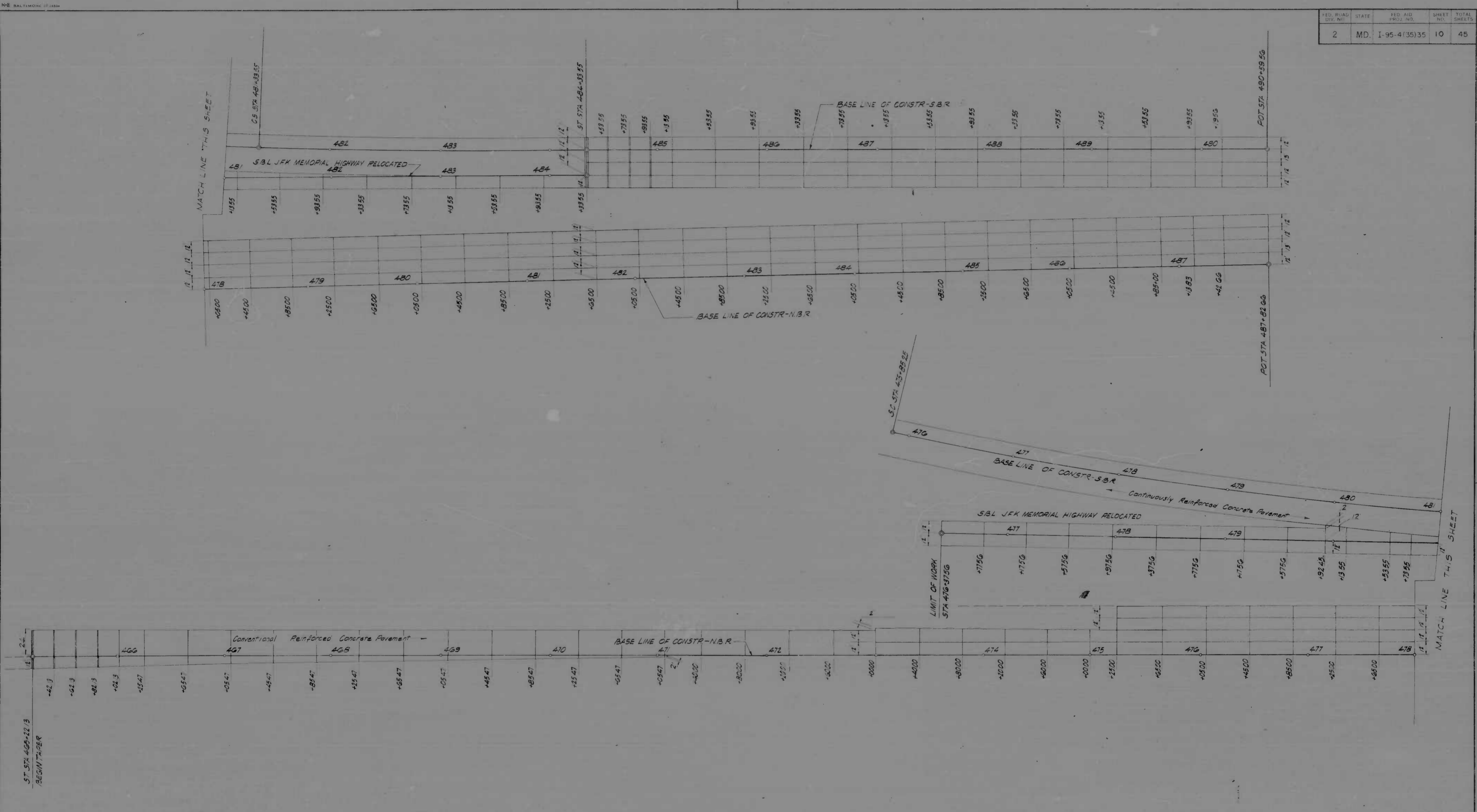


FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35)35	10	45

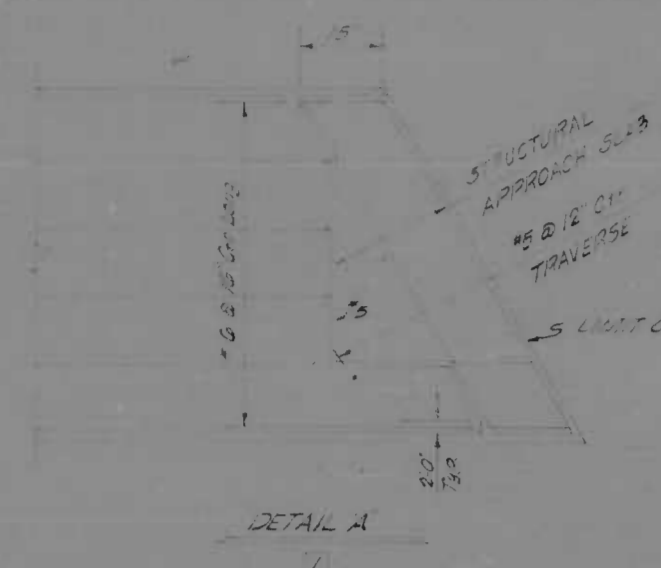


LEGEND

TERMINAL JOINT FOR LONGITUDINAL JOINT LOCATIONS
 EXPANSION JOINT PAVEMENT AND JOINT DETAILS, SEE
 CONTRACTION JOINT TYPICAL SECTIONS & PAVEMENT DETAILS
 SHEET 2, 3, 4, 7, 8 & 9

TRANSVERSE JOINT LOCATIONS

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KNOXLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
	SCALE 1" = 40'	DATE	DRAWN BY PWZ TRACED BY PWZ P. A. P. NO. I-95-4(35)35 S. R. C. NO. BC 246-32-815 BALTO. CITY NO. 1983
			DES. BY J.L.C. CHK. BY R.W.G. SHEET NO. 10 OF 45



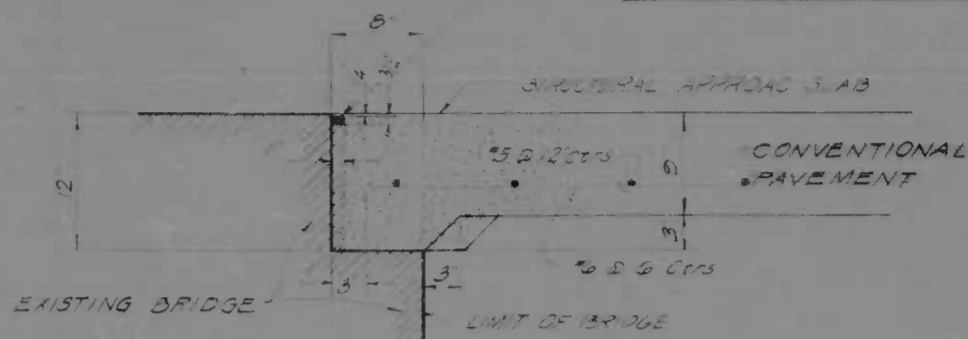
RUBBER JOINT SEALING COMPOUND SHALL BE NOVIFLEX 502 RB AS MANUFACTURED BY AC HOORN COMPANY 2133 BETH STREET NORTH BERGEN NEW JERSEY 07041-1008 OR AS MANUFACTURED BY SIKA CHEMICAL CORPORATION 375 VALLEY BROOK AVENUE LYNDHURST NEW JERSEY 07037 OR EQUAL.

PRIMER SHALL BE USED WITH THE JOINT SEALING COMPOUND OF THE RESPECTIVE MANUFACTURERS SHALL BE NOVIFLEX PRIMER AS MANUFACTURED BY AC HOORN COMPANY COLMA SOLUBLE AS MANUFACTURED BY SIKA CHEMICAL CORPORATION OR APPROVED EQUAL.

THE JOINT SEALING COMPOUND SHALL BE FURNISHED FOR TWO APPLICATION TEMPERATURE RANGES 50 DEGREES F TO 80 DEGREES F AND 70 DEGREES F TO 100 DEGREES F AND SHALL NOT BE PLACED AT TEMPERATURES BELOW 50 DEGREES F OR OVER 100 DEGREES F (ALL TEMPERATURES ARE AMBIENT AIR TEMPERATURES).

THE PRODUCTS OF ONLY ONE MANUFACTURER SHALL BE USED ON THE PROJECT AND THE CONTRACTOR SHALL SUBMIT FOR THE APPROVAL OF THE ENGINEER THE NAME OF THE PRODUCT HE PROPOSES TO FURNISH.

SHEET NO.	DATE	REV. NO.	BY	CHK.	TOTAL SHEETS
2	MD	95-4(35)35	11	45	



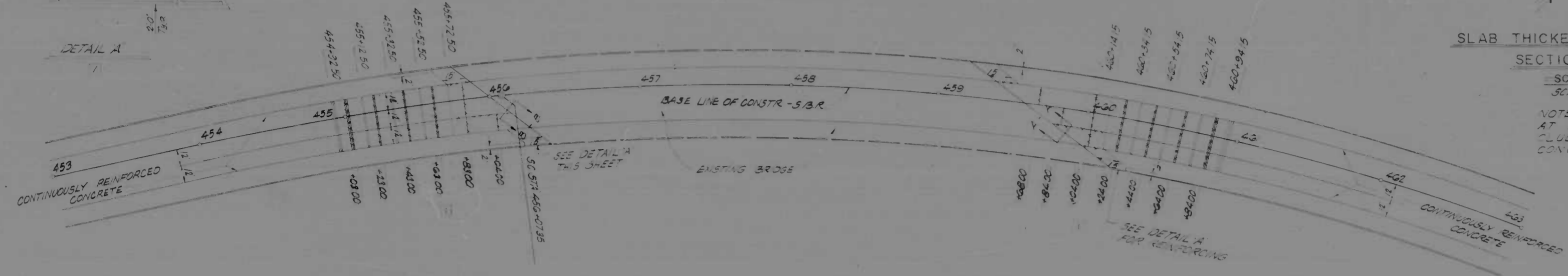
SLAB THICKENING AT BRIDGE SECTION A-A & B-B
SCALE: 1" = 1'-0"
SCALE: 1" = 10'-0"

NOTE: COST OF SLAB THICKENING AT APPROACH SLAB TO BE INCLUDED IN PRICE BID FOR CONCRETE PAVEMENT.

LEGEND

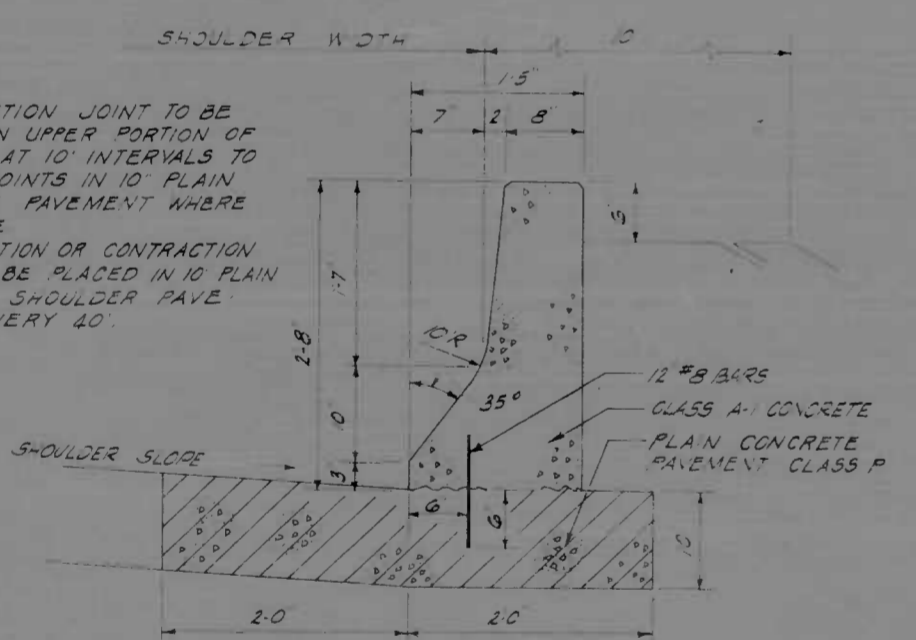
TERMINAL JOINT
EXPANSION JOINT
CONTRACTION JOINT

FOR LONGITUDINAL JOINT LOCATIONS PAVEMENT AND JOINT DETAILS SEE TYPICAL SECTIONS AND PAVEMENT DETAILS SHEET NO. 2, 3, 4, 7, 8 & 9

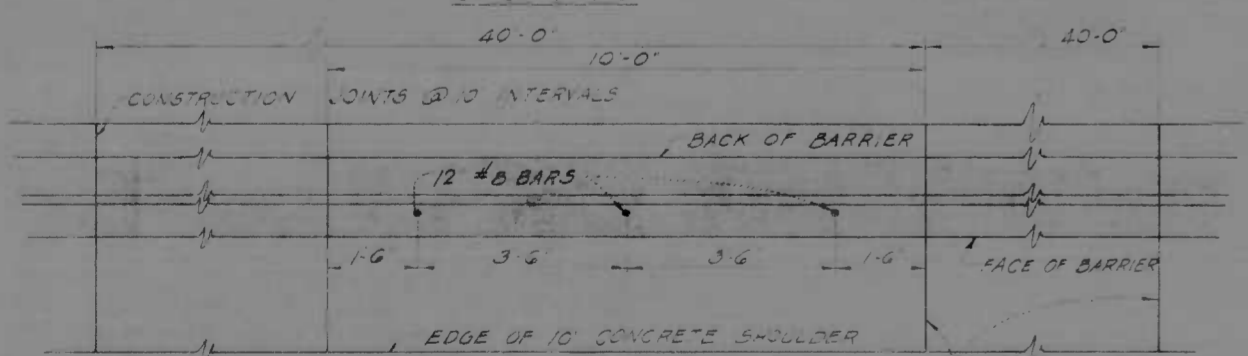


TRANSVERSE JOINT LOCATIONS
SCALE: 1" = 40'

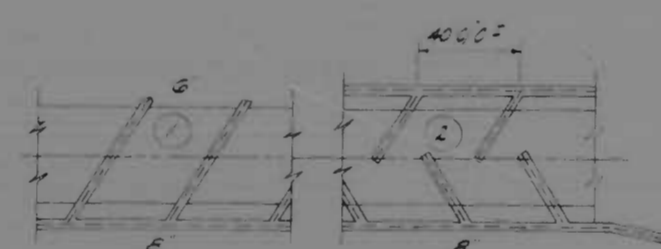
NOTE:
1. CONSTRUCTION JOINT TO BE PLACED IN UPPER PORTION OF BARRIER AT 10' INTERVALS TO MATCH JOINTS IN 10' PLAIN CONCRETE PAVEMENT WHERE POSSIBLE.
2. CONSTRUCTION OR CONTRACTION JOINT TO BE PLACED IN 10' PLAIN CONCRETE SHOULDER PAVEMENT EVERY 40'.



