

MATCH LINE SEE SHEET NO. 3

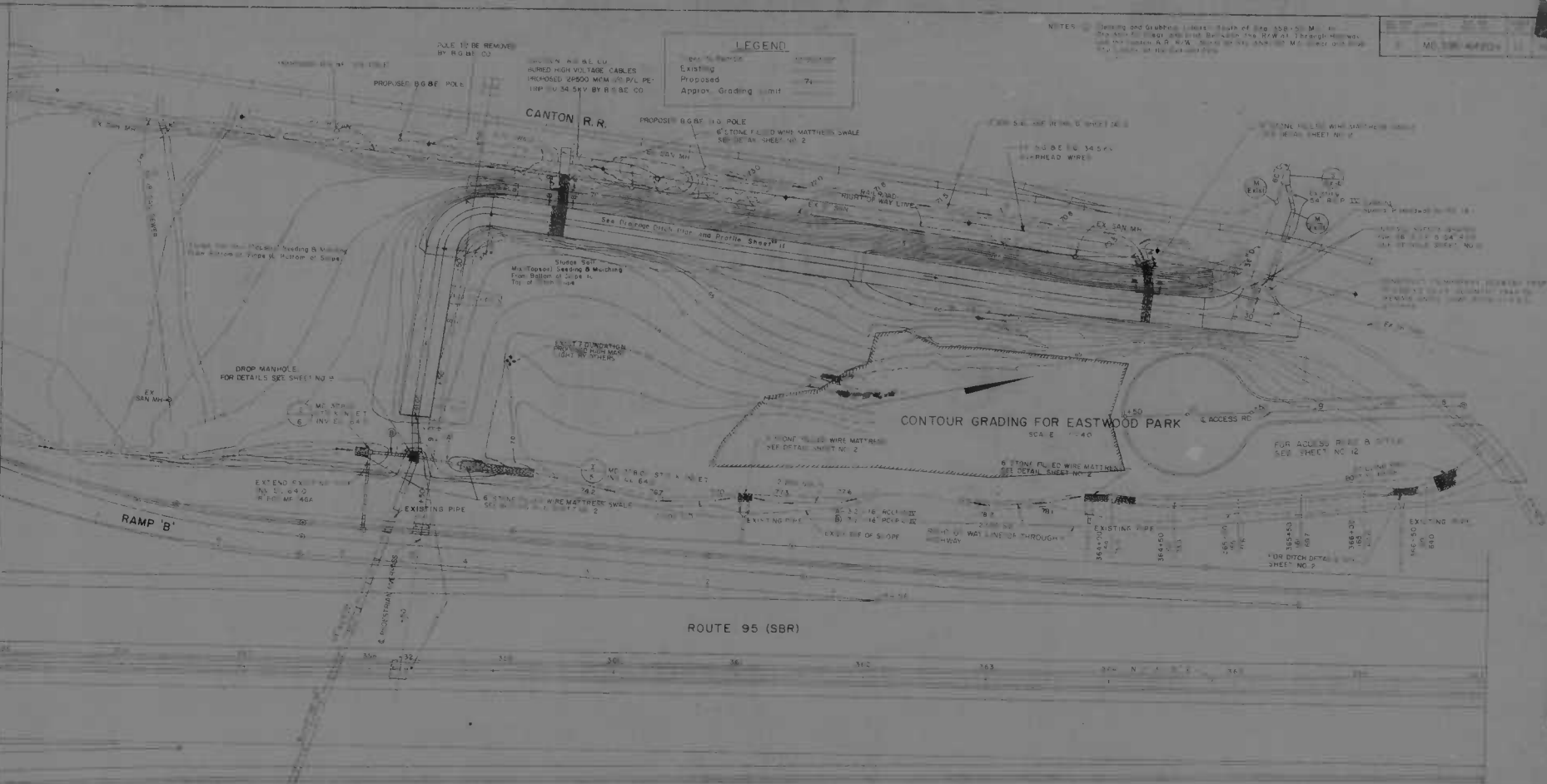
POLE TO BE REMOVED BY R.G.B. CO.
PROPOSED 600E POLE
PROPOSED 600E 110 POLE
EXISTING 6" STONE FILL WITH MATTHEWS SWALE
EXISTING 6" STONE FILL WITH MATTHEWS SWALE
EXISTING 6" STONE FILL WITH MATTHEWS SWALE

LEGEND

| | |
|-----------------------|-------|
| Existing | --- |
| Proposed | - - - |
| Approx. Grading Limit | --- |

NOTES
1. Grading and Grubbing Limits: South of Sta. 458+50 M. to Sta. 460+00 M. and East of Station 460+00 M. to Station 460+00 M. N.W. 1/4 Sec. 10, T. 11 N., R. 10 E., MD. DIST. 24. The Station A.P. N.W. 1/4 Sec. 10, T. 11 N., R. 10 E., MD. DIST. 24. The Station A.P. N.W. 1/4 Sec. 10, T. 11 N., R. 10 E., MD. DIST. 24.

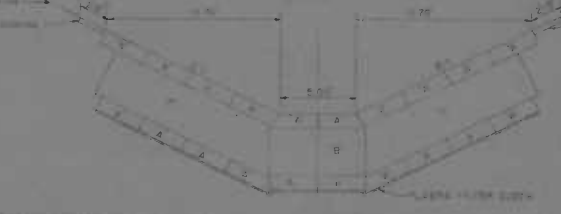
MD. DIST. 24



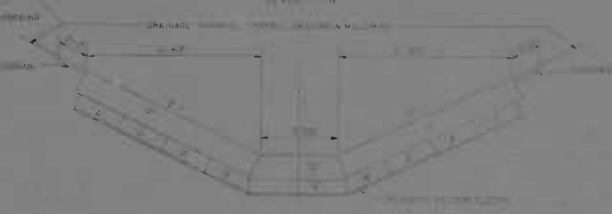
LONGITUDINAL SECTION STILLING POOL



SECTION A-A—STILLING POOL



SECTION B-B—STILLING POOL



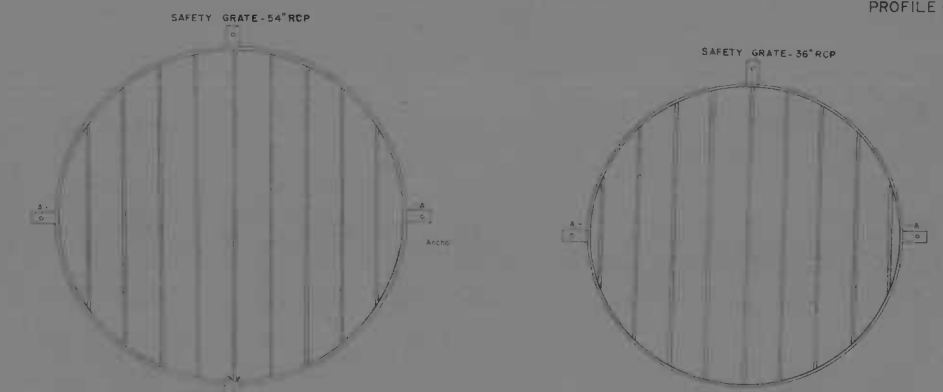
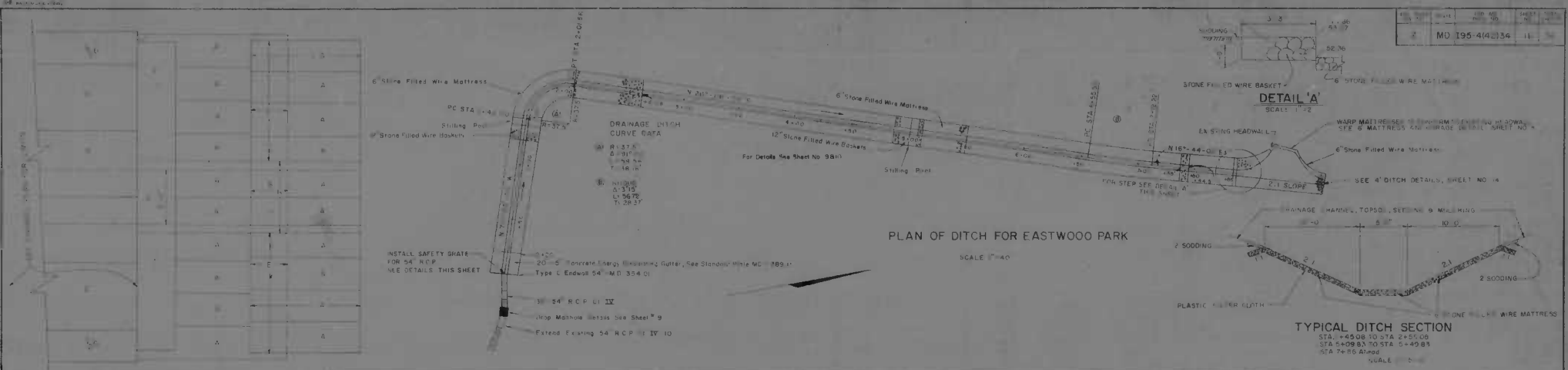
| NO. | DATE | DESCRIPTION |
|-----|---------|-------------|
| 1 | 11-1-66 | PRELIMINARY |
| 2 | 11-1-66 | REVISED |
| 3 | 11-1-66 | REVISED |
| 4 | 11-1-66 | REVISED |
| 5 | 11-1-66 | REVISED |
| 6 | 11-1-66 | REVISED |
| 7 | 11-1-66 | REVISED |
| 8 | 11-1-66 | REVISED |
| 9 | 11-1-66 | REVISED |
| 10 | 11-1-66 | REVISED |

| REVISIONS |
|-----------|
| |
| |
| |
| |
| |
| |
| |
| |
| |
| |

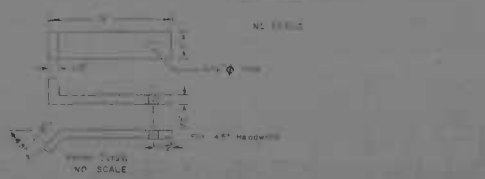
| CONSULTANT |
|------------|
| |
| |
| |
| |
| |
| |
| |
| |
| |
| |

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
INTERSTATE ROUTE 95
PEDESTRIAN OVERPASS KANE ST TO QUINTON ST
SCALE AS SHOWN

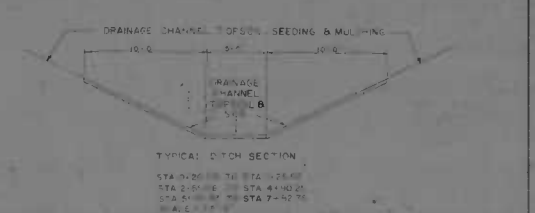
STATE ROADS COMMISSION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY
DRAWN BY: ELK
CHECKED BY: W. B. BARKER
TRACED BY: J. M. BARKER
DATE: 11-1-66
BALTO. CITY NO. 2071



WEST END GRADING
 Slope shall be Robert Frost 12\"/>



NO SCALE



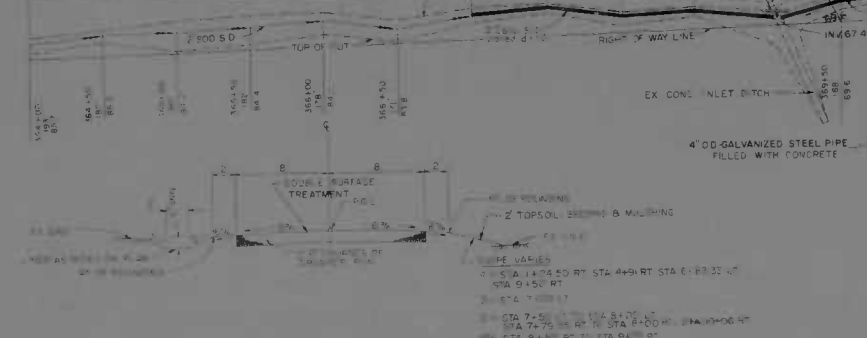
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|------------|---|--|
| | | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO QUINTON ST. | DRAWN BY: ELK CHECKED BY: J.H.H. DES. BY: DCH, B.H.M.A. F.A.P. NO. 1-35-44(2)34 SHEET NO. 30 |

CONSTR STA 7497
 18" Ø B.C.M. PIPE TYPE #12 SLAB
 1' D END SUPPORT WALL FOR 48" PIPE
 1' D TYP. EX. WA. FOR 48" PIPE

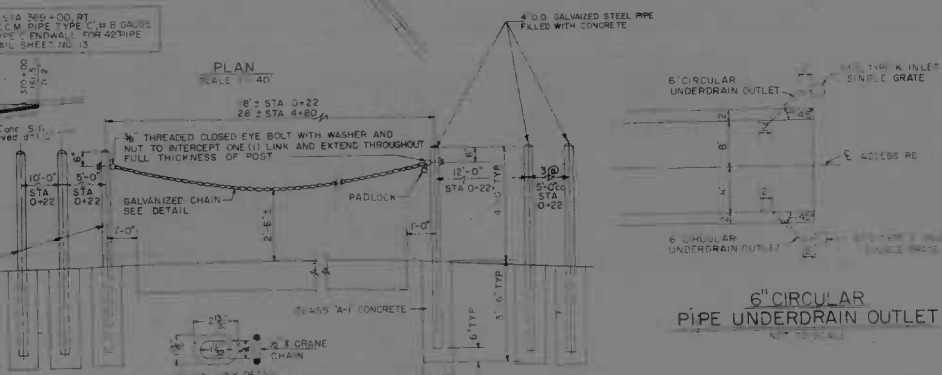


MATCH LINE SEE SHEET NO. 13

| CURVE NO. | PC | PT | PI | TA | EA | EA |
|-----------|-------------|----------|---------|----------|----------|----|
| 1 | 47' 20' 09" | 25' 00" | 54' 98" | 108' 60" | 56' | |
| 2 | 52' 20' 39" | 136' 67" | 67' 29" | 125' 06" | 156' 67" | |
| 3 | 31' 56' 14" | 00' 00" | 38' 22" | 73' 02" | 706' | |
| 4 | 40' 18' 30" | 137' 34" | 5' 18" | 96' 22" | 888' | |



TYPICAL SECTION ACCESS ROAD



POST AND CHAIN BARRIER FOR ACCESS ROAD
 CROSSING AT CANTON RAILROAD & AT KANE ST

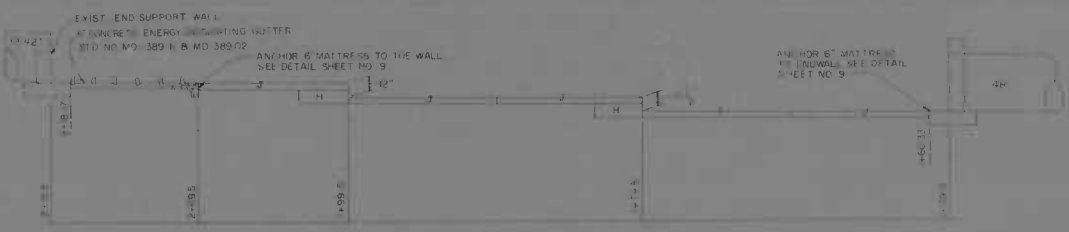
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|--|
| | CONVALL, BEYER, STONE & ASSOC., INC. 711 WEST 40TH STREET BALTIMORE, MARYLAND 21221 | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST. 10 QUINCY ST. | DRAWN BY: J.E. CHECKED BY: J.E. F.A.P. NO. 1-95-4142-34 S.H.A. NO. BS-246-39-R16 BALTO. CITY NO. 207 |

| | | | | |
|---------------------|-------|--------------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 2 | MD | 1-08-4123.04 | 14 | 58 |



PROFILE 4' OUTLET DITCH
LEFT & RIGHT OF ACCESS ROAD

SCALE: HORIZ. = 40'-0"
VERT. = 4'-0"



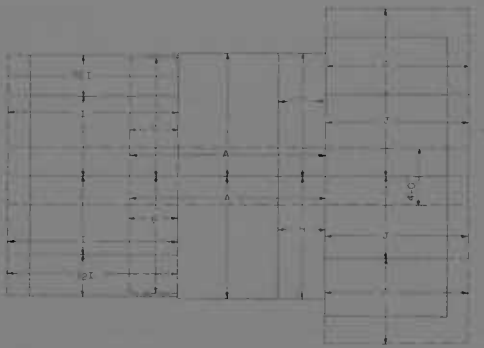
PROFILE OUTLET DITCH - STA. 1+59.00 TO STA. 2+19.50

SCALE: 1" = 4'-0"



STEP DETAIL - STA. 0+00

SCALE: 1" = 4'-0"



PLAN - STILLING POOL

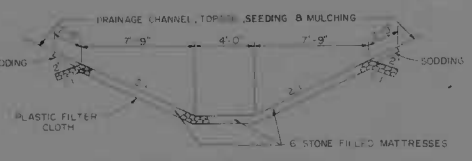
SCALE: 1" = 4'-0"

| TYP. SIZE (BEFORE SETTING) | |
|----------------------------|----------------------|
| A | 3'-3" X 13'-0" X 12" |
| G | 3'-3" X 9'-9" X 8" |
| H | 4'-0" X 9'-9" X 12" |
| I | 12'-0" X 6'-6" X 10" |
| J | 10'-0" X 6'-6" X 6" |



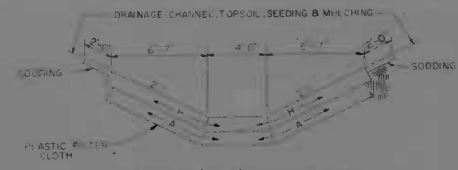
SECTION A-A STILLING POOL

SCALE: 1" = 4'-0"



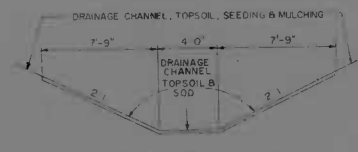
TYPICAL DITCH SECTION
LEFT & RIGHT OF ACCESS ROAD

STA 0+00 TO STA 0+08
STA 0+31.17 TO STA 0+45.17
STA 1+60.33 TO STA 2+09.50
SCALE 1" = 4'-0"



SECTION B-B STILLING POOL

SCALE: 1" = 4'-0"



TYPICAL DITCH SECTION
RIGHT OF ACCESS ROAD

STA 0+08 TO STA 0+31.17
STA 0+65 TO STA 0+85
N.T.S.



