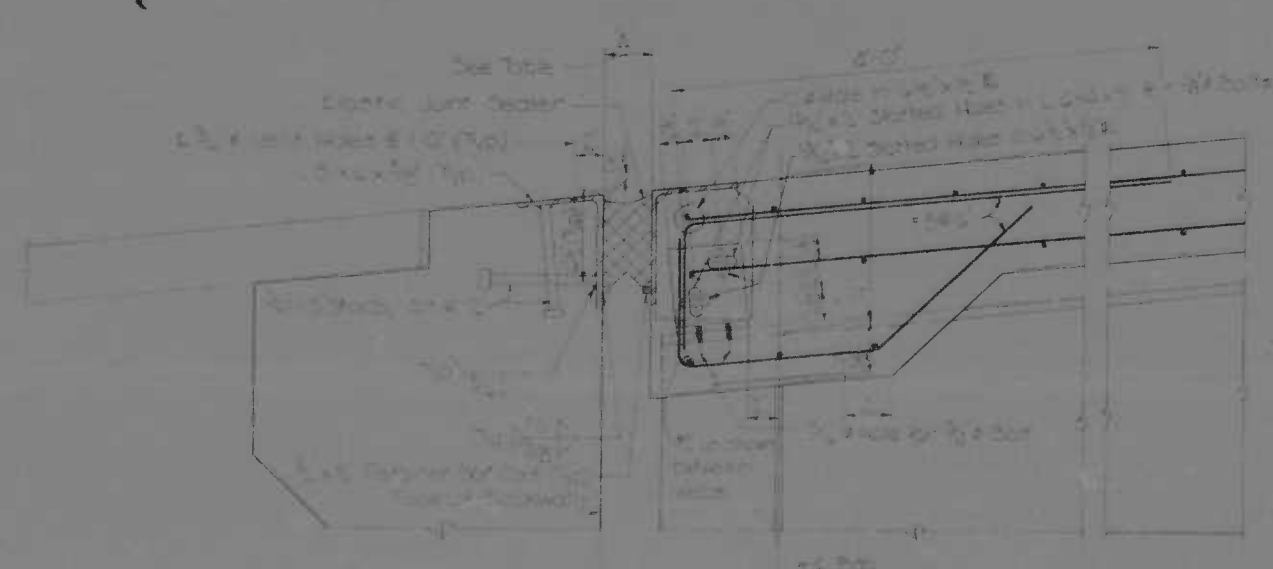


BRACE DETAILS



DETAIL A-A

BARRICADE DETAILS

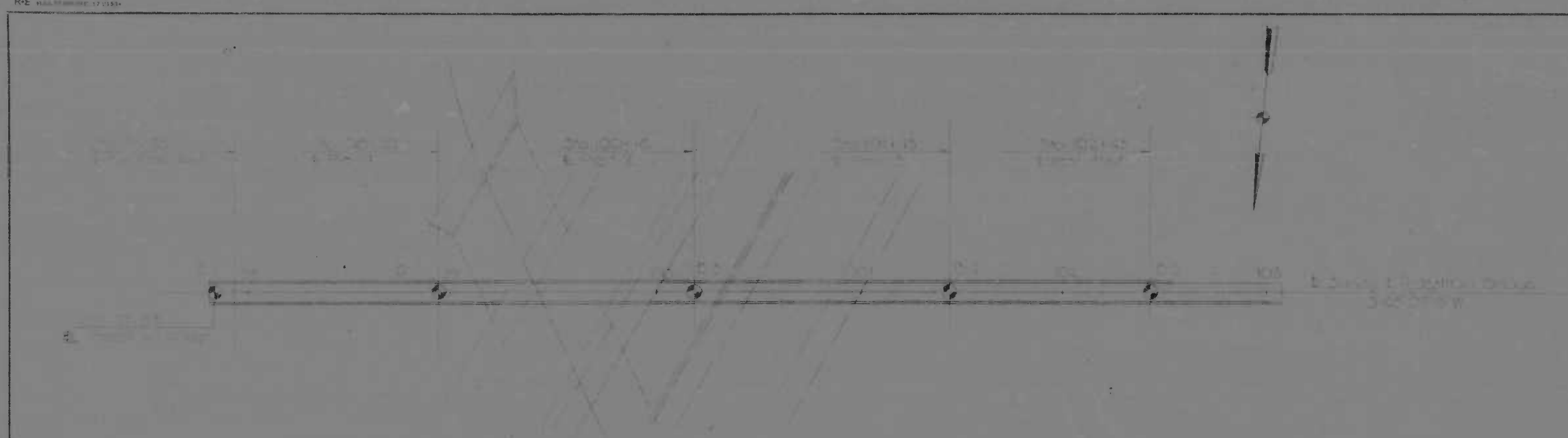


SECTION C-C

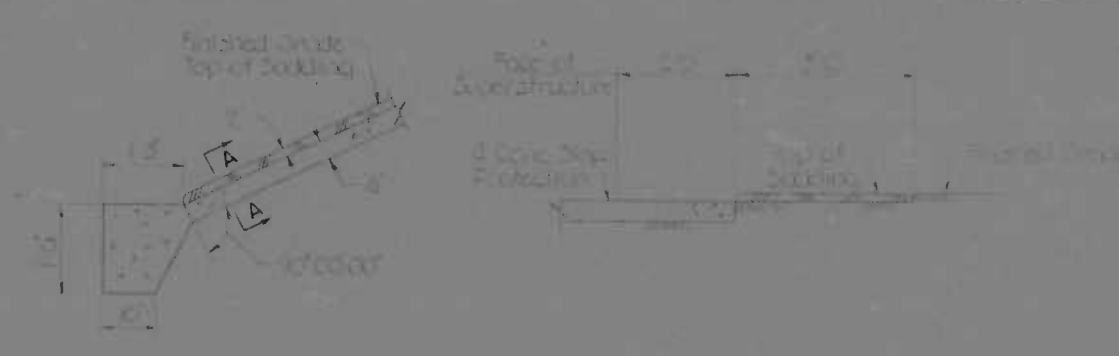
PREFORMED COMPRESSION SEAL EXPANSION DAM AT ABUTMENTS

NO.	DESCRIPTION	DATE
1	As shown	
2	As shown	
3	As shown	
4	As shown	
5	As shown	
6	As shown	
7	As shown	
8	As shown	
9	As shown	
10	As shown	

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 EXPANSION DAM AND MISCELLANEOUS DETAILS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		SCALE: AS SHOWN	DATE: MAY 11, 1973
		DRAWN BY: L.M.W.	DES. BY: J.H.
		TRACED BY: L.M.W.	CHK. BY: G.M.P.
		F.P.R. NO.: I-95-4(52)35	SHEET NO.: 14 OF 18
		S.P.C. NO.: BR 146-50-B15	BALTS. CITY NO. 2400



BORING LOCATION PLAN



TOE WALL DETAILS

SECTION A-A

SLOPE PROTECTION DETAILS

**BORING NO 1**  
Sta 97 + 83 CONSTR  
PEDESTRIAN BRIDGE

**BORING NO 2**  
Sta 98 + 93 CONSTR  
PEDESTRIAN BRIDGE

**BORING NO 3**  
Sta 100 + 18 CONSTR  
PEDESTRIAN BRIDGE

**BORING NO 4**  
Sta 101 + 43 CONSTR  
PEDESTRIAN BRIDGE

**BORING NO 5**  
Sta 102 + 43 CONSTR  
PEDESTRIAN BRIDGE



BORING LOGS  
Vert. 1" = 10'-0"

Notes:  
1. Boring locations are shown on plan view.  
2. Depth of boring is shown on log.  
3. Soil description is shown on log.  
4. Number of logs is shown on log.  
5. Date of boring is shown on log.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 BORING DATA	DRAWN BY: MCF CHECKED BY: MSF F.A.P. NO.: 1-95-45035 S.R.C. NO.: RC 248-49-80 BALTO. CITY NO.: 2140
		SCALE: AS SHOWN	DATE: MAY 11, 1973
			DWG. BY: J.H. CHK. BY: D.M.P. SHEET NO.: 15 OF 18



EXISTING CONTOUR  
 PROPOSED BY OTHERS CONTOUR  
 PROPOSED CONTOUR  
 PROPOSED PARK BENCH  
 PROPOSED PLAYER BENCH

STA 21+12 @ 2nd ST LT  
 16" 4" R.C.P. CL 4  
 1' STD B-48 HEADWALL

STA 27+74 PED BRIDGE  
 6" TRENCH DRAIN WITH GRATED COVER  
 28" 4" C.I. PIPE  
 1' 4" C.I.P. ELBOW  
 16" 3/4" COPPER WATER SERVICE

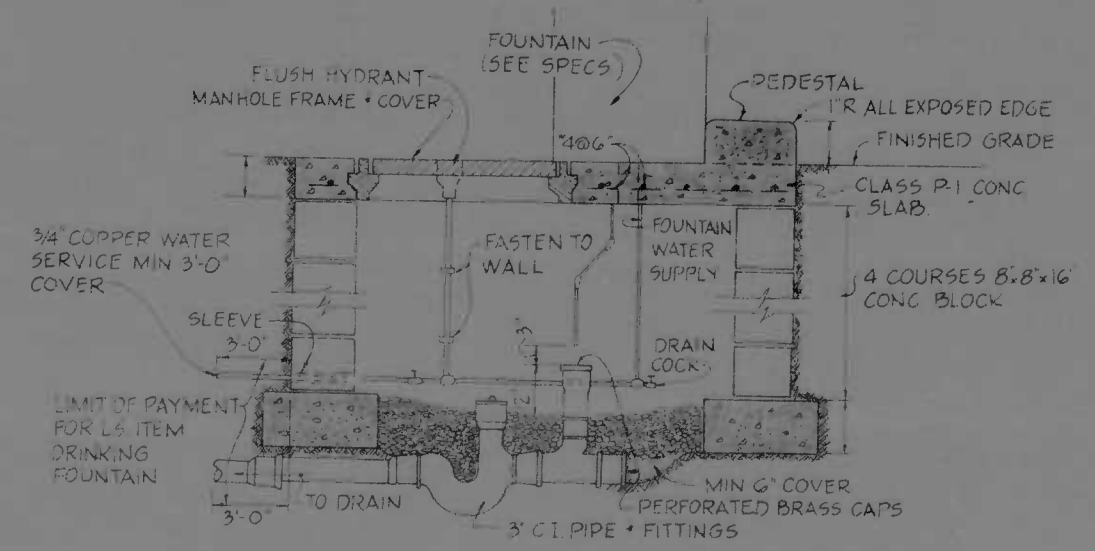
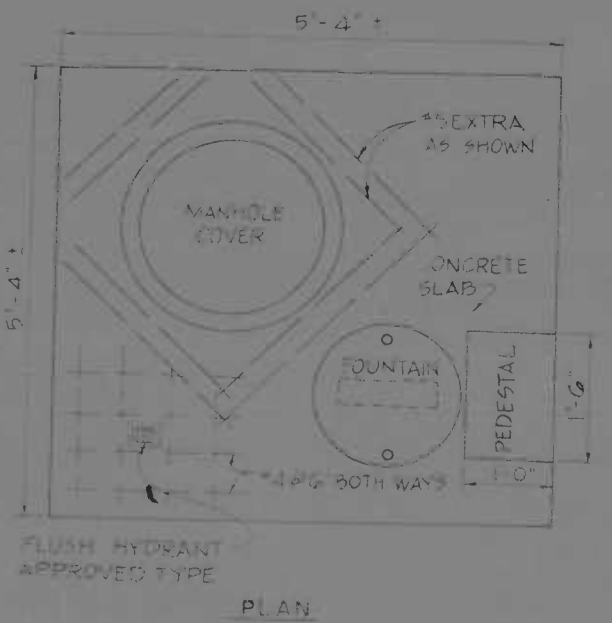
STA 26+70 PED BRIDGE RT+LT  
 1" DRINKING FOUNTAIN  
 42" 3" C.I. PIPE

CONCRETE SIDE DITCH  
 STA 23+50 @ 2nd ST LT  
 STA 25+60 TO 28+10 PED BRIDGE  
 W=15' D=10'

STA 17+70 TO 23+70 LT @ 2nd ST  
 REMOVE AND RESET EXIST FENCE

- 1 EDOR SEAL COAT
- 2 1" BITUMINOUS CONC SURFACE COURSE BAND ST
- 3 PRIME COAT
- 4 1" BITUMINOUS CONC LEVELING COURSE BAND SN
- 5 4" CR-G CRUSHER RUN AGGREGATE BASE COURSE