SECTION
SCALE: 1" = 1'-0"



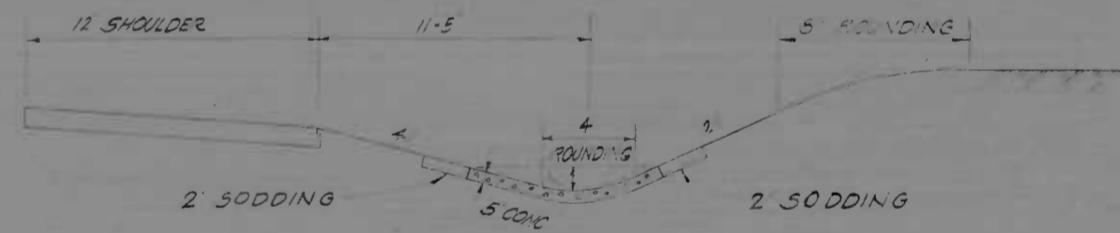
PLAN VIEW
CONCRETE BARRIER
SCALE: 1" = 2'-0"



CONTINGENT UNDERDRAIN PATTERN
SEE S.R.C. STD. PLATE U-11 NOT TO SCALE
USING SUB-SURFACE DRAINAGE DITCH SECTION

6 PERFORATED ROUND UNDERDRAIN LATERALS 8 BULLET PILES AS REQUIRED
2 UNDERDRAIN LINE ONE SIDE WITH LATERALS
2 UNDERDRAIN LINE BOTH SIDES WITH STAGGERED LATERALS
TO BE CONSTRUCTED IF DIRECTED BY THE ENGINEER FOR CONTROL OF UNDERGROUND SPRING AND SUB-GRADE DRAINAGE PROBLEMS IN NET CUT AREAS LOCATIONS AND TYPE AS DIRECTED BY THE ENGINEER

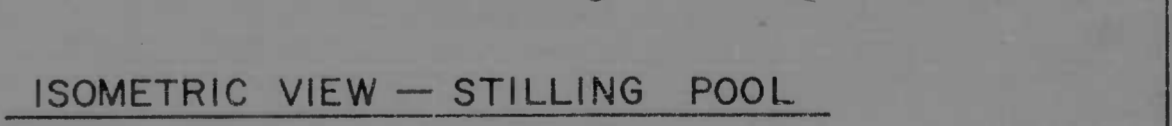
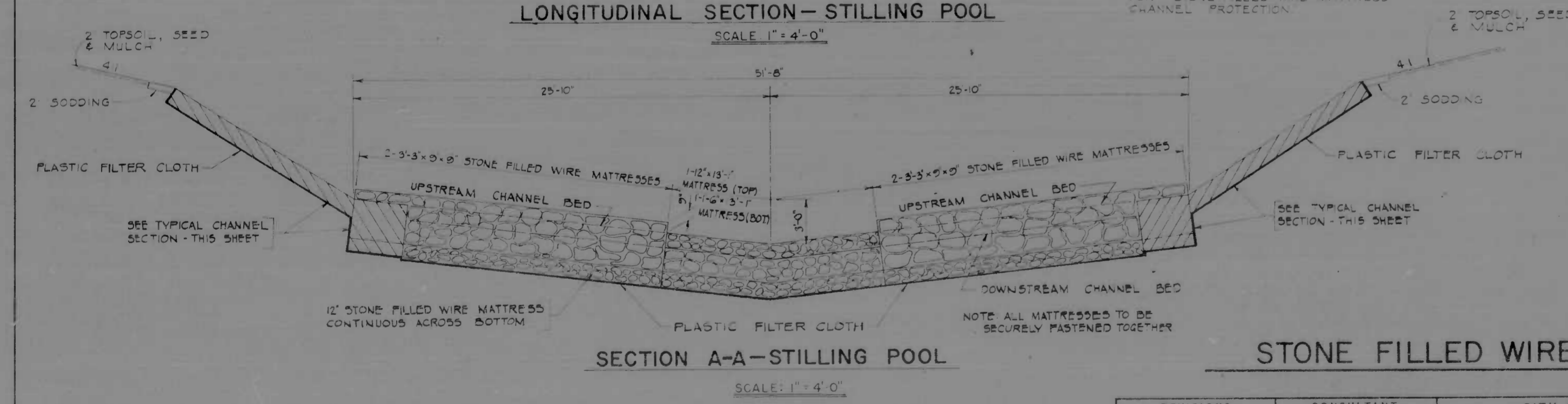
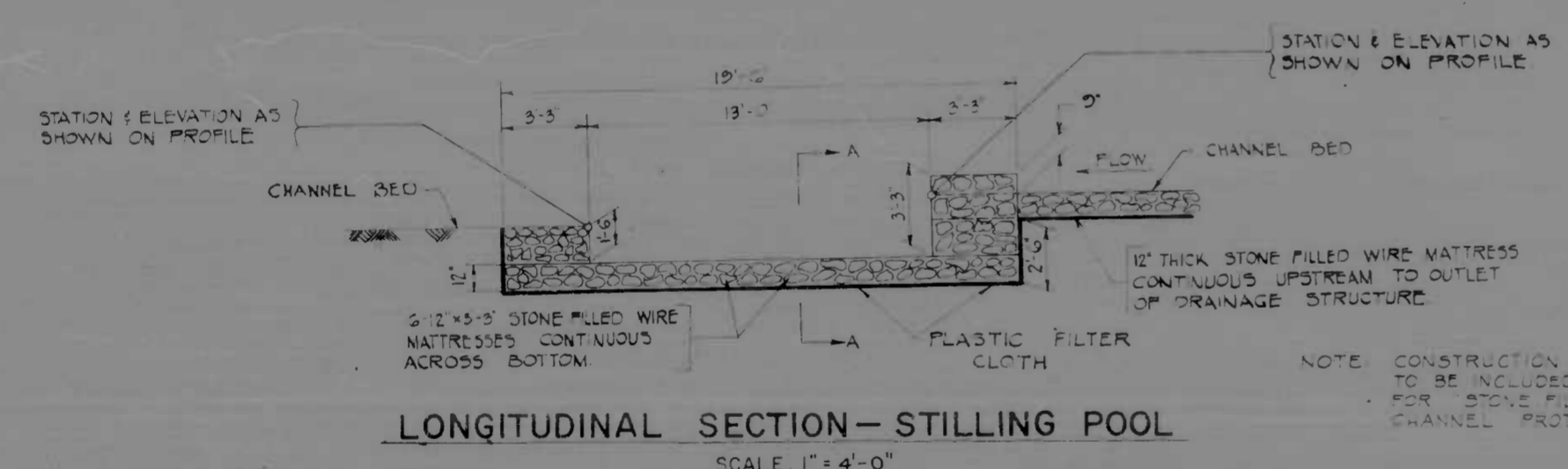
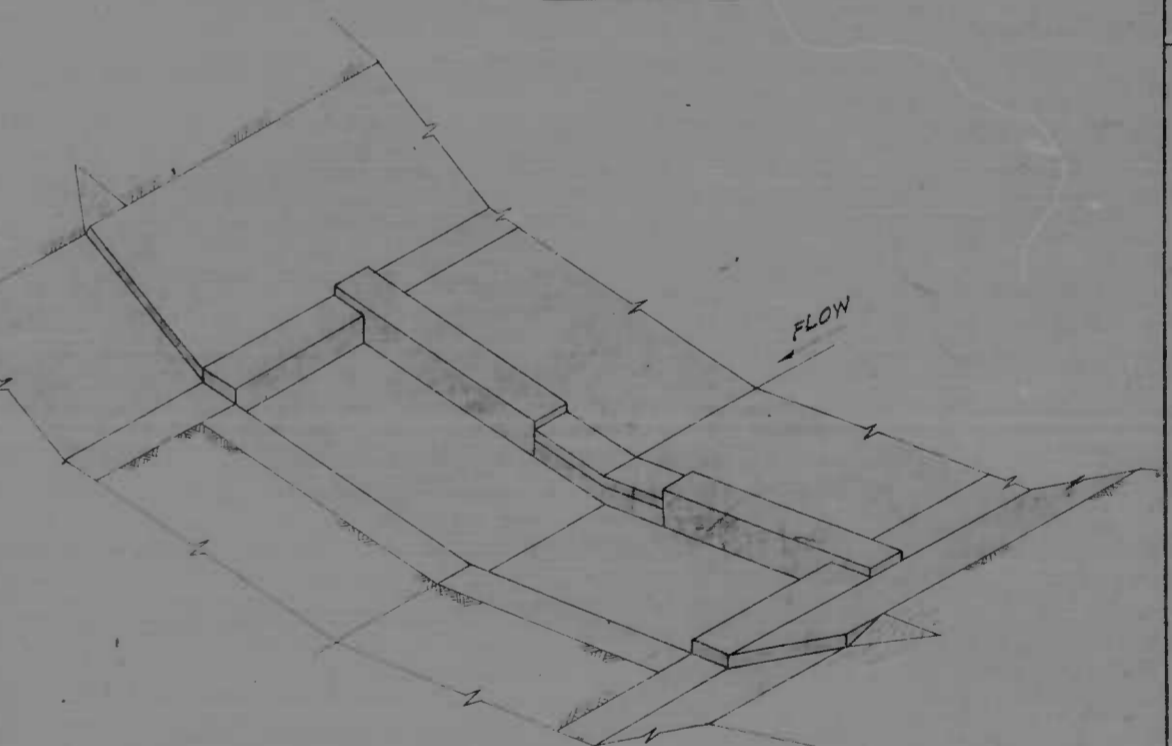
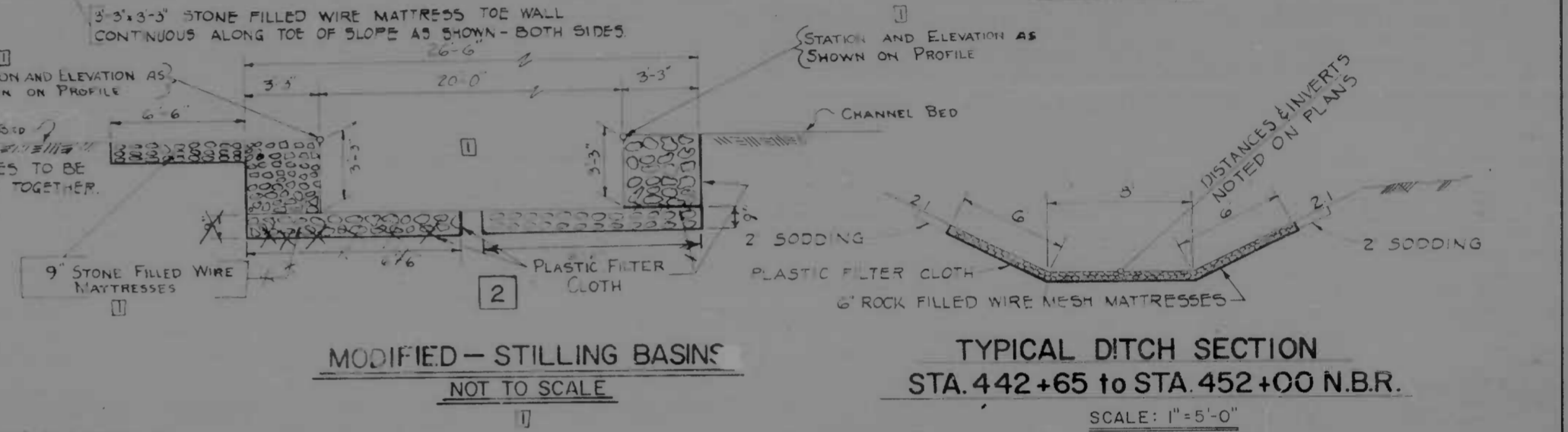
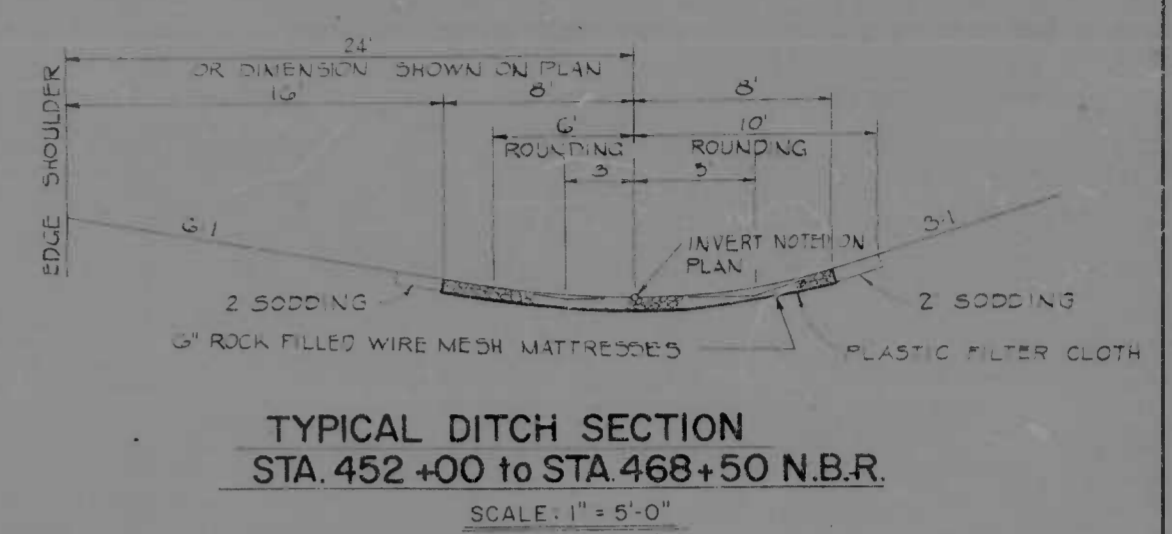
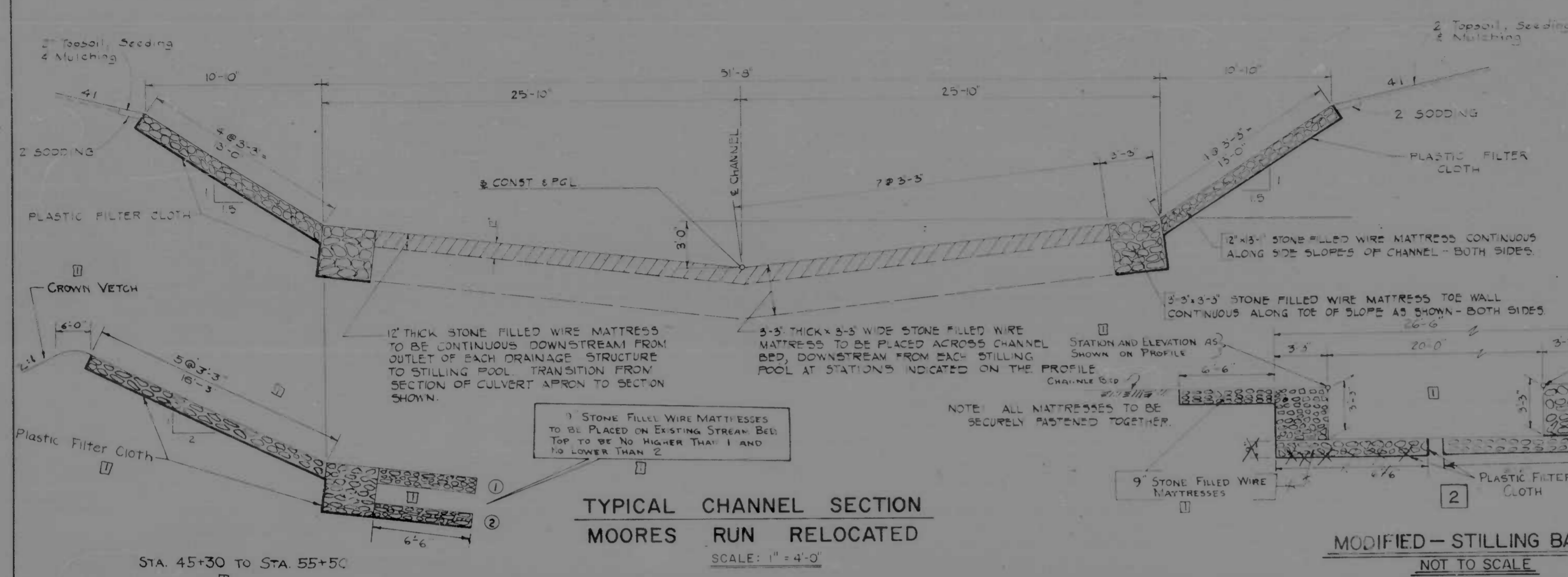
CONTINGENT UNDERDRAIN PATTERN
NO SCALE



TYPICAL DITCH SECTION
STA 468+50 TO STA 479+00
SCALE: 1" = 50'