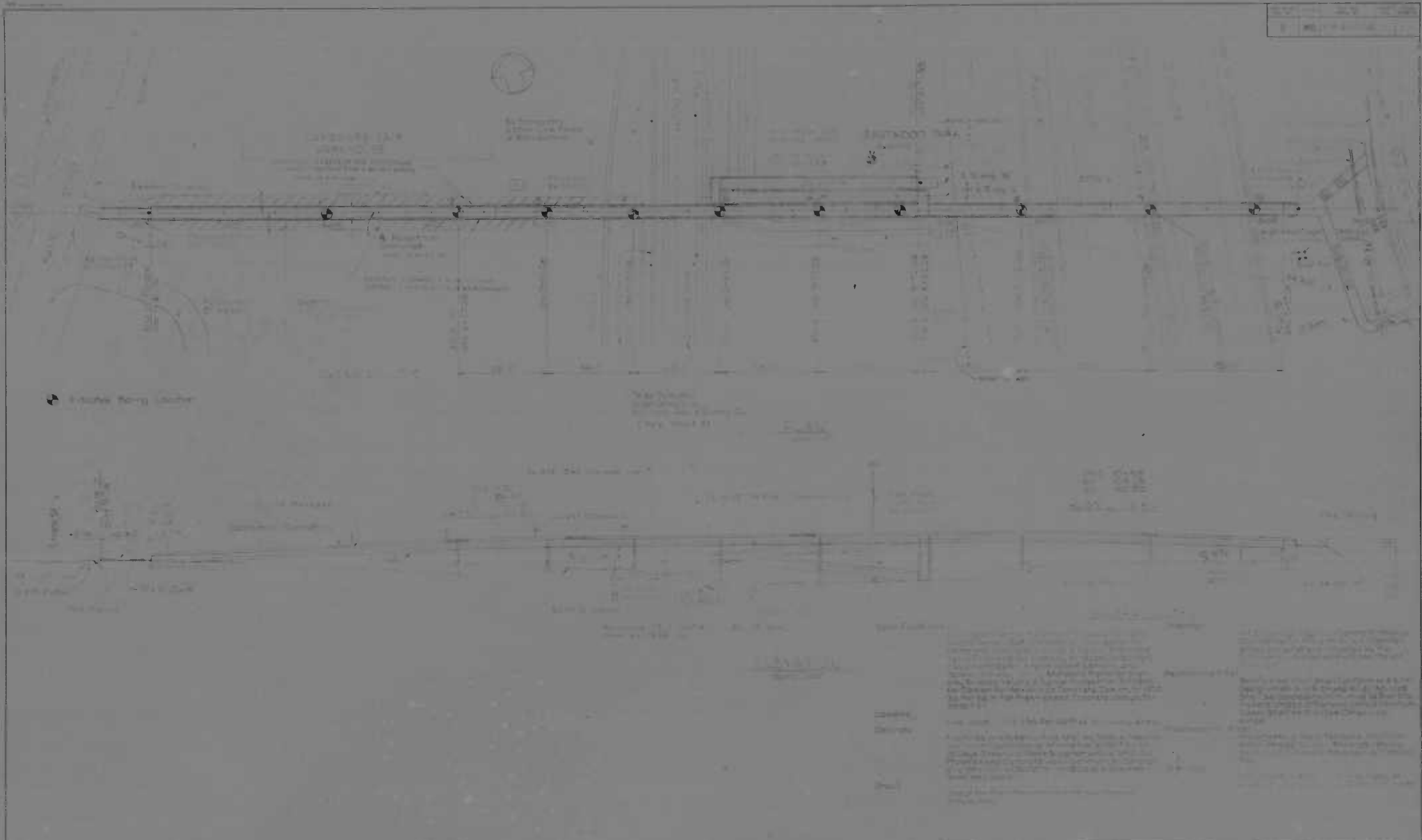
LONGITUDINAL SECTION - STILLING POOL

SCALE: 1" = 4'-0"

THE WIRE BASKETS SHOWN IN THESE DETAILS ARE ON THE BASIS OF 3" X 3" SIZE. IF 3" WIRE BASKETS ARE USED, THEY SHALL MEET OR EXCEED THE MEASUREMENTS AS SHOWN HEREIN.

DRAINAGE DETAILS
LEFT & RIGHT OF ACCESS ROAD

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | KIMBLE, DENNIS, SMITH & ASSOC., INC. AND MATZ, COLLOP & ASSOC., INC. CONSULTING ENGINEERS 711 WEST BOSTON STREET BALTIMORE, MARYLAND 21201 | PROJECT NO. 1-08-4123.04 PEDESTRIAN OVERPASS KANE ST. TO QUINTON ST. | DRAWN BY: H.M.A. CHECKED BY: H.M.A. P.A.P. NO. 1-95-4102-74 S.R.C. NO. 1-95-4102-74 BALDWIN NO. |
| | | SCALE: 1" = 4'-0" | SHEET NO. 14 OF 58 |



TITLE: BRIDGE OVER ...
 SHEET NO. ...
 DATE: ...

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
|-----------|--|---|-------------------------|--|-----------|
| | KIMBLE, HENDER, STUM & ASSOC., INC. CONSULTING ENGINEERS 100 WEST ADAM STREET BALTIMORE, MARYLAND | PROJECT NO. ... DRAWING NO. ... | SCALE: ... DATE: ... | DRAWN BY: ... CHECKED BY: ... DATE: ... | DATE: ... |

Check Sheet (Rev. 10/1/67)

Sheet No. 10

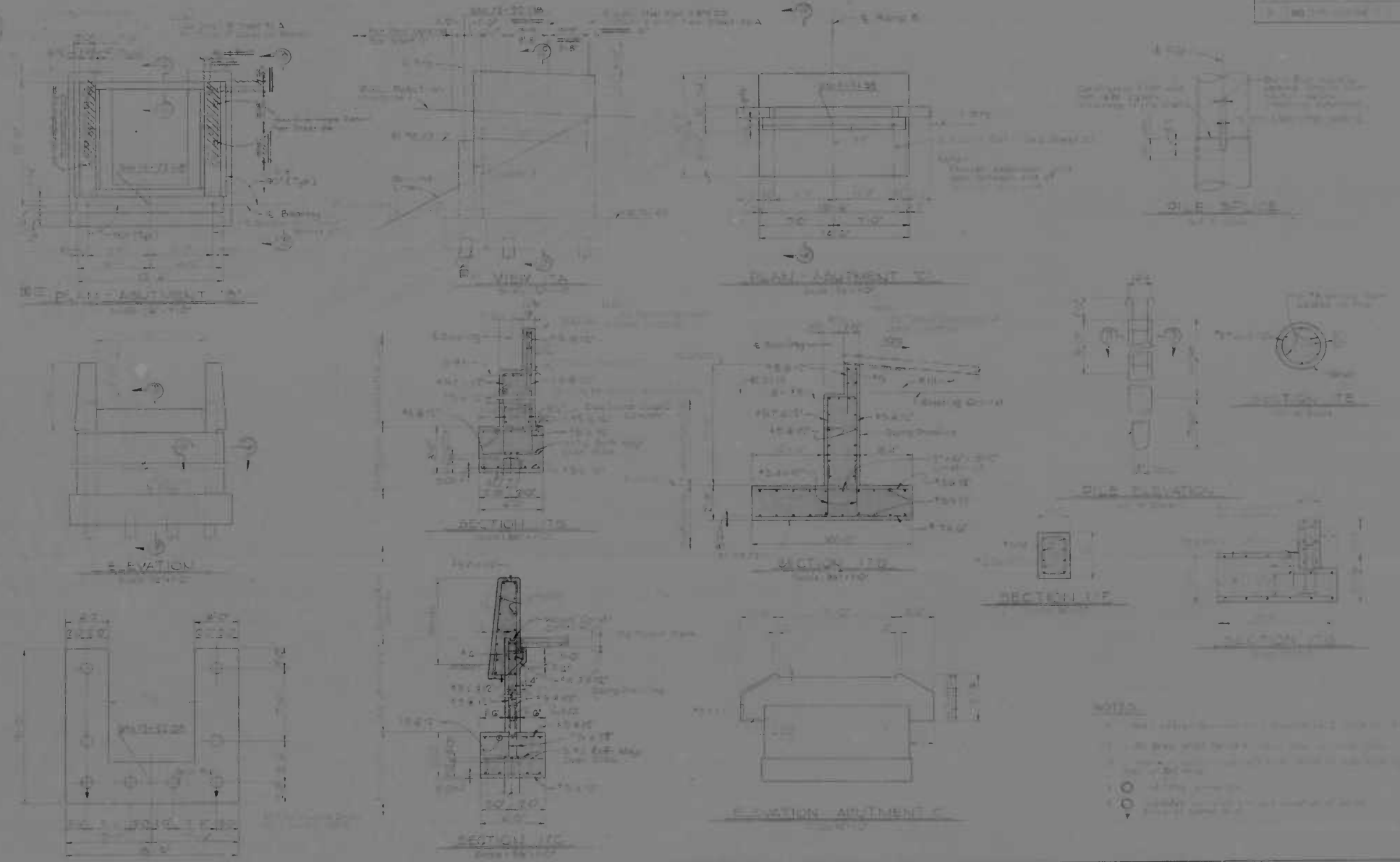


Upper Note 7129c
 1/2" x 1/2" x 1/2" x 1/2"
 TYPICAL DECK JOINT
 Located where Deck meets Vertical
 Scale: 1" = 10'

TYPICAL SECTION

SECTION

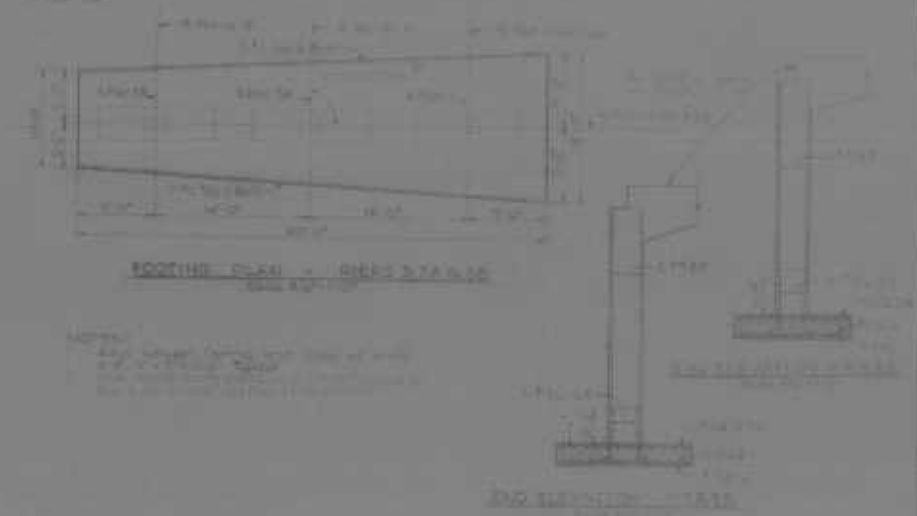
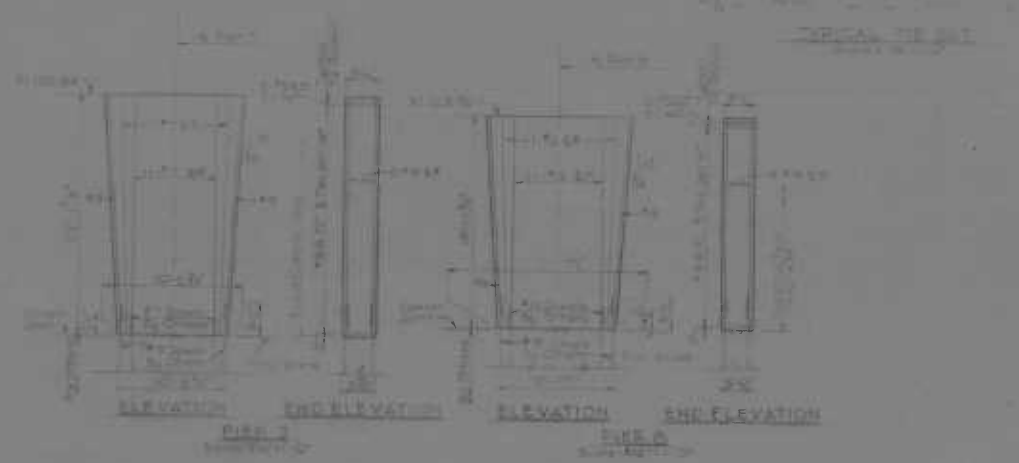
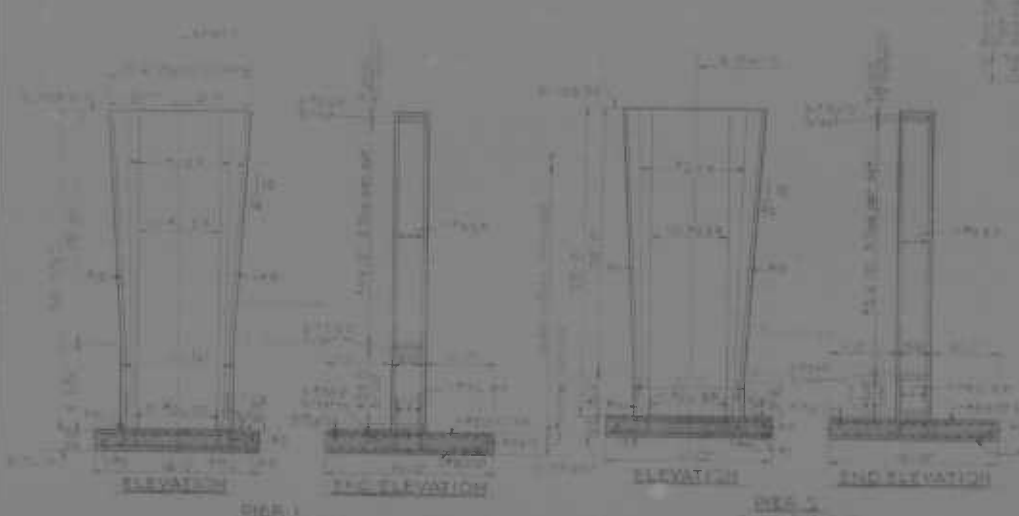
| | | | |
|-------------|------|---|---|
| DESIGNED BY | DATE | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE SUPERVISOR OF HIGHWAYS BALTIMORE DISTRICT OFFICE |
| | | | |



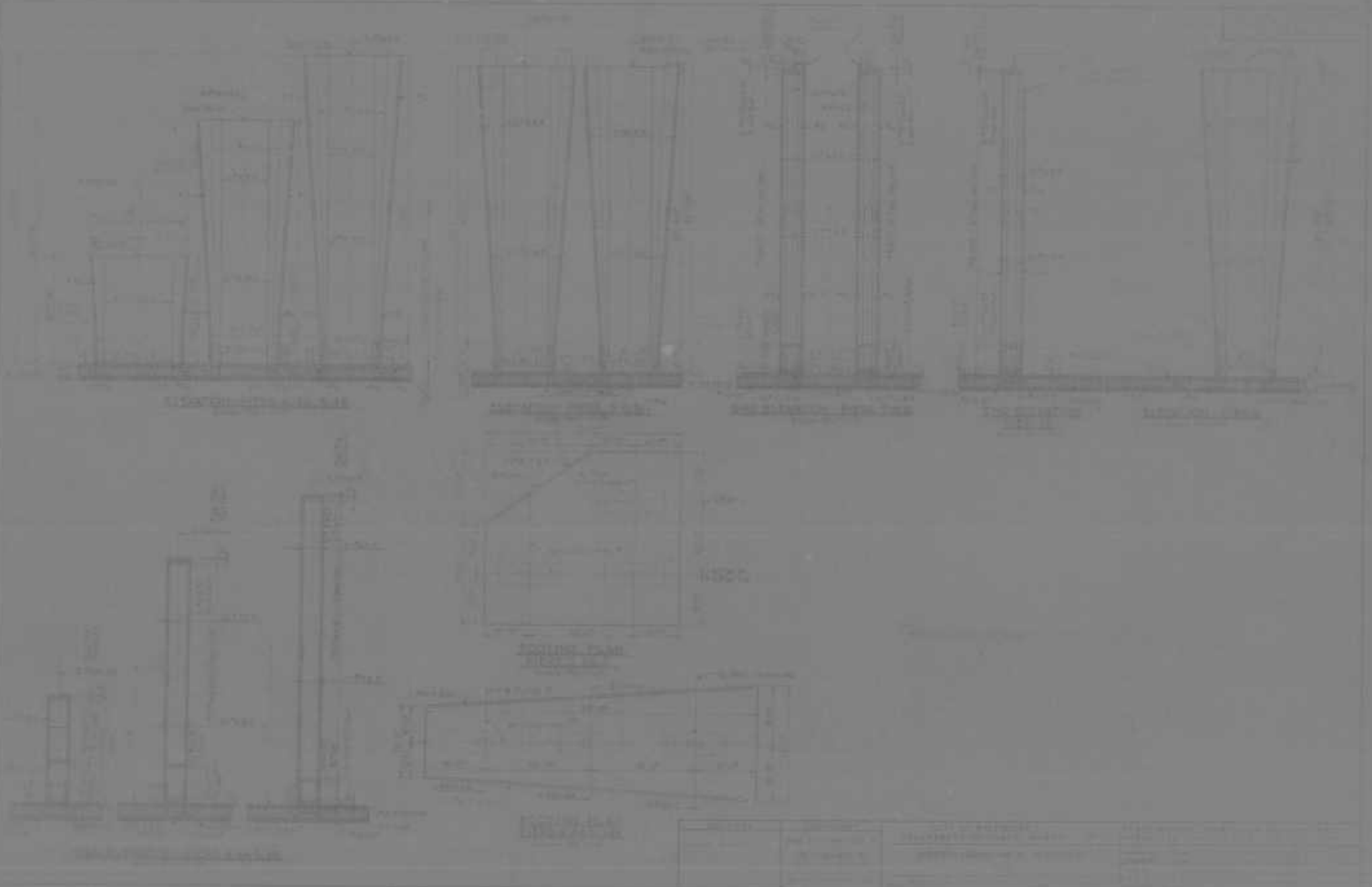
NOTES

1. See notes on drawing 3-100-100-2.
2. All work shall conform to the specifications for manholes.
3. All work shall conform to the specifications for manhole covers.
4. All work shall conform to the specifications for manhole frames.
5. All work shall conform to the specifications for manhole risers.
6. All work shall conform to the specifications for manhole bases.
7. All work shall conform to the specifications for manhole inlets.
8. All work shall conform to the specifications for manhole outlets.
9. All work shall conform to the specifications for manhole manholes.
10. All work shall conform to the specifications for manhole manholes.

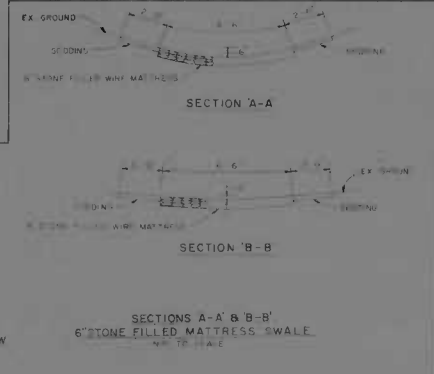
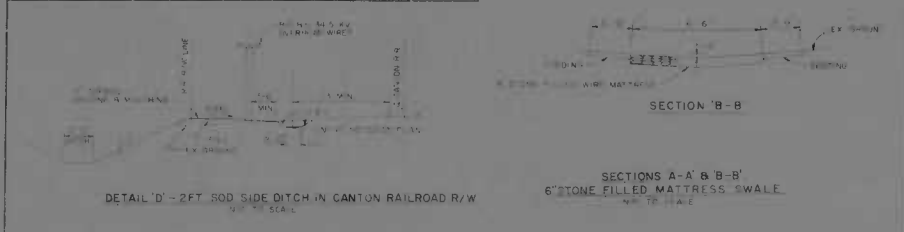
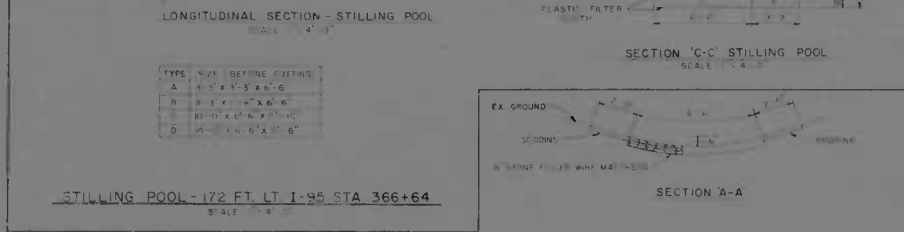
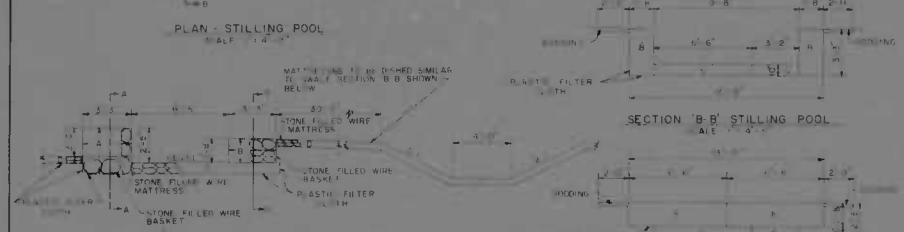
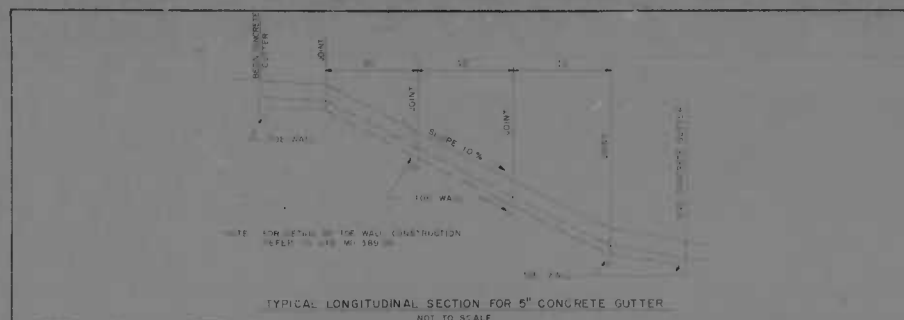
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROAD COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE DIST. |
|--------------|---------------------|---|--|
| 1. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 2. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 3. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 4. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 5. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 6. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 7. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 8. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 9. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |
| 10. 11/15/50 | W. H. B. & C. E. W. | 11/15/50 | 11/15/50 |



| | | | |
|--|--|---|---|
| MILLER 1000 N. BROAD ST. PHILADELPHIA, PA. | ENGINEER 1000 N. BROAD ST. PHILADELPHIA, PA. | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS WATER DIVISION 100 N. CALVERT ST. BALTIMORE, MD. | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS WATER DIVISION 100 N. CALVERT ST. BALTIMORE, MD. |
|--|--|---|---|



| NO. | DESCRIPTION | DATE | BY |
|-----|-------------|------|-----------|
| 1 | DESIGNED | 1911 | J. H. ... |
| 2 | REVISED | 1911 | J. H. ... |
| 3 | REVISED | 1911 | J. H. ... |
| 4 | REVISED | 1911 | J. H. ... |



ABBREVIATIONS

- B.M. = Bench Mark
- B.L.S. = Base Line Survey
- C.L.S. = Center Line Survey
- Elev. = Elevation
- P.G.L. = Profile Ground Line
- P.G.L. = Profile Grade Line
- P/C = Point of Crown
- P/R = Point of Rotation
- P/G.E. = Profile Grade Elevation
- S.S.D. = Sighting Sight Distance
- H.S.D. = Headlight Sight Distance
- S.E. = Superelevation
- S.D.D. = Surface Drain Ditch
- S.D. = Side Ditch
- S.F.D.P.L. = Standard Plate

- P.S.F. = Pounds Per Square Foot
- N.T.S. = Not to Scale
- P.S.I. = Pounds Per Square Inch
- Proposed Inlet
- Inlet No.