PAVING DETAIL - BASKETBALL COURT

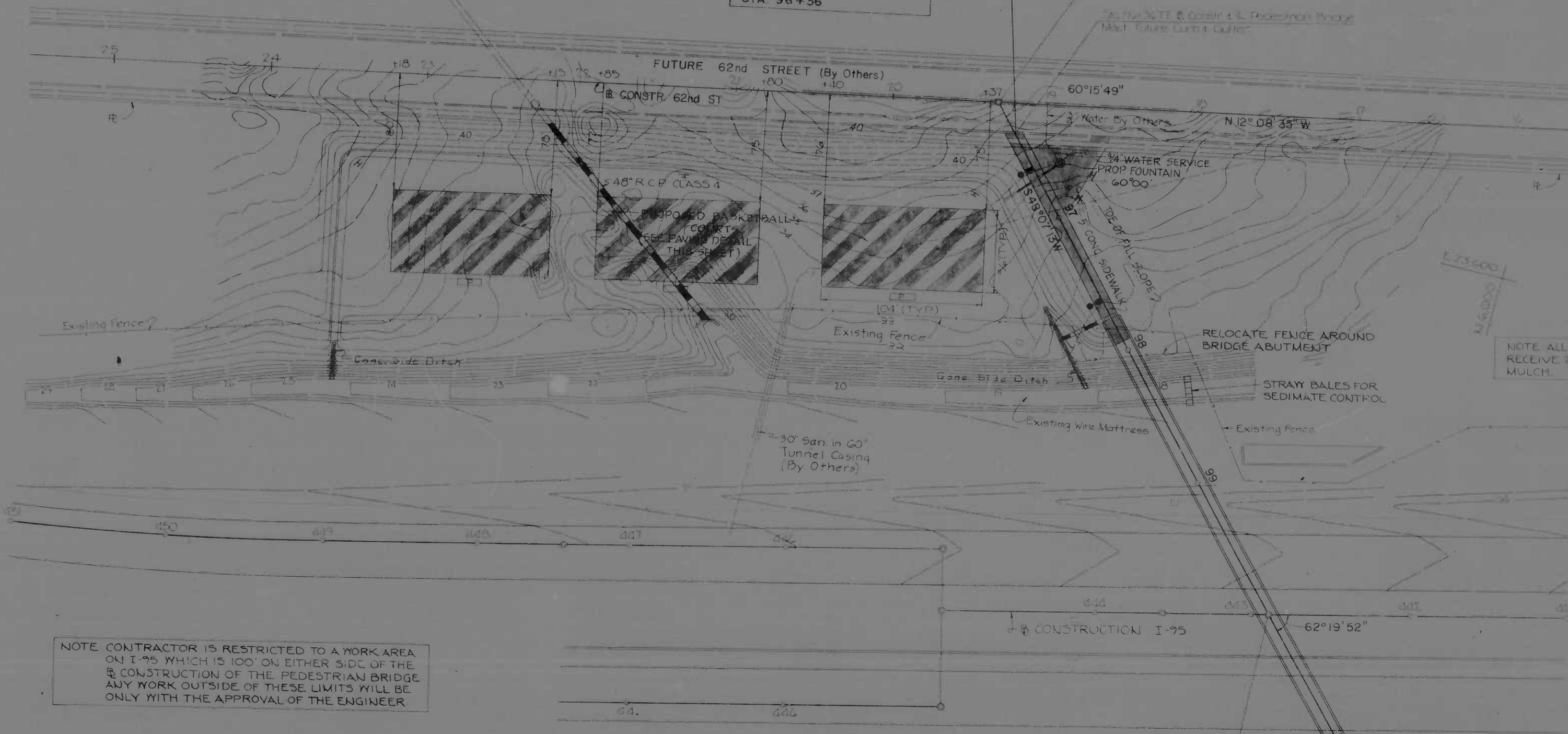


DRINKING FOUNTAIN

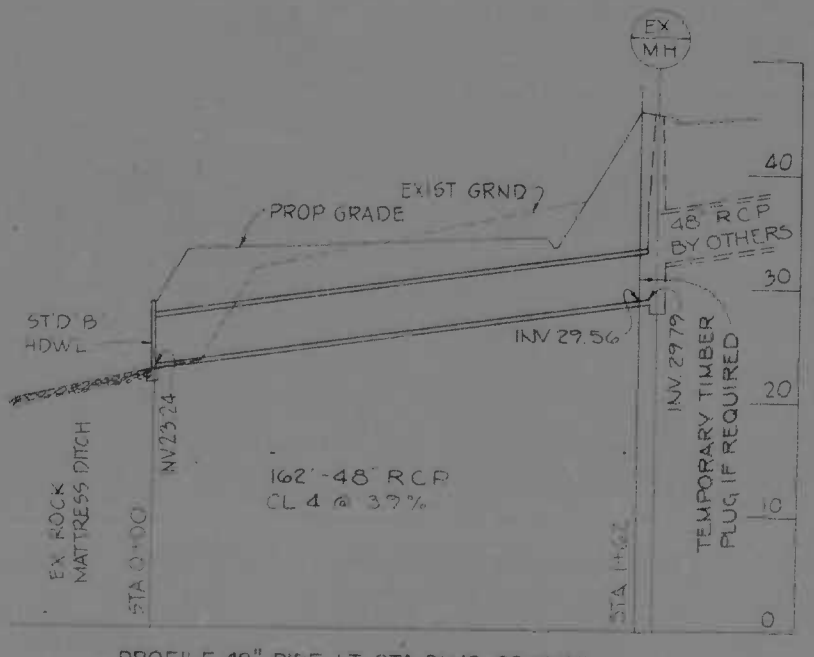
NOTE BASKETBALL COURT LT 62nd STREET  
 STA 19+27 TO STA 20+40 NOT TO BE  
 CONSTRUCTED UNTIL PROPOSED  
 30" SANITARY SEWER BY OTHERS  
 IS IN PLACE

LIMIT OF WORK  
 CONTRACT BC 246-49-815  
 F.A.P. NO. 1-95-4(52)35  
 STA 96+36

STA 96+13.43 @ SURVEY & CONSTR. PEDESTRIAN BRIDGE  
 STA 19+34.92 @ 62nd STREET



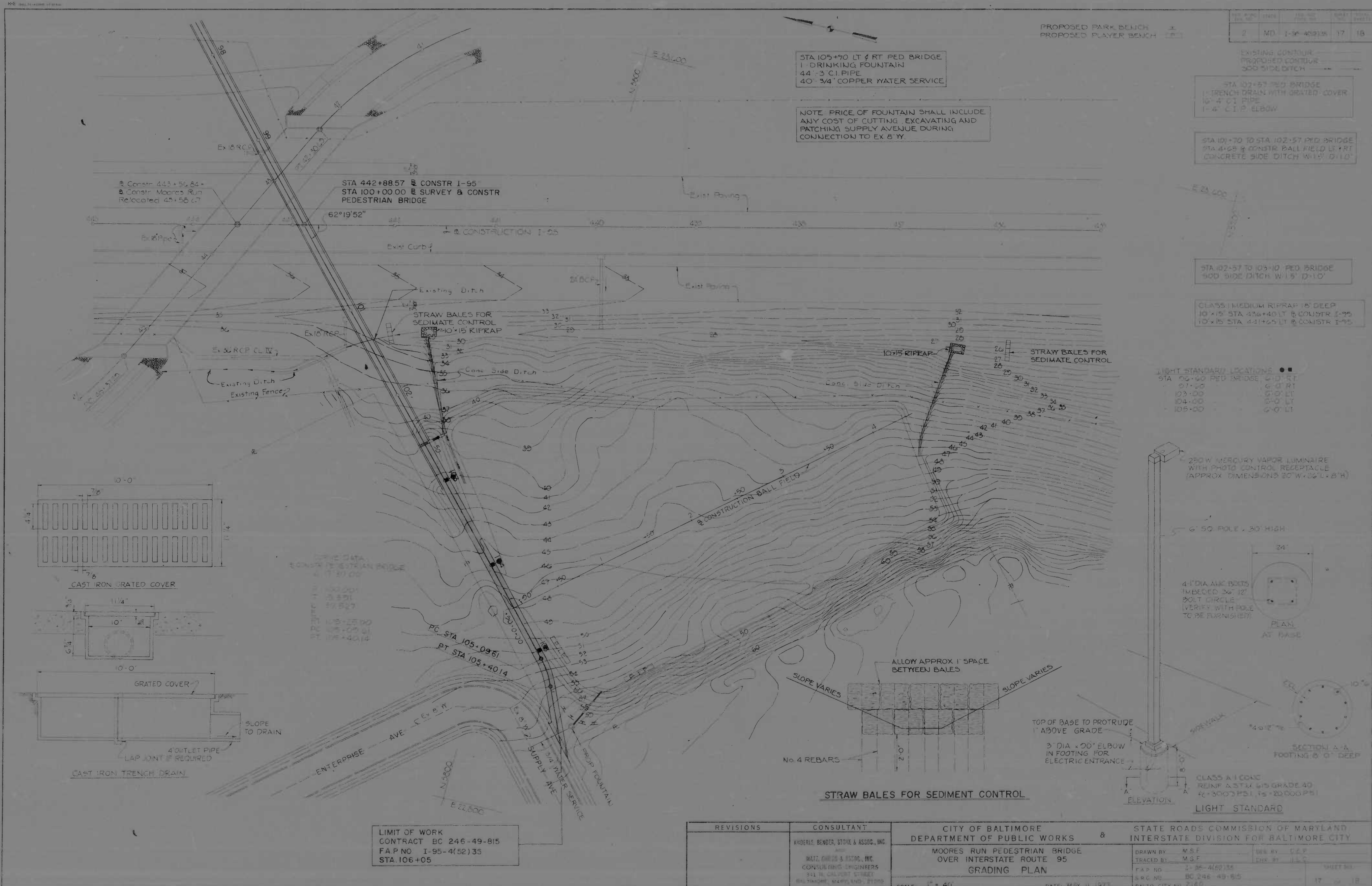
NOTE CONTRACTOR IS RESTRICTED TO A WORK AREA  
 ON I-95 WHICH IS 100' ON EITHER SIDE OF THE  
 CONSTRUCTION OF THE PEDESTRIAN BRIDGE  
 ANY WORK OUTSIDE OF THESE LIMITS WILL BE  
 ONLY WITH THE APPROVAL OF THE ENGINEER



PROFILE 48" PIPE LT STA 21+12 @ 62nd ST.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	MOORE, BENDER, STINE & ASSOC., INC. 917 E. CALVERT STREET BALTIMORE, MARYLAND 21202	MOORE RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 GRADING PLAN		DRAWN BY: L.M.W.	DES. BY: C.E.P.
				TRACED BY: L.M.W.	CHE. BY: A.L.C.
		SCALE: 1"=40'		F.A.P. NO. 1-95-4(52)35	SHEET NO. 16
		DATE: MAY 11, 1973		S.R.C. NO. BC 246-49-815	
				BALTO. CITY NO. 2140	







# SUMMARY OF QUANTITIES

2 MD 1-95-40235 1B 1B

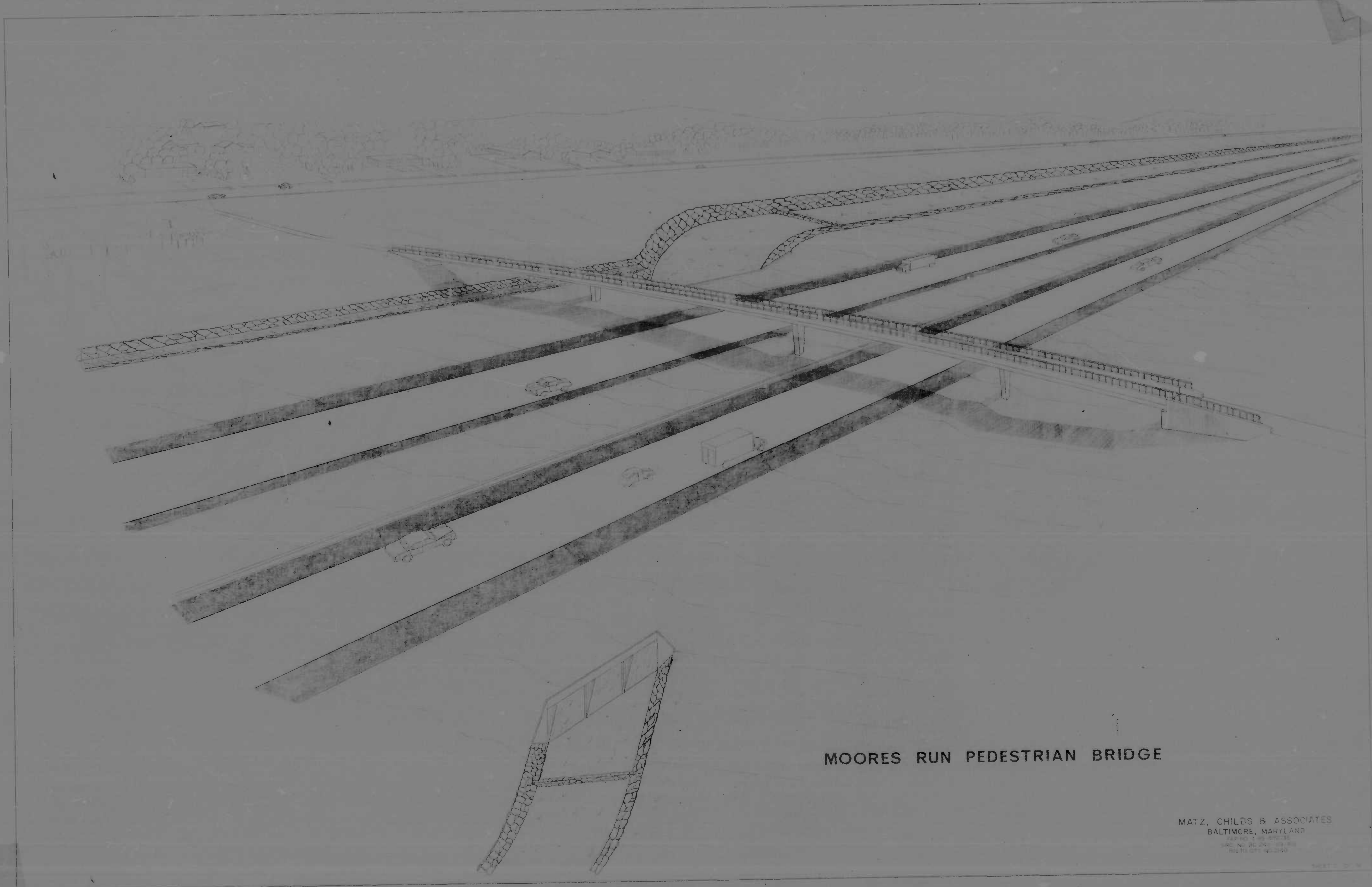
UNIT NO	PAY ITEMS	UNIT	EST QUANT	CONT QUANT	PROP QUANT	FINAL QUANT
<b>PRELIMINARY</b>						
101	CLEARING AND GRUBBING	L.S.	LUMP SUM		LUMP SUM	
102	ENGINEERS FACILITY	L.S.	LUMP SUM		LUMP SUM	
103	CONSTRUCTION STAKEOUT	L.S.	LUMP SUM		LUMP SUM	
104	MOBILIZATION	L.S.	LUMP SUM		LUMP SUM	
105	ON THE JOB TRAINING	HRC.	3000		3000	
<b>GRADING</b>						
201	CLASS 1 EXCAVATION	C.Y.	13000		13100	
202	CONTINGENT CLASS 1 EXCAVATION	C.Y.		5000	5000	
203	CLASS 2 EXCAVATION	C.Y.	190		210	
204	CONTINGENT BORROW EXCAVATION	C.Y.		5000	5000	
<b>DRAINAGE</b>						
301	48" RC PIPE CLASS 4	L.F.	162		162	
302	3" CAST IRON PIPE AND FITTINGS	L.F.	88		88	
303	4" CAST IRON PIPE AND FITTINGS	L.F.	50		50	
304	REMOVE OLD PIPE CULVERTS ANY SIZE	L.F.		50	50	
305	STANDARD B-48 HEADWALL	EA.	1		1	
306	6" PERFORATED CIRCULAR PIPE UNDERDRAIN	L.F.		500	500	
307	6" CIRCULAR PIPE UNDERDRAIN OUTLETS	L.F.		200	200	
308	5" CONCRETE GUTTER	S.Y.	215		215	
309	CAST IRON TRENCH DRAIN WITH GRATED COVER	EA.	2		2	
310	CLASS 1 MEDIUM RIPRAP	S.Y.	40		40	
311	STRAW BALES FOR SEDIMENT CONTROL	EA.	50		50	
<b>STRUCTURES</b>						
401	CLASS 3 EXCAVATION FOR STRUCTURES	C.Y.	330		330	
402	SUB FOUNDATION DRILLING	L.F.	10		10	
403	12" STEEL BEARING PILES AT 53' FURNISHED AND DRIVEN	L.F.	1300		1300	
404	12" STEEL BEARING TEST PILES AT 53' FURNISHED AND DRIVEN	L.F.	250		250	
405	12" STEEL BEARING PILE SPLICES	EA.	40		40	
406	CLASS "C" CONCRETE	C.Y.	20		20	
407	FOOTING CONCRETE	C.Y.	125		125	
408	SUBSTRUCTURE CONCRETE FOR PIERS	L.S.	LUMP SUM		LUMP SUM	
409	SUBSTRUCTURE CONCRETE FOR ABUTMENTS	L.S.	LUMP SUM		LUMP SUM	
410	SUPERSTRUCTURE CONCRETE FOR BRIDGE	L.S.	LUMP SUM		LUMP SUM	
411	CONTINGENT CONCRETE FOR BRIDGE	C.Y.	30		30	
412	FABRICATED STRUCTURAL STEEL (A-588)	L.S.	LUMP SUM		LUMP SUM	
413	SUBSTRUCTURE STEEL (A-588)	L.S.	LUMP SUM		LUMP SUM	
414	4" CONCRETE SLOPE PROTECTION	S.Y.	40		40	
415	PEDESTRIAN BARRIER	L.S.	LUMP SUM		LUMP SUM	
416	EPOXY PROTECTIVE COATING FOR PARAPETS	L.S.	LUMP SUM		LUMP SUM	
417	STEEL STUD SHEAR DEVELOPERS FOR BRIDGE	L.S.	LUMP SUM		LUMP SUM	
<b>PAVEMENT</b>						
501	4" CR-6 CRUSHER RUN BASE COURSE	S.Y.	3091		3100	
502	BITUMINOUS MATERIAL FOR PRIME COAT	GAL.	380		380	
503	1" BITUMINOUS CONCRETE (SPEC. B USING BAND ST. STONE)	TON	108		110	
504	1" BITUMINOUS CONCRETE (SPEC. B USING BAND SN. STONE)	TON	108		110	
505	COLD SEAL COAT	S.Y.	1941		1950	