MISC. DETAILS

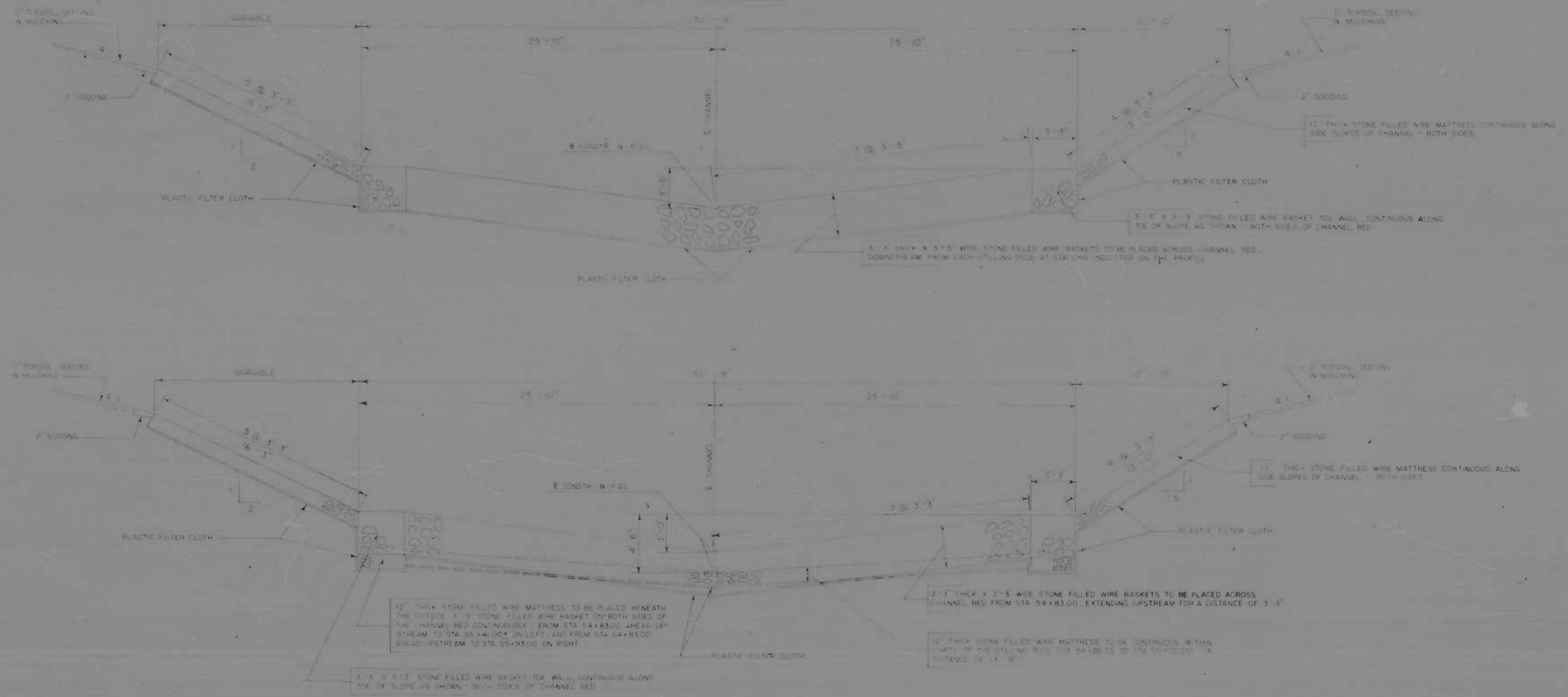
REVISIONS 1. APPROACH SLAB AND JOINT LOCATION DETAILS 12/24/70	CONSULTANT EMPERLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: P.W.Z. TRACED BY: P.W.Z. P.A.P. NO. I-95 4(35) 35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983	DES. BY: J.L.C. CHK. BY: R.W.G. SHEET NO. 11 OF 45



STONE FILLED WIRE MATTRESS DETAILS

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION I OR BALTIMORE CITY
1 Revised wire baskets 9-10-31	KROEGER, BENDER, STONE & ASSOC., INC. AND WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21206	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: DLT. TRACED BY: DLT. DES. BY: TEL CHK. BY: RWG
2 Revised Stilling Pool			
		SCALE AS SHOWN	DATE 1983
			SHEET NO. 12 OF 45

TYPICAL CHANNEL SECTION

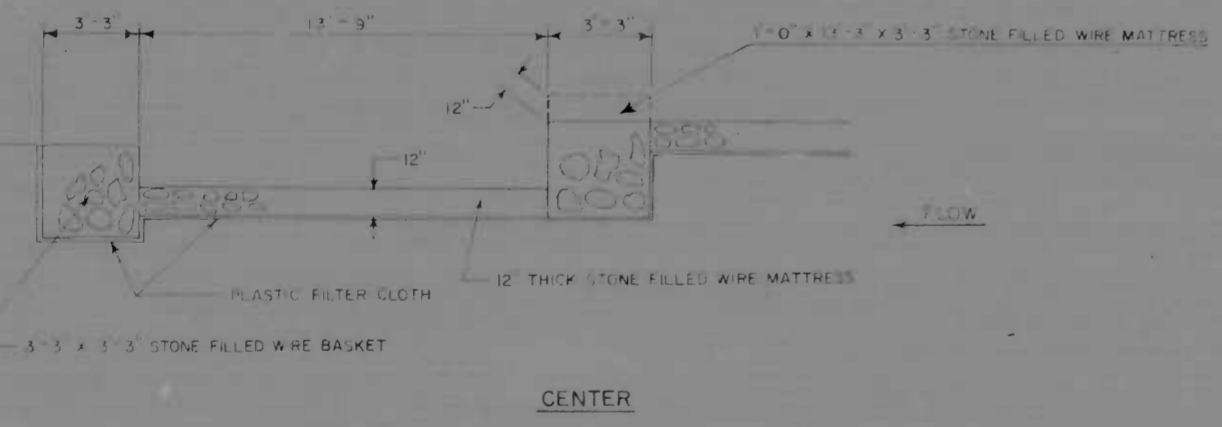
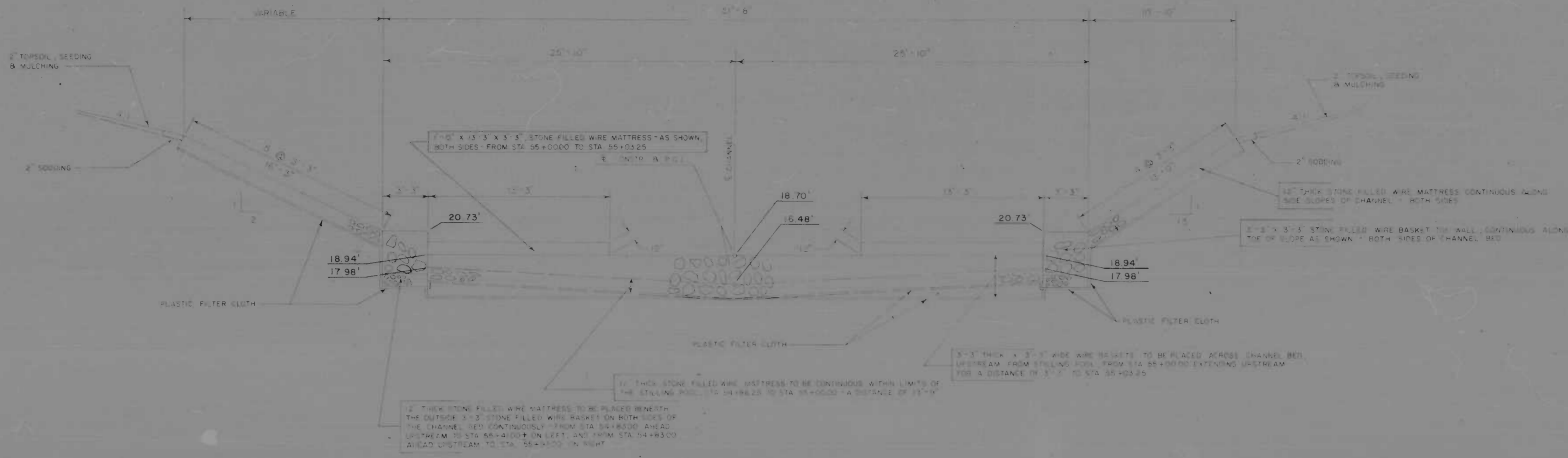


STA. 54+86.25

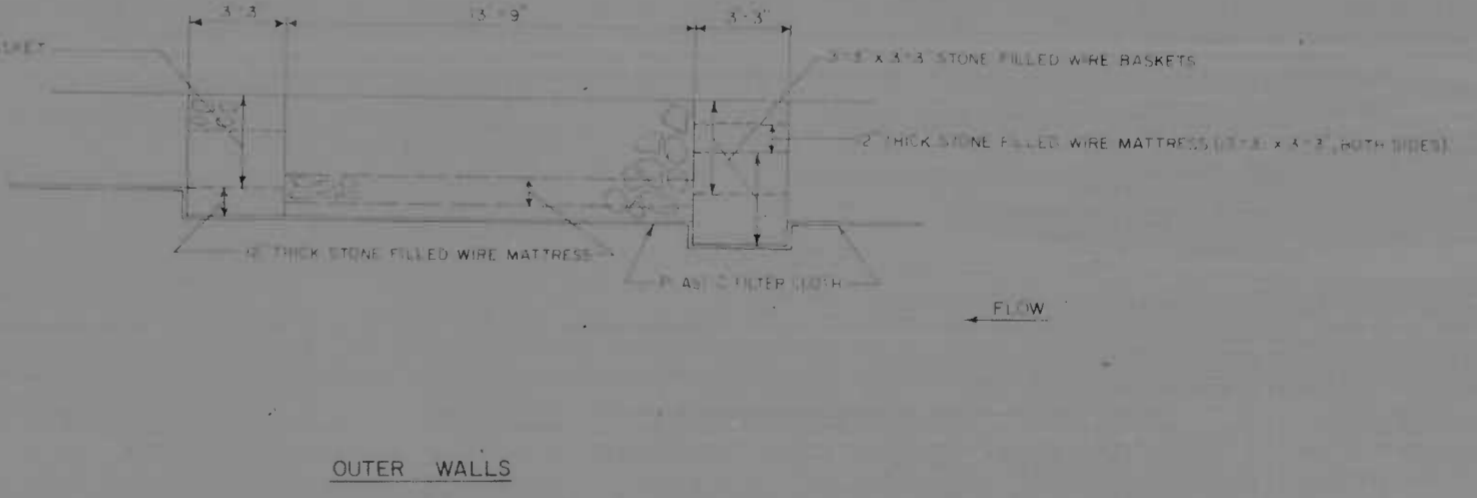
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
ADDITION TO EXISTING CONTRACT		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: D.A. KNOTT	DES. BY: I.D.B.C.
		SCALE: 1" = 4' - 0"		DATE: 1983	SHEET NO.: 12 A OF 45

DES. NO.	SCALE	DES. NO.	SHEET	TOTAL SHEETS
2	MD I-95-4(35)35	12 B	45	

STA 55+00.00



CENTER

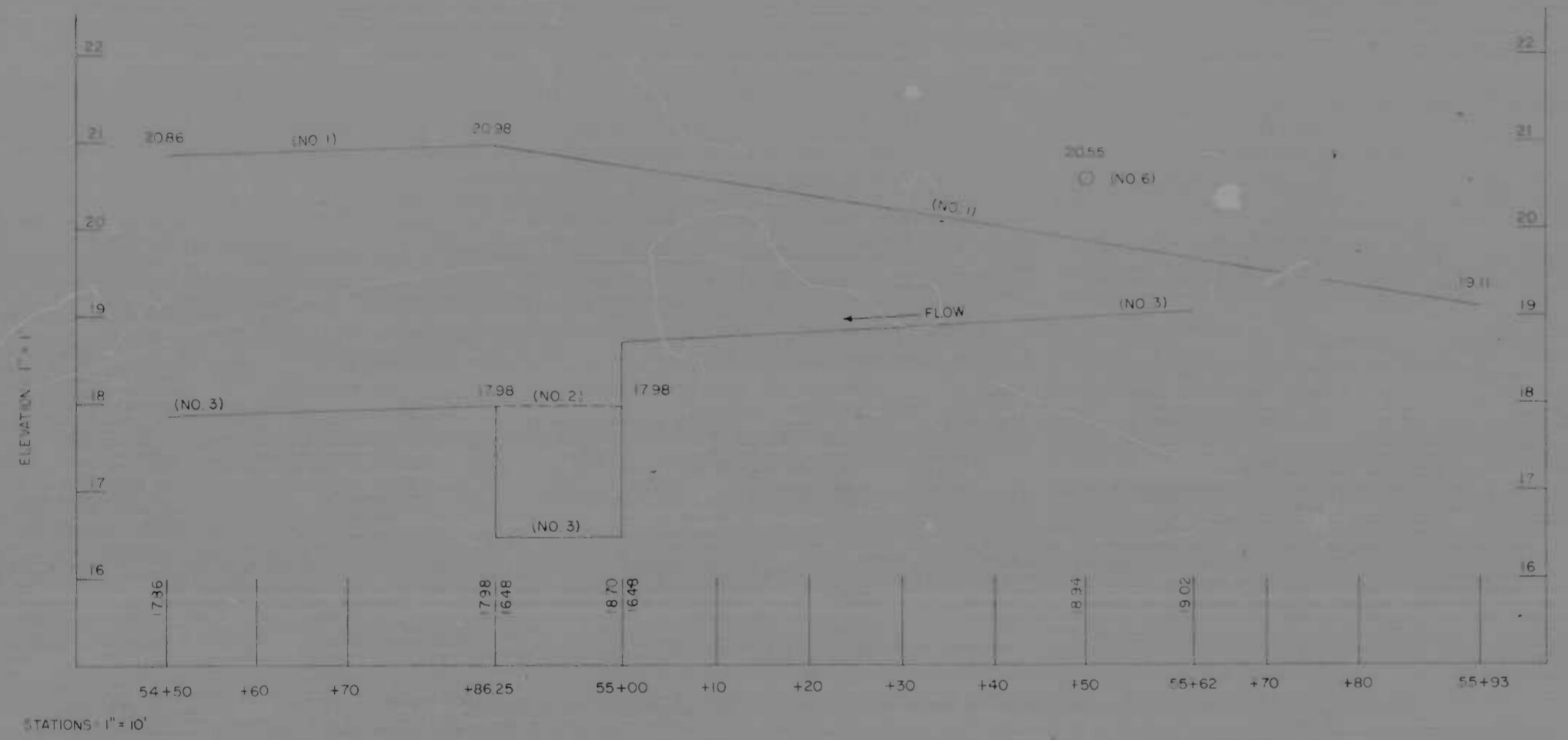


OUTER WALLS

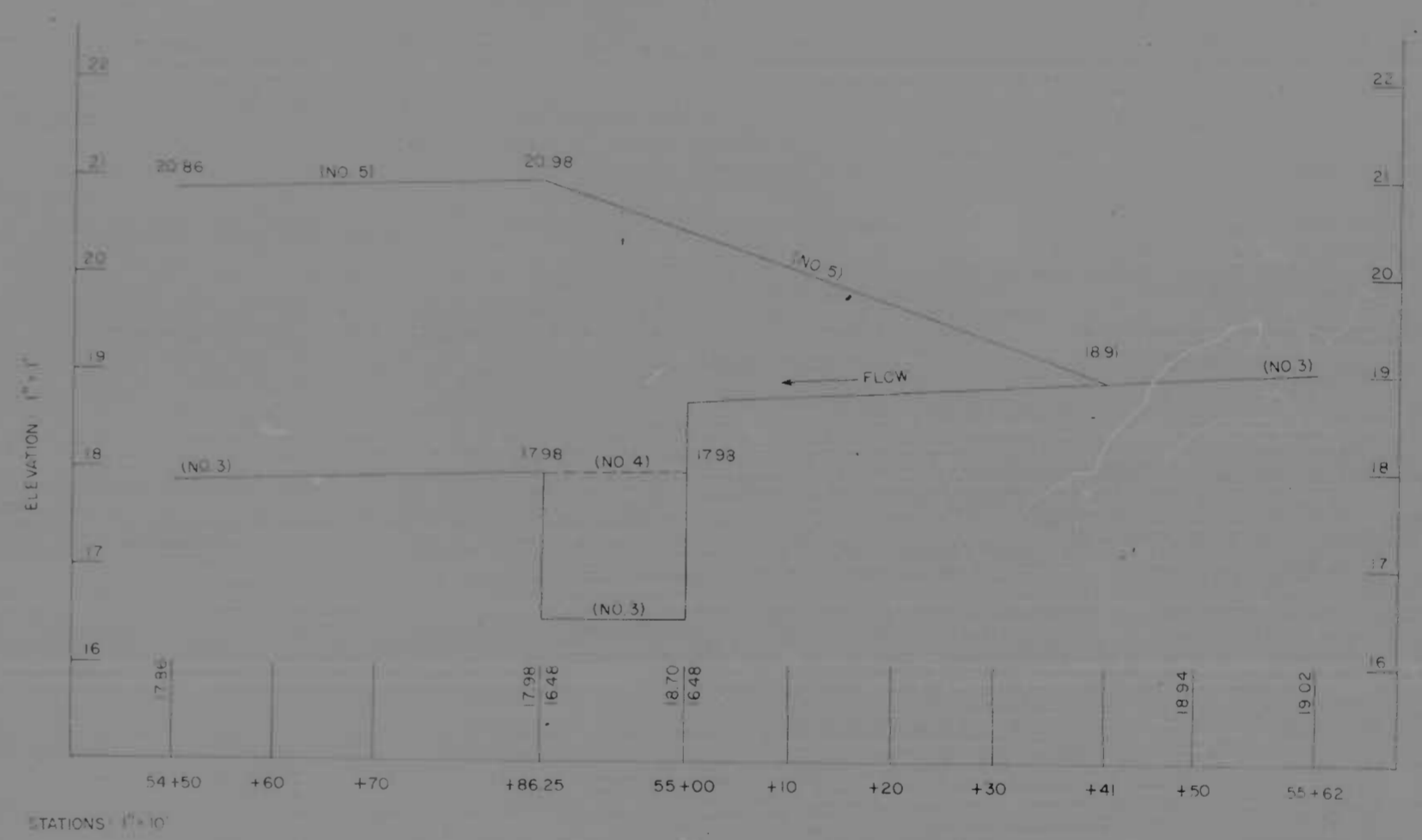
LONGITUDINAL SECTIONS - STILLING POOL

REVISIONS LOCATION TO EXISTING CONTRACT 11-10-71	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: D.A. KNOTTIS CHECKED BY: D.A. KNOTTIS	DES. BY: I.D.B.C. I.C.H.K. BY: I.D.B.C.
SCALE: 1" = 4'-0"		DATE:		F.A.P. NO.: I-95-4(35)35 S.R.C. NO.: BC 246-32-815	SHEET NO.: 42 B OF 45