TOPOGRAPHY LEGEND

- FOUN. OF FOUNDATIONS
- FLAGPOLE
- ALL TYPE FENCES
- FENCE ON RETAINING WALL
- RETAINING WALL
- GUARD POSTS & LADLE
- POSTS OR GUARD POSTS
- STEEL GUARD RAIL
- RAILROAD
- R.R. SWITCH
- R.R. HUMBER
- R.R. SIGNAL
- R.P. SIGNAL OR R. TRAFFIC CONTROL BOX
- TRAFFIC LIGHT
- LIGHT POLE
- UTILITY AND LIGHT POLE
- UTILITY, LIGHT POLE WITH TRANSFORMER
- UTILITY POLE
- UTILITY POLE WITH TRANSFORMER
- SUP. POLE
- WIRELESS SIGN
- LIGHT POLES WITH LATERAL TRANSFORMERS
- UTILITY POLES WITH LATERAL TRANSFORMERS
- TELEPHONE
- OVERHEAD TRAFFIC CONTROL SWITCH
- FIRE HYDRANT
- DRAIN INLET
- TRASH BASKET
- BILLBOARD
- MANHOLE
- SOIL VENT PIPE
- WATER OR GAS VALVE, WATER METER
- PIPE OR POLE (SIGNAL, CABLE OR TELEPHONE) POLE
- GRATE
- UNDERGROUND P.C.
- SEWER ENTRANCE
- NO. 25 SIGN LAYOUT
- R.R. CROSSING GATE
- UTILITY, LIGHT POLE WITH FIRE OR POWER WIRE
- RATE OF GRADE - 1/4"
- DITCH (SPEL AND MOUNT)
- DITCH (SPEL) MOUNTING
- DITCH (PAVED)
- HEAD
- TREE
- TRAIL (LARGE) (D)
- ULVERT - EXISTING
- STREAM OR SHOULDER
- INTERMEDIATE CONTOUR
- INDEX CONTOUR
- DEPRESSION - NO CROWN AREA
- B.M. 5
- STA. A / Z
- PHOTOGRAMMETRIC ELEVATION
- P.L. 1055
- FIELD ELEVATION
- AVERAGE
- BTM
- BTM
- BR
- BR
- CONC
- DWG
- FLAGPOLE
- F.F.
- FRAME
- GAP
- G.A.
- L.P.
- M.S.
- M.A.S.
- M.T.
- P.T. ST.
- R.W. P.T.W.
- U.S.N.
- SH
- STA
- SIC
- STY
- TRANSFORMER
- AV. 254
- PLACED RIPRAP INLET
- ULVERT - PROPOSED
- ULVERT - EXISTING
- ULVERT - GRADE ELEVATION
- ULVERT - GRADE ELEVATION
- WATER

EXPLANATORY NOTES AND REFERENCES

VERTICAL CONTROL
The Location and Elevations of Bench Marks are Shown on the Plans. All Elevations Shown are Based on Baltimore Datum.

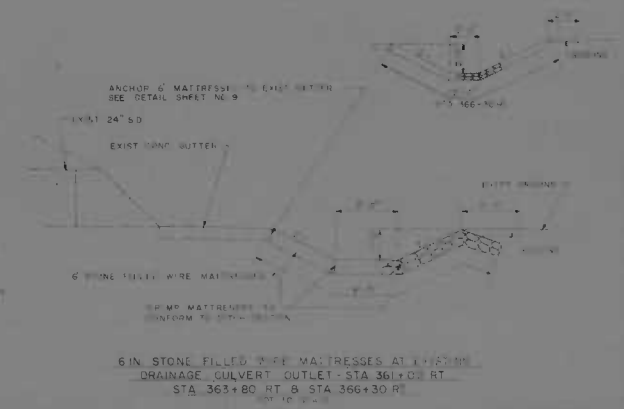
DITCHES
All Side Ditches on this Project will be 2' in Width Unless Otherwise Noted on the Plans.

INVERT ELEVATIONS
All Invert Elevations are Approximate and may be Modified to Meet Conditions Encountered during Installation of Drainage Structures.

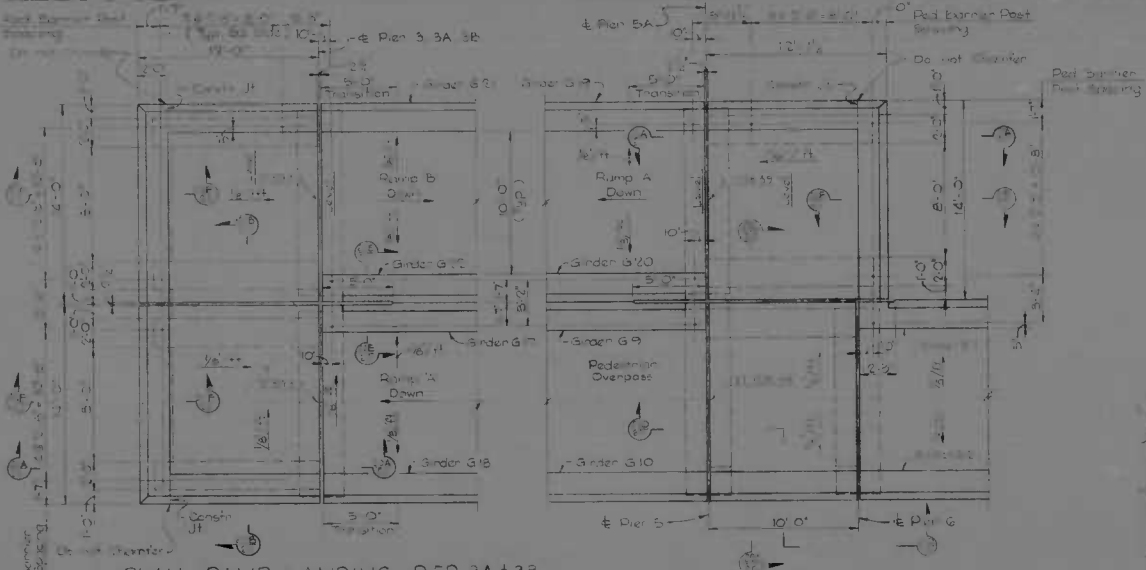
MATERIALS SALVAGED IN CONSTRUCTION
These Materials shall become the Property of the Contractor and shall be Removed from the Site of Construction Except for Those Items Provided for Salvage as S.P.C. Property in the Special Provisions.

HORIZONTAL CONTROL
The Project is Oriented in Conformity with the Baltimore City Grid System.

SPECIFICATIONS
Specifications for Materials, Highways, Bridges and Incidental Structures, Dated March, 1968 and Revised Thereof or Additions Therein included in the Proposal and Special Provisions.

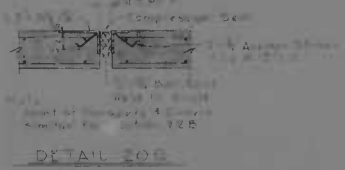


| | | | |
|-----------|---|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | KIMBLE, POWELL, STORE & ASSOC., INC. AND WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 7 WEST 40TH STREET BALTIMORE, MARYLAND 21211 | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO QUINTON ST. | DRAWN BY J.J.K. CHECKED BY J.J.K. DES. BY H.V.A. CHK. BY |
| | | SCALE - As Shown | DATE |
| | | | SHEET NO. |

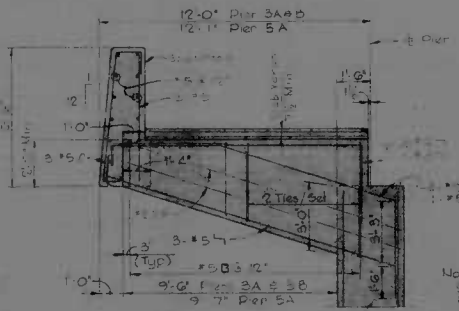


PLAN - RAMP LANDING - PIER 3A & 3B
Scale: 1/4" = 1'-0"

PLAN - RAMP LANDING - PIER 5 & 5A
Scale: 1/2" = 1'-0"

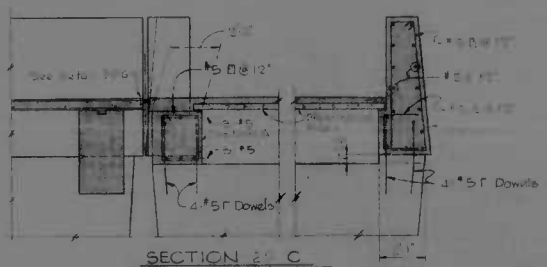


DETAIL 20B

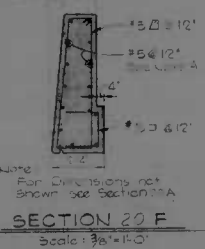


SECTION 20A
Scale: 3/8" = 1'-0"

Note: See sheet #11 for Pier Details

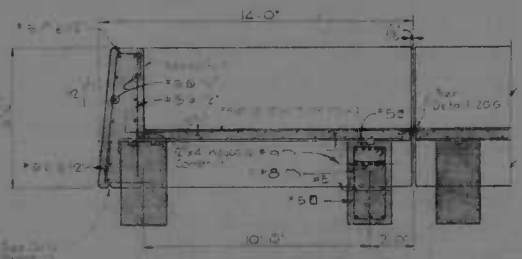


SECTION 20C
Scale: 3/8" = 1'-0"

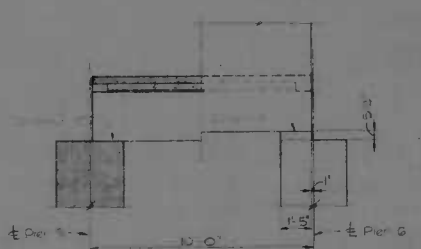


Note: For dimensions not shown see Section 20A

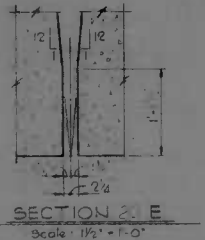
SECTION 20F
Scale: 3/8" = 1'-0"



SECTION 20B
Scale: 3/8" = 1'-0"

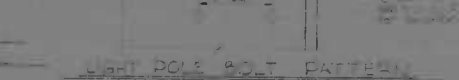
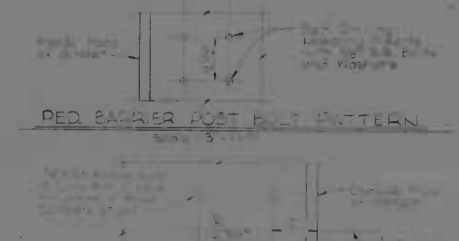
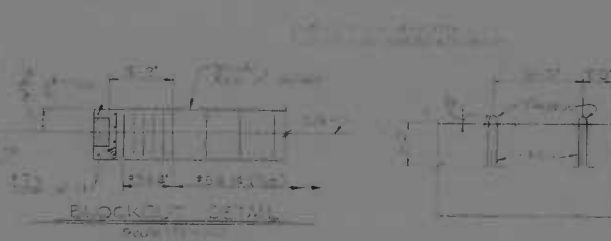
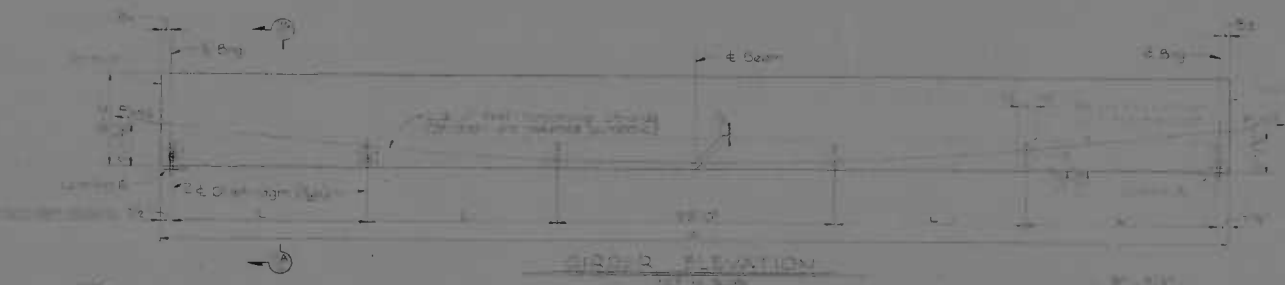
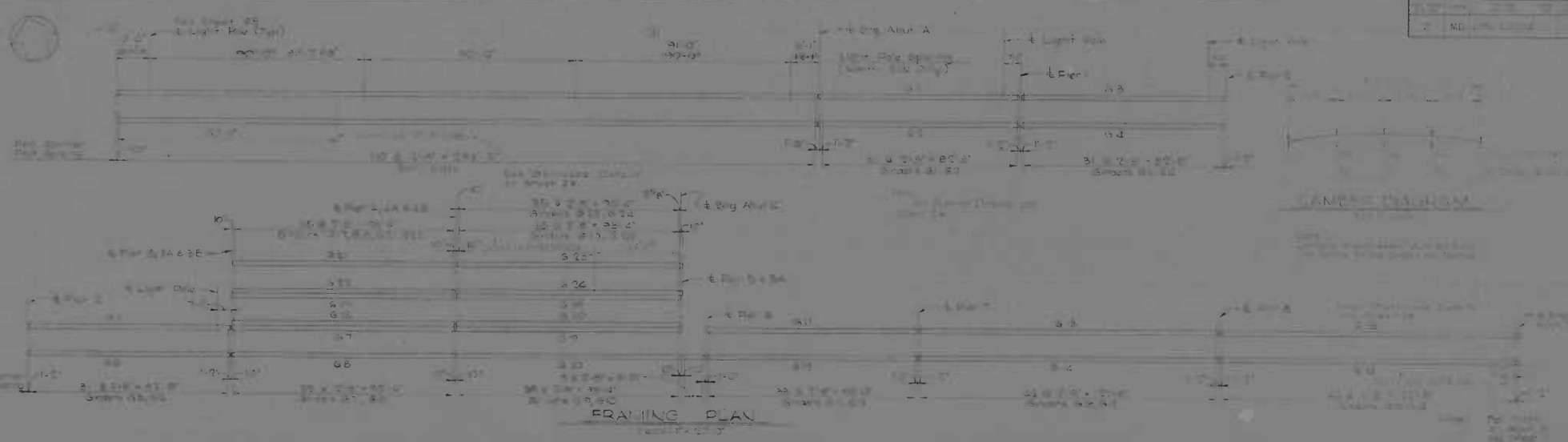


SECTION 20D
Scale: 3/8" = 1'-0"



SECTION 20E
Scale: 1/2" = 1'-0"

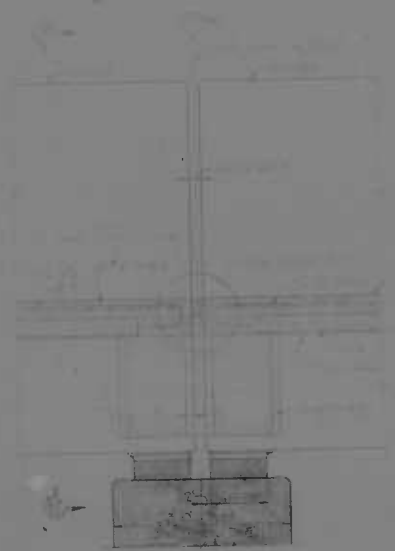
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | WARREN, BECKER & ASSOC., INC. CONSULTING ENGINEERS 125 WEST 40th STREET NEW YORK, N.Y. 10018 | PEDESTRIAN OVERPASS KANE ST. TO GUNTON ST. RAMP LANDINGS | DRAWN BY: F.B.O. CHECKED BY: T.J.S. TRACED BY: F.B.O. DATE: 2-26-54 F.P. NO. 2-26-54-220 SHEET NO. 2 C.E. NO. 54-2-220 |
| | | SCALE: AS SHOWN DATE: 2-26-54 | BALTO. CITY NO. 54-1 |



POST-TENSIONED PRESTRESSING NOTES (CONTD)
 GENERAL
 2. Tendons to be jacked to 0.75fs for G13 to G16, 0.74fs for G7 to G12 and G17 to G24, 0.75fs for G1 to G6. Anchorage tendons at 0.70fs (equivalent anchor set = 70%).
 Final stress in tendons shall be 0.81fs max.

| NO. | DESCRIPTION | DATE | BY | CHECKED |
|-----|-------------|------|----|---------|
| 1 | AS SHOWN | | | |
| 2 | | | | |
| 3 | | | | |
| 4 | | | | |
| 5 | | | | |
| 6 | | | | |
| 7 | | | | |
| 8 | | | | |
| 9 | | | | |
| 10 | | | | |
| 11 | | | | |
| 12 | | | | |
| 13 | | | | |
| 14 | | | | |
| 15 | | | | |
| 16 | | | | |
| 17 | | | | |
| 18 | | | | |
| 19 | | | | |
| 20 | | | | |
| 21 | | | | |
| 22 | | | | |
| 23 | | | | |
| 24 | | | | |
| 25 | | | | |
| 26 | | | | |
| 27 | | | | |
| 28 | | | | |
| 29 | | | | |
| 30 | | | | |
| 31 | | | | |
| 32 | | | | |
| 33 | | | | |
| 34 | | | | |
| 35 | | | | |
| 36 | | | | |
| 37 | | | | |
| 38 | | | | |
| 39 | | | | |
| 40 | | | | |
| 41 | | | | |
| 42 | | | | |
| 43 | | | | |
| 44 | | | | |
| 45 | | | | |
| 46 | | | | |
| 47 | | | | |
| 48 | | | | |
| 49 | | | | |
| 50 | | | | |

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROAD COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|--------------------------|------------|---|---|
| 1. 10/72 Addendum A | | | |
| 2. 4/73 Light Pole | | | |
| 3. 5/73 Dimension Change | | | |



SECTION THROUGH DECK JOINT



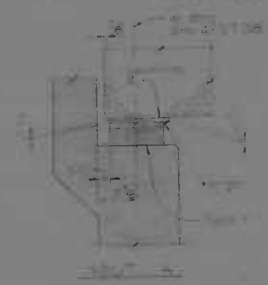
SECTION 1A



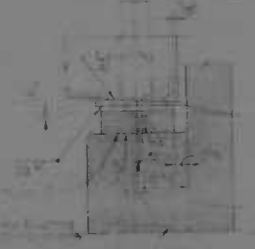
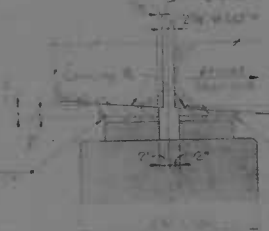
ELEVATION