UNIT NO	PAY ITEMS	UNIT	EST QUANT	CONT QUANT	PROP QUANT	FINAL QUANT
<b>SHOULDERS</b>						
601	5" CONCRETE SIDEWALK	S.F.	6,350		6,400	
602	REMOVE & RESET EXISTING FENCE	L.F.	50		50	
<b>LANDSCAPING</b>						
701	TOPSOIL FURNISHED AND PLACED 2 IN. DEPTH	S.Y.	20,000		20,000	
702	SEEDING AND MULCHING	S.Y.	20,000		20,000	
703	SOLID SODDING	S.Y.	40		40	
<b>UTILITIES</b>						
801	LIGHTING BASE	EA.	5		5	
802	30 FT ANCHOR BASE STANDARD	EA.	5		5	
803	250 WATT LUMINAIR BUILT IN BALLAST & AUXILIARY EQUIPMENT	EA.	5		5	
804	2.4" WATER SERVICE	L.F.	56		56	
805	DRINKING FOUNTAIN	EA.	2		2	
806	PARK BENCHES	EA.	9		9	
807	PLAYER BENCHES	EA.	7		7	

SUMMARY		OF		EARTHWORK	
CLASS "1" EXCAVATION		CLASS "2" EXCAVATION			
CUT	13,000 C.Y.	FROM CROSS SECTIONS	180 C.Y.		
		TOTAL CLASS "2" EXCAVATION	180 C.Y.		
		LOSS DUE TO HANDLING & DENSIFICATION (10%)	19 C.Y.		
		TOTAL CLASS "2" AVAIL. FOR EMBANKMENT	161 C.Y.		
EXCAVATION AVAILABLE FOR EMBANKMENT		EMBANKMENT REQUIRED	11,800 C.Y.		
TOTAL CLASS "1" EXCAVATION	13,000 C.Y.	EXCAVATION AVAILABLE FOR EMBANKMENT	11,800 C.Y.		
		WASTE	200 C.Y.		
CUT ADJUSTED	13,000 C.Y.	TOPSOIL REQUIRED	20,000 C.Y.		
CUT DENSIFIED	11,709 C.Y.	+ 10%	2,318 C.Y.		
PLUS CLASS "2" EXCAV. AVAIL. FOR EMBANKMENT	171 C.Y.	TOTAL TOPSOIL REQUIRED	20,000 C.Y.		
TOTAL EXCAV. AVAIL. FOR EMBANKMENT	11,880 C.Y.				

PROPOSAL	QUANTITIES
ITEM	PROPOSAL QUANTITY
CLASS 1 EXCAVATION	13,100 C.Y.
CLASS 2 EXCAVATION	210 C.Y.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 SUMMARY OF QUANTITIES	SCALE: NONE
		DATE: MAY 11, 1973	DATE: MAY 11, 1973



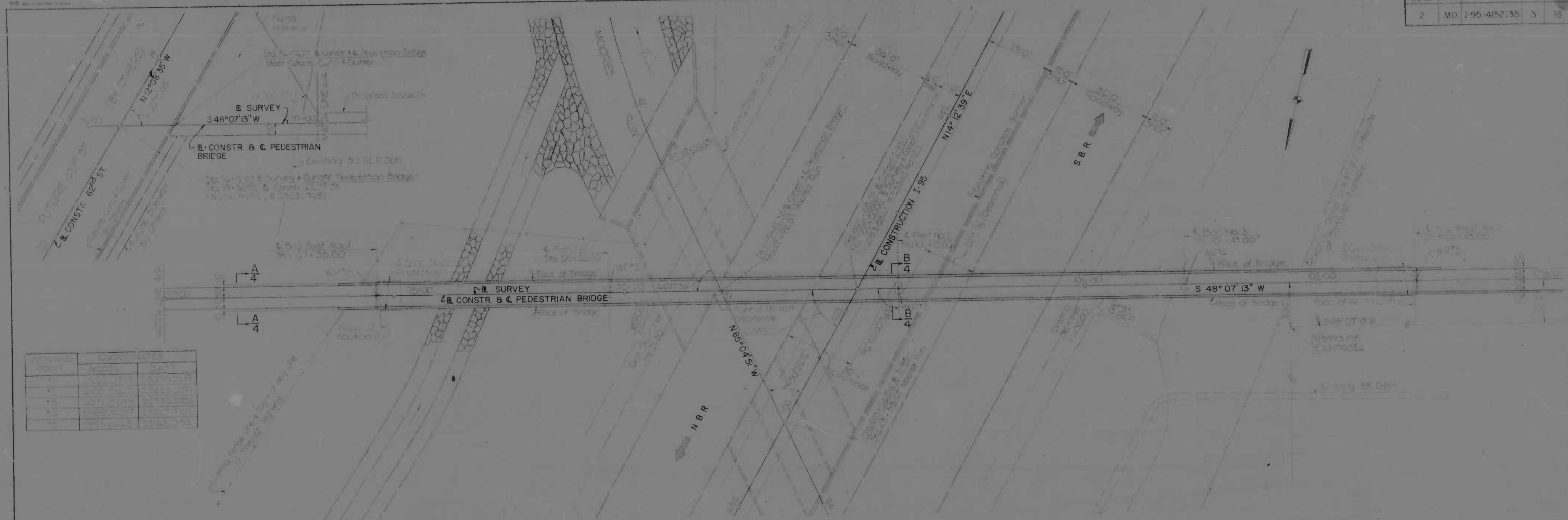
MOORES RUN PEDESTRIAN BRIDGE

MATZ, CHILDS & ASSOCIATES  
BALTIMORE, MARYLAND  
P.O. BOX 108 500 33  
DRC. NO. RC 297-43 80  
BAL. TO CITY WASHDC

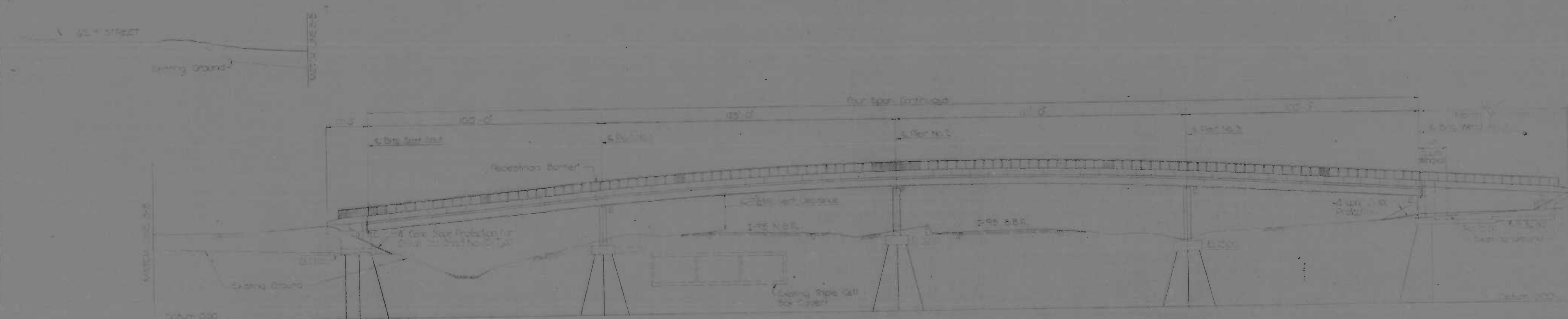
SHEET 1 OF 1



F.P. NO.	STATE	DATE	SHEET NO.
2	MD.	1-95-4(52)35	3 of 18



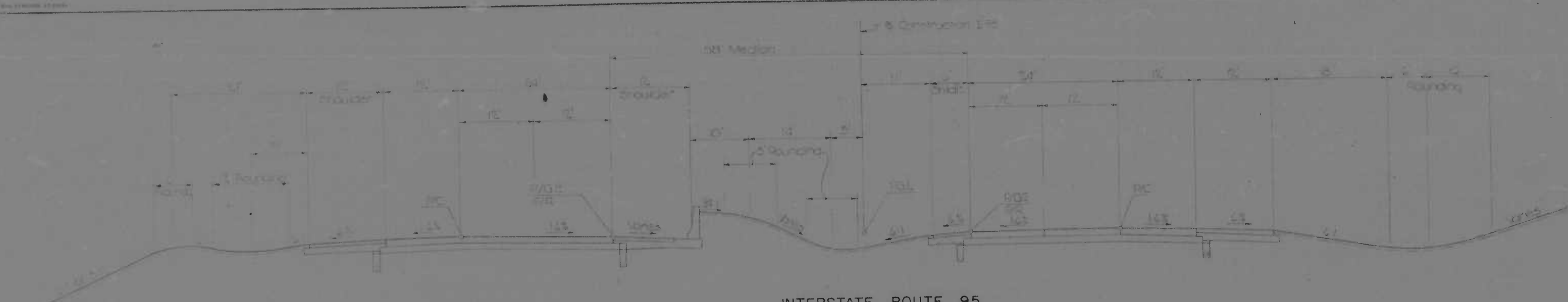
PLAN



ELEVATION

NO.	DESCRIPTION	DATE

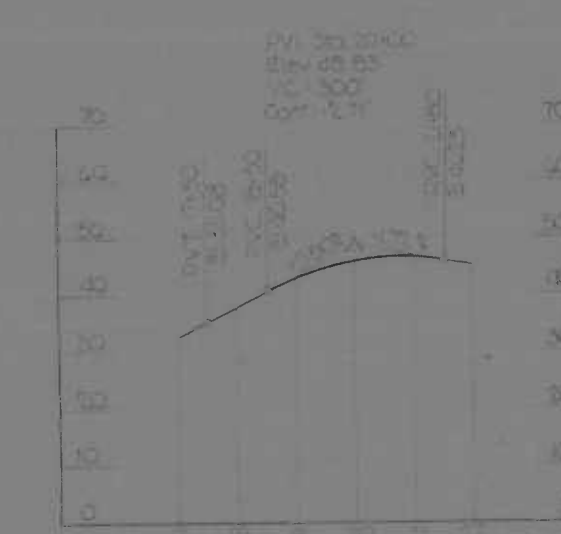
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNOERLE, BENDER, STONE & ASSOC., INC. AND WATZ, LARLOS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND, 21208	MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 GENERAL PLAN AND ELEVATION	DRAWN BY: L.M.W. TRACED BY: L.M.W. F.P. NO.: 1-95-4(52)35 S.R.C. NO.: BC 246-49-85 BALTO. CITY NO. 2140
		DATE: MAY 11, 1973	DES. BY: J.H. CHK. BY: D.M.P. SHEET NO.: 3 of 18



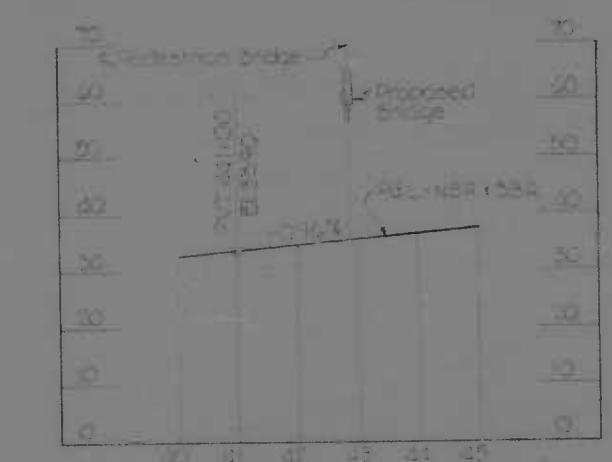
INTERSTATE ROUTE 95  
STA 434+00 TO STA 445+00  
Scale 1"=100'



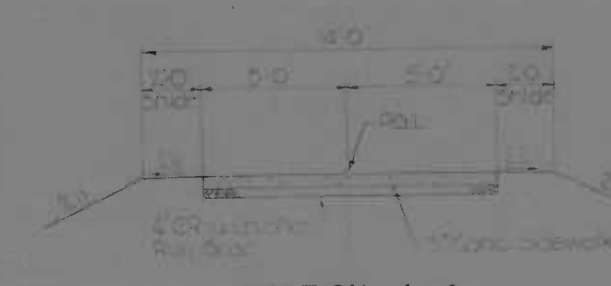
TYPICAL SECTION-FUTURE 62<sup>ND</sup> ST.  
Scale 1"=100'



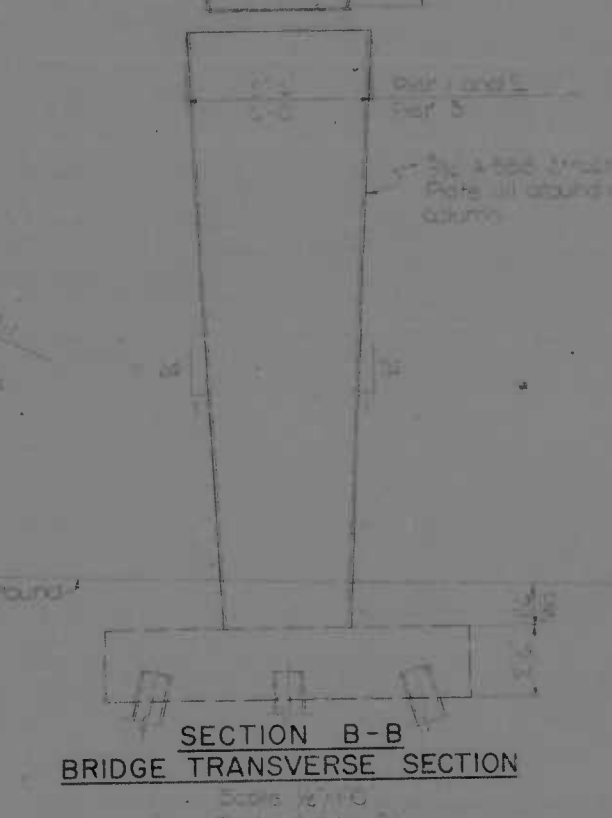
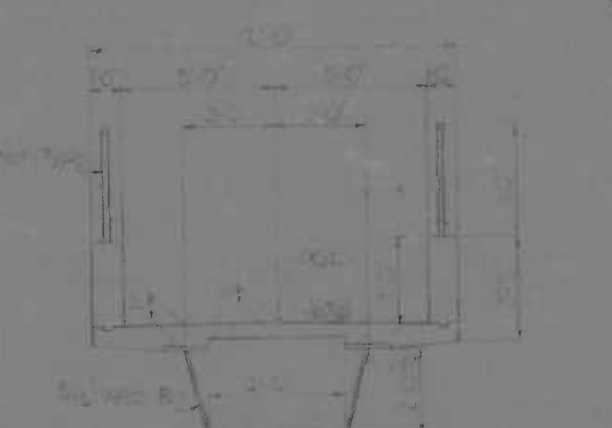
PROFILE-FUTURE 62<sup>ND</sup> ST.  
Scale 1"=100'