2	MD	7-95-4(35)35	12C	45
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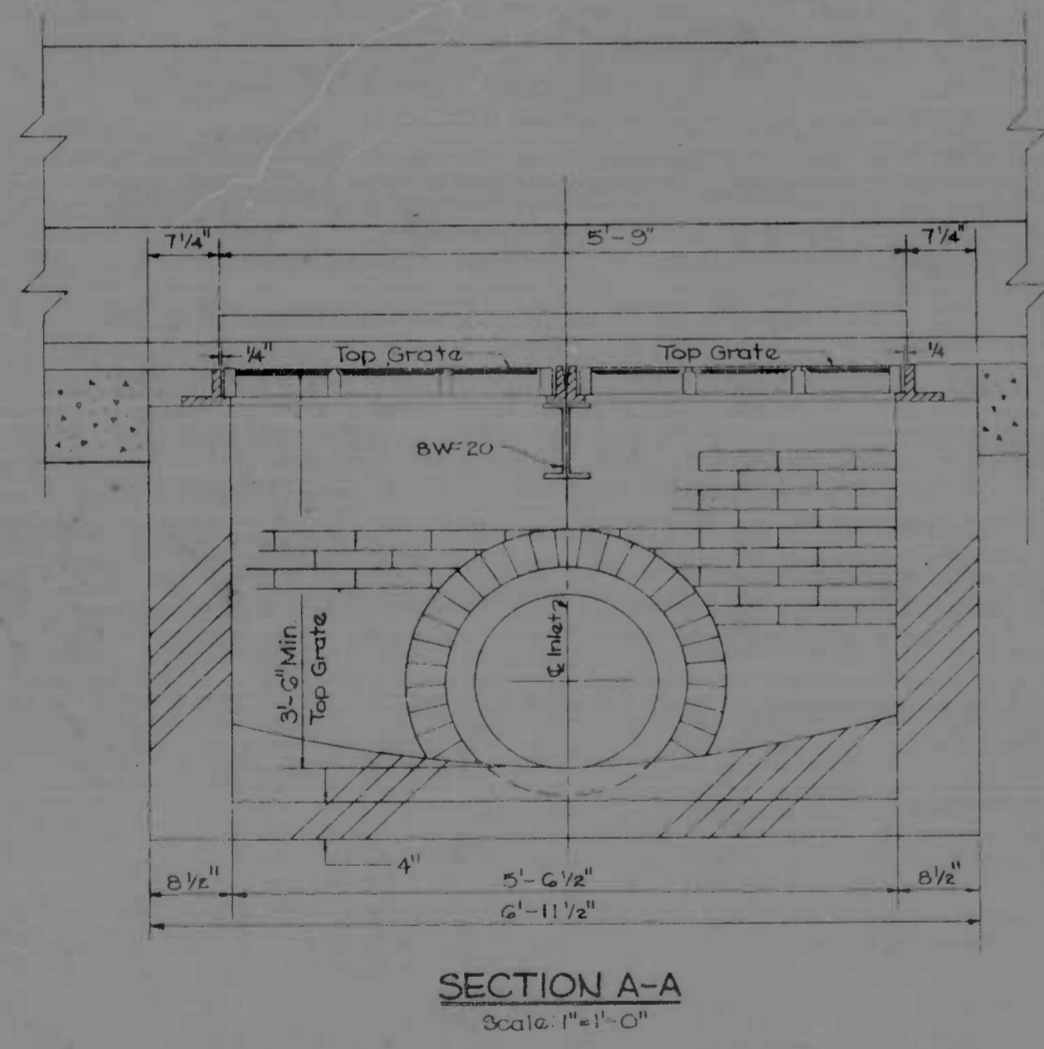
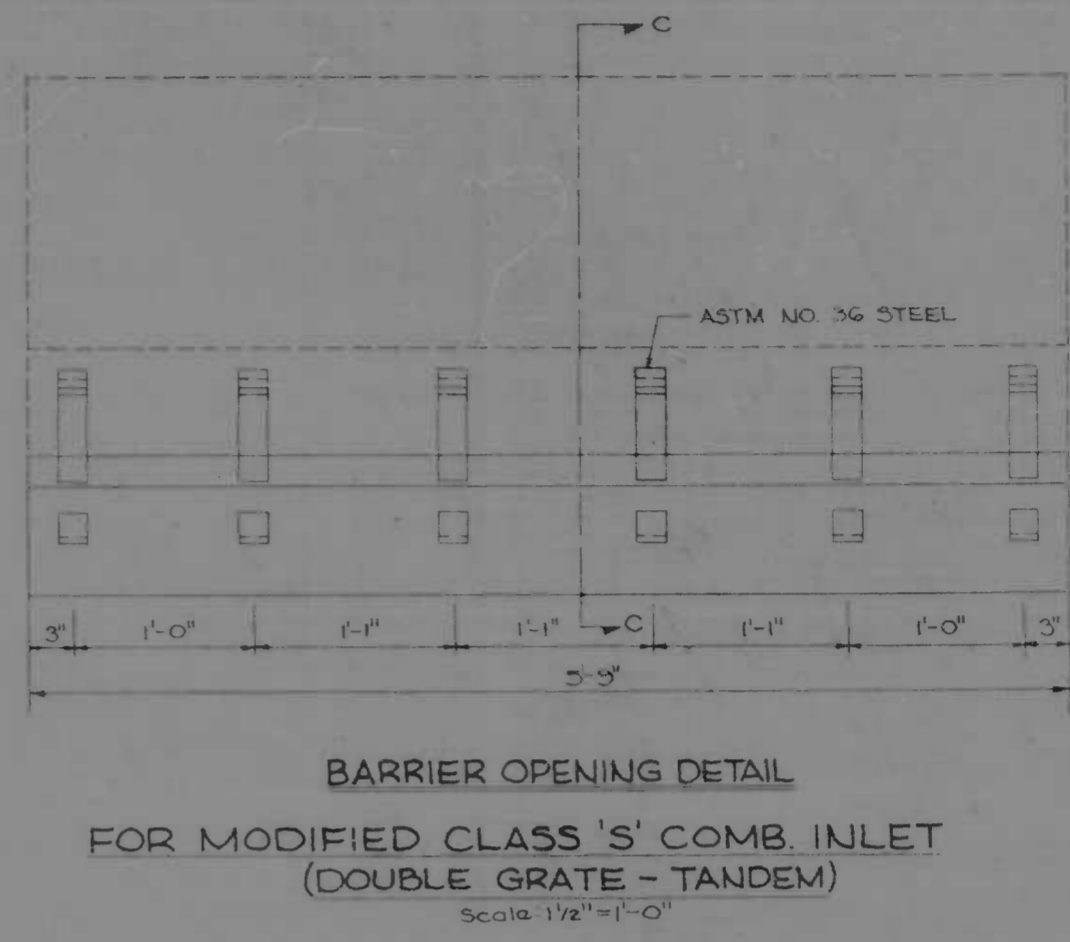
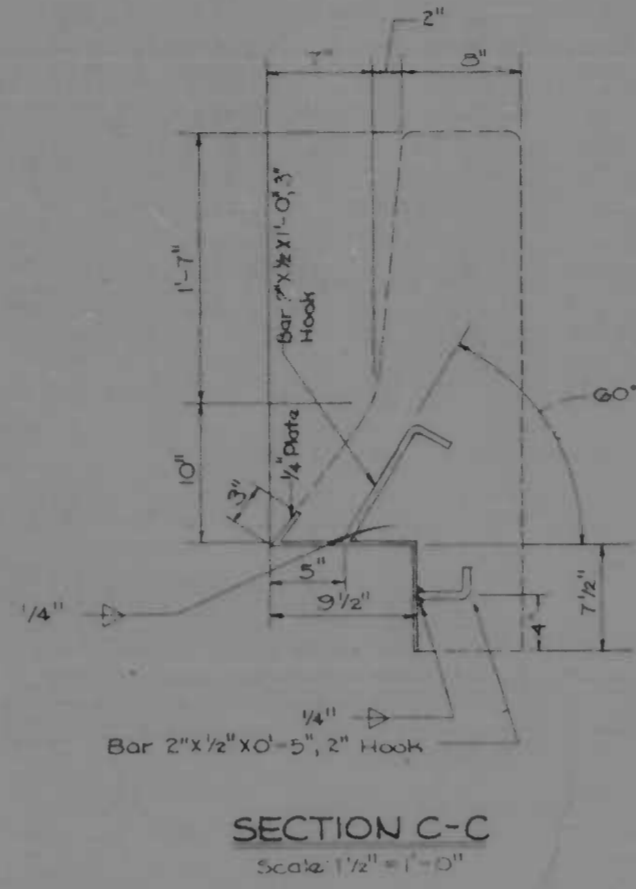
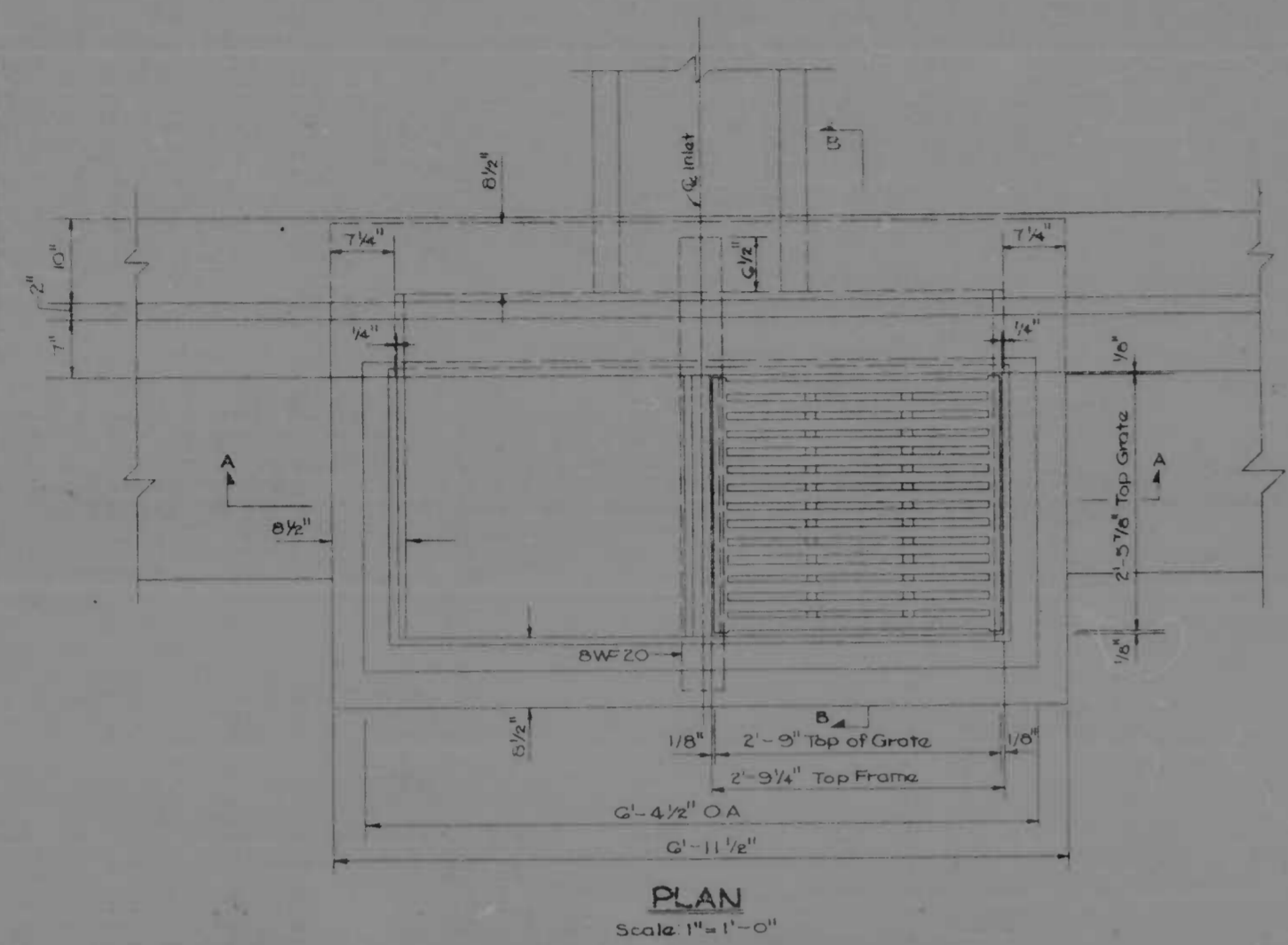
PROFILE, CENTER & RIGHT SIDE - AREA OF STILLING POOL



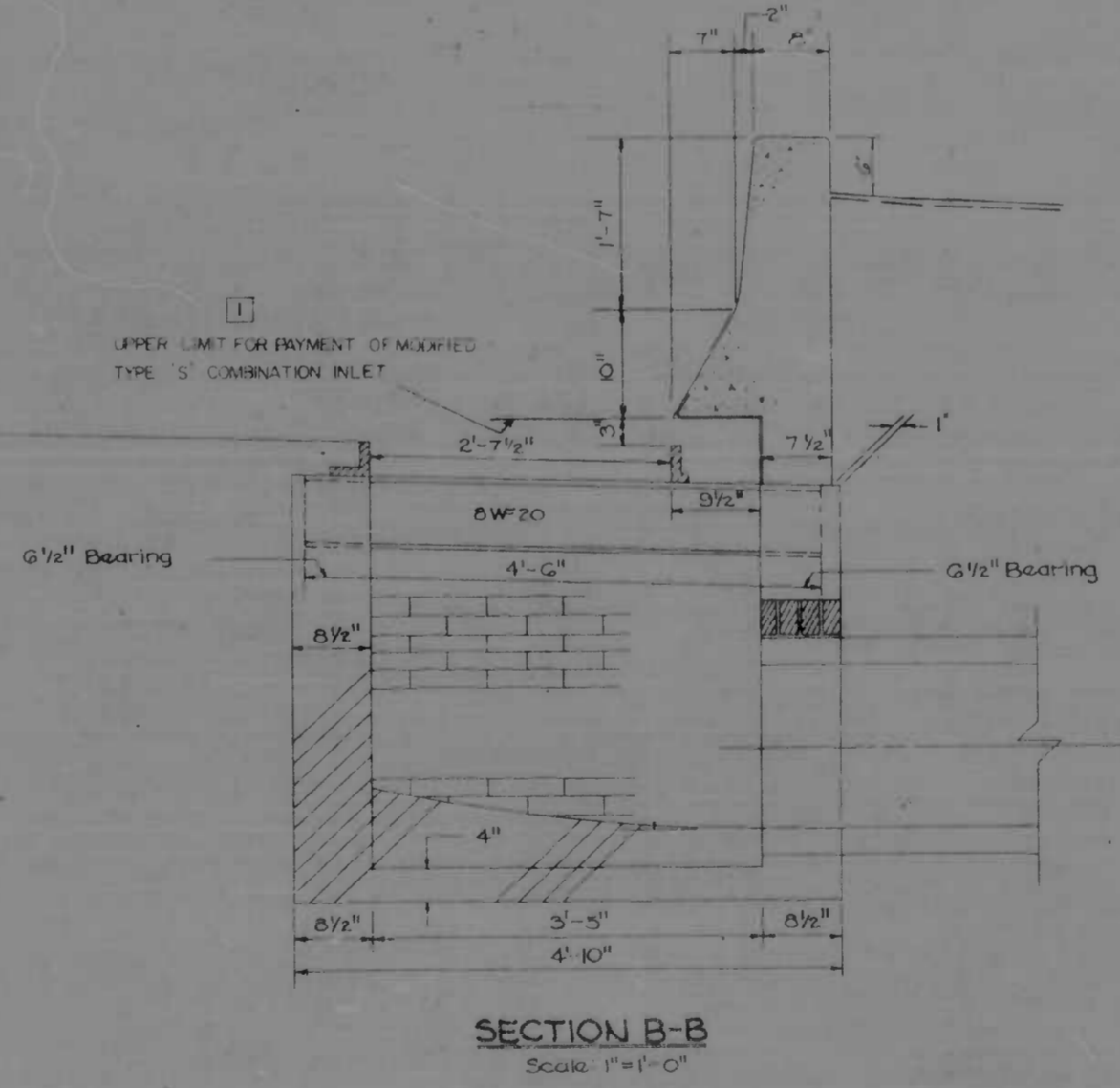
PROFILE, CENTER & LEFT SIDE - AREA OF STILLING POOL

- NO 1 - TOP OF TOE WALL, RIGHT SIDE OF CHANNEL - SOLID LINE
- NO 2 - P.G.L. STILLING POOL - DASHED LINE
- NO 3 - CENTER OF CHANNEL - SOLID LINE
- NO 4 - P.G.L. STILLING POOL - DASHED LINE
- NO 5 - TOP OF TOE WALL, LEFT SIDE OF CHANNEL - SOLID LINE
- NO 6 - POINT OF INVERT, 21" R.C.C.P. - ON RIGHT