ELASTOMERIC SANDWICH SEAL DETAILS



PIER BEARING DETAIL



PIER BEARING DETAIL



ELEVATION



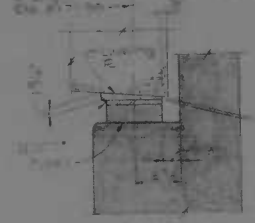
ELEVATION



ELEVATION

LEVEL 3 REINFORCEMENT

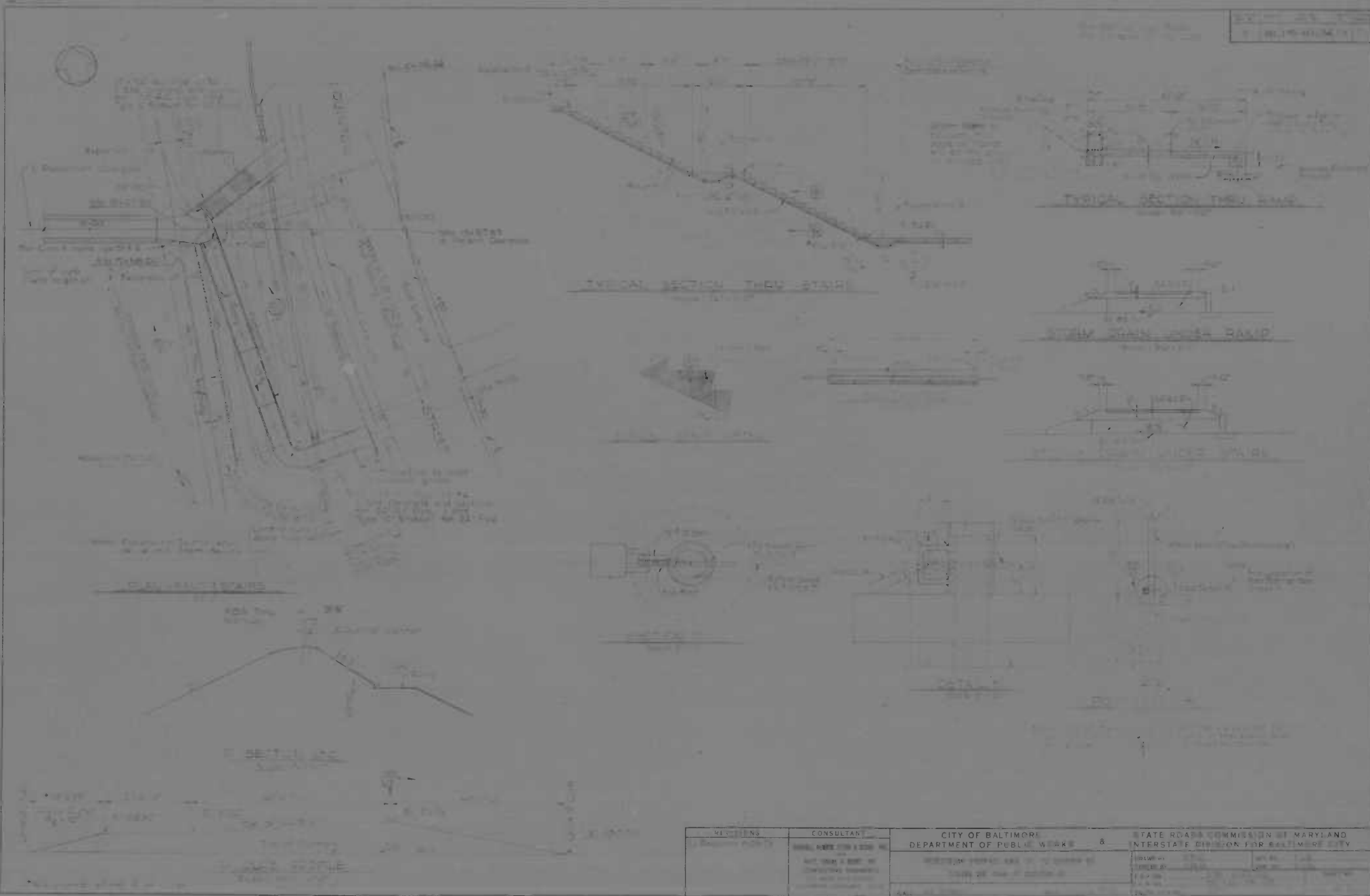
| NO. | DESCRIPTION | QUANTITY | UNIT | TOTAL |
|-----|-------------|----------|------|-------|
| 1 | ... | ... | ... | ... |
| 2 | ... | ... | ... | ... |
| 3 | ... | ... | ... | ... |
| 4 | ... | ... | ... | ... |
| 5 | ... | ... | ... | ... |
| 6 | ... | ... | ... | ... |
| 7 | ... | ... | ... | ... |
| 8 | ... | ... | ... | ... |
| 9 | ... | ... | ... | ... |
| 10 | ... | ... | ... | ... |
| 11 | ... | ... | ... | ... |
| 12 | ... | ... | ... | ... |
| 13 | ... | ... | ... | ... |
| 14 | ... | ... | ... | ... |
| 15 | ... | ... | ... | ... |
| 16 | ... | ... | ... | ... |
| 17 | ... | ... | ... | ... |
| 18 | ... | ... | ... | ... |
| 19 | ... | ... | ... | ... |
| 20 | ... | ... | ... | ... |
| 21 | ... | ... | ... | ... |
| 22 | ... | ... | ... | ... |
| 23 | ... | ... | ... | ... |
| 24 | ... | ... | ... | ... |
| 25 | ... | ... | ... | ... |
| 26 | ... | ... | ... | ... |
| 27 | ... | ... | ... | ... |
| 28 | ... | ... | ... | ... |
| 29 | ... | ... | ... | ... |
| 30 | ... | ... | ... | ... |
| 31 | ... | ... | ... | ... |
| 32 | ... | ... | ... | ... |
| 33 | ... | ... | ... | ... |
| 34 | ... | ... | ... | ... |
| 35 | ... | ... | ... | ... |
| 36 | ... | ... | ... | ... |
| 37 | ... | ... | ... | ... |
| 38 | ... | ... | ... | ... |
| 39 | ... | ... | ... | ... |
| 40 | ... | ... | ... | ... |
| 41 | ... | ... | ... | ... |
| 42 | ... | ... | ... | ... |
| 43 | ... | ... | ... | ... |
| 44 | ... | ... | ... | ... |
| 45 | ... | ... | ... | ... |
| 46 | ... | ... | ... | ... |
| 47 | ... | ... | ... | ... |
| 48 | ... | ... | ... | ... |
| 49 | ... | ... | ... | ... |
| 50 | ... | ... | ... | ... |



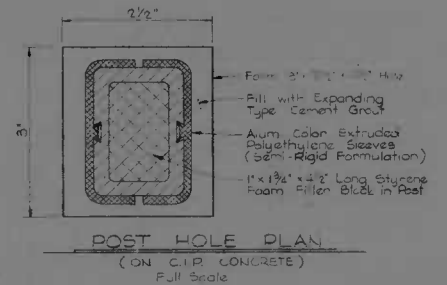
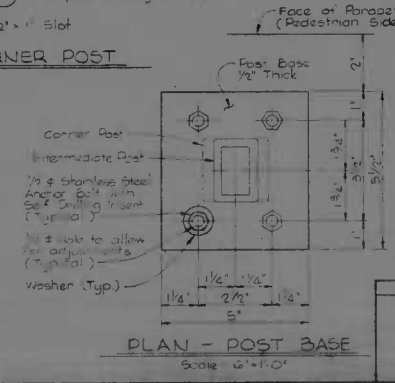
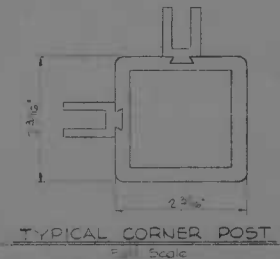
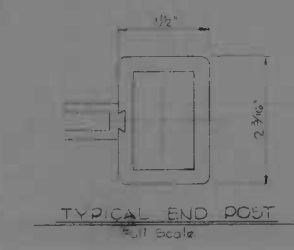
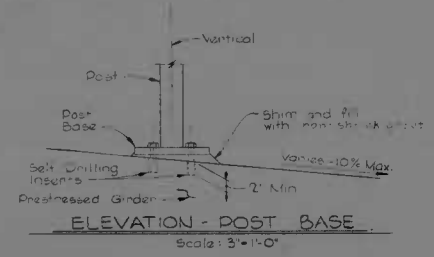
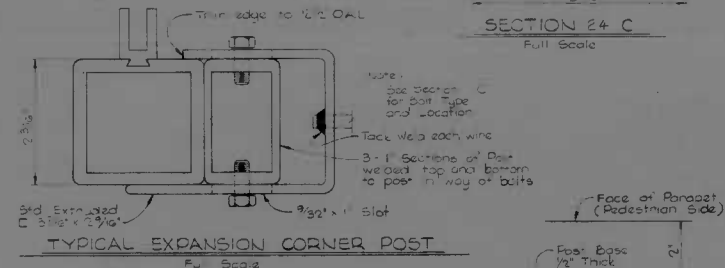
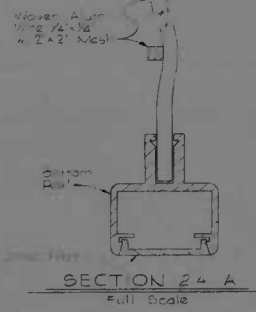
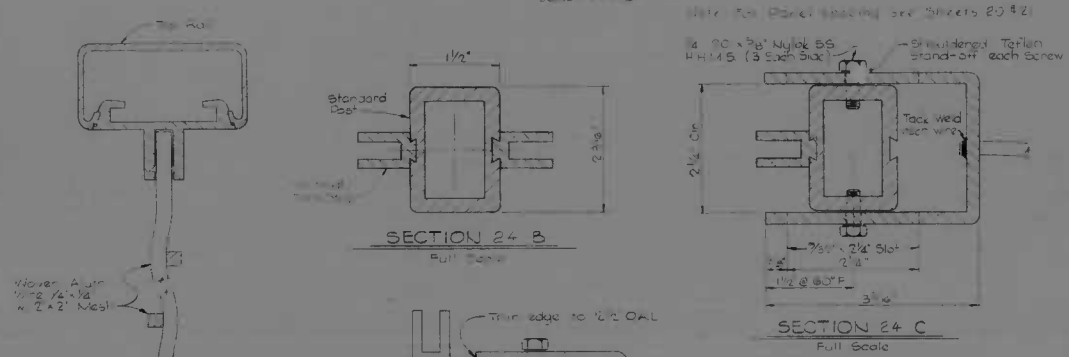
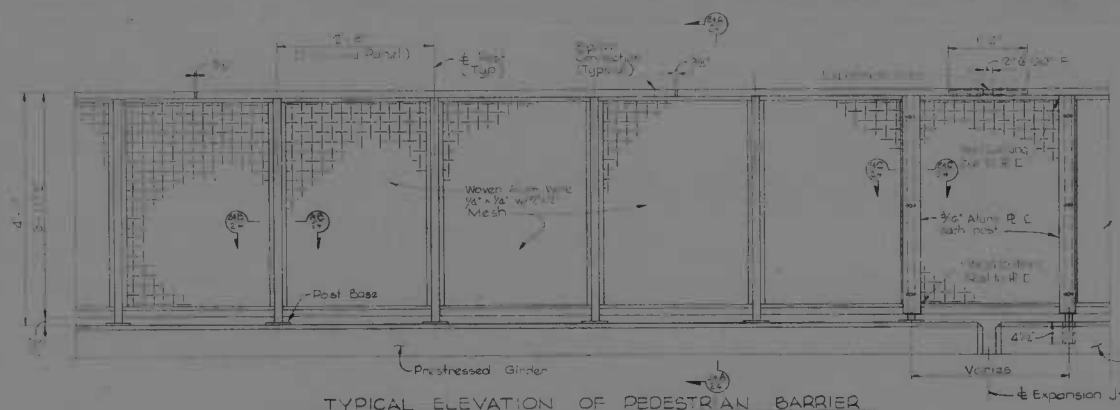
PIER BEARING DETAIL

| REVISION | DATE | DESCRIPTION | BY | CHECKED |
|----------|--------|-------------------------|-----|---------|
| 1 | 8/2/70 | Issued for construction | ... | ... |

| | |
|---|---|
| CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROAD COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| PROJECT NO. 100-100000-01 | CONTRACT NO. 100-100000-01 |
| DATE: 8/2/70 | SCALE: AS SHOWN |



| | | | |
|-----------|---|---|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROAD COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | W. H. HARRIS & SONS, INC. 1000 BALTIMORE AVENUE BALTIMORE, MARYLAND | 8 | |
| | | | |
| | | | |



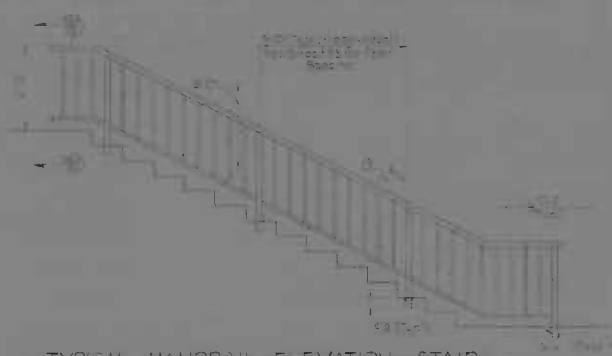
Notes for Sheet 24 for Post Barrier for use with Pedestrian Protective Barrier.

NOTES

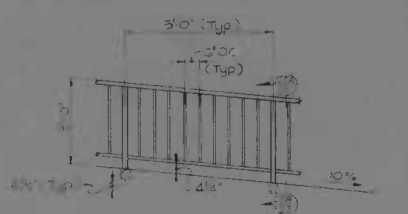
- The barrier shall be fabricated and erected as indicated on the contract drawings.
- Posts shall be set vertical.
- Material for extruded components shall conform to ASTM Designation B-121, Alloy 6063-T6.
- Material for bars for woven wire mesh shall conform to ASTM Designation B-61, Alloy 3043-T6.
- Material for other plate and pipe shall conform to ASTM Designation B-209, Alloy 6061-T6.
- Material for anchor bolts shall conform to ASTM Designation A-276 Type 304, Alloy 304, A-307 Hot Finished, Ultimate Strength 55,000 psi, minimum 20% minimum elongation.
- Material for washers and bolts shall conform to ASTM Designation B-109, Alloy A307, Grade 2.
- Material for washers and bolts shall conform to ASTM Designation B-109, Alloy A307, Grade 2.
- Bottom of post bases and shims shall be thoroughly coated with an approved waterproofing compound as approved and checked in place.
- Form of concrete shall be approved by the Engineer and shall be in accordance with the approved drawings.

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|--|--|--|
| | ENGINEER: ROYER STON & ASSOC., INC. 1425 N. WASHINGTON ST. BALTIMORE, MARYLAND 21201 | PEDESTRIAN OVERPASS KAME ST. TO QUINTON ST. PEDESTRIAN PROTECTIVE BARRIER DETAILS | DRAWN BY: J.S. TRACED BY: J.S. DATE: 10/1/74 SHEET NO. 2 OF 2 |

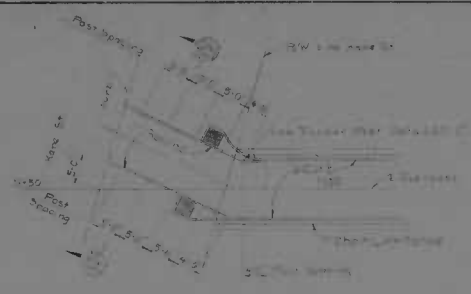
| | | | |
|----------|------|----|-----|
| REV. NO. | DATE | BY | CHK |
| 2 | MD | | |



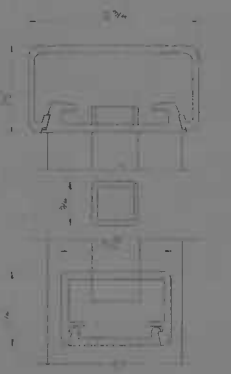
TYPICAL HANDRAIL ELEVATION - STAIRS
Scale: 1/2" = 1'-0"



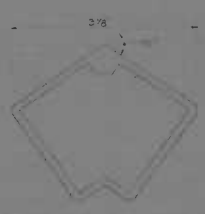
TYPICAL HANDRAIL ELEVATION - RAMP
Scale: 1/2" = 1'-0"



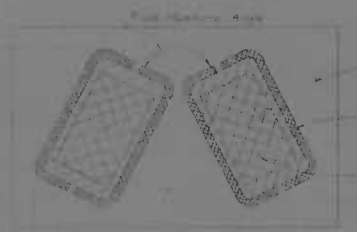
PART PLAN
SCALE



SECTION 25 A
Full Scale

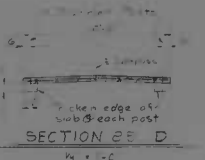


CORNER POST DETAIL B
Full Scale

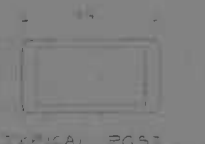


CORNER POST DETAIL C
Full Scale

- Fill with Expanding
Tee Cement Grout
- 4 in. Dia. Extruded
Polystyrene Sleeves
(2000 High Performance)
- 1/4" x 1/2" Long Styrene
Foam Filler Strips