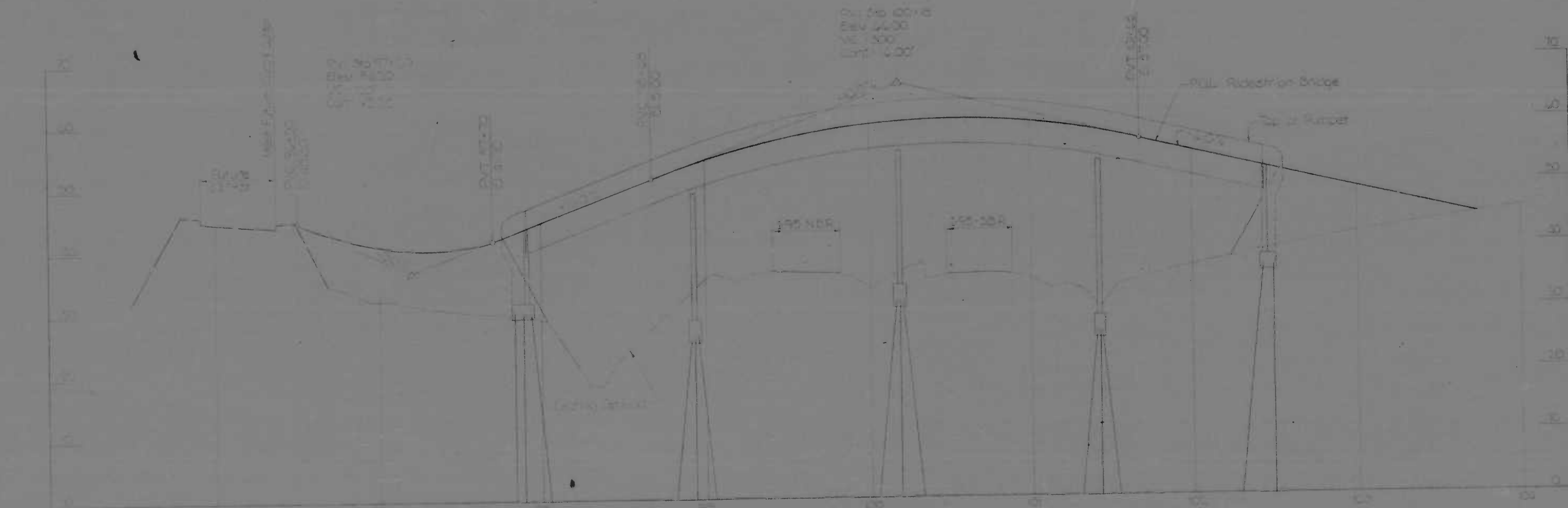
PROFILE-I-95  
Scale 1"=100'



SECTION A-A  
RAMP TRANSVERSE SECTION  
Scale 1"=100'  
Sheet No. 3



SECTION B-B  
BRIDGE TRANSVERSE SECTION  
Scale 1/2"=100'  
Sheet No. 3



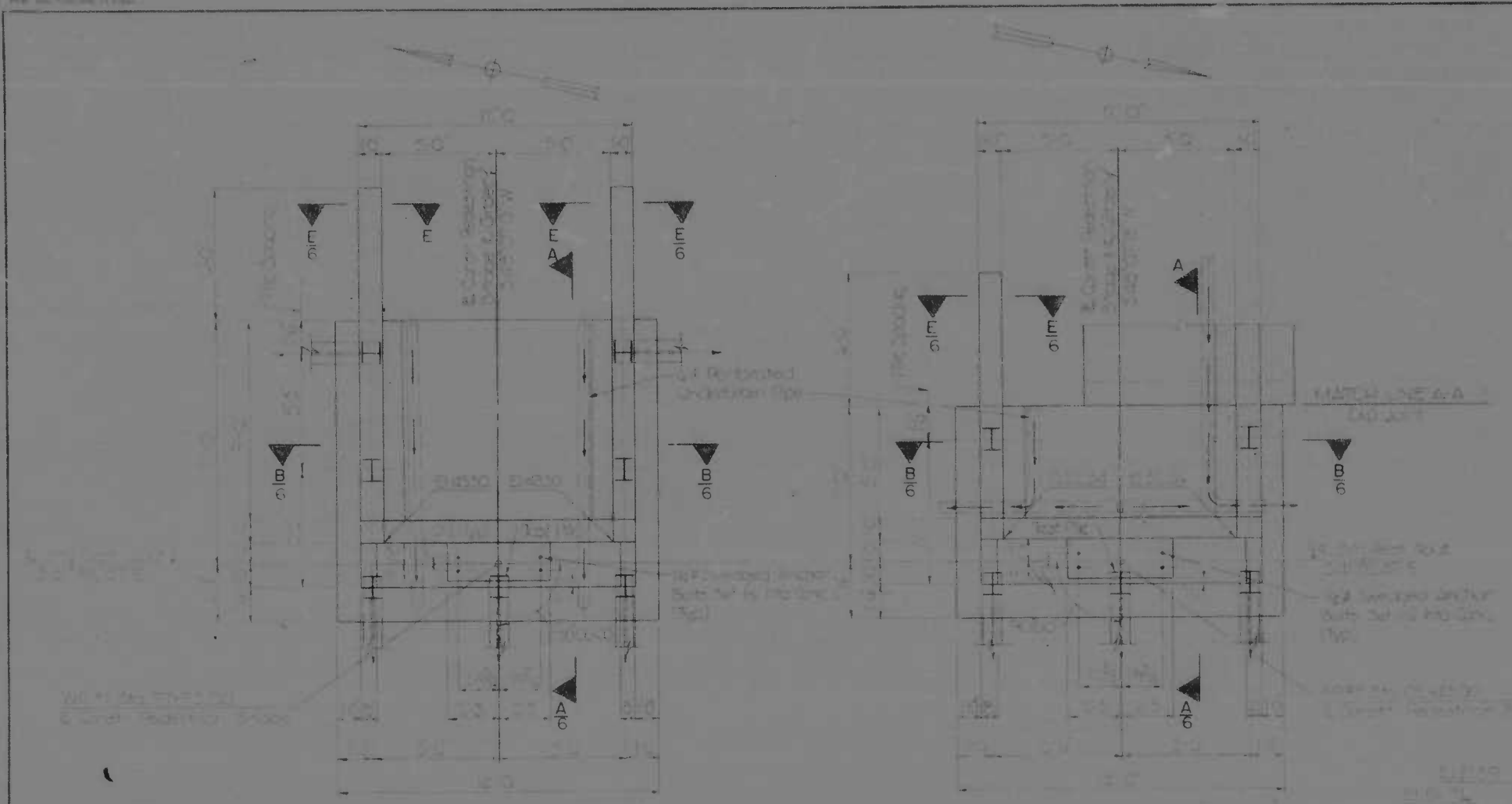
PROFILE-PEDESTRIAN BRIDGE  
Scale 1"=100'

- GENERAL NOTES**
- SPECIFICATIONS: Maryland S.R.C. Specifications and Errata to Specifications dated March, 1968 and special provisions for materials and construction. (ASHI) Standard Specifications for Highway Bridges, dated 1969, for design including 1970, 1971, 1972 interim Specifications.
  - LOADING: Live load 85 lbs. per square foot or HS-20 which ever governs, with provisions for a future 2' wearing surface.
  - CONCRETE: Class "A"-1 Concrete shall have a minimum compressive strength of 3,000 p.s.i. & 28 days for special provisions. For reinforced concrete design, fc=1,200 p.s.i., except that the concrete in bridge deck slabs supported by steel beams, has an fc=1,500 p.s.i.
  - CHAMFER: All exposed corners of concrete shall be chamfered 3/4" x 3/4" with milled chamfer strips except where indicated by the following notation on the plans: "Do Not Chamfer".
  - REINFORCING STEEL: Reinforcing steel shall conform to A.S.T.M. Designation A-615 Grade 40. All splices shall be lapped a minimum of 24 Bar diameters unless other wise noted. Minimum cover for all bars shall be 2" unless otherwise noted.
  - STRUCTURAL STEEL: Structural steel shall conform to A.S.T.M. Designation - A 588.
  - EXCAVATION: See Special Provisions.
  - One coat of pure epoxy coating shall be applied to all abutment walls & pedestals, and abutment backwalls.
  - FOUNDATIONS: Recommendations are given on individual foundation.
- REFERENCES**
- | REFERENCE   | SHEET NO. |
|---|-----------|
| General Plan and Elevations                             | 1         |
| Abutments   | 2         |
| Abutment Sections and Details                           | 3         |
| Plan 1 and 2  | 4         |
| Plan 3 and Details                                      | 5         |
| Frame Plan and Girder Details                           | 6         |
| Diaphragm and Lateral Bracing Details                   | 7         |
| Typical Deck Section and Reinforcement Barriers Details | 8         |
| Bar Joist Details                                       | 9         |
| Center Girder and Superstructure Details                | 10        |
| Elevation and Section Details                           | 11        |
| Bar Joist Details                                       | 12        |

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	ANDRIE, BENDER, STONE & ASSOC., INC. AND KATZ, CHLOS & ASSOC., INC. CONSULTING ENGINEERS 841 N. CALVERT STREET BALTIMORE, MARYLAND 21202	MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 SECTIONS, PROFILES & GENERAL NOTES	DRAWN BY: L.M.W. DES. BY: J.H. TRACED BY: L.M.W. CHK. BY: D.M.P. F.P. NO. 1-95-4102135 SHEET NO. S.R.C. NO. BC 246-49-85 BALTR. CITY NO. 2140 4 of 18
		SCALE AS SHOWN	DATE MAY 11, 1973