REVISIONS APPROX. TO EXISTING CONTRACT	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY			
SCALE VERT. 1" = 1' HOR. 1" = 10'		DATE	DRAWN BY: D.A. KNOTTS	CHK. BY: I.D.B.C.	SHEET NO.
			TRACED BY: D.A. KNOTTS	CHK. BY: I.D.B.C.	45
			I.A.P. NO. 7-95-4(35)35		
			S.W. NO. BC 246-32-85		
			BALTO. CITY NO. 1983		

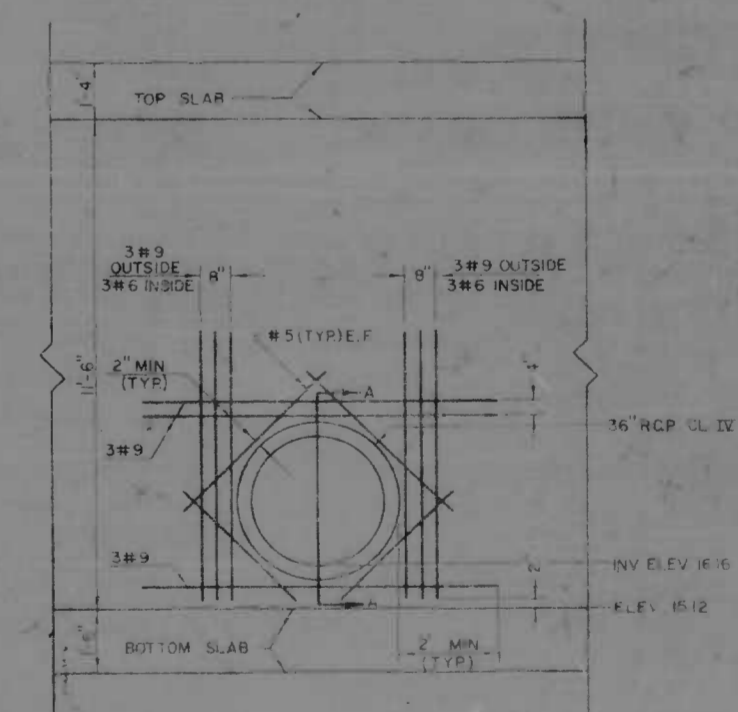
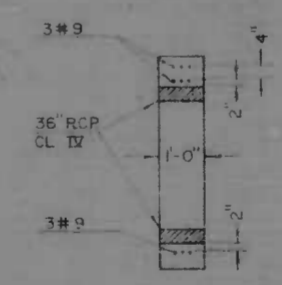


MODIFIED CLASS 'S' COMBINATION (DOUBLE GRATE-TANDEM)

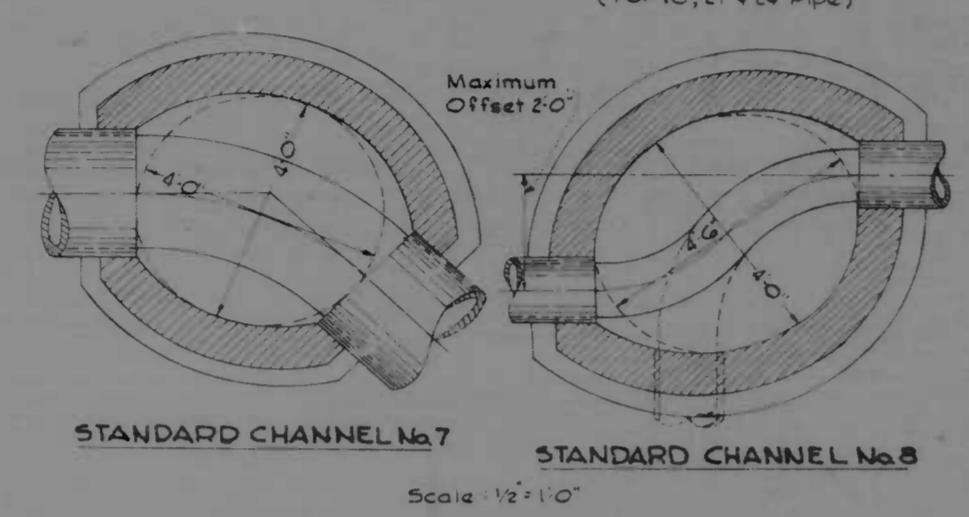
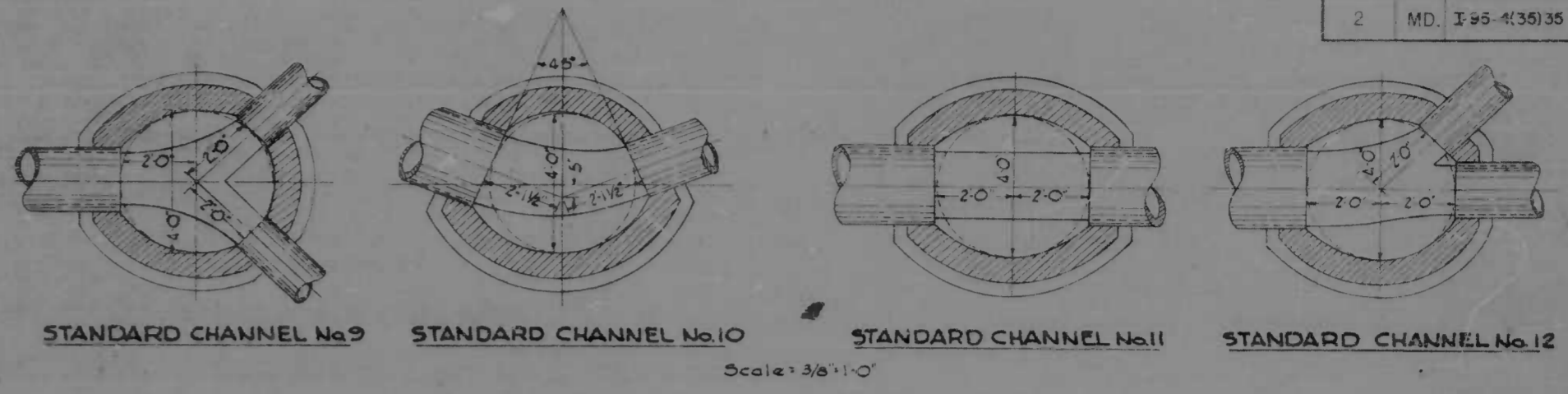
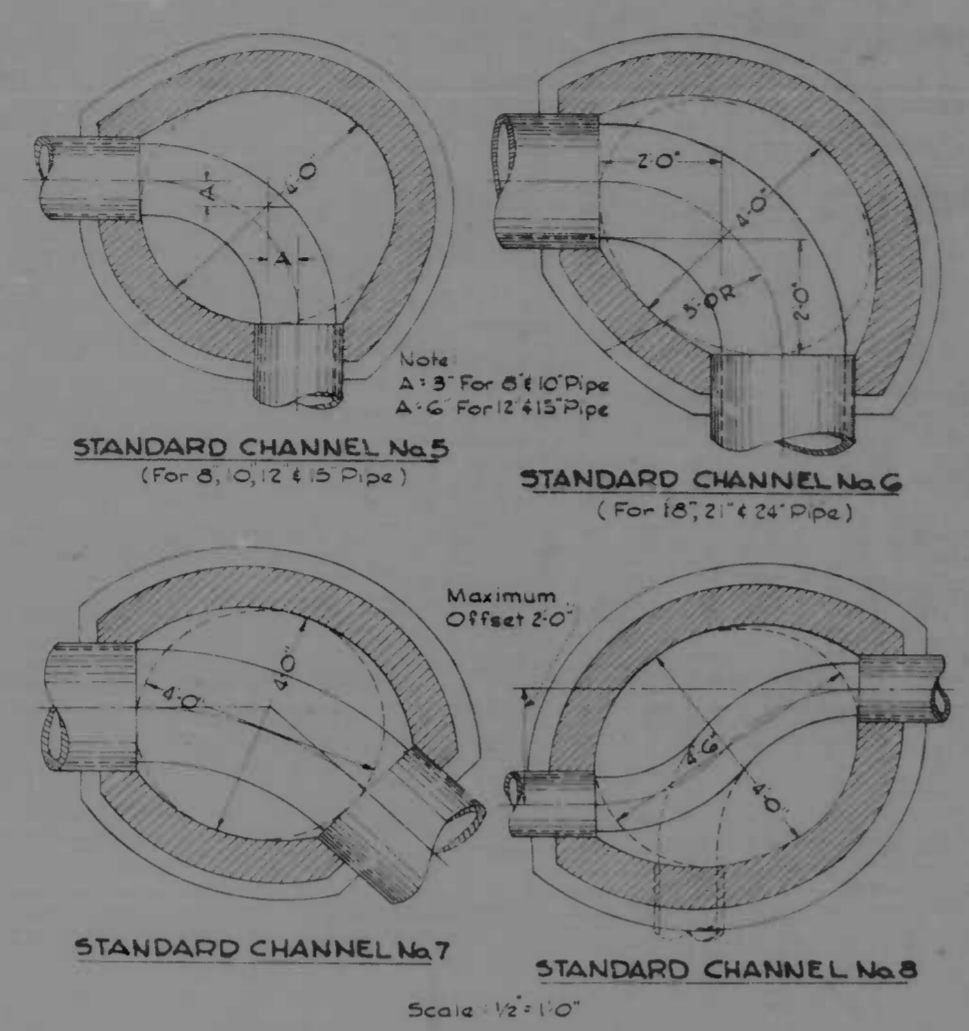
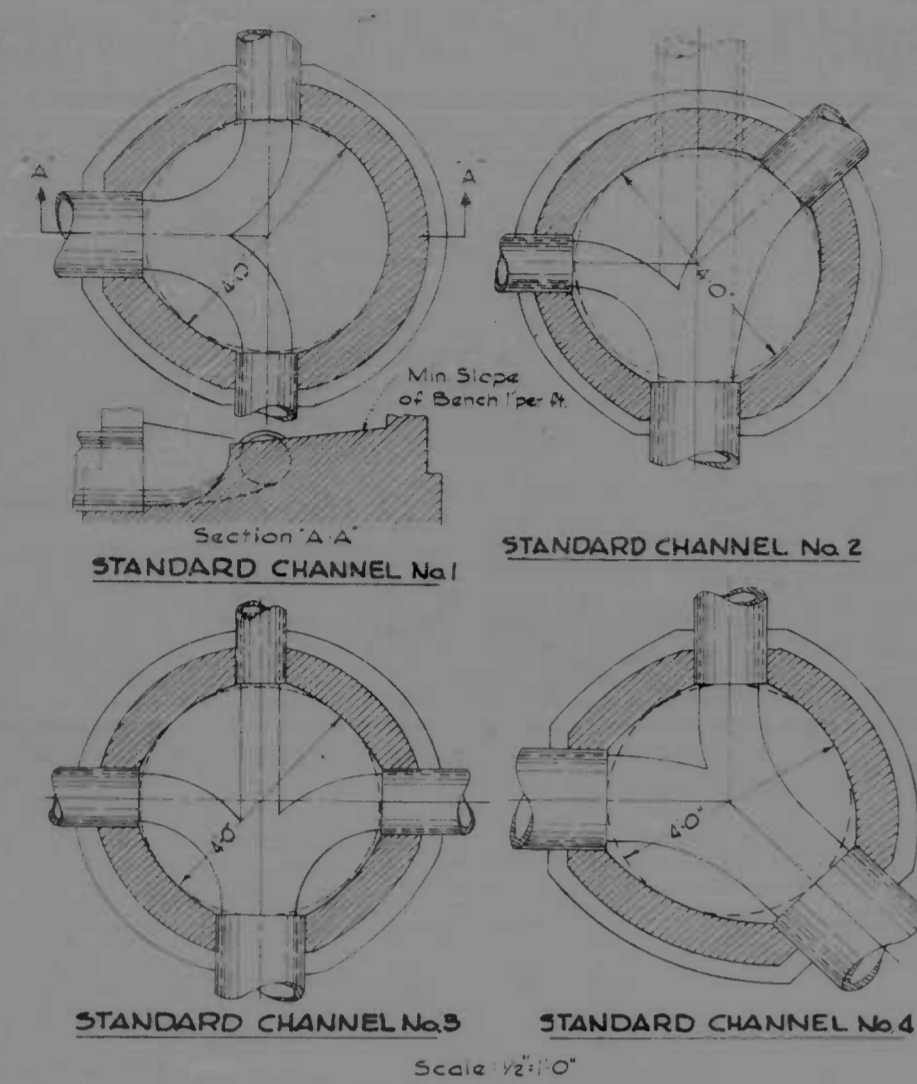