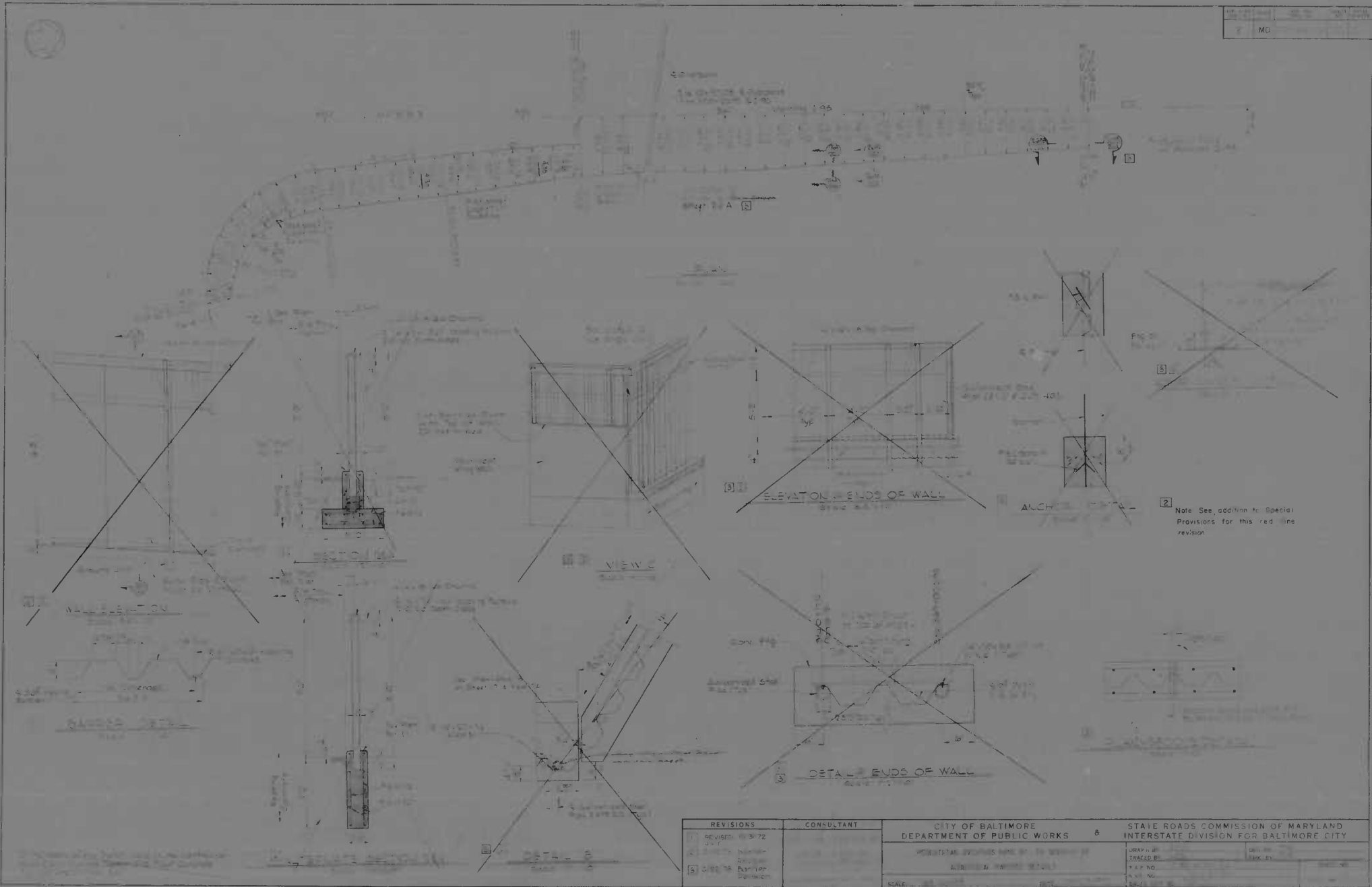
SECTION 25 D
Full Scale



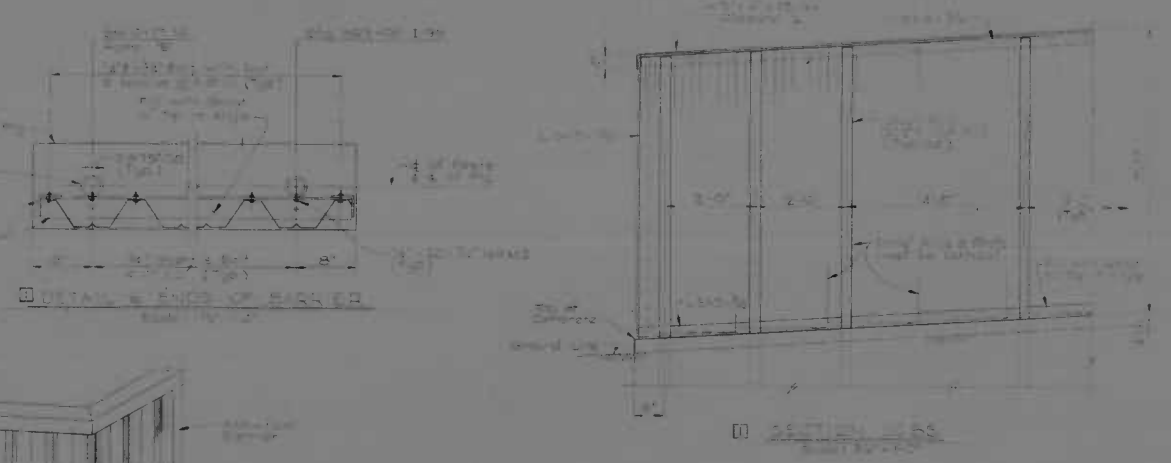
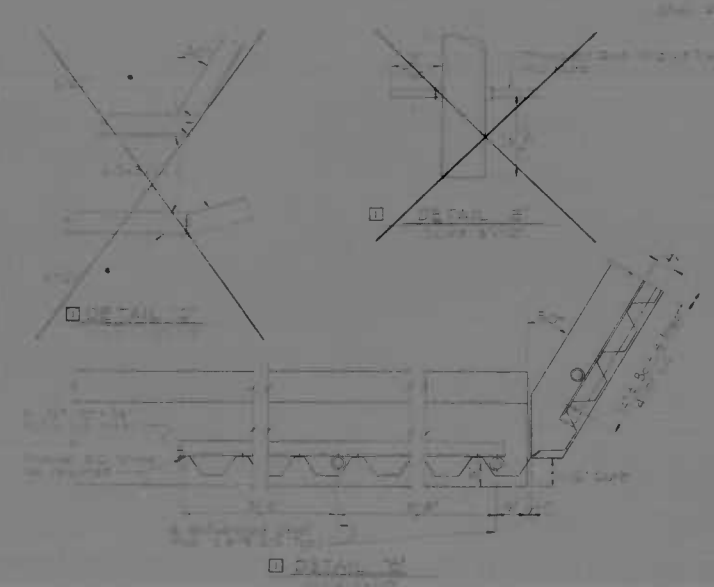
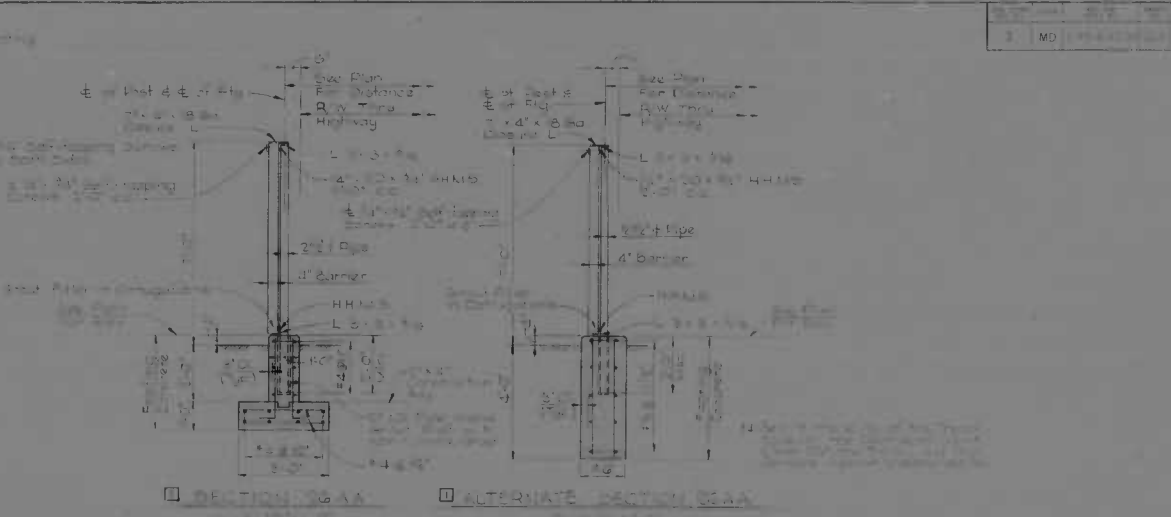
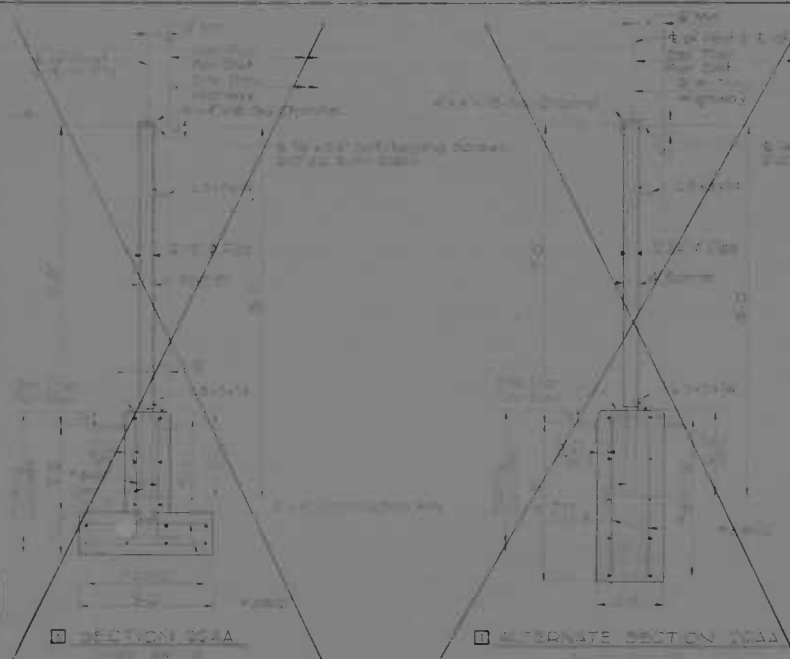
TYPICAL POST
Full Scale

NOTES
Refer to the Detail C.

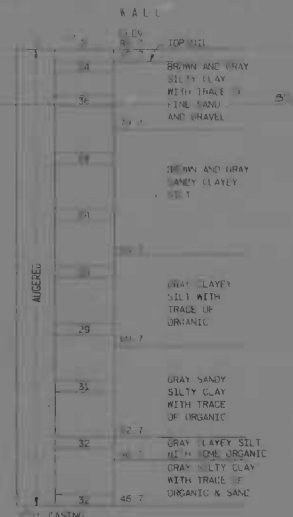
| | | | |
|-----------|--|---|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | SPENCER, BEAR, ROSE & ASSOC., INC. 401 BAYVIEW AVE. Baltimore, MD 21214 Tel: 336-1100 | PEDESTRIAN OVERPASS KANE ST. TO DITCHON ST PEDESTRIAN SAFETY RAILING DETAILS | DRAWN BY: [Signature] TRACED BY: [Signature] F.P. NO. 10-2-84 S.I.A. NO. 10-2-84-20 BALTIMORE CITY NO. [Number] |
| | | SCALE: [Scale] | DATE: [Date] |

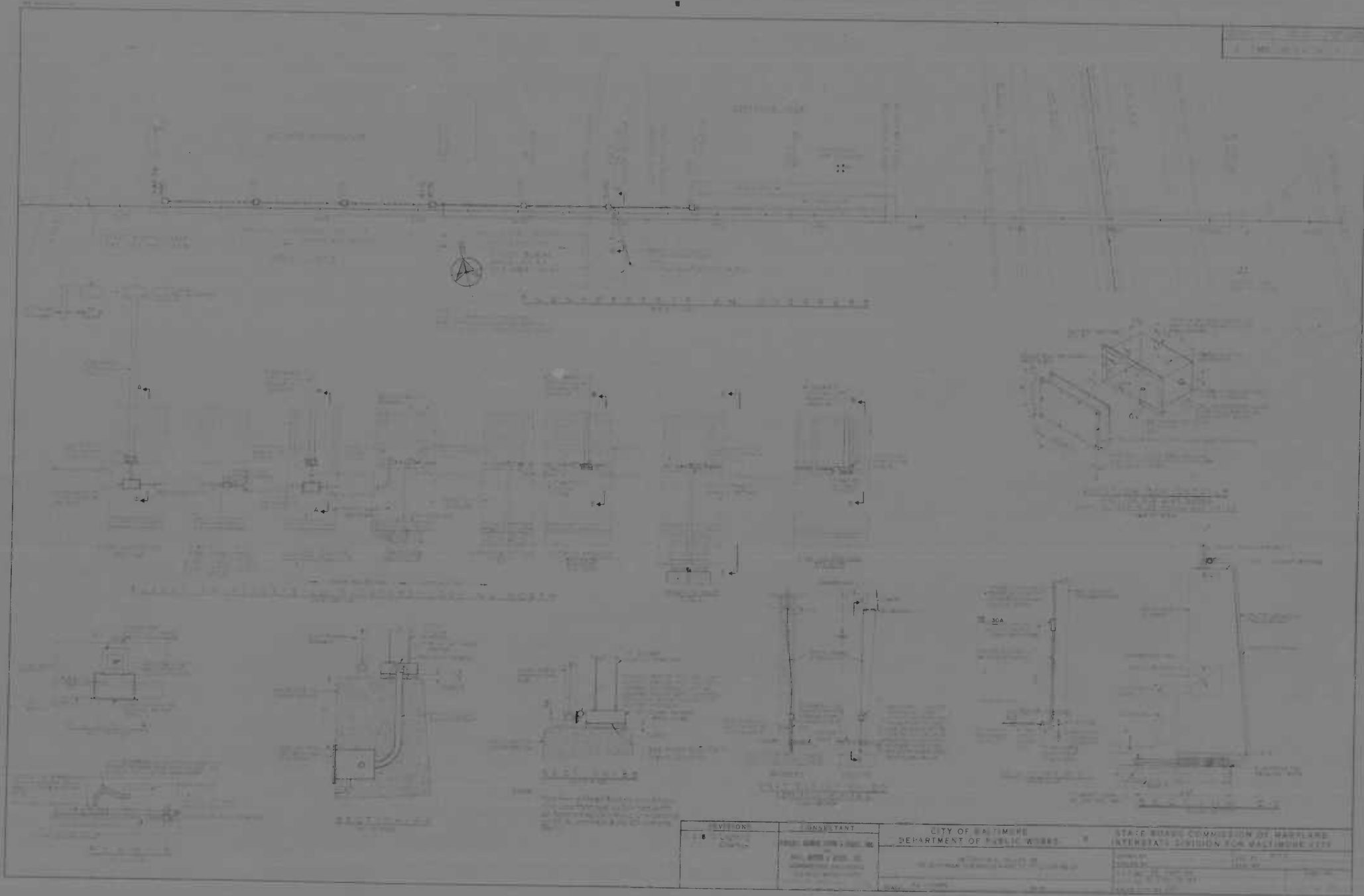


| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|----------------------------|------------|---|--|
| 1) REVISED 11/3/72 | | | |
| 2) CHECKED BY: [Signature] | | | |
| 3) DATE: 11/3/72 | | | |
| | | SCALE: 1/8" = 1'-0" | DRAWN BY: [Signature] CHECKED BY: [Signature] |



| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|--------------------------|------------|---|--|
| 1 6. 2000 Revision | | | |
| | | DRAWN BY TRACED BY F.A.P. NO. DATE | DESIGNED BY CHECKED BY DATE |





| | | | |
|-----------------------------|--|--|---|
| DIVISION STREET LIGHTING | CONSULTANT THE BURNS & MCDONNELL CO. 1100 MARKET STREET PHILADELPHIA, PA. | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS 100 BALTIMORE STREET BALTIMORE, MARYLAND | STATE BOARD OF COMMISSIONERS OF HIGHWAYS INTERSTATE DIVISION FOR BALTIMORE CITY 1100 MARKET STREET PHILADELPHIA, PA. |
|-----------------------------|--|--|---|



| DATE | | BY | | FOR | |
|------|----------|-----------------|----------|----------|----------|
| 1 | 10/15/68 | J. J. [unclear] | 10/15/68 | 10/15/68 | 10/15/68 |
| 2 | 10/22/68 | J. J. [unclear] | 10/22/68 | 10/22/68 | 10/22/68 |
| 3 | 11/1/68 | J. J. [unclear] | 11/1/68 | 11/1/68 | 11/1/68 |
| 4 | 11/15/68 | J. J. [unclear] | 11/15/68 | 11/15/68 | 11/15/68 |
| 5 | 11/22/68 | J. J. [unclear] | 11/22/68 | 11/22/68 | 11/22/68 |
| 6 | 12/1/68 | J. J. [unclear] | 12/1/68 | 12/1/68 | 12/1/68 |
| 7 | 12/15/68 | J. J. [unclear] | 12/15/68 | 12/15/68 | 12/15/68 |
| 8 | 12/22/68 | J. J. [unclear] | 12/22/68 | 12/22/68 | 12/22/68 |

| | | | |
|------------------------|------------|----------------------------|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE | STATE ROAD COMMISSION OF MARYLAND |
| | | DEPARTMENT OF PUBLIC WORKS | INTERSTATE DIVISION FOR BALTIMORE CITY |
| PROJECT: [unclear] | | 8 | |
| DRAWN BY: [unclear] | | DATE: [unclear] | SCALE: [unclear] |
| CHECKED BY: [unclear] | | DATE: [unclear] | SCALE: [unclear] |
| APPROVED BY: [unclear] | | DATE: [unclear] | SCALE: [unclear] |

SUMMARY OF QUANTITIES

| | | | |
|---------------------|-------|----------------|--------------|
| FED. ROAD DIST. NO. | COUNT | SHEET NO. | TOTAL SHEETS |
| 2 | MD | 1-95-442(3) 29 | 36 |

| STATION | GRADING | | | | | | | | | | DRAINAGE | | | | | | | | | | REMARKS |
|---------|---------|-----|-----|-----|----|----|----|----|----|----|----------|----|----|----|----|----|----|----|----|----|---------|
| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 | 19 | 20 | |
| 1+00 | | | | | | | | | | | | | | | | | | | | | |
| 1+10 | | | | | | | | | | | | | | | | | | | | | |
| 1+20 | | | | | | | | | | | | | | | | | | | | | |
| 1+30 | | | | | | | | | | | | | | | | | | | | | |
| 1+40 | | | | | | | | | | | | | | | | | | | | | |
| 1+50 | | | | | | | | | | | | | | | | | | | | | |
| 1+60 | | | | | | | | | | | | | | | | | | | | | |
| 1+70 | | | | | | | | | | | | | | | | | | | | | |
| 1+80 | | | | | | | | | | | | | | | | | | | | | |
| 1+90 | | | | | | | | | | | | | | | | | | | | | |
| 2+00 | | | | | | | | | | | | | | | | | | | | | |
| 2+10 | | | | | | | | | | | | | | | | | | | | | |
| 2+20 | | | | | | | | | | | | | | | | | | | | | |
| 2+30 | | | | | | | | | | | | | | | | | | | | | |
| 2+40 | | | | | | | | | | | | | | | | | | | | | |
| 2+50 | | | | | | | | | | | | | | | | | | | | | |
| 2+60 | | | | | | | | | | | | | | | | | | | | | |
| 2+70 | | | | | | | | | | | | | | | | | | | | | |
| 2+80 | | | | | | | | | | | | | | | | | | | | | |
| 2+90 | | | | | | | | | | | | | | | | | | | | | |
| 3+00 | | | | | | | | | | | | | | | | | | | | | |
| 3+10 | | | | | | | | | | | | | | | | | | | | | |
| 3+20 | | | | | | | | | | | | | | | | | | | | | |
| 3+30 | | | | | | | | | | | | | | | | | | | | | |
| 3+40 | | | | | | | | | | | | | | | | | | | | | |
| 3+50 | | | | | | | | | | | | | | | | | | | | | |
| 3+60 | | | | | | | | | | | | | | | | | | | | | |
| 3+70 | | | | | | | | | | | | | | | | | | | | | |
| 3+80 | | | | | | | | | | | | | | | | | | | | | |
| 3+90 | | | | | | | | | | | | | | | | | | | | | |
| 4+00 | | | | | | | | | | | | | | | | | | | | | |
| 4+10 | | | | | | | | | | | | | | | | | | | | | |
| 4+20 | | | | | | | | | | | | | | | | | | | | | |
| 4+30 | | | | | | | | | | | | | | | | | | | | | |
| 4+40 | | | | | | | | | | | | | | | | | | | | | |
| 4+50 | | | | | | | | | | | | | | | | | | | | | |
| 4+60 | | | | | | | | | | | | | | | | | | | | | |
| 4+70 | | | | | | | | | | | | | | | | | | | | | |
| 4+80 | | | | | | | | | | | | | | | | | | | | | |
| 4+90 | | | | | | | | | | | | | | | | | | | | | |
| 5+00 | | | | | | | | | | | | | | | | | | | | | |
| 5+10 | | | | | | | | | | | | | | | | | | | | | |
| 5+20 | | | | | | | | | | | | | | | | | | | | | |
| 5+30 | | | | | | | | | | | | | | | | | | | | | |
| 5+40 | | | | | | | | | | | | | | | | | | | | | |
| 5+50 | | | | | | | | | | | | | | | | | | | | | |
| 5+60 | | | | | | | | | | | | | | | | | | | | | |
| 5+70 | | | | | | | | | | | | | | | | | | | | | |
| 5+80 | | | | | | | | | | | | | | | | | | | | | |
| 5+90 | | | | | | | | | | | | | | | | | | | | | |
| 6+00 | | | | | | | | | | | | | | | | | | | | | |
| 6+10 | | | | | | | | | | | | | | | | | | | | | |
| 6+20 | | | | | | | | | | | | | | | | | | | | | |
| 6+30 | | | | | | | | | | | | | | | | | | | | | |
| 6+40 | | | | | | | | | | | | | | | | | | | | | |
| 6+50 | | | | | | | | | | | | | | | | | | | | | |
| 6+60 | | | | | | | | | | | | | | | | | | | | | |
| 6+70 | | | | | | | | | | | | | | | | | | | | | |
| 6+80 | | | | | | | | | | | | | | | | | | | | | |
| 6+90 | | | | | | | | | | | | | | | | | | | | | |
| 7+00 | | | | | | | | | | | | | | | | | | | | | |
| 7+10 | | | | | | | | | | | | | | | | | | | | | |
| 7+20 | | | | | | | | | | | | | | | | | | | | | |
| 7+30 | | | | | | | | | | | | | | | | | | | | | |
| 7+40 | | | | | | | | | | | | | | | | | | | | | |
| 7+50 | | | | | | | | | | | | | | | | | | | | | |
| 7+60 | | | | | | | | | | | | | | | | | | | | | |
| 7+70 | | | | | | | | | | | | | | | | | | | | | |
| 7+80 | | | | | | | | | | | | | | | | | | | | | |
| 7+90 | | | | | | | | | | | | | | | | | | | | | |
| 8+00 | | | | | | | | | | | | | | | | | | | | | |
| 8+10 | | | | | | | | | | | | | | | | | | | | | |
| 8+20 | | | | | | | | | | | | | | | | | | | | | |
| 8+30 | | | | | | | | | | | | | | | | | | | | | |
| 8+40 | | | | | | | | | | | | | | | | | | | | | |
| 8+50 | | | | | | | | | | | | | | | | | | | | | |
| 8+60 | | | | | | | | | | | | | | | | | | | | | |
| 8+70 | | | | | | | | | | | | | | | | | | | | | |
| 8+80 | | | | | | | | | | | | | | | | | | | | | |
| 8+90 | | | | | | | | | | | | | | | | | | | | | |
| 9+00 | | | | | | | | | | | | | | | | | | | | | |
| 9+10 | | | | | | | | | | | | | | | | | | | | | |
| 9+20 | | | | | | | | | | | | | | | | | | | | | |
| 9+30 | | | | | | | | | | | | | | | | | | | | | |
| 9+40 | | | | | | | | | | | | | | | | | | | | | |
| 9+50 | | | | | | | | | | | | | | | | | | | | | |
| 9+60 | | | | | | | | | | | | | | | | | | | | | |
| 9+70 | | | | | | | | | | | | | | | | | | | | | |
| 9+80 | | | | | | | | | | | | | | | | | | | | | |
| 9+90 | | | | | | | | | | | | | | | | | | | | | |
| 10+00 | | | | | | | | | | | | | | | | | | | | | |
| 10+10 | | | | | | | | | | | | | | | | | | | | | |
| 10+20 | | | | | | | | | | | | | | | | | | | | | |
| 10+30 | | | | | | | | | | | | | | | | | | | | | |
| 10+40 | | | | | | | | | | | | | | | | | | | | | |
| 10+50 | | | | | | | | | | | | | | | | | | | | | |
| 10+60 | | | | | | | | | | | | | | | | | | | | | |
| 10+70 | | | | | | | | | | | | | | | | | | | | | |
| 10+80 | | | | | | | | | | | | | | | | | | | | | |
| 10+90 | | | | | | | | | | | | | | | | | | | | | |
| TOTAL | 541 | 148 | 142 | 109 | 40 | 30 | 74 | 60 | 16 | 7 | 113 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | |