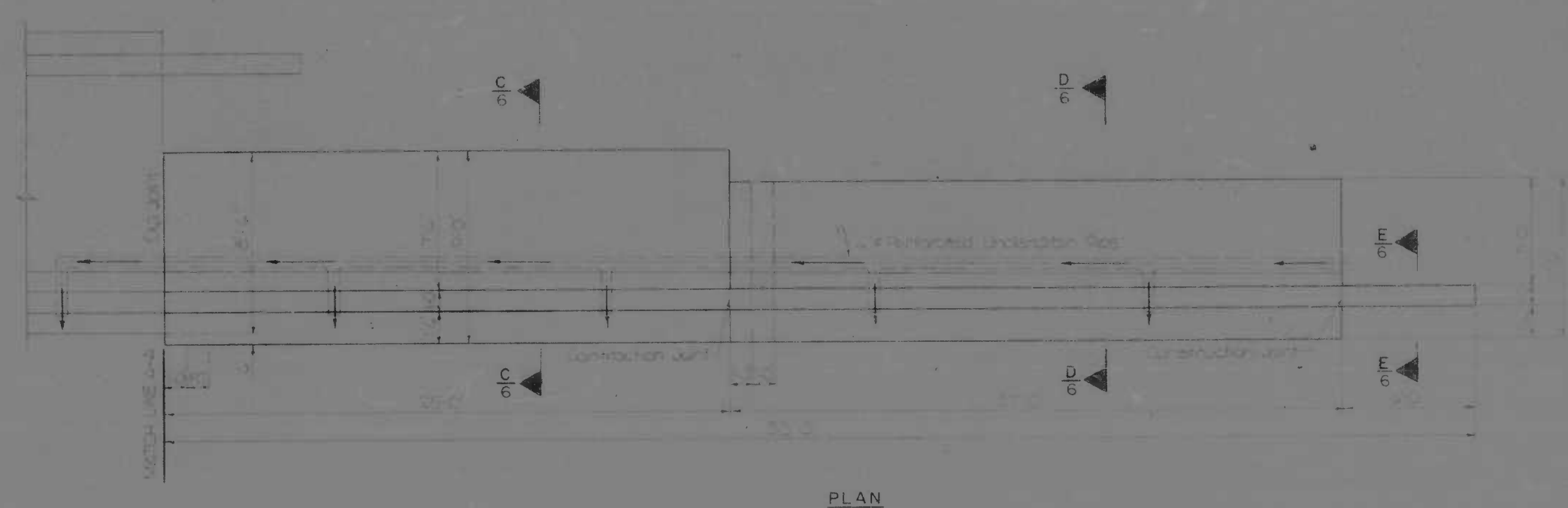


FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	1-95-4(52)35	5	18

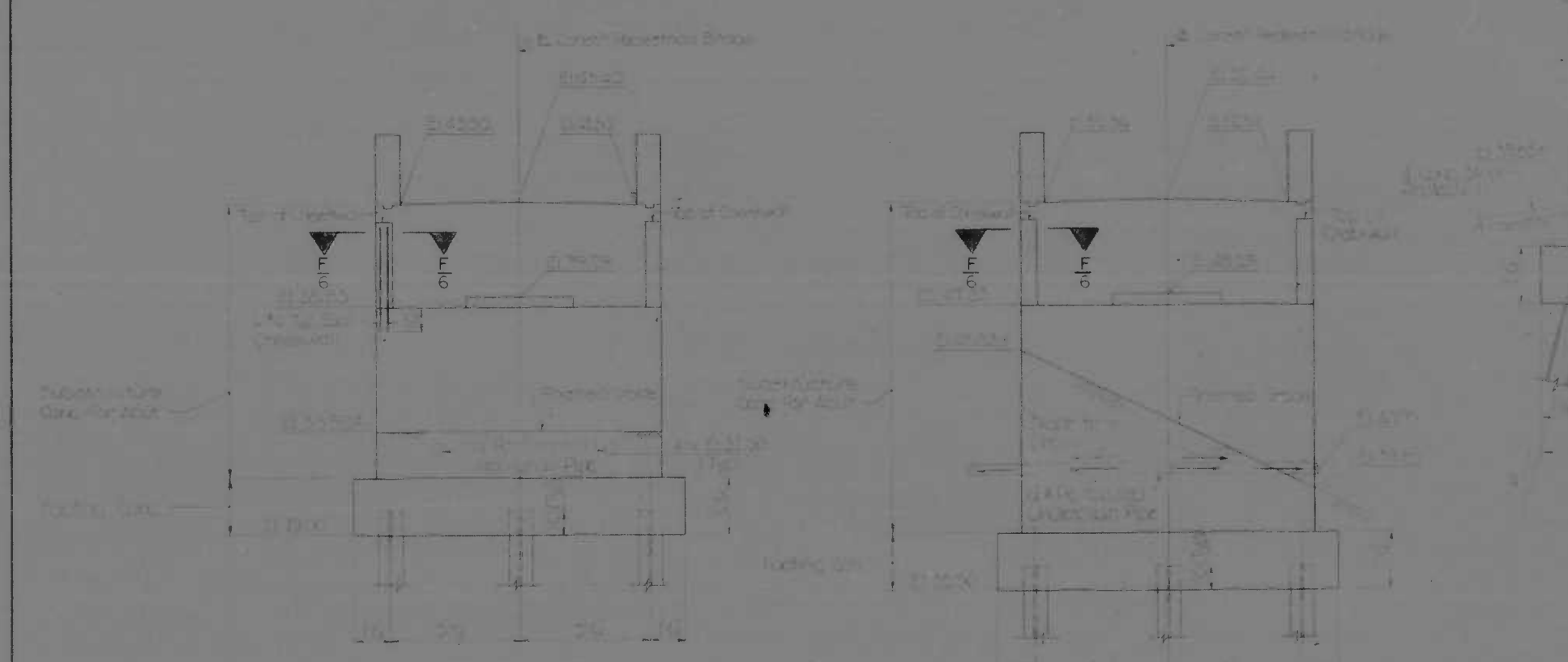


PLAN

PLAN

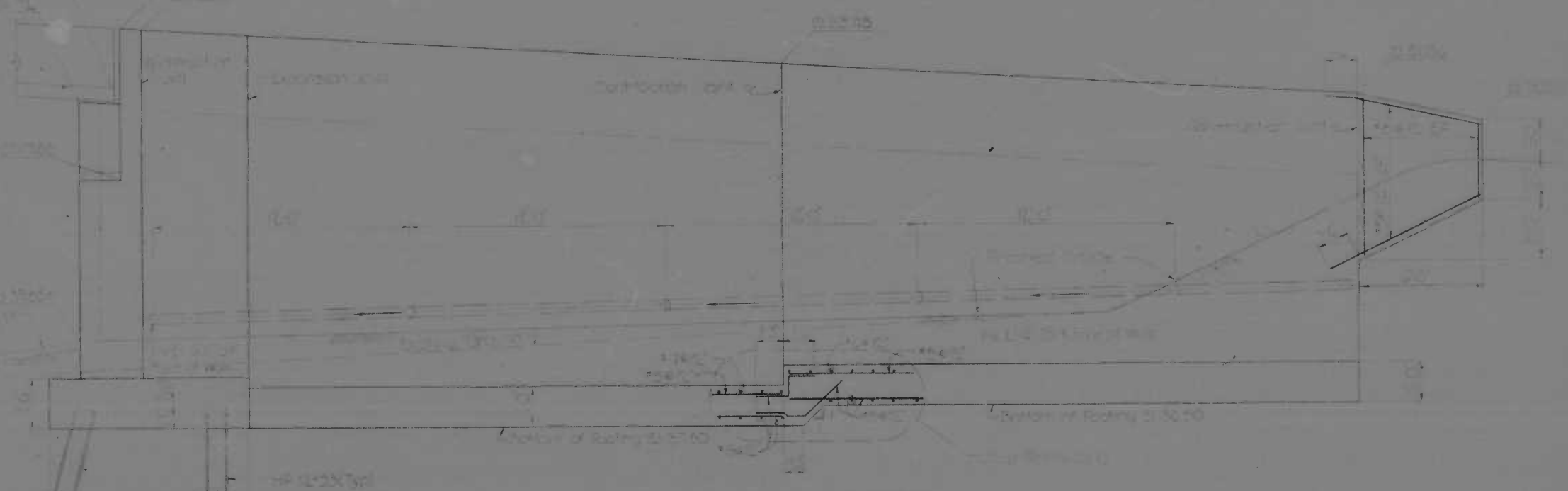


PLAN

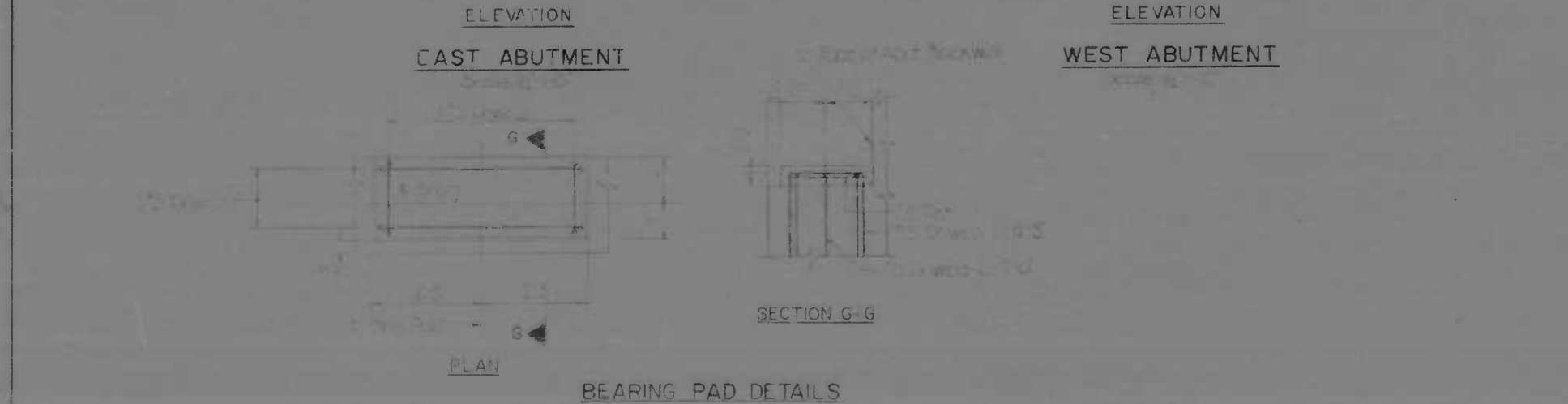


ELEVATION  
WEST ABUTMENT

ELEVATION  
EAST ABUTMENT



ELEVATION  
NORTH WINGWALL - WEST ABUTMENT



BEARING PAD DETAILS

NOTES:  
1. Minimum depth of concrete for bridge piers shall be 10 feet below the lowest water level.  
2. All steel reinforcement shall be lap welded and shall be placed in accordance with the provisions of the Specification for Structural Steel Buildings, AISC, Inc., 1989.

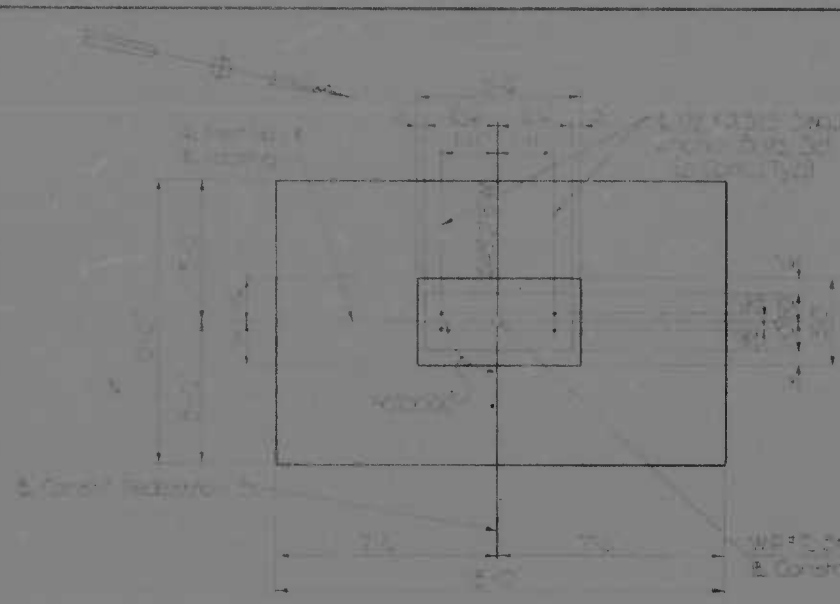
REVISIONS:  
1. Revised drawing and elevation.  
2. Check and correct errors.  
3. Revise and correct errors.  
4. Revise and correct errors.  
5. Revise and correct errors.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 ABUTMENTS	DRAWN BY: L M W CHECKED BY: L M W F.A.P. NO.: 1-95-4(52)35 S.R.C. NO.: BC 246-49-815 BALTS. CITY NO. 7140
		SCALE: AS SHOWN	DATE: MAY 11, 1971 DES. BY: LH CHK. BY: DMP SHEET NO.: 5 OF 18

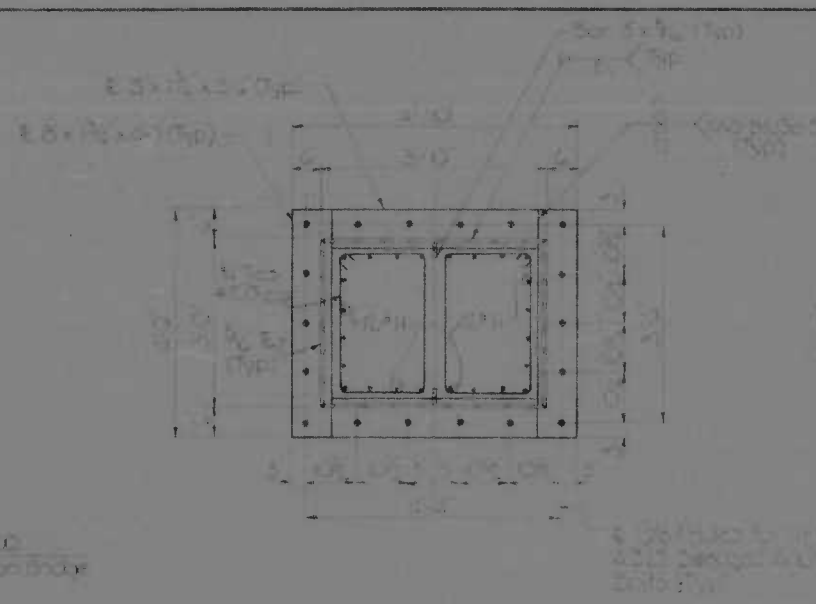




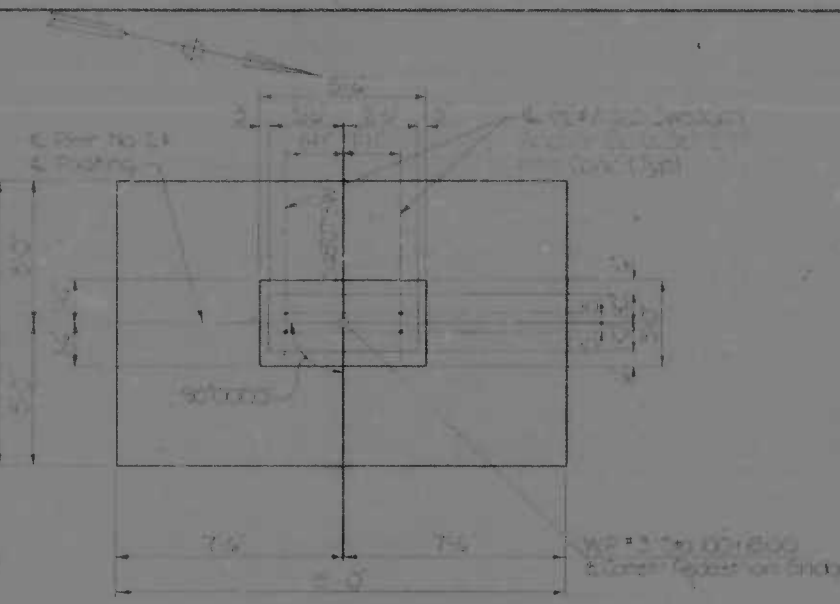
FIG. NO.	DATE	REV. NO.	BY	CHKD.
2	MD I-95-4(52)35	7	IB	



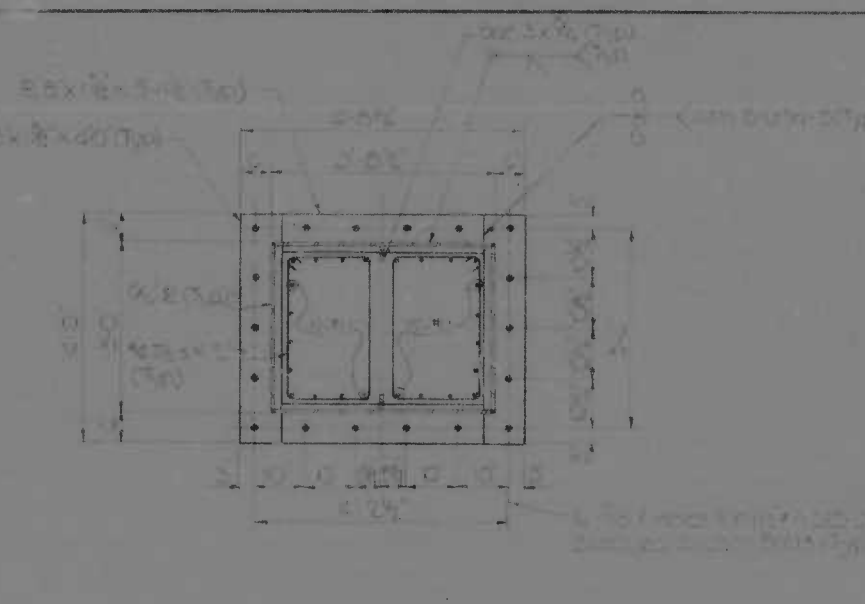
PLAN - PIER NO. 1  
Scale 1/4" = 1'-0"



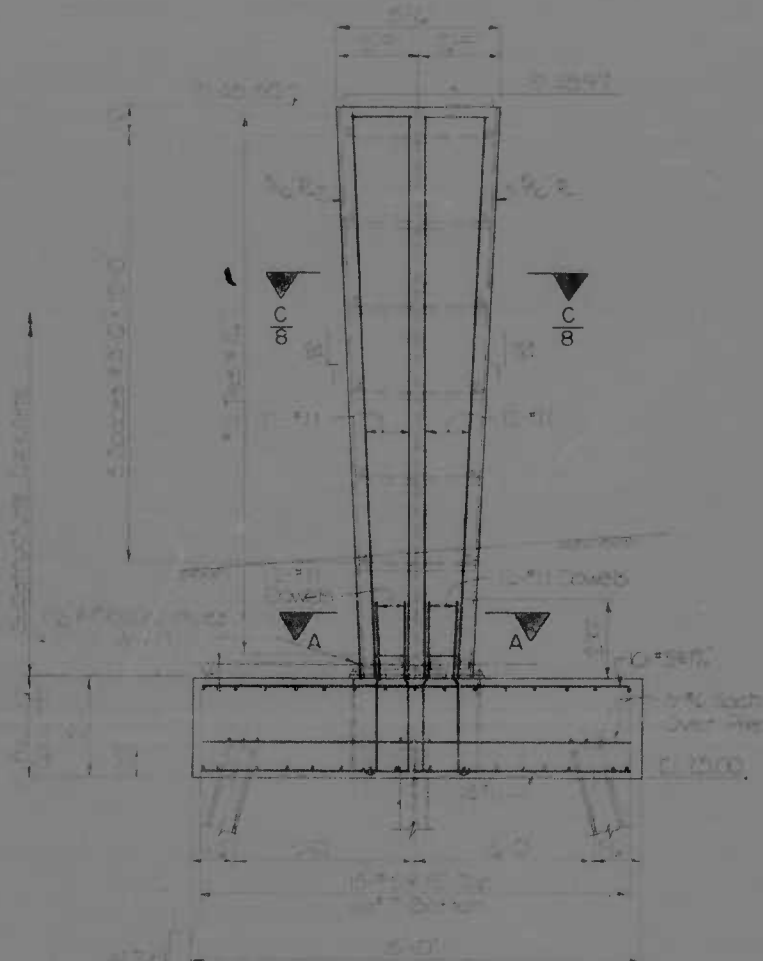
SECTION A-A  
Scale 1/4" = 1'-0"