REINFORCING AT 36" DIA. PIPE ENTRANCE SOUTH EXTERIOR WALL OF BOX CULVERT
SEE SHEETS NOS. 14 & 20 OF 45

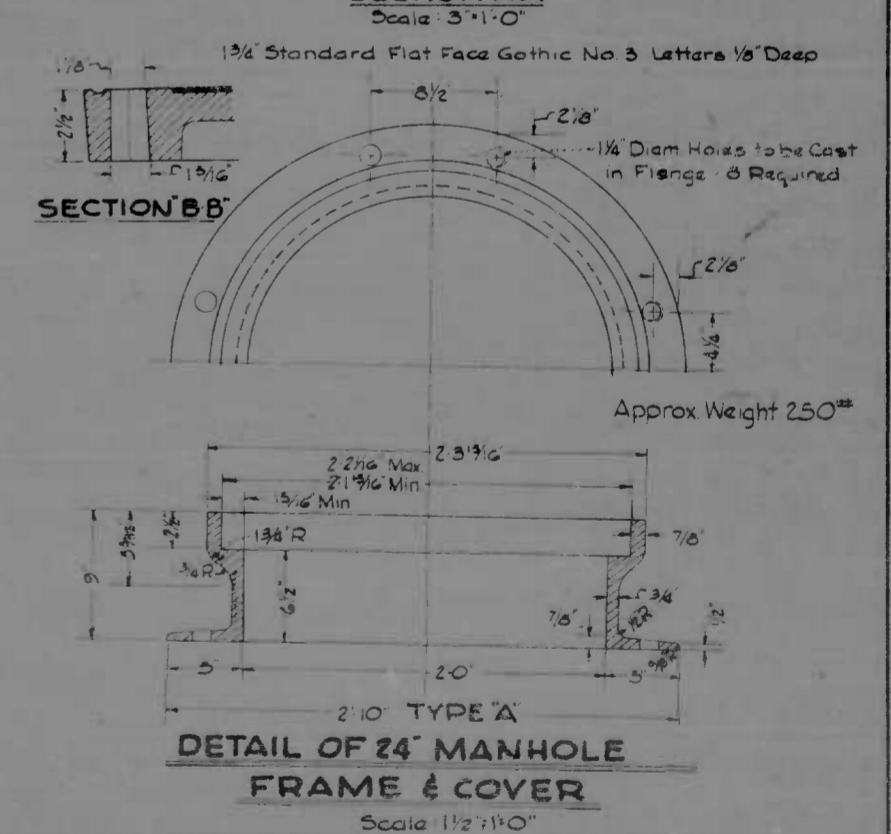
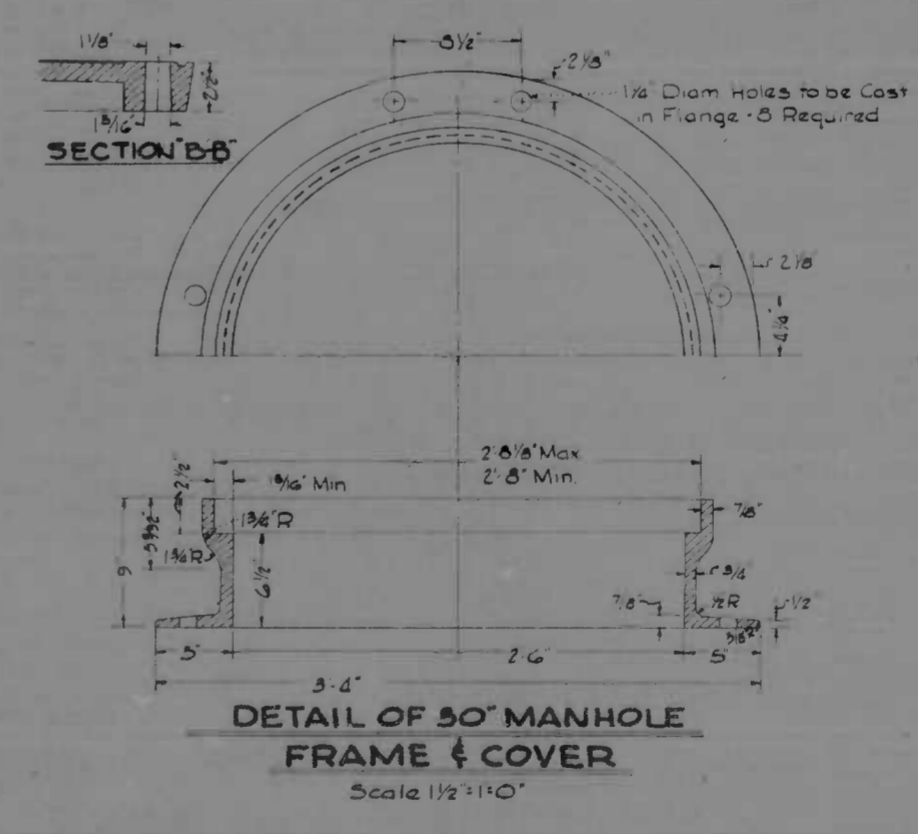
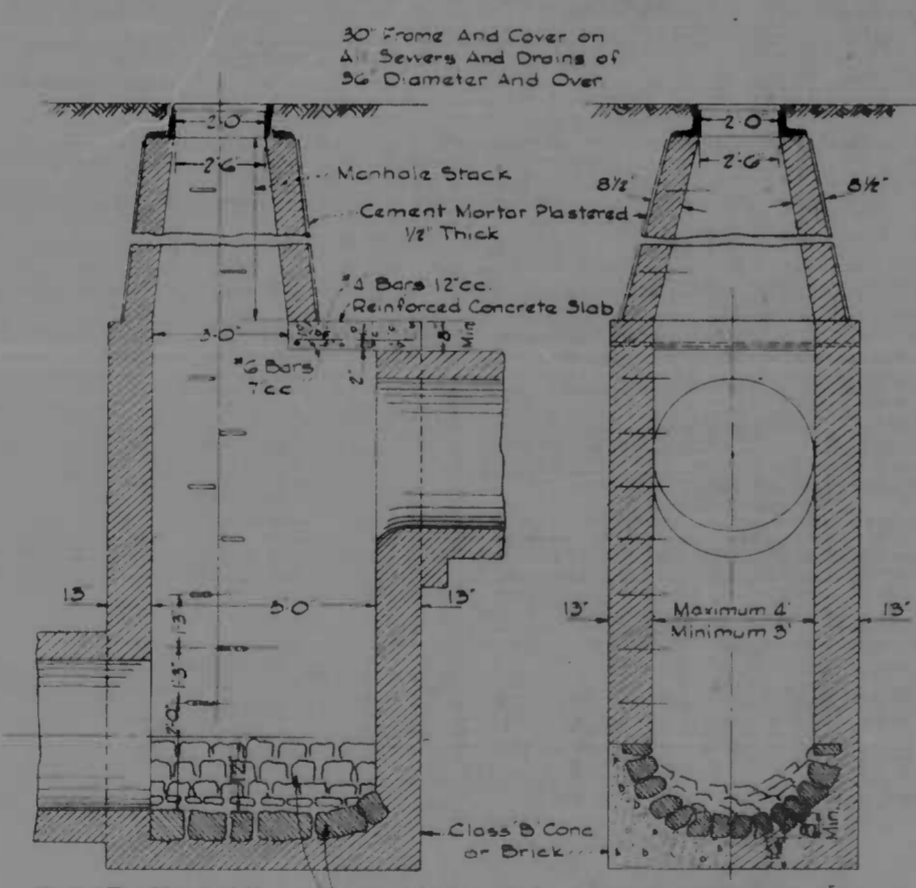
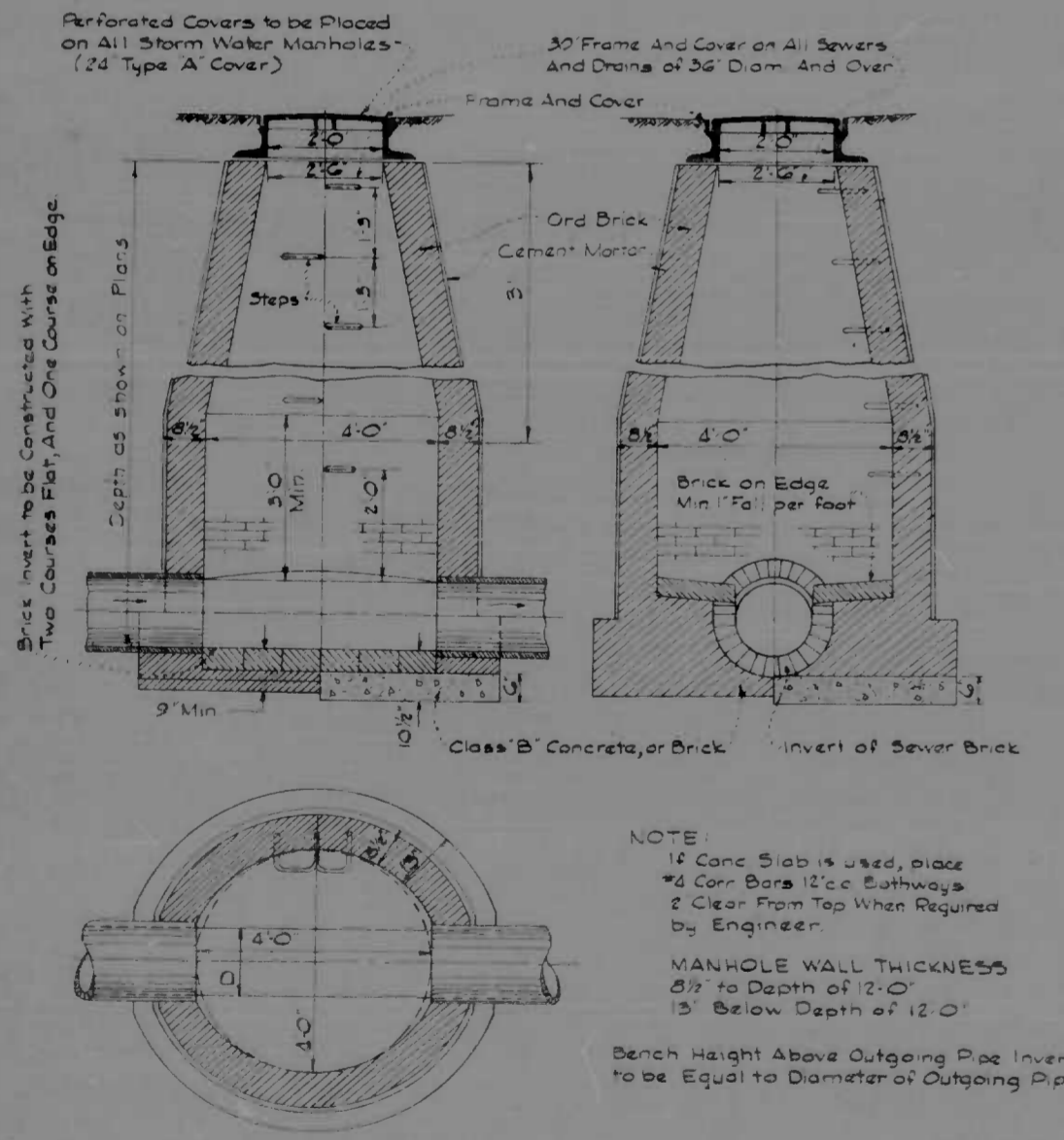
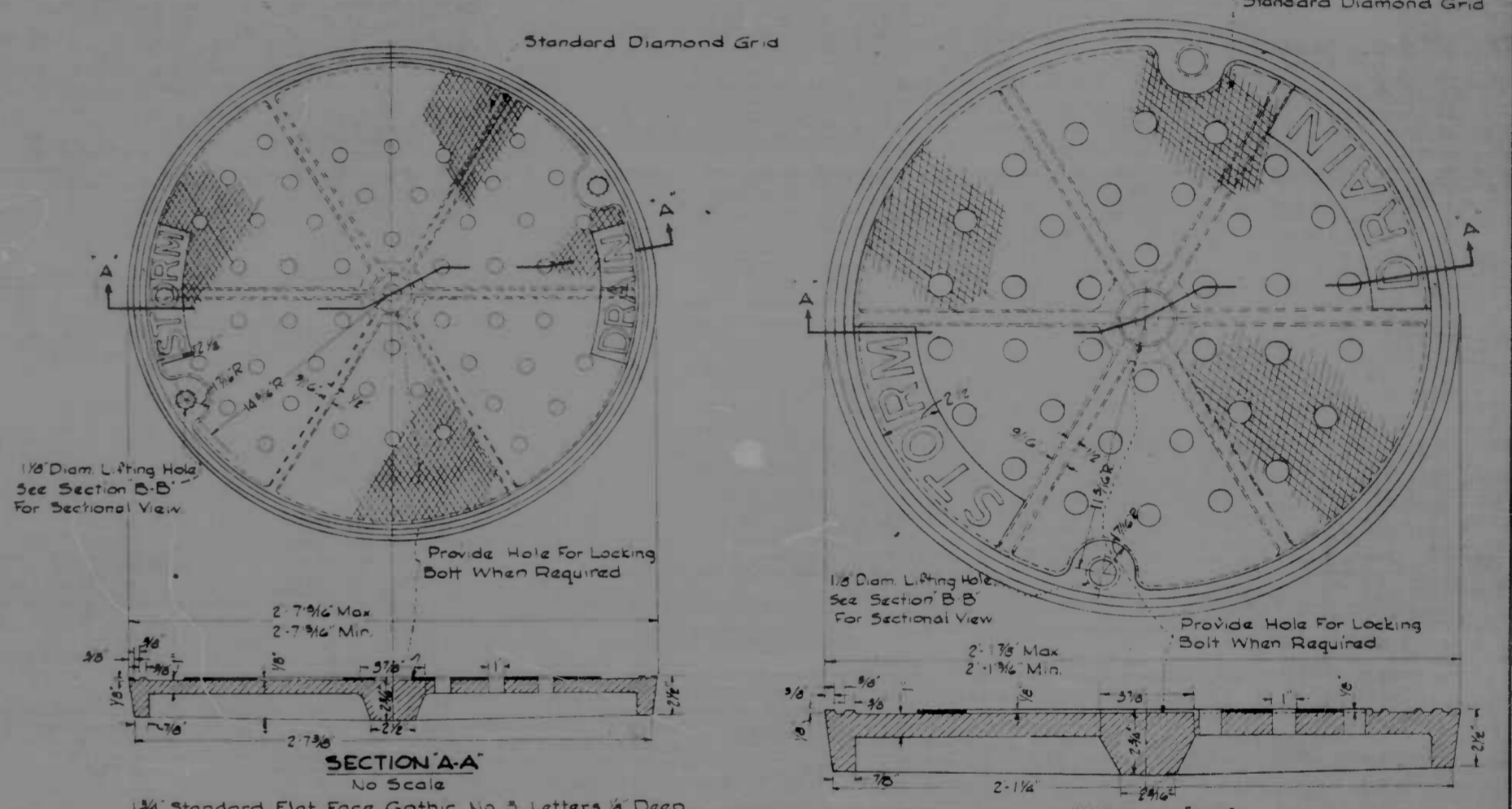
NOTE
1 REINFORCING SHOWN IS IN ADDITION TO CONVENTIONAL REINFORCING INDICATED ON THE CONTRACT DRAWINGS. CONVENTIONAL REINFORCING WILL BE CUT TO PROVIDE 2" CLEARANCE AROUND ENTERING PIPE.
2 PIPE TO BE CUT OFF FLUSH WITH INSIDE FACE OF EXTERIOR WALL.



REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1 ADDENDUM NO. 1 07-09-70	KROCKEL, BENTON, STONE & ASSOC., INC.	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY
2 REVISION-REINFORCEMENT FOR BOX CULVERT 12/24/70	MATZ, GIBBS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYL. MD 21202	SCALE: As Shown DATE: _____
		DRAWN BY: W.F.F. DES. BY: D.E.S. TRACED BY: J.D.M. CHK. BY: S.W.H. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		SHEET NO. 13 OF 45



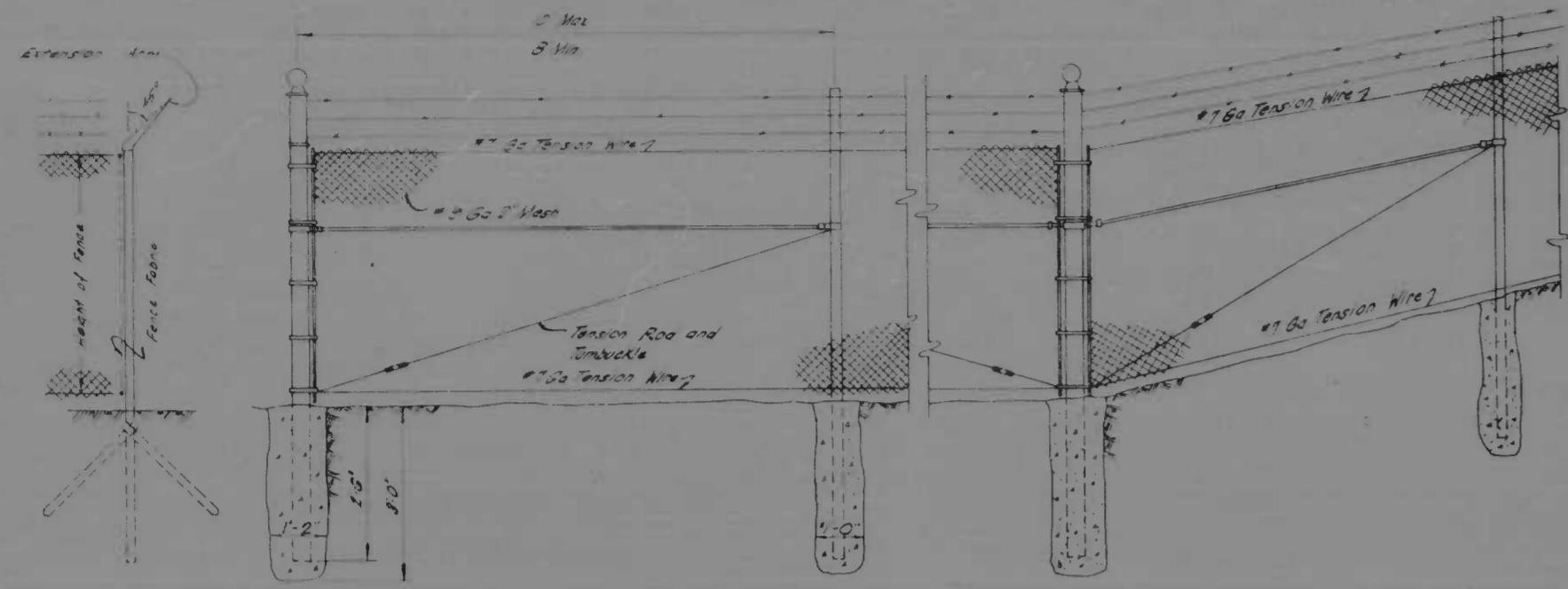
TYPICAL MANHOLE CHANNELS



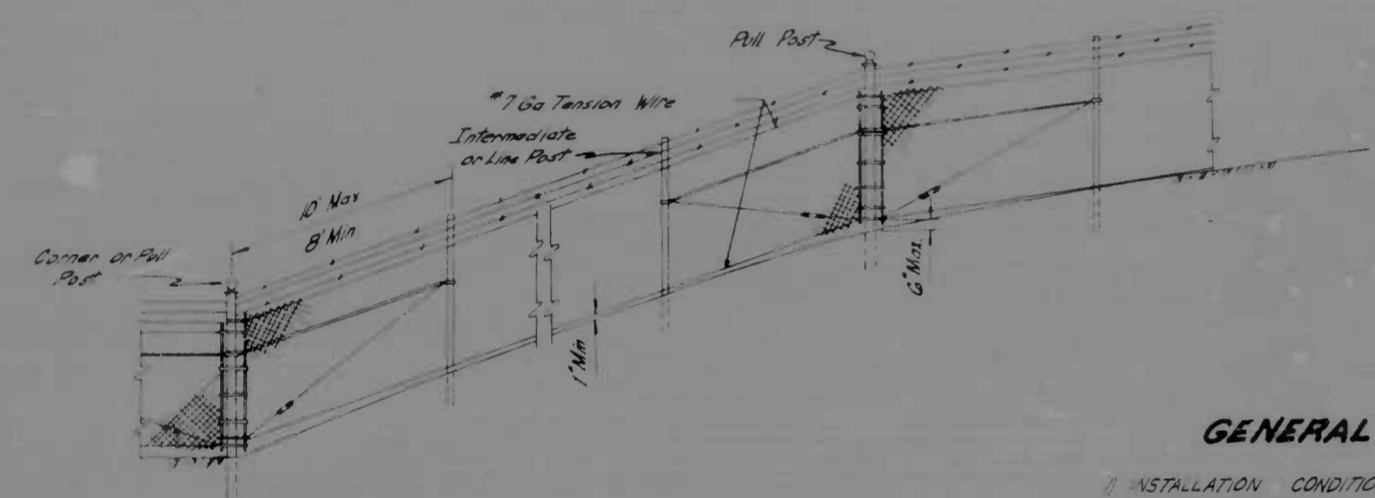
STANDARD STORM DRAIN DROP MANHOLE
Scale: 3/8" = 1'-0"