| | | | |
|-----------|--|--|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | BIRDALE, BENTON, STONE & ASSOC., INC. 711 WEST 40TH STREET BALTIMORE, MARYLAND 21211 | INTERSTATE ROUTE 95 FROM I-95 TO I-83 (JUNCTION) SCALE: 1" = 40' DATE: | DRAWN BY: [] TRACED BY: [] F.A.P. NO. 95-44-36 SHEET NO. 29 OF 36 BALTO. CITY NO. |

PLAN VIEW
 SCALE: 1" = 40'
 DATE: 11/15/12



MATCH LINE SEE SHEET NO 4

- NOTES:**
1. CONSTR STA 4+27.76 TO STA 4+40.00 REMOVE AND REINSTALL 6\"/>
 2. AFTER STRUCTURE HAS BEEN COMPLETED REMOVE THE 6\"/>
 3. CONSTR STA 4+27.76 TO STA 4+40.00 SEE SPECIAL PROVISIONS

I-1812



| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|--|
| | ROBERT J. BALTIMORE, MARYLAND 21211 | WINTERHYTE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO CANTON ST | DATE: ... BALTIMORE, MD |

SUMMARY OF QUANTITIES

| | | | | |
|---------|-------|--------------------|-----------|--------------|
| JOB NO. | STATE | FED. AID PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 2 | MD | 1-95-4142134 | 33 | 36 |

| ITEM NO. | QUANTITIES | | | | | REMARKS |
|----------|------------|---|---|---|---|---------|
| | 1 | 2 | 3 | 4 | 5 | |
| 1 | | | | | | |
| 2 | | | | | | |
| 3 | | | | | | |
| 4 | | | | | | |
| 5 | | | | | | |
| 6 | | | | | | |
| 7 | | | | | | |
| 8 | | | | | | |
| 9 | | | | | | |
| 10 | | | | | | |
| 11 | | | | | | |
| 12 | | | | | | |
| 13 | | | | | | |
| 14 | | | | | | |
| 15 | | | | | | |
| 16 | | | | | | |
| 17 | | | | | | |
| 18 | | | | | | |
| 19 | | | | | | |
| 20 | | | | | | |
| 21 | | | | | | |
| 22 | | | | | | |
| 23 | | | | | | |
| 24 | | | | | | |
| 25 | | | | | | |
| 26 | | | | | | |
| 27 | | | | | | |
| 28 | | | | | | |
| 29 | | | | | | |
| 30 | | | | | | |
| 31 | | | | | | |
| 32 | | | | | | |
| 33 | | | | | | |
| 34 | | | | | | |
| 35 | | | | | | |
| 36 | | | | | | |
| 37 | | | | | | |
| 38 | | | | | | |
| 39 | | | | | | |
| 40 | | | | | | |
| 41 | | | | | | |
| 42 | | | | | | |
| 43 | | | | | | |
| 44 | | | | | | |
| 45 | | | | | | |
| 46 | | | | | | |
| 47 | | | | | | |
| 48 | | | | | | |
| 49 | | | | | | |
| 50 | | | | | | |
| 51 | | | | | | |
| 52 | | | | | | |
| 53 | | | | | | |
| 54 | | | | | | |
| 55 | | | | | | |
| 56 | | | | | | |
| 57 | | | | | | |
| 58 | | | | | | |
| 59 | | | | | | |
| 60 | | | | | | |
| 61 | | | | | | |
| 62 | | | | | | |
| 63 | | | | | | |
| 64 | | | | | | |
| 65 | | | | | | |
| 66 | | | | | | |
| 67 | | | | | | |
| 68 | | | | | | |
| 69 | | | | | | |
| 70 | | | | | | |
| 71 | | | | | | |
| 72 | | | | | | |
| 73 | | | | | | |
| 74 | | | | | | |
| 75 | | | | | | |
| 76 | | | | | | |
| 77 | | | | | | |
| 78 | | | | | | |
| 79 | | | | | | |
| 80 | | | | | | |
| 81 | | | | | | |
| 82 | | | | | | |
| 83 | | | | | | |
| 84 | | | | | | |
| 85 | | | | | | |
| 86 | | | | | | |
| 87 | | | | | | |
| 88 | | | | | | |
| 89 | | | | | | |
| 90 | | | | | | |
| 91 | | | | | | |
| 92 | | | | | | |
| 93 | | | | | | |
| 94 | | | | | | |
| 95 | | | | | | |
| 96 | | | | | | |
| 97 | | | | | | |
| 98 | | | | | | |
| 99 | | | | | | |
| 100 | | | | | | |

| | | | | | |
|--------------------|-----|-----|-----|-----|-----|
| TOTAL THIS SHEET | 2 | 1 | 2 | 1 | 3.3 |
| TOTAL | 2 | 1 | 2 | 1 | 3.3 |
| IDENTIFICATION NO. | 200 | 240 | 240 | 240 | 214 |

| | | | |
|-----------|---|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | WINDFORD, BENDER, STONE & ASSOC., INC. AND HATZ, CARLOS & ASSOC., INC. CONSULTING ENGINEERS 711 WEST 40TH STREET BALTIMORE, MARYLAND 21211 | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO DANTON ST | DRAWN BY: C.F. TRACED BY: R.S.K. P.A.P. NO. 95-4142134 S.P.A. NO. BC 240-30-B10 BALTS. CITY NO. 2071 |
| | | SCALE: NONE | DATE: _____ |
| | | | DES. BY: C.F. CHK. BY: V.F.H. SHEET NO. 33 OF 36 |

EARTHWORK SUMMARY

2 MD 1-95-1142354 3 4 3 6

| STATION | | CUT | FILL | TOPSOIL | | UNSUITABLE MATERIAL | | CLASS 2' | CUT ADJUSTED | CUT DENSIFIED | REMARKS |
|--|--------|-------|--------|---------|-------|---------------------|------|----------|--------------|---------------|---|
| FROM | TO | | | CUT | FILL | CUT | FILL | | | | |
| 4+91 | 4+91 | 44 | | | | | | | 427 | 0.1 0.97 | |
| 2+00 | 4+50 | 196 | | | | | | | | | |
| 2+00 | 4+91 | | 711 | | | | | | | | |
| 3+00 | 6+00 | | | | | | | | | | |
| 3+00 | 7+00 | | | | | | | | 52 | | |
| 6+00 | 7+00 | 226 | | | | | | | 226 | 0.1 0.97 | |
| 6+00 | 9+00 | | | | | | | | | | |
| 7+00 | 10+00 | | | | | | | | 637 | | |
| 7+75 | 10+50 | 749 | | | | | | | 126 | 0.1 0.97 | |
| 8+00 | 10+50 | | | | | | | | | | |
| <p style="text-align: center;">ACQUISITIONAL BARRIER TO PLAN ELEVATION</p> | | | | | | | | | | | |
| 5+00 | 5+00 | | 174 | | | | | | | | |
| 5+00 | 7+00 | | 97 | | | | | | | | |
| 7+00 | 50+00 | | | | | | | | | | |
| 350+00 | 350+00 | | 85 | | | | | | 85 | 0.1 0.97 | |
| 350+00 | 360+00 | | | | | | | | | | |
| <p style="text-align: center;">ACQUISITIONAL BARRIER TO PLAN STA. 370+00</p> | | | | | | | | | | | |
| 362+00 | 370+00 | | 35.577 | | | | | | 681 | | |
| <p style="text-align: center;">EASTWOOD PARK</p> | | | | | | | | | | | |
| 0+00 | 0+15 | 99 | | | | | | | 99 | 0.1 0.97 | |
| 0+36 | 1+05 | 212 | | | | | | | | | |
| 0+50 | 0+50 | 66 | | | | | | | 66 | 0.1 0.97 | |
| 0+50 | 1+02 | | | | | | | | 0.3 | | |
| 0+50 | 2+04 | 1,374 | | | | | | | 1,374 | 1.374 | 0.1 0.97 |
| 353+70 | 353+50 | 354 | | | | | | | 333 | 324 | |
| 352+00 | 365+00 | | 6,337 | | | | | | | | 0.1 0.97 |
| 353+00 | 361+93 | 4,091 | | | | | | | 4,091 | 4,566 | 0.1 0.97 |
| 358+50 | 365+00 | | | | | | | | 2,017 | | |
| 361+00 | 365+00 | 331 | | | | | | | 331 | 331 | 0.1 0.97 |
| 361+70 | 365+00 | | | | | | | | 1,910 | 2,000 | |
| | | | | | | | | | 2,824 | 7,176 | 50% TOTAL EASTWOOD PARK |
| | | | | | | | | | 1,280 | | |
| | | | | | | | | | 5,431 | 6,320 | EASTWOOD PARK TOTAL CUT ADJUSTED CUT DENSIF |
| <p style="text-align: center;">RAMP AT QUINTON STREET</p> | | | | | | | | | | | |
| 8+76 | 10+75 | 237 | | | | | | | 237 | 250 | 0.1 0.97 |
| 8+75 | 10+50 | | 406 | | | | | | | | |
| <p style="text-align: center;">STREET</p> | | | | | | | | | | | |
| 0+00 | 3+00 | | 108 | | | | | | | | |
| 0+00 | 1+00 | | 229 | | | | | | | | 0.1 0.97 |
| 0+00 | 0+00 | | | | | | | | | | |
| 0+00 | 1+00 | | 27 | | | | | | | | |
| 0+00 | 1+00 | | | | | | | | | | |
| 0+00 | 1+67.5 | | | | | | | | | | |
| 0+00 | 1+00 | | 27 | | | | | | 27 | 27 | 0.1 0.97 |
| 0+00 | 1+00 | | | | | | | | | | |
| 0+00 | 1+00 | | 1,800 | 8,400 | 4,700 | 7,847 | | | | | |

CLASS 1' EXCAVATION

| | |
|-----------------------------------|---------------------|
| CUT | 4,337 CU YD |
| PLUS ROOT MAT REMOVED UNDER PIPES | 2,408 CU YD |
| TOTAL CLASS 1' EXCAVATION | 11,968 CU YD |

EXCAVATION AVAILABLE FOR EMBANKMENT

| | |
|--|---------------------|
| TOTAL CLASS 1' EXCAVATION | 11,968 CU YD |
| ROOT MAT REMOVED IN CUT | 1,910 CU YD |
| ROOT MAT REMOVED UNDER FILL | 2,408 CU YD |
| CUT ADJUSTED | 7,647 CU YD |
| CUT DENSIFIED | 7,416 CU YD |
| PLUS CLASS 2' EXCAVATION AVAILABLE FOR EMBANKMENT | 2,749 CU YD |
| TOTAL EXCAVATION AVAILABLE FOR EMBANKMENT | 10,165 CU YD |

EMBANKMENT REQUIRED

| | |
|--------------------------------------|---------------------|
| EMBANKMENT | 88,120 CU YD |
| NEED FOR ROOT MAT REMOVED UNDER FILL | 2,408 CU YD |
| EMBANKMENT REQUIRED | 90,528 CU YD |
| EXCAVATION AVAILABLE FOR EMBANKMENT | 10,165 CU YD |
| BORROW REQUIRED | 80,363 CU YD |
| BORROW DENSIFICATION (10%) | 8,830 CU YD |
| TOTAL BORROW | 89,193 CU YD |

PROPOSED QUANTITIES

| | |
|---------------------|--------------|
| CLASS 1' EXCAVATION | 11,968 CU YD |
| CLASS 2' EXCAVATION | 4,700 CU YD |
| BORROW EXCAVATION | 89,193 CU YD |

CLASS 2' EXCAVATION

| | |
|---|--------------------|
| FROM CRADLE TABLE | 4,159 CU YD |
| FROM PIPE SHEET | 541 CU YD |
| FROM BRIDGE SHEET | 0 CU YD |
| TOTAL CLASS 2' EXCAVATION | 4,700 CU YD |
| LOSS DUE TO HANDLING AND DENSIFICATION (30%) | 1,410 CU YD |
| FROM PIPE SHEET NOT AVAILABLE FOR FILL | 541 CU YD |
| FROM BRIDGE SHEET NOT AVAILABLE FOR FILL | 0 CU YD |
| TOTAL CLASS 2' EXCAVATION AVAILABLE FOR EMBANKMENT | 2,749 CU YD |

| | | | |
|-----------|---|---|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | BROADBENT, BERGER, STONE & ASSOC., INC. AND MATZ, BILLEN & ASSOC., INC. CONSULTING ENGINEERS 721 WEST ACHES STREET BALTIMORE, MARYLAND 21221 | INTERSTATE ROUTE 95 OVERPASS KANE ST. TO QUINTON ST. | DRAWN BY: [] CHECKED BY: [] DATE: [] |
| | | SCALE: NONE | BALTO. CITY NO. 217 |

SUMMARY OF QUANTITIES

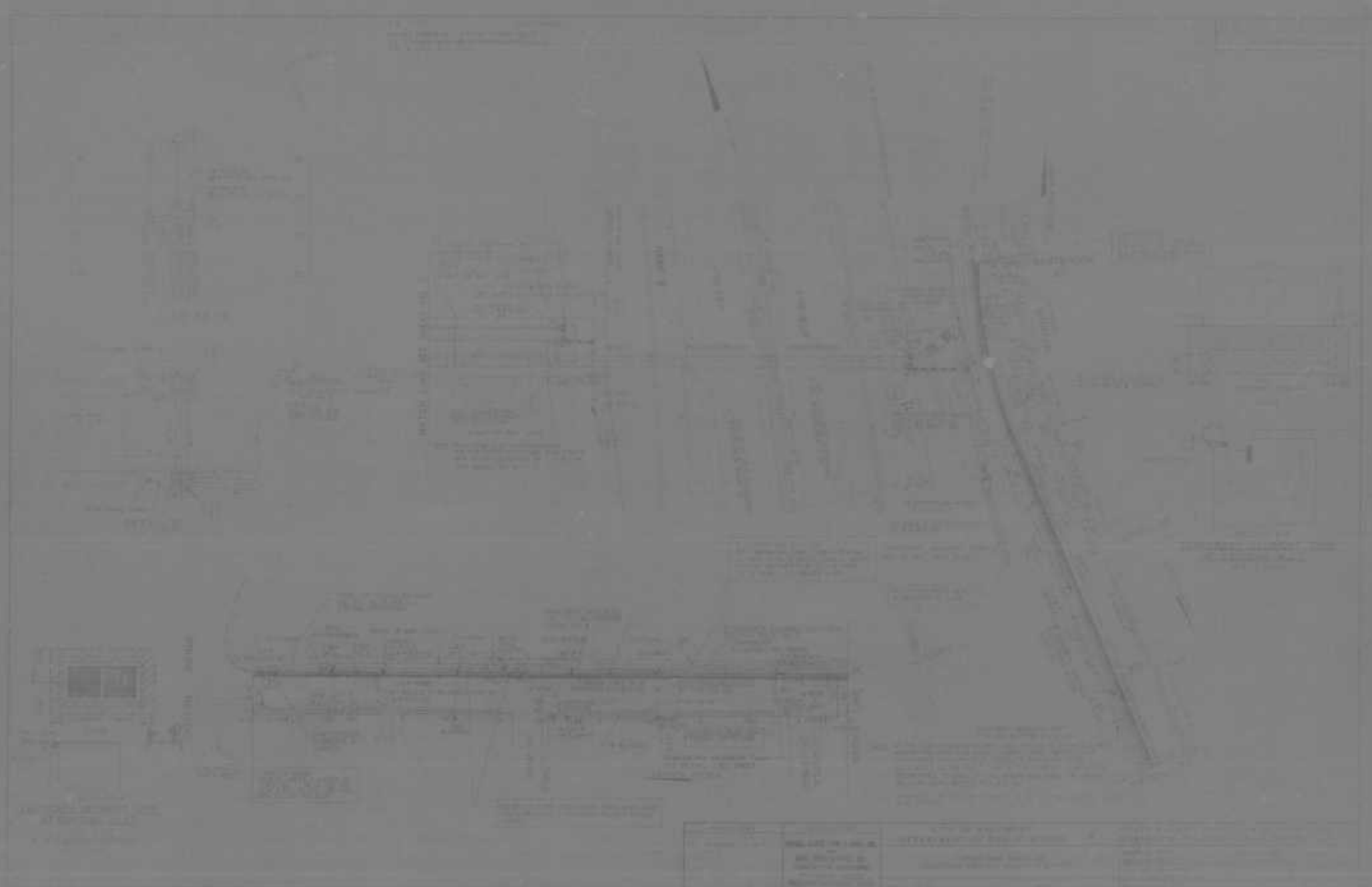
| | | | | |
|---------------------|-------|----------------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | FED. AID PARTIAL NO. | SHEET NO. | TOTAL SHEETS |
| 2 | MD | 1-95-4(42)34 | 35 | 36 |