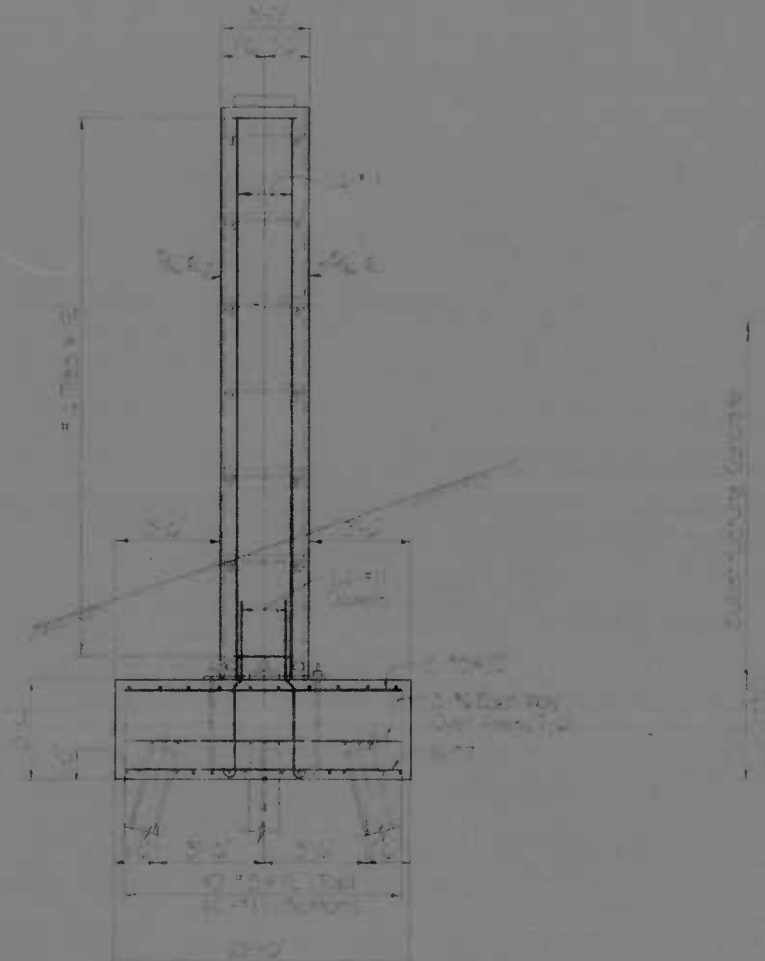
PLAN - PIER NO. 2  
Scale 1/4" = 1'-0"



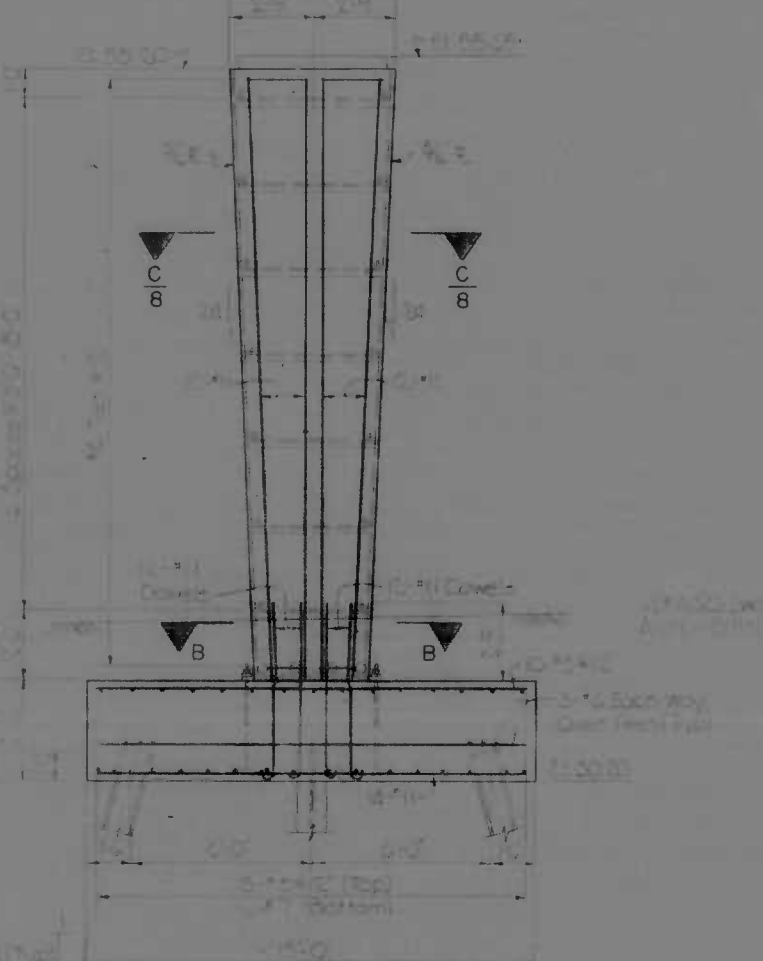
SECTION B-B  
Scale 1/4" = 1'-0"



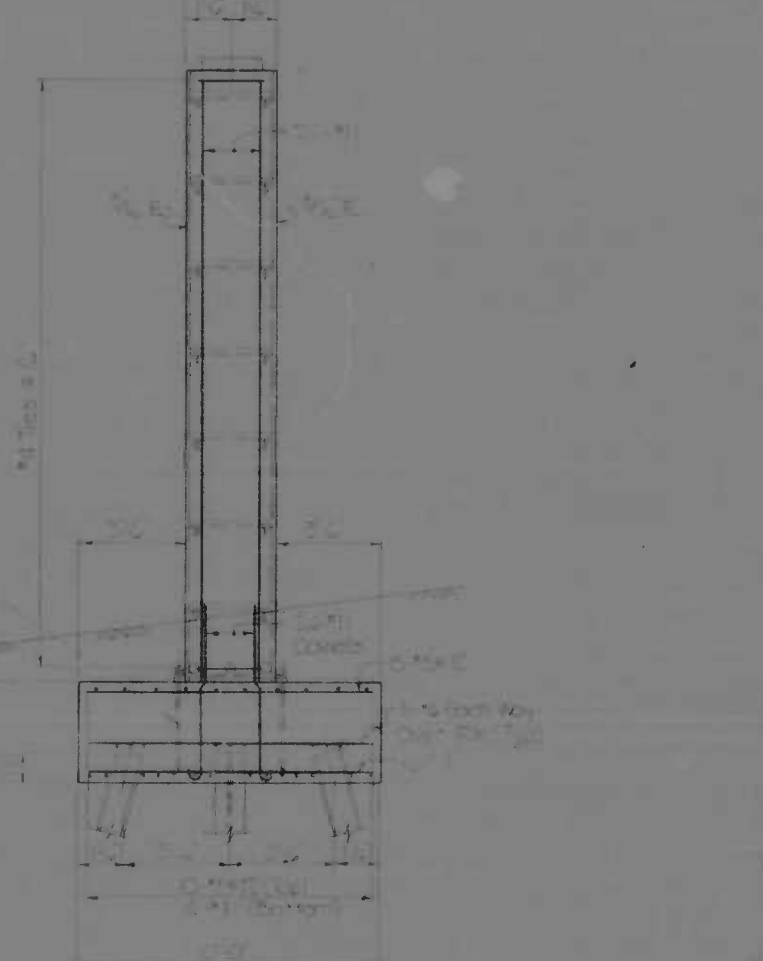
ELEVATION - PIER NO. 1  
Scale 1/4" = 1'-0"



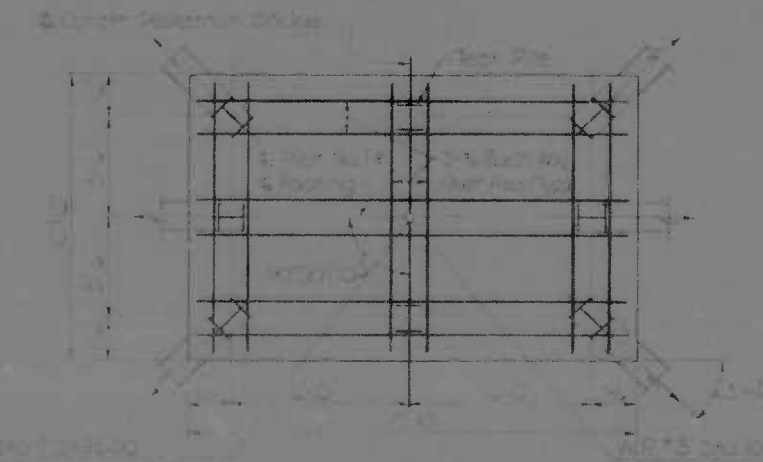
END VIEW  
Scale 1/4" = 1'-0"



ELEVATION - PIER NO. 2  
Scale 1/4" = 1'-0"



END VIEW  
Scale 1/4" = 1'-0"



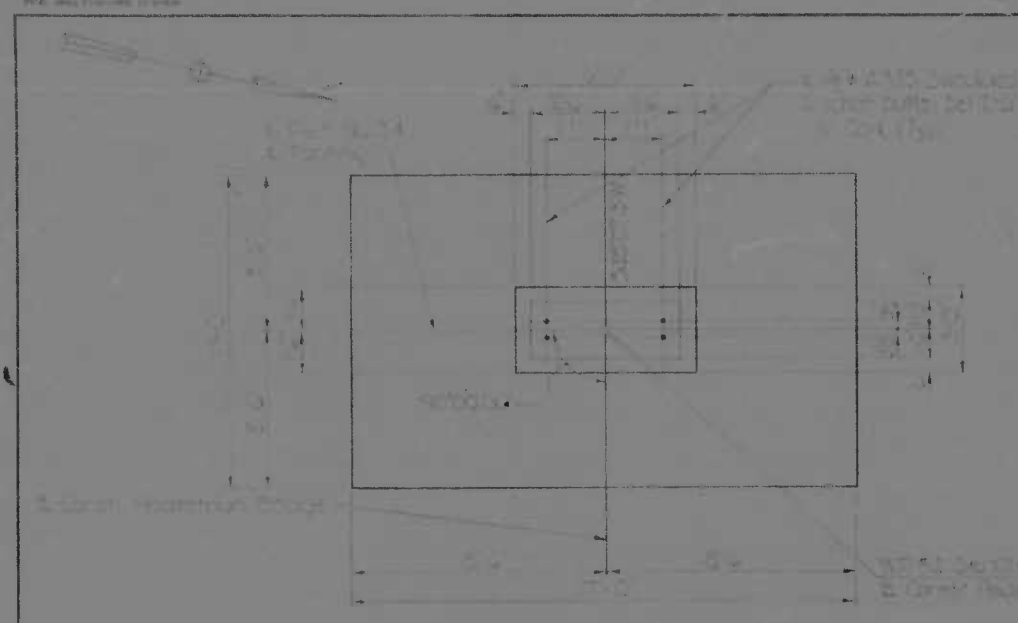
FOOTING PLAN - PIERS NO. 1 & 2  
Scale 1/4" = 1'-0"

Notes:  
1. All steel reinforcement shall conform to ASTM Designation A-601.  
2. All structural steel shall conform to ASTM Designation A-500.  
3. Clear height from top of footing to above finish grade shall be 100'-0". This requires 100'-0" of clear height for deck conforming to NSRF Specifications 301.2.  
4. Scale of drawings shall be as shown. Inadequate pressure applied from 100'-0". This requires 100'-0" of clear height for deck conforming to NSRF Specifications 301.2.  
5. Scale of drawings shall be as shown. Inadequate pressure applied from 100'-0". This requires 100'-0" of clear height for deck conforming to NSRF Specifications 301.2.

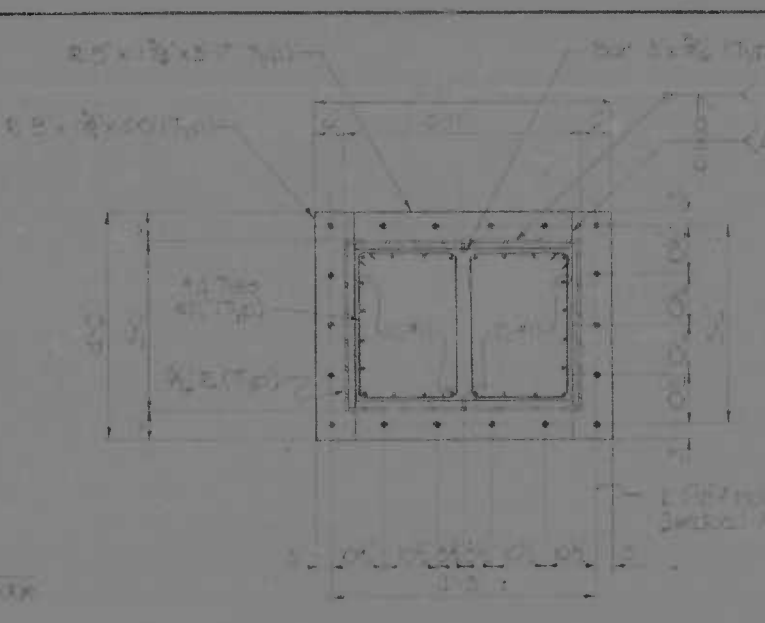
REVISIONS  
1. Initial design  
2. Final design  
3. Construction details  
4. Final design  
5. Construction details

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 PIERS 1 AND 2	DRAWN BY: L.M.W.    PER BY: J.H. TRACED BY: L.M.W.    CHK BY: D.M.P. F.P. NO.: I-95-4(52)35 S.P. NO.: DC 246-49-815 BALTO. CITY NO. 2140
SCALE AS SHOWN		DATE: MAY 11, 1973	SHEET NO. 7 OF 18

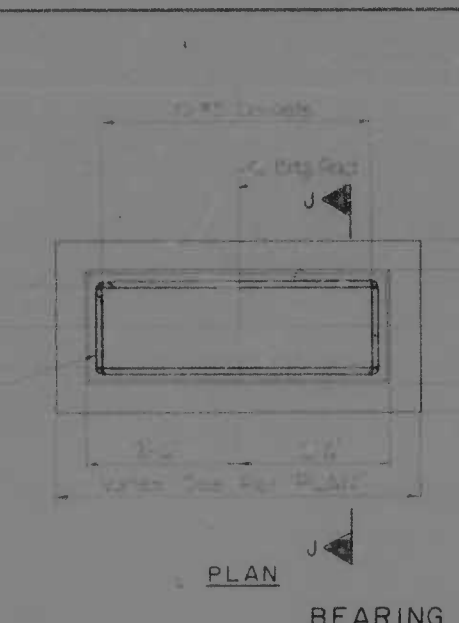
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	PROJ. DIST.	SHEET NO.
2	MD.	I-95-4(52)35	8	18



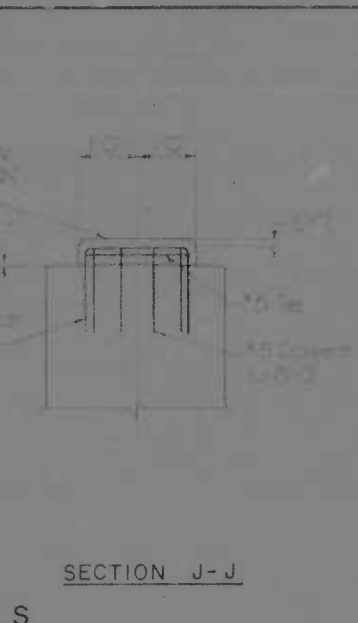
PLAN - PIER NO 3  
SCALE 1/4" = 1'-0"