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNORLE, BENDER, STONE & ASSOC., INC. AND HATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: M.F.H. TRACED BY: M.F.H. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC-246-32-815 BALTO. CITY NO. 1983
		SCALE	DATE
			DES. BY: D.E.S. CHK. BY: W.F.F. SHEET NO. 14 of 45

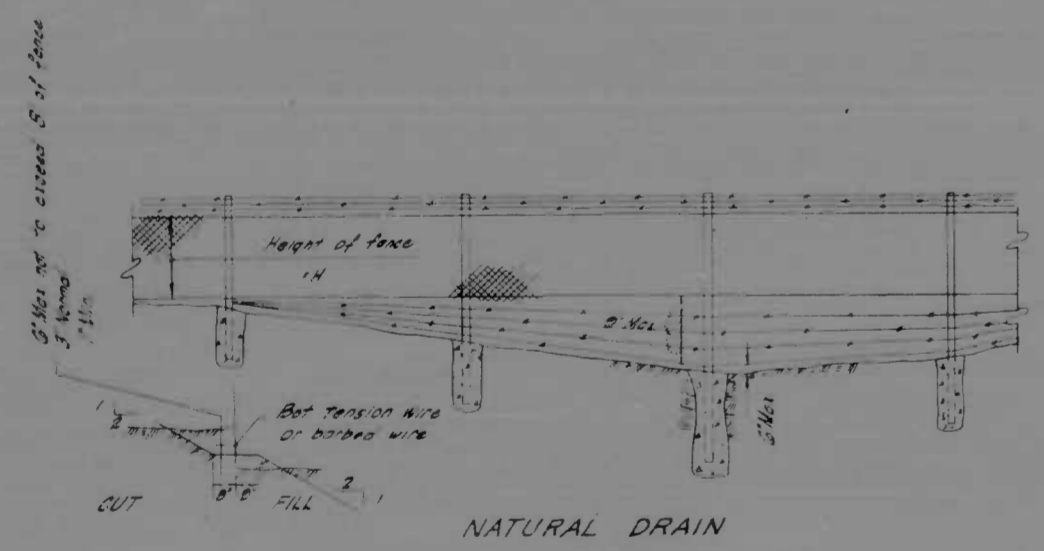
FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35)36	15	45



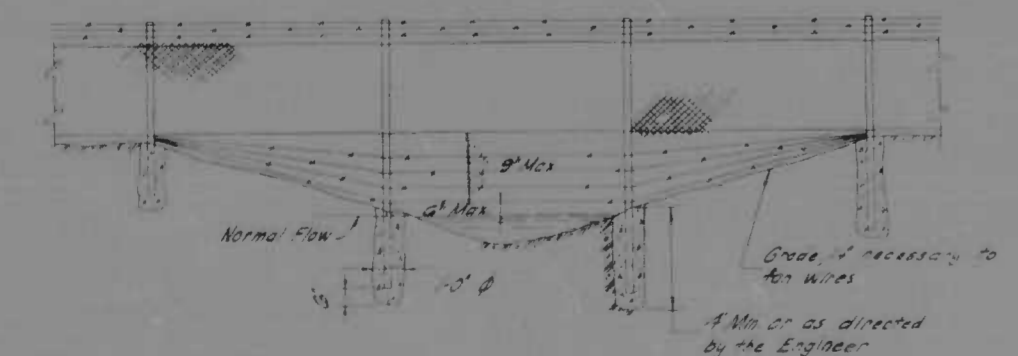
END POST
LINE POST
CORNER POST
DRIVEN BLADE ANCHOR



AT GRADE CHANGES



NATURAL DRAIN



DITCHES
CROSS OVER DRAINAGE OUTLETS

GENERAL NOTES

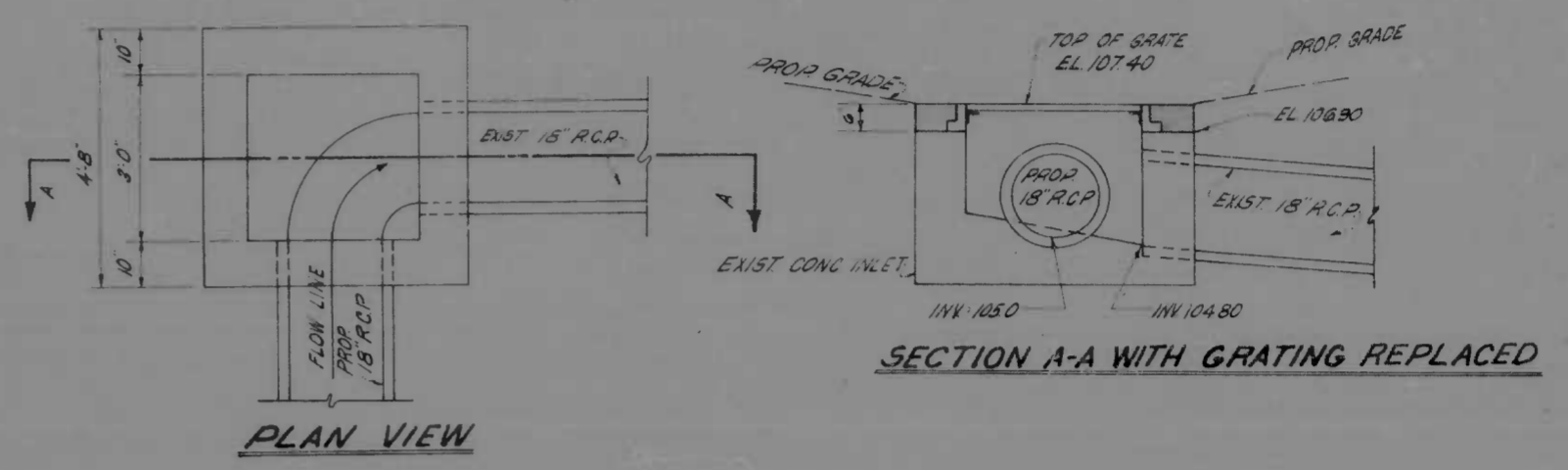
- 1) INSTALLATION CONDITIONS SHOWN ARE TYPICAL AND NOT TO BE CONSIDERED AS REPRESENTATIVE OF ALL CONDITIONS CONSTRUCTION WILL BE VARIED AS REQUIRED OR DIRECTED TO MEET FIELD CONDITIONS
- 2) LOCATE FENCE WHERE SHOWN USING NATURAL DRAIN CROSSING AS INDICATED ON THE DETAILS OF THIS SHEET.
- 3) AN ITEM HAS BEEN INCLUDED IN THE PROPOSAL FOR ADDITIONAL POST ASSEMBLIES WHICH MAY BE REQUIRED DUE TO THE INCREASED NUMBER OF HORIZONTAL BOUNDS FOR THE PROPOSED REALIGNMENT OF THE EXISTING FENCE SEE SPECIAL PROVISIONS
- 4) UNLESS OTHERWISE DIRECTED CONCRETE FOOTINGS SHALL BE PROVIDED FOR ALL LINES CORNER END AND PULL POSTS
- 5) LINE POSTS SUPPORTING FENCE AND BARBED WIRE ACROSS DRAINAGE OUTLETS AND SUBJECTED TO STREAM ACTION SHALL BE SET IN CONCRETE FOOTINGS AS SHOWN. POSTS OF ADDITIONAL LENGTH WILL BE RESET WHERE REQUIRED
- 6) DRIVEN BLADE ANCHORS MAY BE REUSED FOR LINE POSTS NOT SUPPORTING FENCE ACROSS DRAINAGE OUTLETS
- 7) LINE POSTS SHALL BE ON APPROACH EXPRESSWAY SIDE OF FENCE FABRIC. EXTENSION ARMS SUPPORTING BARBED WIRE ON NON-CLIMBABLE THE FENCE SHALL PROJECT TOWARDS THE APPROACH EXPRESSWAY TRANSITIONS OF EXTENSION ARMS SUPPORTING BARBED WIRE FROM 45° OFF VERTICAL TO VERTICAL SHALL BE MADE IN A DISTANCE OF 16 FT TO 20 FT ±
- 8) PULL POSTS SHALL BE INSTALLED AT SHARP BREAKS IN VERTICAL ALIGNMENT AND ON APPROX. 500' CENTERS IN STRAIGHT RUNS OF FENCE OR AS DIRECTED BY THE ENGINEER SEE DETAIL AT GRADE CHANGES
- 9) ALL END POSTS, PULL POSTS & CORNER POSTS SHALL BE BRACED AS SHOWN.

6" CHAIN LINK FENCE WITH BARBED WIRE
NO SCALE

NOTE
EXISTING INLET TO BE REGRANULIZED AND COST TO BE INCLUDED IN PRICE BID FOR ADJUSTING INLET.

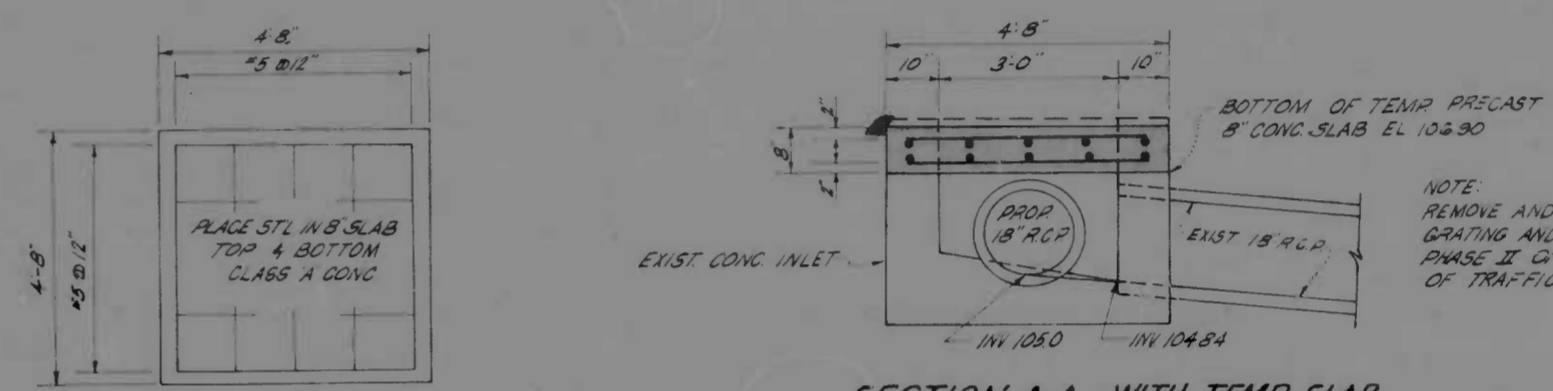
NOTE
WALLS TO BE RAISED AND EXISTING GRATING TO BE REPLACED. COST TO BE INCLUDED IN PRICE BID FOR ADJUSTING INLET.

NOTE
SLAB TO BE PRECAST AND SET IN PLACE AFTER INLET IS ADJUSTED



PLAN VIEW

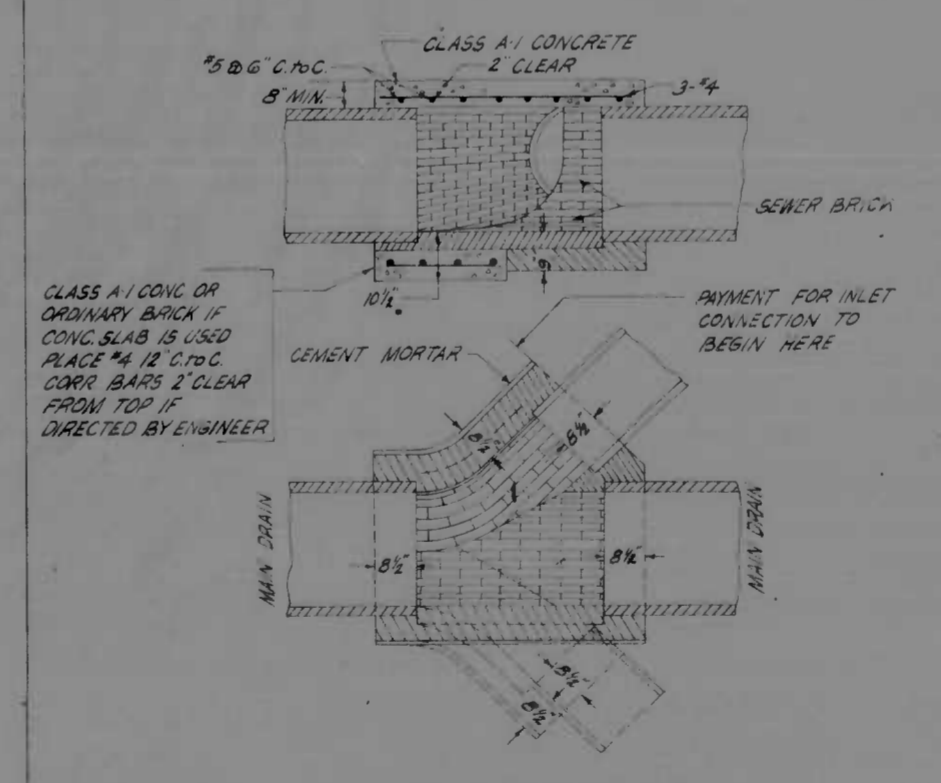
SECTION A-A WITH GRATING REPLACED



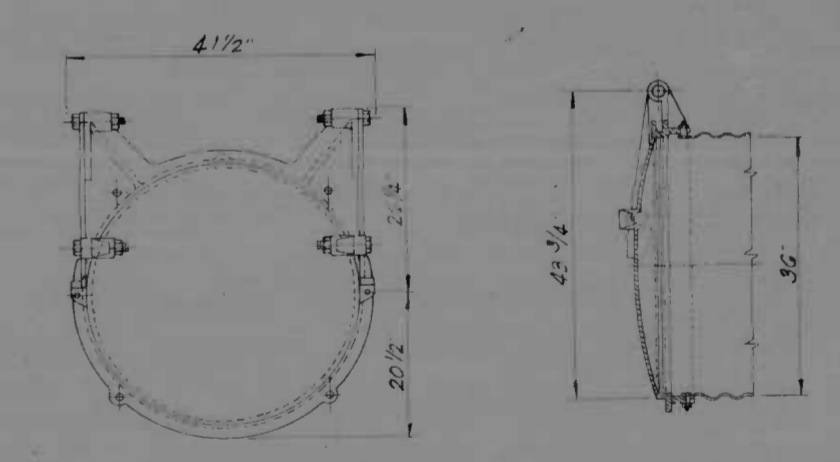
DETAIL OF TEMP CONCRETE SLAB FOR EXISTING 'K' INLET

SECTION A-A WITH TEMP SLAB

SCALE: 1/2" = 1'-0"
DETAIL OF ADJUSTED INLET
RT. STA 490+08 BASE LINE OF CONST. S.B.R.