| ITEM NO. | DESCRIPTION | UNIT | 90% FED AID PART | 50% FED AID PART | TOTAL | PROPOSAL QUANTITY |
|-------------------------------|---|------|------------------|------------------|--------|-------------------|
| CATEGORY NO. 2 LUMP SUM ITEMS | | | | | | |
| 101 | RESURFACING GRUBBING | L.S. | | | | L.S. |
| 102 | CONCRETE FACILITIES | L.S. | | | | L.S. |
| 103 | MAINTENANCE OF TRAFFIC | L.S. | | | | L.S. |
| 104 | CONSTRUCTION STAFFING | L.S. | | | | L.S. |
| 105 | MOBILIZATION | L.S. | | | | L.S. |
| 106 | ON THE JOB TRAINING | HR | 3,000 | | 3,000 | 3,000 |
| CATEGORY NO. 3 GRADING | | | | | | |
| 201 | CLASS 1 EXCAVATION | Y | 11,770 | 280 | 12,050 | 12,300 |
| 202 | CLASS 2 EXCAVATION | Y | 4,730 | | 4,730 | 4,750 |
| 203 | BORROW EXCAVATION | Y | 41,166 | | 41,166 | 41,200 |
| 204 | THICK MAT EXCAVATION | C.Y. | 7 | 3 | 10 | 10 |
| 205 | REMOVAL OF EXISTING COMBINATION CURB AND GUTTER | L.F. | 159 | | 159 | 160 |
| 206 | REMOVAL OF EXISTING PAVEMENT | S.Y. | 603 | 189 | 792 | 792 |
| CATEGORY NO. 3 DRAINAGE ITEMS | | | | | | |
| 301 | 18 INCH R.C. PIPE, CLASS 4 | L.F. | 212 | | 212 | 212 |
| 302 | 24 INCH R.C. PIPE, CLASS 4 | L.F. | 32 | | 32 | 32 |
| 303 | 36 INCH R.C. PIPE, CLASS 4 | L.F. | 40 | | 40 | 40 |
| 304 | 8 INCH R.C. M. PIPE, TYPE A, 1/2 G.A.P. | L.F. | 32 | | 32 | 32 |
| 305 | 12 INCH R.C. M. PIPE, TYPE A, 1/2 G.A.P. | L.F. | 74 | | 74 | 74 |
| 306 | 18 INCH R.C. M. PIPE, TYPE A, 1/2 G.A.P. | L.F. | 50 | | 50 | 50 |
| 307 | 24 INCH R.C. M. PIPE, TYPE A, 1/2 G.A.P. | L.F. | 16 | | 16 | 16 |
| 308 | STANDARD 12C SUPPORT RING FOR 18 INCH PIPE | EA | 1 | | 1 | 1 |
| 309 | STANDARD 12C RINGS FOR 18 INCH PIPE | EA | 2 | | 2 | 2 |
| 310 | STANDARD TYPE K INLET FOR 18 INCH PIPE | EA | 1 | | 1 | 1 |
| 311 | STANDARD TYPE K ENDWALL FOR 18 INCH PIPE | EA | 1 | | 1 | 1 |

| ITEM NO. | DESCRIPTION | UNIT | 90% FED AID PART | 50% FED AID PART | TOTAL | PROPOSAL QUANTITY |
|---|---|------|------------------|------------------|-------|-------------------|
| CATEGORY NO. 3 DRAINAGE ITEMS (CONTINUED) | | | | | | |
| 312 | STANDARD TYPE K ENDWALL FOR 24 INCH PIPE | EA | 1 | | 1 | 1 |
| 313 | STANDARD CONCRETE END SECTION FOR 24 INCH PIPE | EA | 2 | | 2 | 2 |
| 314 | STANDARD METAL END SECTION FOR 18 INCH C.M. PIPE | EA | 1 | | 1 | 1 |
| 315 | STANDARD TYPE K INLET, SINGLE GRATE, MINIMUM DEPTH | L.F. | 12 | | 12 | 12 |
| 316 | STANDARD TYPE K INLET, SINGLE GRATE, VERTICAL DEPTH | L.F. | 24.6 | | 24.6 | 25 |
| 317 | ADJUST EXISTING INLET | EA | 1 | | 1 | 1 |
| 318 | PROP MANHOLE | L.S. | L.S. | | L.S. | L.S. |
| 319 | ADJUST EXISTING MANHOLE | EA | 6 | | 6 | 6 |
| 320 | CLASS 1 CONCRETE FOR MISCELLANEOUS STRUCTURES | C.Y. | 1.0 | | 1.0 | 1.0 |
| 321 | 6 INCH CIRCULAR PIPE UNDERDRAIN OUTLETS | L.F. | 28 | | 28 | 28 |
| 322 | 6 INCH CONCRETE GUTTER | S.Y. | 667 | | 667 | 670 |
| 323 | 6 INCH CONCRETE ENERGY DISSIPATING FILTER | S.Y. | 132 | | 132 | 135 |
| 324 | 6 INCH THICK STONE FILLED WIRE MATTRESS DRAINAGE CHANNEL | S.Y. | 1,400 | | 1,400 | 1,400 |
| 325 | 12 INCH THICK STONE FILLED WIRE MATTRESS DRAINAGE CHANNEL | S.Y. | 37 | | 37 | 40 |
| 326 | STONE FILLED WIRE BASKET DRAINAGE CHANNEL | C.Y. | 378 | | 378 | 380 |
| 327 | SPECIAL STEEL GRATES FOR 54 INCH & 36 INCH PIPES | L.S. | L.S. | | L.S. | L.S. |
| 328 | SELECTED BACKFILL USING #6 AGGREGATE | C.Y. | 30 | | 30 | 30 |
| 329 | SELECTED BACKFILL USING CRUSHER RUN | C.Y. | 30 | | 30 | 30 |
| CATEGORY NO. 4 STRUCTURES | | | | | | |
| 401 | CLASS 3 EXCAVATION FOR STRUCTURES | C.Y. | 2,130 | | 2,130 | 2,130 |
| 402 | 50% SAND/FILL FOR FILLING | L.F. | 100 | | 100 | 100 |
| 403 | 14 INCH C.I.P. CONCRETE PILE | L.F. | 210 | | 210 | 210 |
| 404 | 14 INCH C.I.P. CONCRETE TIE PILE | L.F. | 30 | | 30 | 30 |
| 405 | 14 INCH C.I.P. CONCRETE PILE SPLICES | EA | 5 | | 5 | 5 |
| 406 | CONCRETE CURBS | C.Y. | 522 | | 522 | 522 |

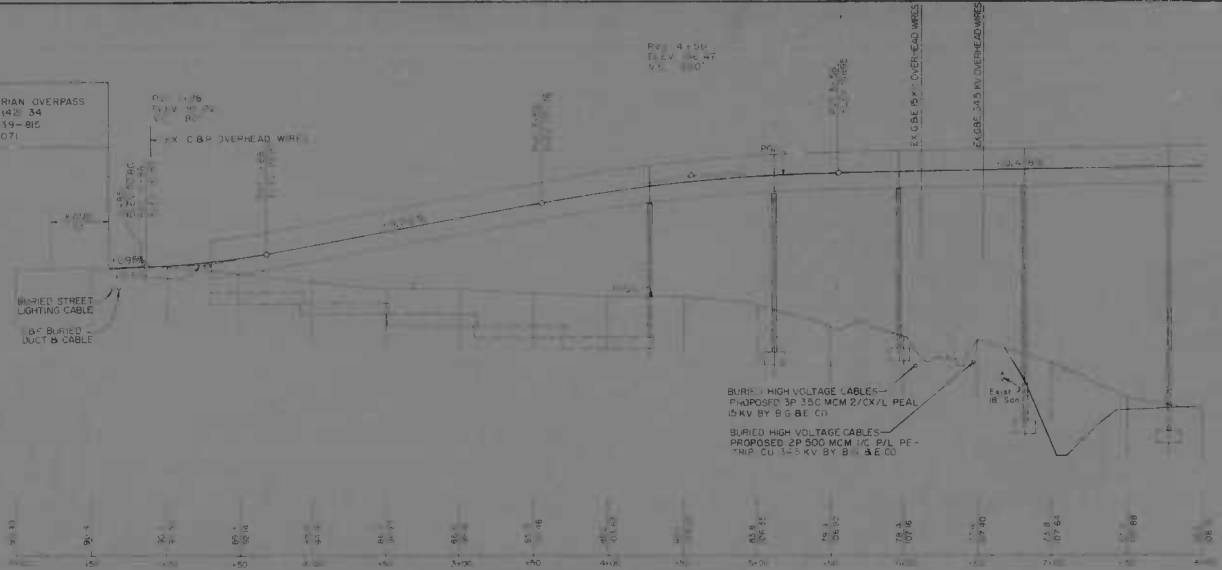
| ITEM NO. | DESCRIPTION | UNIT | 90% FED AID PART | 50% FED AID PART | TOTAL | PROPOSAL QUANTITY |
|---------------------------------------|---|------|------------------|------------------|-------|-------------------|
| CATEGORY NO. 4 STRUCTURES (CONTINUED) | | | | | | |
| 407 | SUBSTRUCTURE CONCRETE | L.S. | L.S. | | L.S. | L.S. |
| 408 | SUPERSTRUCTURE CONCRETE | L.S. | L.S. | | L.S. | L.S. |
| 409 | CONTINGENT CONCRETE FOR BRIDGES | C.Y. | 20 | | 20 | 20 |
| 410 | CLASS C CONCRETE SUBFOUNDATION | C.Y. | 10 | | 10 | 10 |
| 411 | PRESTRESSED CEMENT CONCRETE STRUCTURES | L.S. | L.S. | | L.S. | L.S. |
| 412 | STAIRWAY AT QUINTON STREET | L.S. | L.S. | | L.S. | L.S. |
| 413 | PEDESTRIAN PROTECTIVE BARRIER | L.S. | L.S. | | L.S. | L.S. |
| 414 | PEDESTRIAN SAFETY RAILING | L.S. | L.S. | | L.S. | L.S. |
| 415 | ACOUSTICAL BARRIER | L.S. | L.S. | | L.S. | L.S. |
| CATEGORY NO. 5 | | | | | | |
| 501 | 6 INCH SUB-BASE, USING CRUSHER RUN | S.Y. | 5,443 | 657 | 6,100 | 6,100 |
| 502 | BITUMINOUS MATERIAL FOR PRIME COAT | GA | 759 | 116 | 875 | 875 |
| 503 | MINERAL AGGREGATE FOR BITUMINOUS SURFACE TREATMENT | TON | 126 | | 126 | 130 |
| 504 | BITUMINOUS MATERIAL FOR SEAL COATS FOR SURFACE TREATMENT | GA | 2,090 | | 2,090 | 2,025 |
| 505 | BITUMINOUS CONCRETE SPEC. 3, USING BAND CSL | TON | | 42 | 42 | 45 |
| 506 | BITUMINOUS CONCRETE SPEC. 3, USING BAND B1 | TON | | 27 | 27 | 30 |
| 507 | 8 INCH PLAIN CEMENT CONCRETE PAVEMENT, CLASS P | S.Y. | 68 | | 68 | 70 |
| 508 | 8 INCH REINFORCED CEMENT CONCRETE PAVEMENT CLASS H.E.S. | S.Y. | 582 | | 582 | 585 |
| CATEGORY NO. 6 SHOULDERS | | | | | | |
| 601 | STANDARD TYPE A COMBINATION CURB AND GUTTER 12 INCH GUTTER PAN & 3 INCH DEPTH | L.F. | 150 | | 150 | 150 |
| 602 | STANDARD TYPE A COMBINATION CURB AND GUTTER 18 INCH GUTTER PAN & 3 INCH DEPTH | L.F. | 1,000 | | 1,000 | 1,000 |

| | | | |
|-----------|--|--|--|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | WHEELBENDER ENGINEERING, INC. WATZ, CHIRIC & ASSOC. INC. CONSULTING ENGINEERS 100 WEST 40TH STREET BALTIMORE, MARYLAND 21201 | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO QUINTON ST | DRAWN BY: [] CHECKED BY: [] F.A.P. NO. 1-95-4(42)34 SHEET NO. 35 OF 36 BALTO. CITY NO. 207 |
| | SCALE: NONE | DATE: [] | |

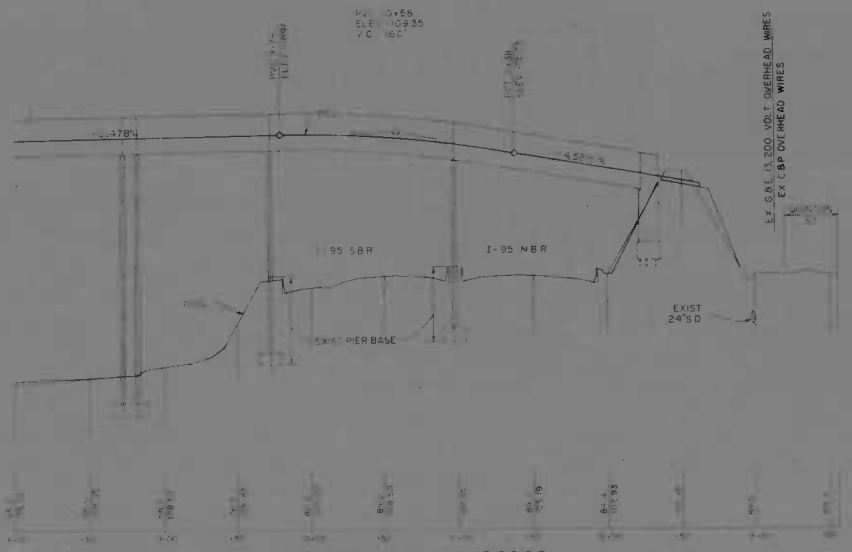


| | | | | |
|-----|------|--------------|------|-----|
| NO. | DATE | BY | APP. | NO. |
| 2 | MD | 1-95-4(42)34 | 5 | 30 |

LIMIT OF WORK
 I-95 PEDESTRIAN OVERPASS
 T.A.P. NO. I-95-4 (42) 34
 I-95 NO. BC 246 151-815
 BALTO. CITY NO. 207



PEDESTRIAN OVERPASS
 DATUM 40



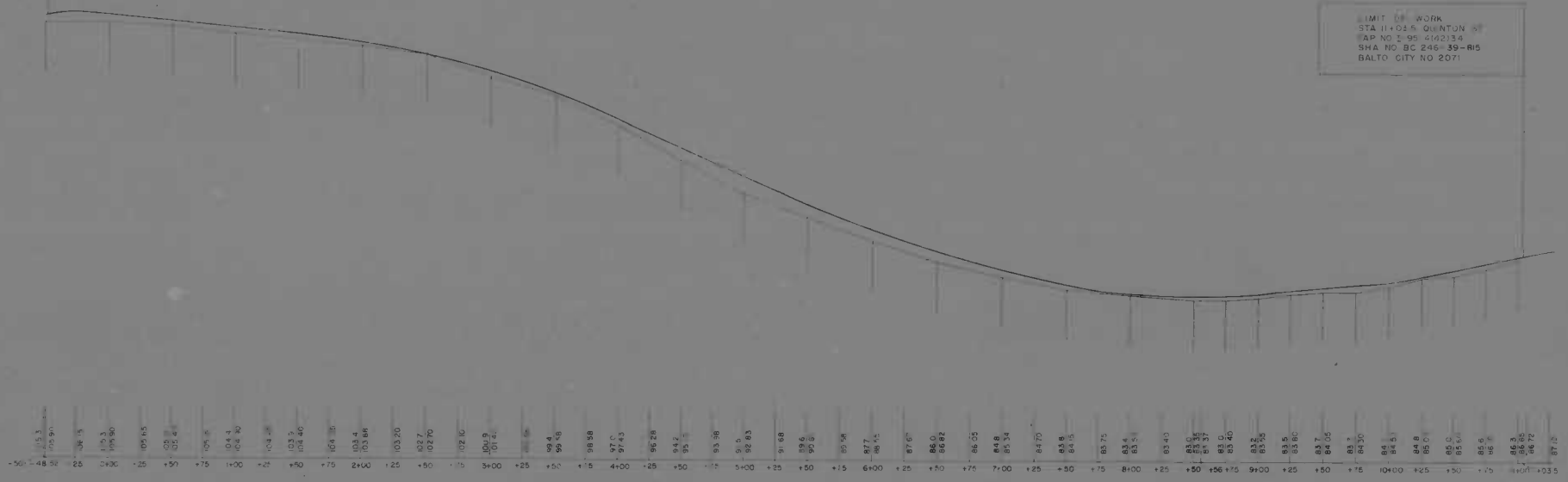
PEDESTRIAN OVERPASS
 DATUM 40

| | | | |
|--------------------------------------|---|--|---|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
| | KENNEDY, DENNER, STONE & ARON, INC. 1417 WEST 40TH STREET BALTIMORE, MARYLAND 21215 | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO QUINTON ST | DRAWN BY J.J.K. CHECKED BY V.F.H. F.A.P. NO. I-95-4(42)34 I-95 NO. BC 246 151-815 BALTO. CITY NO. 207 |
| SCALE: HOR. 1" = 40', VERT. 1" = 10' | | DATE: | SHEET NO. |

LIMIT OF WORK
 STA 0+00 ACCESS RD
 F.A.P. NO. I-95-4142134
 SHA NO. BC 246-39-815
 BALTO. CITY NO. 2071

DATE: 11/11/95
 DRAWN BY: J.E.E.
 CHECKED BY: Y.F.H.
 DESIGNED BY: J.E.E.
 STATE: MD.

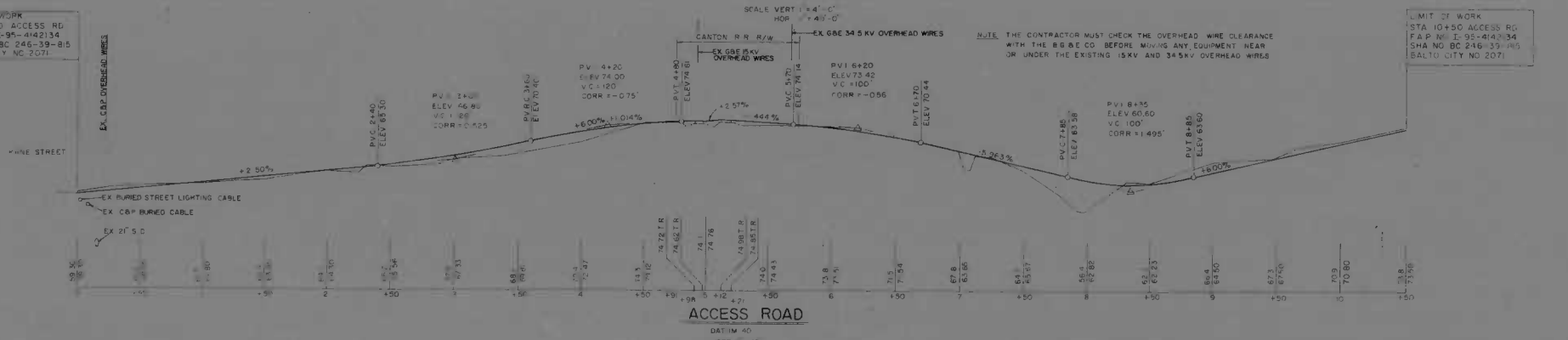
LIMIT OF WORK
 STA 10+50 QUINTON ST
 F.A.P. NO. I-95-4142134
 SHA NO. BC 246-39-815
 BALTO. CITY NO. 2071



QUINTON STREET - TOP OF CURB PROFILE
 DATUM '0

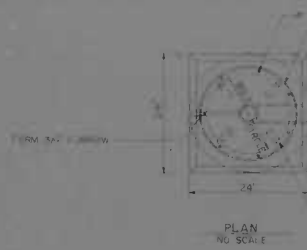
LIMIT OF WORK
 STA 0+00 ACCESS RD
 F.A.P. NO. I-95-4142134
 SHA NO. BC 246-39-815
 BALTO. CITY NO. 2071

LIMIT OF WORK
 STA 10+50 ACCESS RD
 F.A.P. NO. I-95-4142134
 SHA NO. BC 246-39-815
 BALTO. CITY NO. 2071



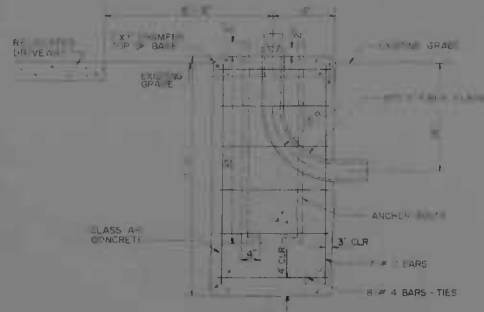
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | THORNTON, HOWELL, STINE & ASSOC., INC. AND WATZ, THORNTON & ASSOC., INC. CONSULTING ENGINEERS 711 WEST 40TH STREET BALTIMORE, MARYLAND 21221 | INTERSTATE ROUTE 95 PEDESTRIAN OVERSPANS KANE ST. @ QUINTON ST | DRAWN BY: J.E.E. CHECKED BY: Y.F.H. DESIGNED BY: J.E.E. F.A.P. NO. I-95-4142134 SHA NO. BC 246-39-815 BALTO. CITY NO. 2071 |
| | | SCALE: AS SHOWN | DATE: 11/11/95 |

| | | | | | | | | |
|----|----------------|----|----|----|----|----|----|----|
| 10 | 11 | 12 | 13 | 14 | 15 | 16 | 17 | 18 |
| | | | | | | | | |
| 2 | MD 196-4142134 | 7 | 30 | | | | | |

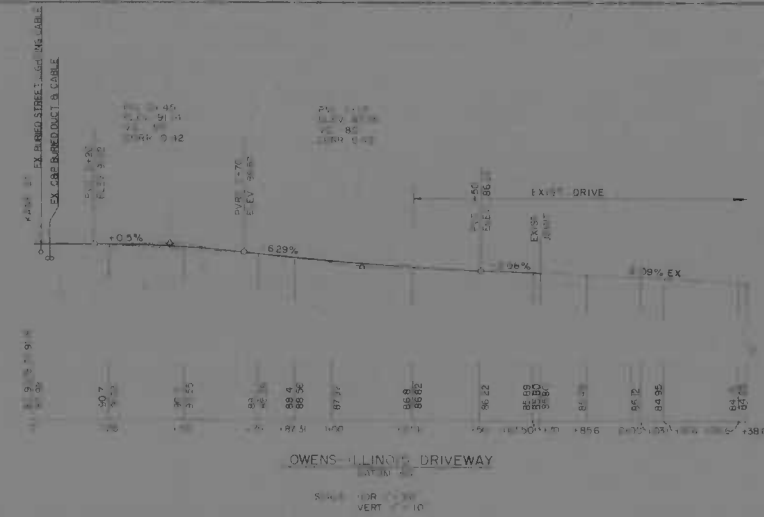


PLAN
NO SCALE

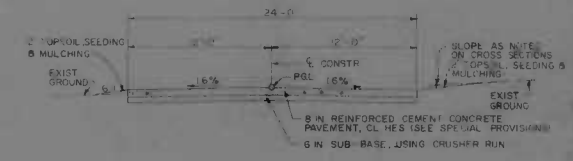
NOTE: ANCHOR BOLTS SHALL BE SPACED 16" ON CENTER WITH 1/2" DIA. ANCHOR BOLTS. VERIFY WITH MANUFACTURER'S STANDARD BASE.