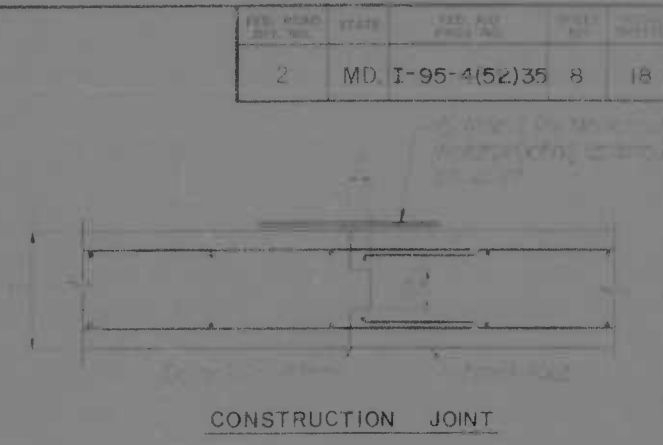
SECTION D-D  
SCALE 1/4" = 1'-0"



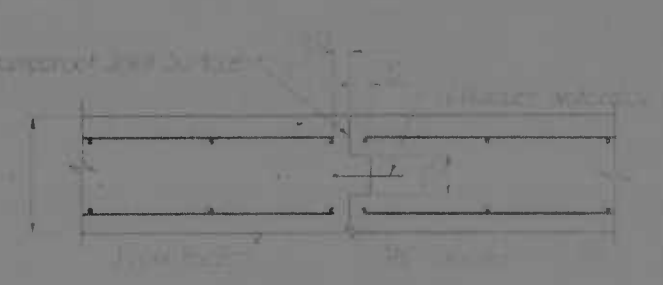
BEARING PAD DETAILS  
SCALE 1/2" = 1'-0"



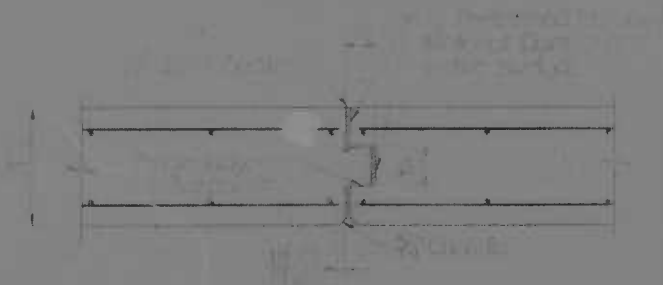
SECTION J-J



CONSTRUCTION JOINT

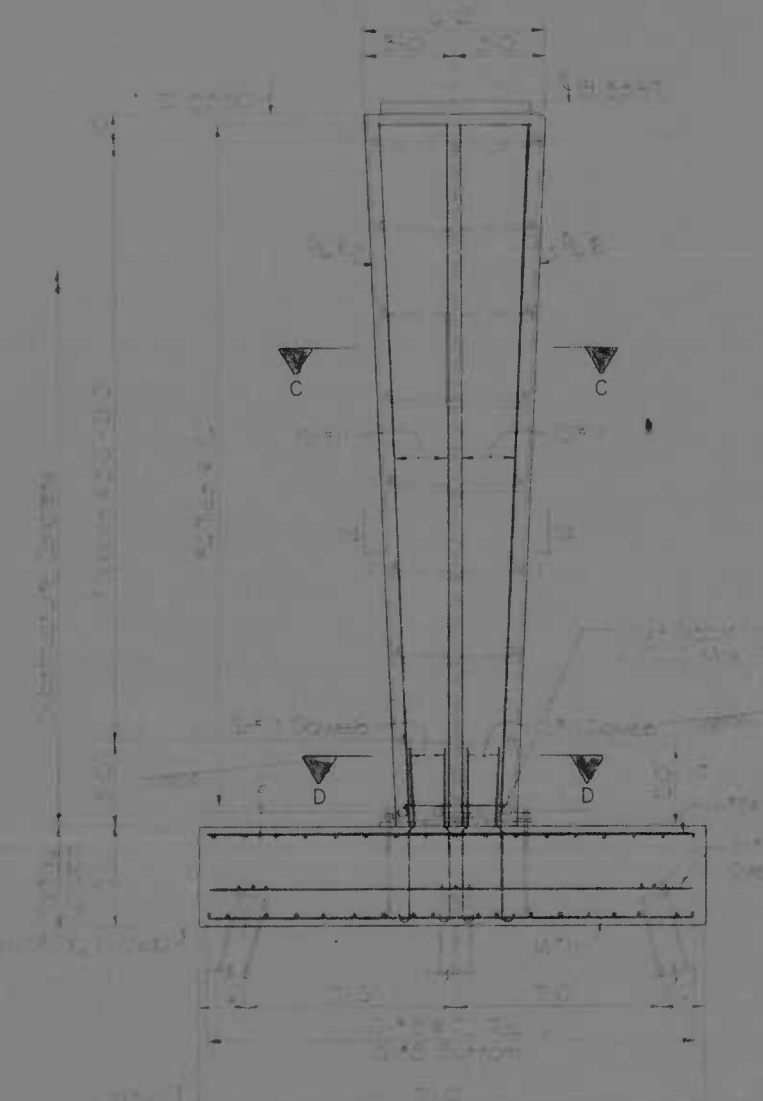


CONTRACTION JOINT

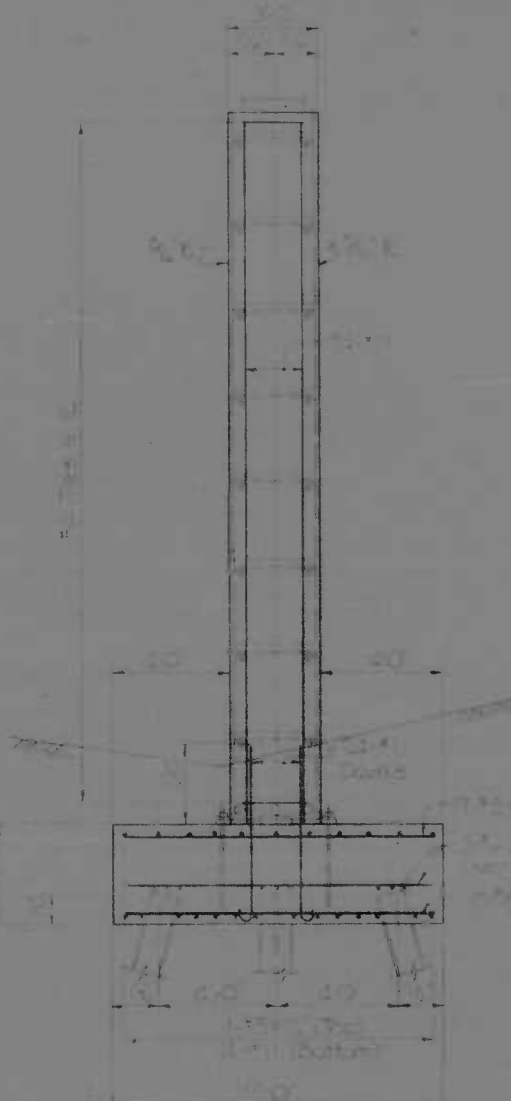


EXPANSION JOINT

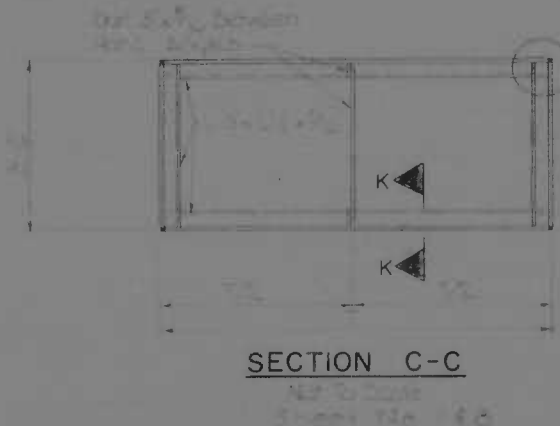
JOINT DETAILS



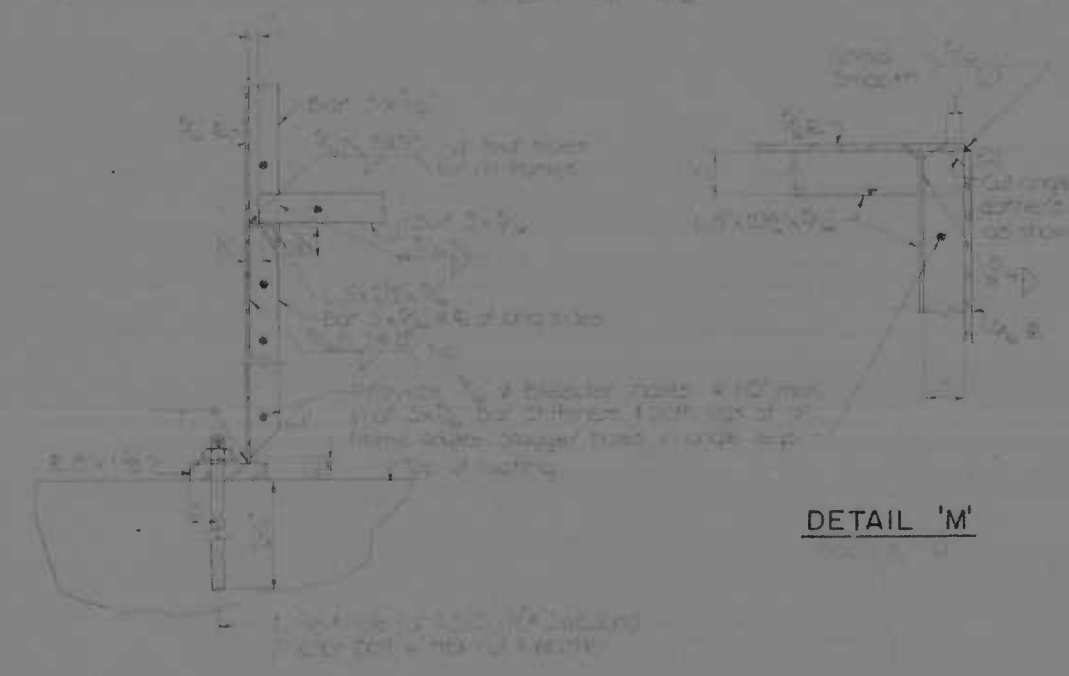
ELEVATION - PIER NO 3  
SCALE 1/4" = 1'-0"



END VIEW  
SCALE 1/4" = 1'-0"

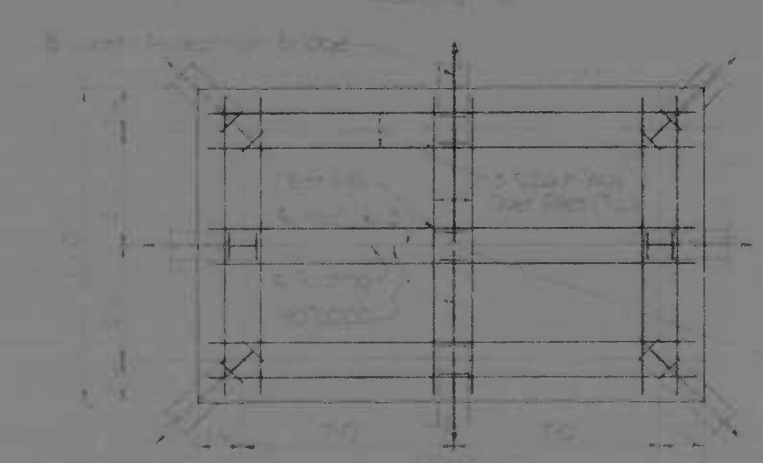


SECTION C-C  
SCALE 1/4" = 1'-0"



SECTION K-K  
SCALE 1/4" = 1'-0"

DETAIL 'M'

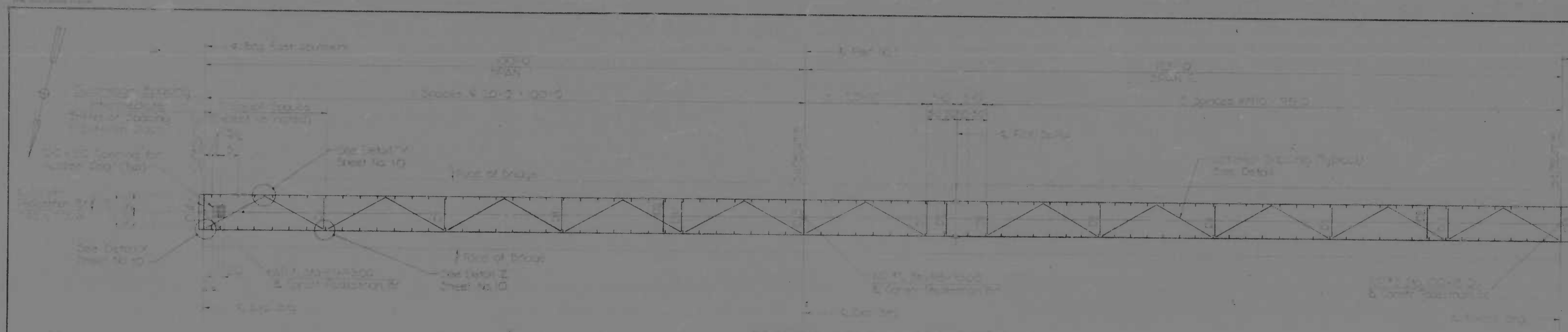


FOOTING PLAN  
SCALE 1/4" = 1'-0"

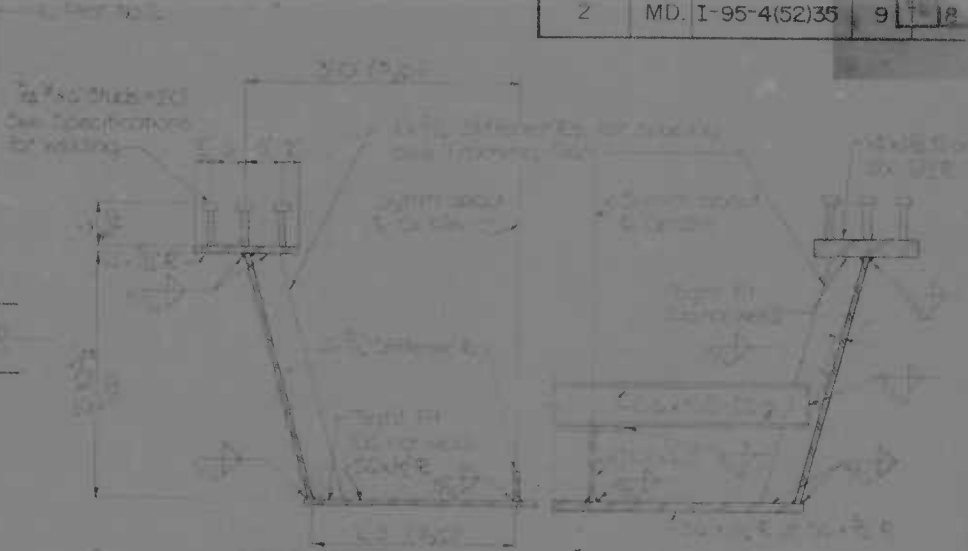
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 PIER 3 AND DETAILS		DRAWN BY: LMW	DES. BY: JH
				TRACED BY: LMW	CHK. BY: D.M.P.
				F.A.P. NO. 1-95-4(52)35	SHEET NO.
				S.R.C. NO. 30 246-42-115	8 of 18
				BALTO. CITY NO. 2140	
		SCALE AS SHOWN	DATE MAY 11, 1973		



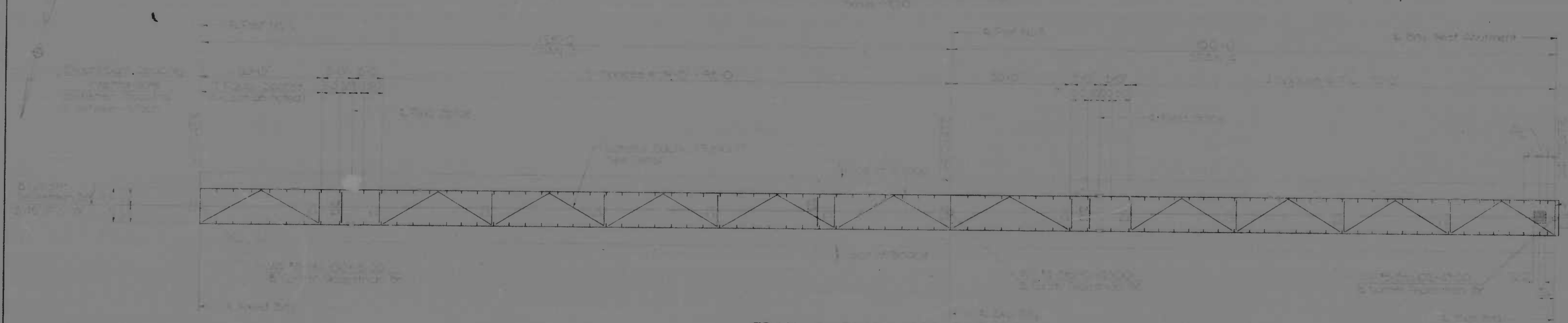
FED. ROAD DIST. NO.	STATE	PROJ. NO.	SHEET NO.
2	MD.	I-95-4(52)35	9



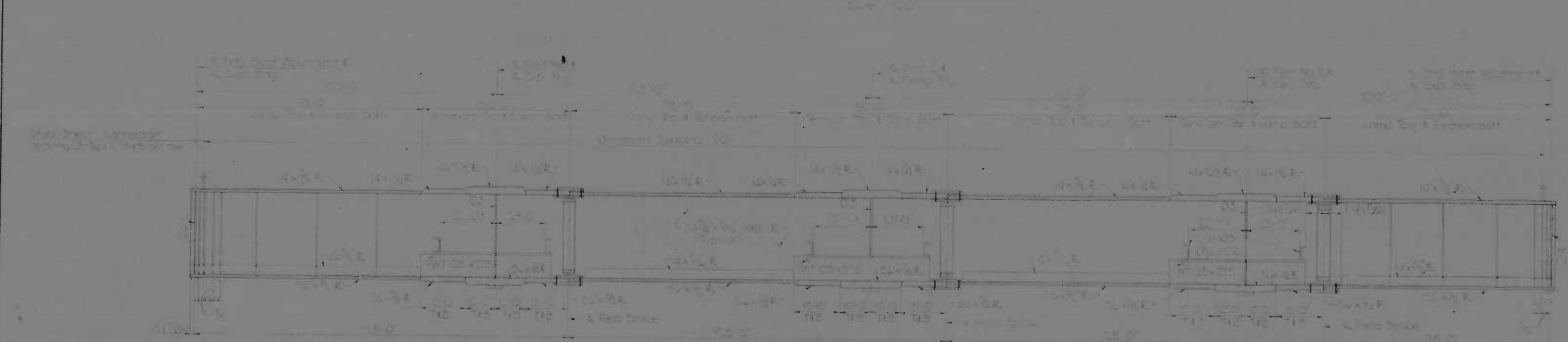
FRAMING PLAN - SPANS 1 & 2



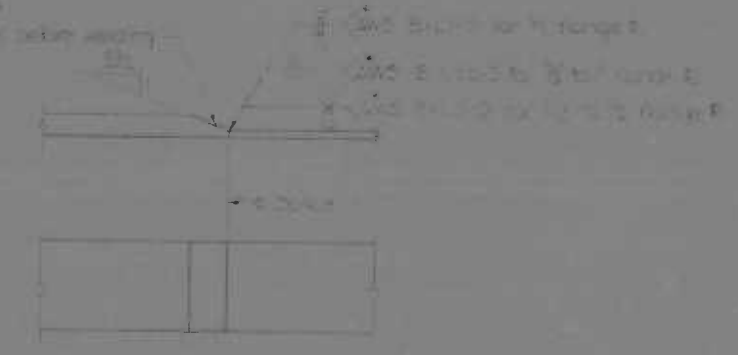
HALF SECTION COMP TOP TENSION BOTTL  
 HALF SECTION TENSION TOP COMP BOT TOM  
 TYPICAL CROSS SECTION OF GIRDER



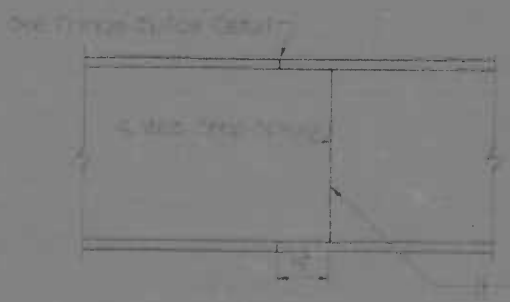
FRAMING PLAN - SPANS 3 & 4



TYPICAL LONGITUDINAL SECTION OF GIRDER



FLANGE SPLICE DETAILS



WELDED SHOP SPLICE

Notes:  
 1. All structural steel shall conform to ASTM designation A 500.  
 2. All dimensions and tolerances of girders shall be verified.

REVISIONS  
 General Notes:  
 Typical Detail Section  
 Disposition and Detailing  
 Welding Details  
 Connection Detail  
 Deck Slab Detail  
 Superstructure Detail  
 Details 1, 2, and 3

This detail is governed by the rules of contract for work for  
 design and construction, and is subject to the provisions  
 of the contract documents, including the specifications,  
 and the contract between the City of Baltimore and the  
 consultant.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	ANDERLE, BENDER, STONE & ASS'CS, INC. AND MATZ GIRDERS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	MOORES RUN PEDESTRIAN BRIDGE OVER INTERSTATE ROUTE 95 FRAMING PLAN AND GIRDER DETAIL		DRAWN BY: L.M.W.	DES. BY: J.H.
		SCALE: AS SHOWN	DATE: MAY 11, 1973	TRACED BY: L.M.W.	CHK. BY: D.M.P.
				F.A.P. NO.: I-95-4(52)35	SHEET NO.: 9 of 18
				S.R.C. NO.: BC 246-90-80	BALTO. CITY NO. 214

INDEX OF SHEETS

1	TITLE SHEET AND LOCATION MAP
2	ARCHITECTURAL RENDERING
3	GENERAL PLAN AND ELEVATION
4	SECTIONS, PROFILES AND GENERAL NOTES
5	ABUTMENTS
6	ABUTMENT SECTIONS AND DETAILS
7	PIERS 1 AND 2
8	PIER 3 AND DETAILS
9	FRAMING PLAN AND GIRDER DETAIL
10	DIAPHRAGM AND LATERAL BRACING DETAILS
11	TYPICAL DECK SECTION & PED. BARRIER DETAIL
12	BEARING DETAILS
13	CAMBER DIAGRAM & SUPERSTRUCTURE ELEVATION
14	EXPANSION DAM AND MISCELLANEOUS DETAILS
15	BORING DATA
16	GRADING PLAN
17	GRADING PLAN
18	SUMMARY OF QUANTITIES

2	MD	I-95-4(52)35	1	18
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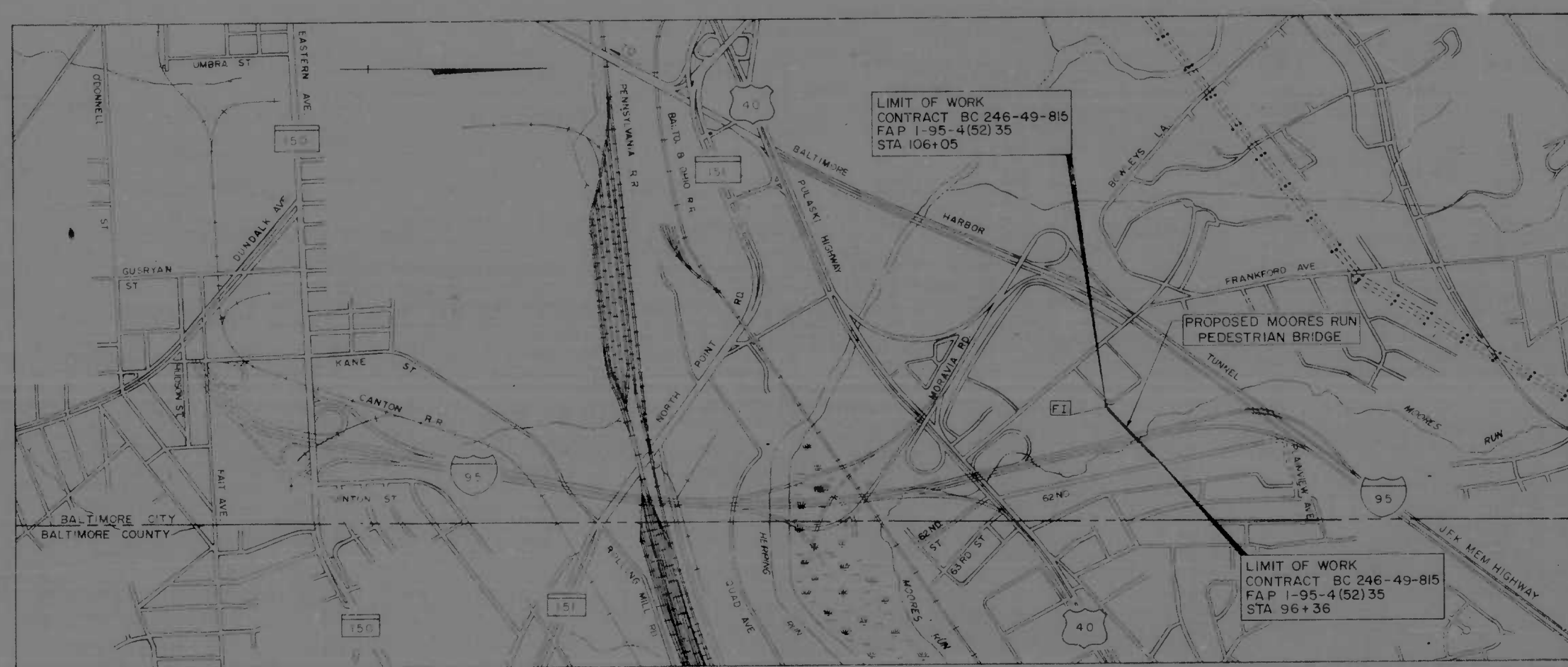
CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
AND  
STATE ROADS COMMISSION OF MARYLAND  
INTERSTATE DIVISION FOR BALTIMORE CITY

FEDERAL AID PROJECT NO. I-95-4(52)35  
STATE ROADS COMMISSION PROJECT NO. BC 246-49-815  
CITY OF BALTIMORE BUREAU OF ENGINEERING,  
HIGHWAY ENGINEERING DIVISION CONTRACT NO.2140

INTERSTATE ROUTE 95  
MOORES RUN PEDESTRIAN BRIDGE

NOTE:  
EFFECTIVE JULY 1, 1971, IN ACCORDANCE WITH THE PROVISIONS OF CHAPTER 526 OF ACTS OF THE 1970 GENERAL ASSEMBLY, WHEREVER THE TITLE "STATE ROADS COMMISSION" AND/OR TERM COMMISSION IS USED, IT SHALL BE CONSTRUED TO BE "THE STATE HIGHWAY ADMINISTRATION."

FIELD BOOKS	
BOOK NO.	DESCRIPTION
I-95-001	ALIGNMENT
I-95-002	"
I-95-005	"
I-95-015	"
I-95-022	"
I-95-026	"
I-95-006	TOPO
I-95-007	"
I-95-008	"
I-95-026	"
I-95-003	CROSS-SECTIONS
I-95-008	"
I-95-015	"



[F] DENOTES INFORMATIONAL SIGNS STATE OF MARYLAND STANDARD 104.41 (5 REQUIRED) COST TO BE INCLUDED IN THE PRICE BID FOR "MAINTENANCE OF TRAFFIC"

LOCATION PLAN  
SCALE 1"=1000'

LENGTH OF PROJECT-0.022MILES

RIGHT OF WAY LINES SHOWN ON THESE PLANS DO NOT INCLUDE EASEMENT. THEY ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THESE LINES DO NOT REPRESENT THE OFFICIAL PROPERTY ACQUISITION LINES FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION. SEE THE APPROPRIATE RIGHT OF WAY PLAT OR PLATS.

CITY OF BALTIMORE  
APPROVED:  
David S. Carlson  
CIVIL & LEASURES  
ADMINISTRATION & TRAFFIC CONTROL  
REPRESENTATIVE

CHECKED BY:	INITIALS	DATE
BUREAU OF ENGINEERING	W.H.B.	4-30-73
WATER DIVISION	W.H.B.	4-30-73
WASTE WATER DIVISION	W.H.B.	4-30-73
HIGHWAY ENGINEERING DIVISION	W.H.B.	4-30-73
SURVEY AND RECORDS DIVISION	W.H.B.	4-30-73
BUREAU OF UTILITY OPERATIONS	W.H.B.	4-30-73
LIGHTING SECTION - HIGHWAY MAINTENANCE DIV.	W.H.B.	4-30-73
CONCRETE SECTION - HIGHWAY MAINTENANCE DIV.	W.H.B.	4-30-73
DEPARTMENT OF TRANSIT AND TRAFFIC	J.W.C.	4-30-73

CITY OF BALTIMORE  
DEPARTMENT OF PUBLIC WORKS  
APPROVAL RECOMMENDED  
HEAD, BUREAU OF ENGINEERING  
APPROVED  
DIRECTOR OF PUBLIC WORKS

PREPARED BY  
ANDERLE, HENDER, SYLVE & ASSOCIATES, INC.  
AND  
MATZ, CHILDS, & ASSOCIATES, INC.  
CONSULTING ENGINEERS  
BALTIMORE, MARYLAND  
Fred F. Mirmiran  
DATE: 4/30/73

STATE ROADS COMMISSION OF MARYLAND  
REVIEWED AND APPROVAL RECOMMENDED  
CHIEF, BUREAU OF ENGINEERING  
APPROVAL RECOMMENDED  
CHIEF, INTERSTATE DIVISION FOR BALTIMORE CITY

STATE ROADS COMMISSION OF MARYLAND  
APPROVAL RECOMMENDED  
DEPUTY CHIEF ENGINEER - DEVELOPMENT  
APPROVED  
CHIEF ENGINEER

U. S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION  
APPROVED  
DIVISION ENGINEER