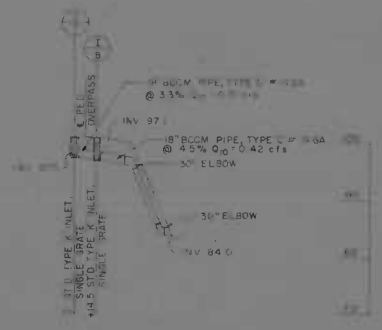
ELEVATION
ROADWAY LIGHT PEDESTAL BASE
NO SCALE



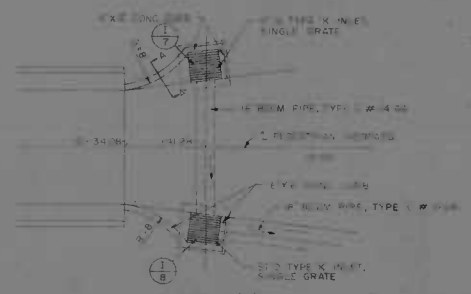
OWENS ILLINOIS DRIVEWAY



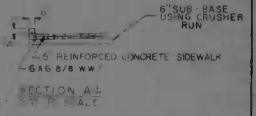
TYPICAL SECTION
OWENS ILLINOIS DRIVEWAY
SCALE: 1" = 5'-0"



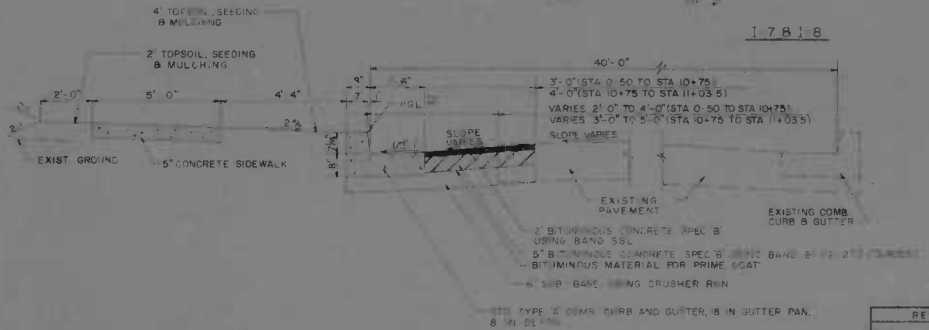
17818



DETAIL 'C'
SCALE: 1" = 1'-0"

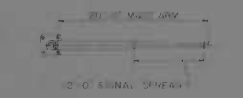


SECTION 'A-A'
SCALE: 1" = 1'-0"

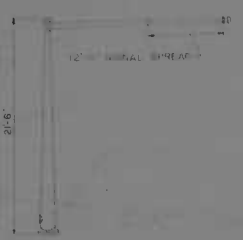


CONCRETE SIDEWALK WIDENING

| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------------|---|---|---|
| REVISED 10/3/72 | KIMBLE, HENDEL, STYRE & ASSOC., INC. DATE, CHECK & SIGN. INC. CONSULTING ENGINEERS 711 WEST 47TH STREET BALTIMORE, MARYLAND 21220 | INTERSTATE 835 PEDESTRIAN OVERPASS PLANE ST TO QUINCY ST | DRAWN BY: JCH CHECKED BY: JCH F.A.P. NO. 1-90-4142-24 BALTO CITY NO. 196-4142134-7 |
| | | SCALE: AS SHOWN | SHEET NO. 7 OF 30 |

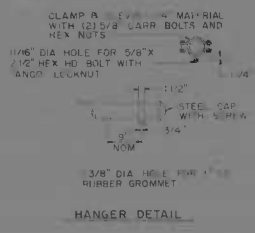


PLAN



ELEVATION

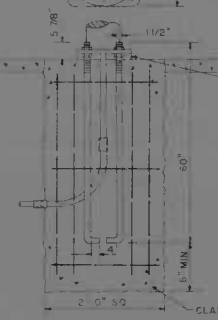
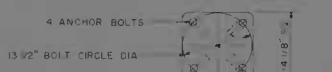
TRANSIT & TRAFFIC 21'-6" TRAFFIC SIGNAL POLE
NOT TO SCALE



HANGER DETAIL



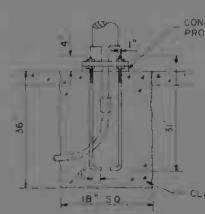
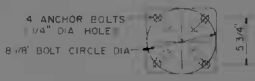
TRANSIT & TRAFFIC 7' STEEL PEDESTAL
NOT TO SCALE



BASE FOR 21'-6" STEEL POLE
NOT TO SCALE

THE TOPS OF ALL ANCHOR BOLTS SHALL BE LEVEL BEFORE POURING CONCRETE

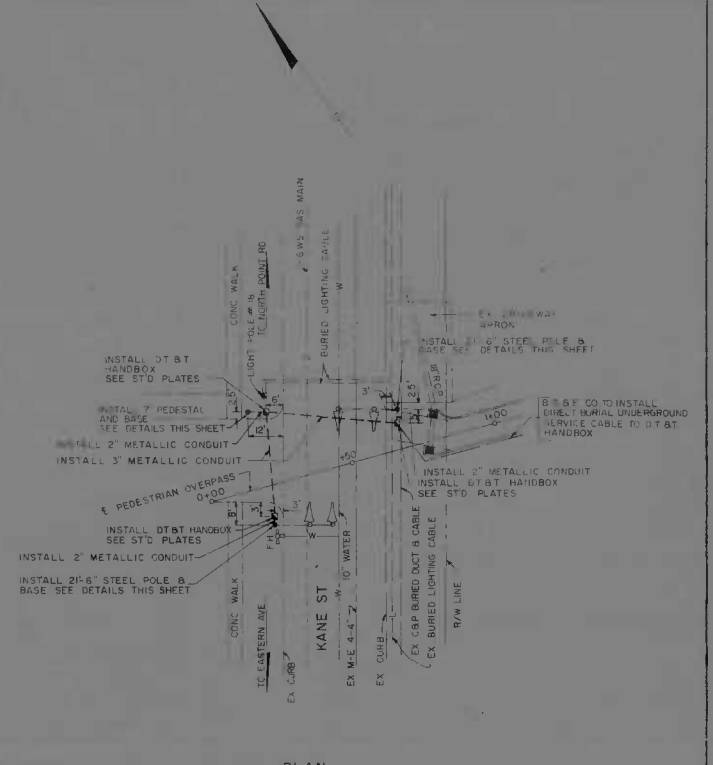
CONCRETE (ROUT - 1" MIN, 1 1/2" MAX PROVIDE FOR 1/2" WEEP HOLE



BASE FOR 7' PEDESTAL
NOT TO SCALE

THE TOPS OF ALL ANCHOR BOLTS SHALL BE LEVEL BEFORE POURING CONCRETE

CONCRETE (ROUT - 1" MIN, 1 1/2" MAX PROVIDE FOR 1/2" WEEP HOLE

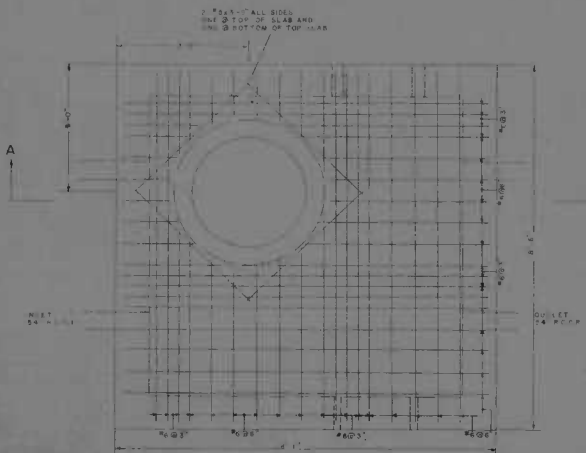


PLAN
SCALE - 20

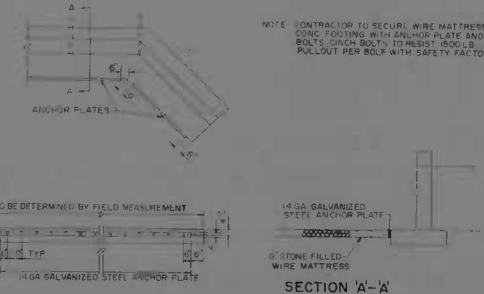
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY |
|-----------|---|---|---|
| | KIMBLE, BECKER, STONE & ASSOC., INC. AND WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 711 WEST 40TH STREET BALTIMORE, MARYLAND 21211 | PEDESTRIAN OVERPASS KANE ST TO QUINTON ST | DRAWN BY: J.E.K. TRACED BY: J.E.K. F.A.P. NO. 1-95-4142-44 S.R.C. NO. 246-30-895 |
| | | SCALE: AS SHOWN | DES. BY: V.P.H. 5-1-8 CHK. BY: J.E.K. SHEET NO. 8 OF 30 |

| | | |
|----------------|-----------|--------------|
| FILE NO. | SHEET NO. | TOTAL SHEETS |
| MD 195-4(42)34 | 9 | |

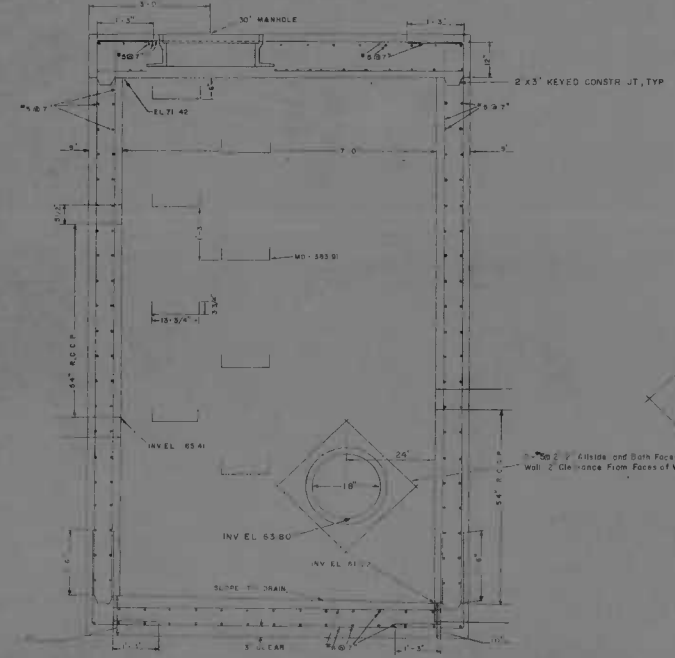
TOP VIEW
SCALE 3/4"=1'-0"



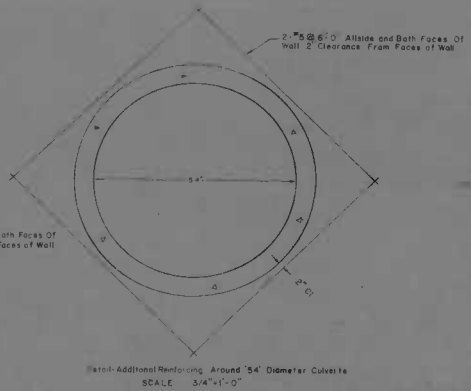
- NOTES**
1. All Exposed Corners To Have 3/4" Chamfer
 2. See Special Provisions For 30" Manhole Cover and Frame Details
 3. Steps are to be Diamond Metal Ladder Rungs - M.D. - 383 91
 4. The Drop Manhole Slab is Constructed With 4" A-1 Concrete With Minimum Comp. Strength @ 28 Days of 3000 psi. The Quantity is 11 ccy
 5. The Reinforcing Bar Sizes and Spacing are the Same as All Walls
 6. Top View Shows Bottom Mat of Reinforcing Steel Only
 7. The Manhole is to be Set on a Lump Sum Basis and is to Include Concrete, Reinforcing Steel, Manhole Frame and Cover and Ladder Rungs
 8. AASHTO Loading H10-A4
 9. See General Notes on Sheet No 15 for Embedding and Reinforcing Steel



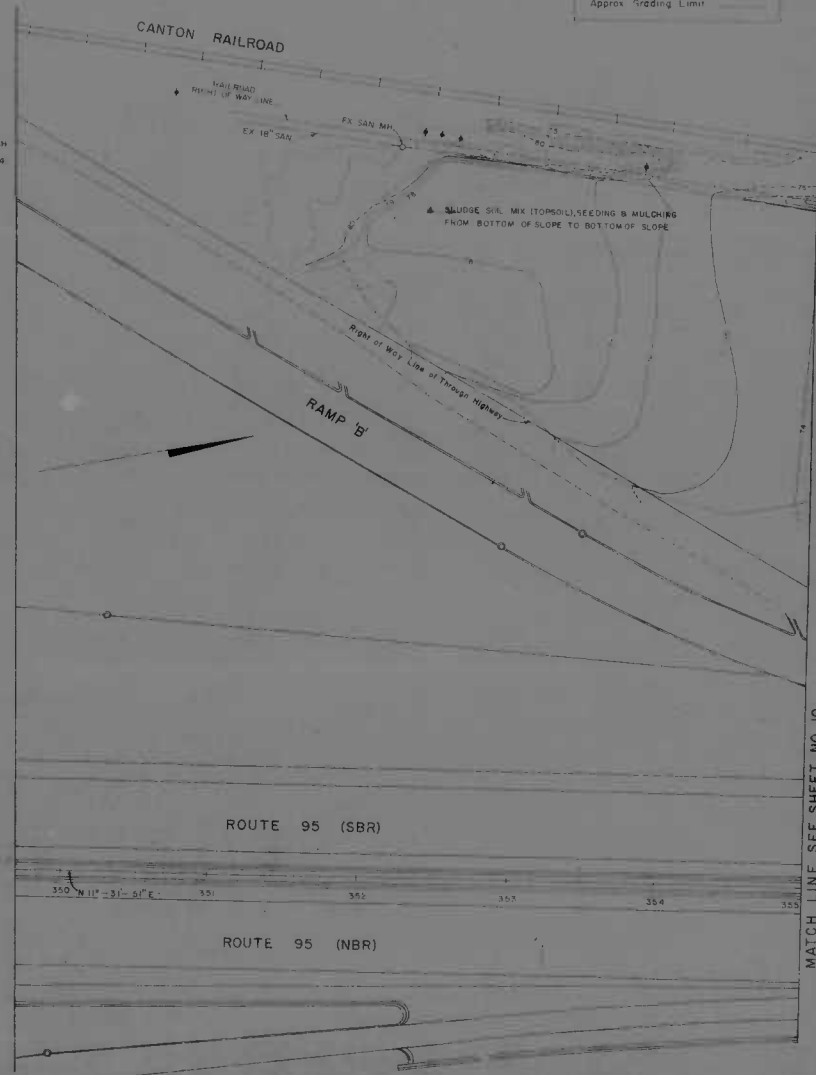
6" MESH ANCHORAGE TO EXISTING HEADWALL
NOT TO SCALE



SECTION "AA" DROP MANHOLE
NOT TO SCALE



CONTOUR GRADING FOR EASTWOOD PARK
SCALE 1"=40'



LEGEND

| | |
|-----------------------|-----|
| Existing Contours | |
| Proposed Contours | --- |
| Approx. Grading Limit | --- |

| | | | | | |
|----------------|------------|--|-----------------------|--|-------------------|
| REVISIONS | CONSULTANT | CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & | | STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY | |
| | | INTERSTATE ROUTE 95 PEDESTRIAN OVERPASS KANE ST TO QUINTON ST | | DRAWN BY: ELK | DES BY: DCH & LEL |
| SCALE AS SHOWN | | DATE: | F.P. NO. I-95-4(42)34 | CHK BY: JBT | SHEET NO. |
| | | | CH. NO. RC 246.39-815 | | |
| | | | BALTO. CITY NO. 407 | | |

MATCH LINE SEE SHEET NO. 10

- INDEX OF SHEETS**
- 1. PLAN, GRADE & ELEVATION
 - 2. PLAN, GRADE & ELEVATION
 - 3. PLAN, GRADE & ELEVATION
 - 4. PLAN, GRADE & ELEVATION
 - 5. PLAN, GRADE & ELEVATION
 - 6. PLAN, GRADE & ELEVATION
 - 7. PLAN, GRADE & ELEVATION
 - 8. PLAN, GRADE & ELEVATION
 - 9. PLAN, GRADE & ELEVATION
 - 10. PLAN, GRADE & ELEVATION
 - 11. PLAN, GRADE & ELEVATION
 - 12. PLAN, GRADE & ELEVATION
 - 13. PLAN, GRADE & ELEVATION
 - 14. PLAN, GRADE & ELEVATION
 - 15. PLAN, GRADE & ELEVATION
 - 16. PLAN, GRADE & ELEVATION
 - 17. PLAN, GRADE & ELEVATION
 - 18. PLAN, GRADE & ELEVATION
 - 19. PLAN, GRADE & ELEVATION
 - 20. PLAN, GRADE & ELEVATION

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
AND
STATE ROADS COMMISSION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

FEDERAL AID PROJECT NO. I-95-4(42)34
STATE HIGHWAY ADMINISTRATION PROJECT NO. BC 246-39-613
CITY OF BALTIMORE BUREAU OF ENGINEERING,
HIGHWAY ENGINEERING DIVISION CONTRACT NO. 2071

INTERSTATE ROUTE 95

PEDESTRIAN OVERPASS - KANE STREET TO QUINTON STREET



FIELD BOOKS

| DATE | DESCRIPTION |
|------|-------------|
| | |
| | |
| | |
| | |

| | |
|-------------|------|
| APPROVED BY | DATE |
| | |
| | |
| | |
| | |

| | |
|-------------|------|
| APPROVED BY | DATE |
| | |
| | |
| | |
| | |

PREPARED BY

STATE ROADS COMMISSION OF MARYLAND

U.S. DEPARTMENT OF TRANSPORTATION

FEDERAL HIGHWAY ADMINISTRATION