PIPE CONNECTION TYPICAL BRICK 'Y' (SINGLE OR DOUBLE)
SCALE 1/2" = 1'-0"

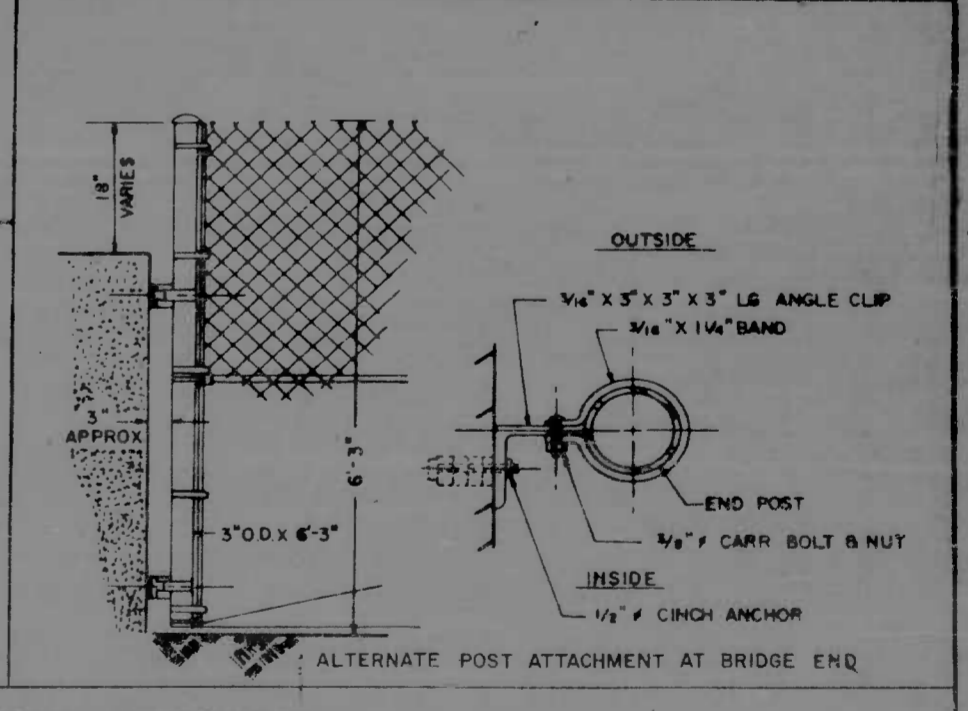
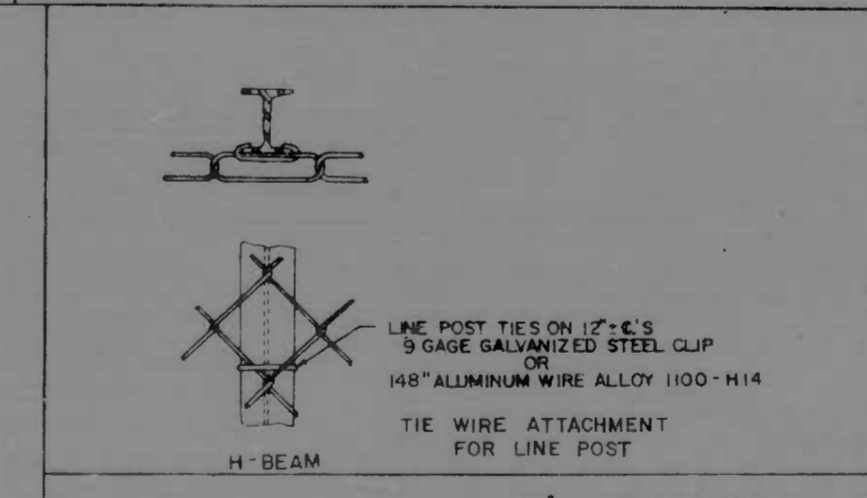
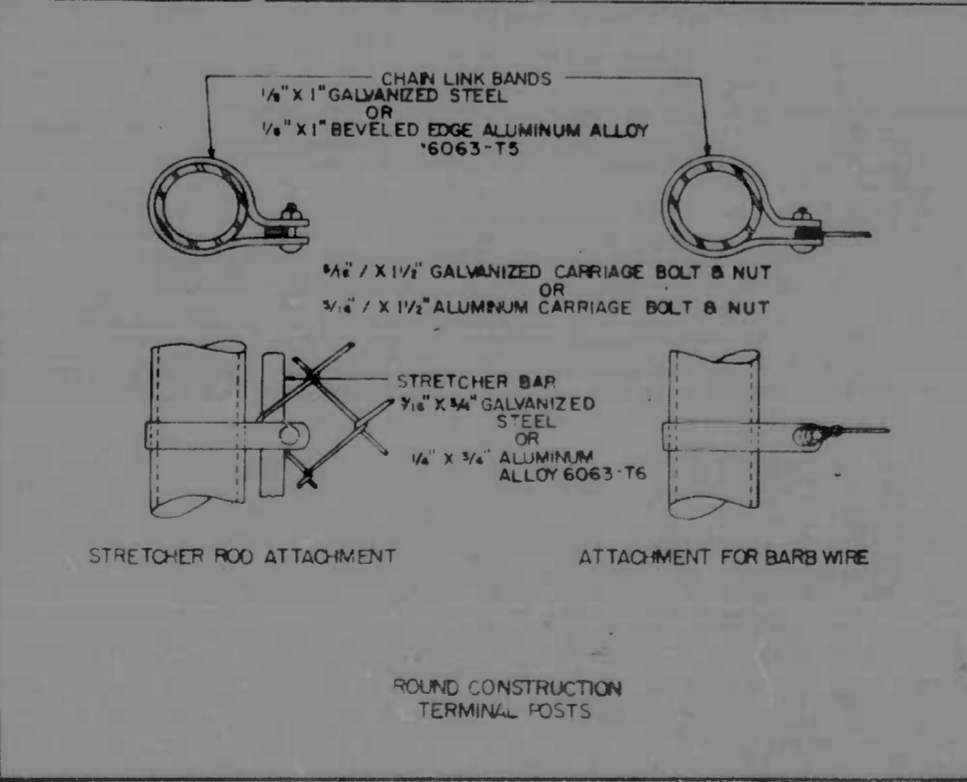
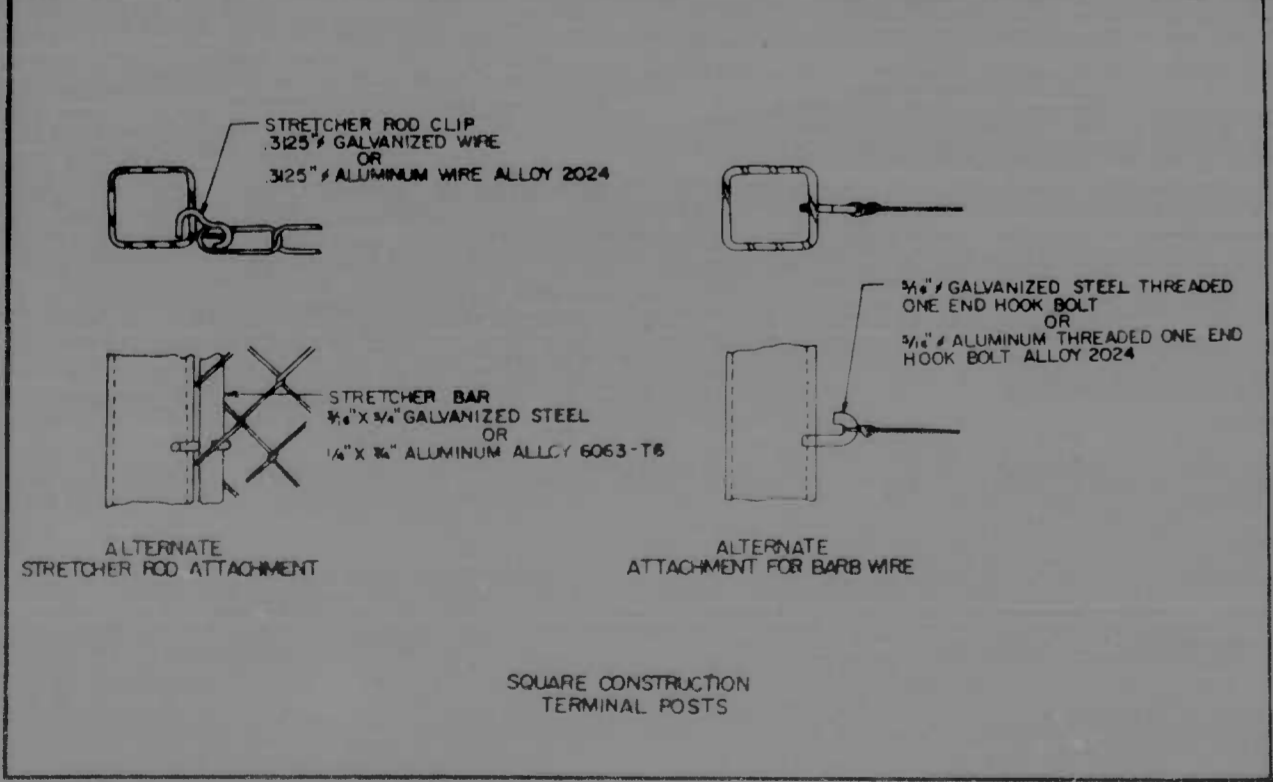
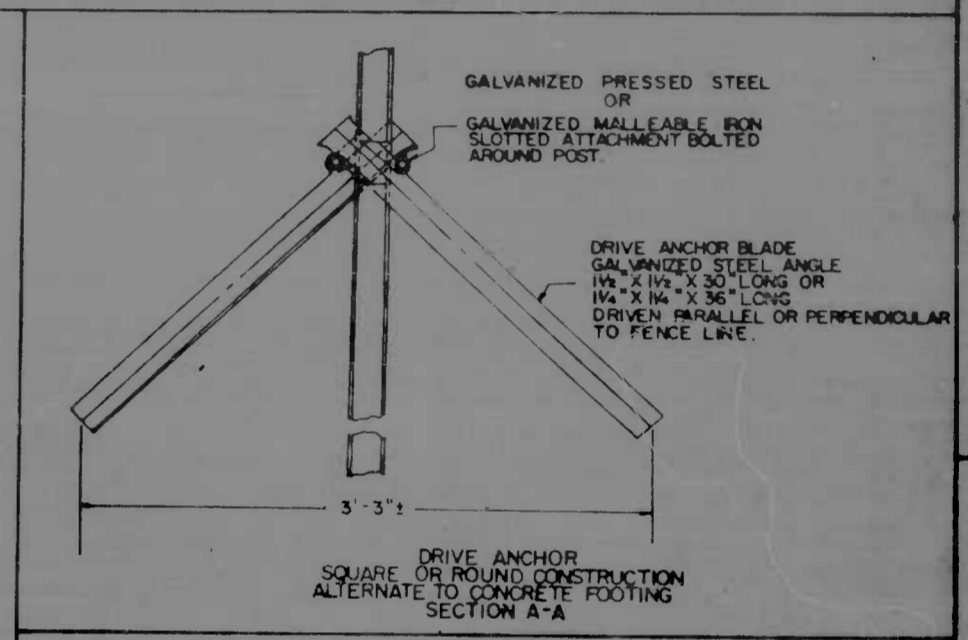
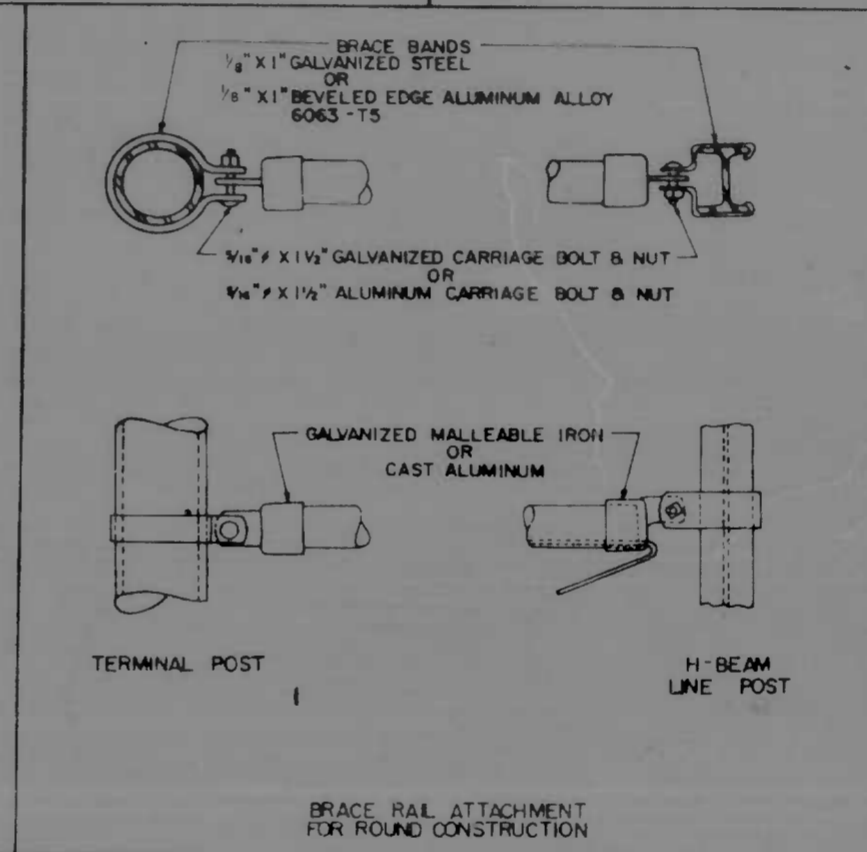
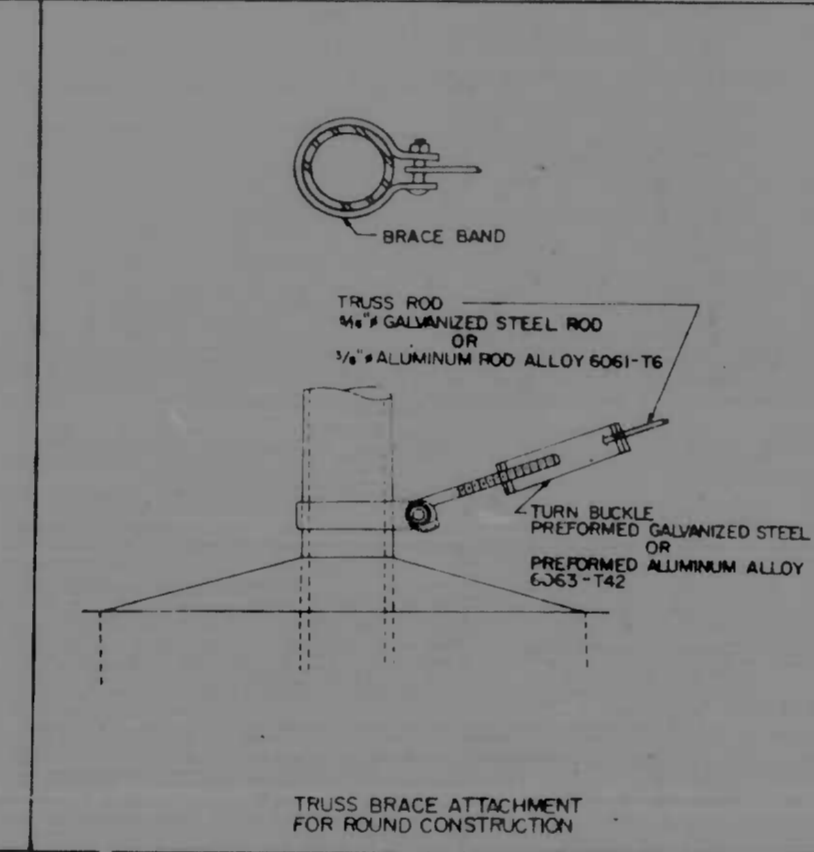
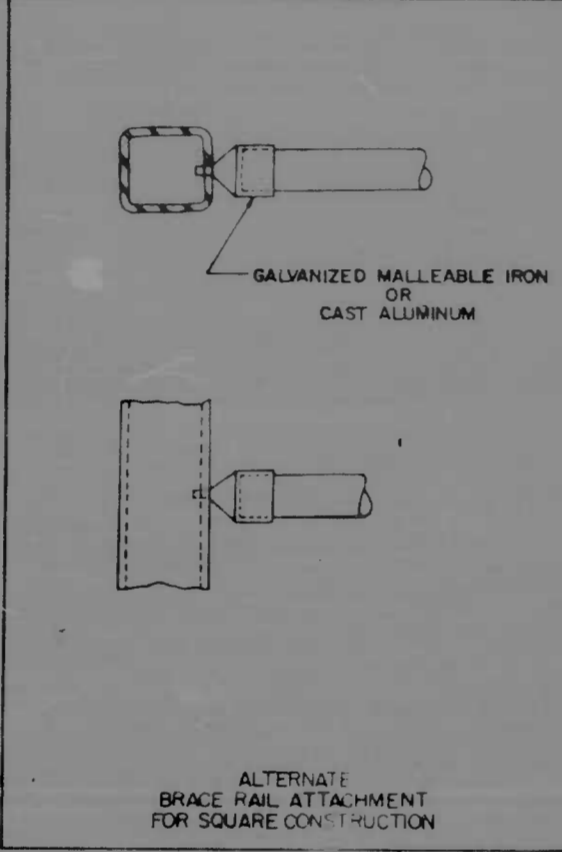
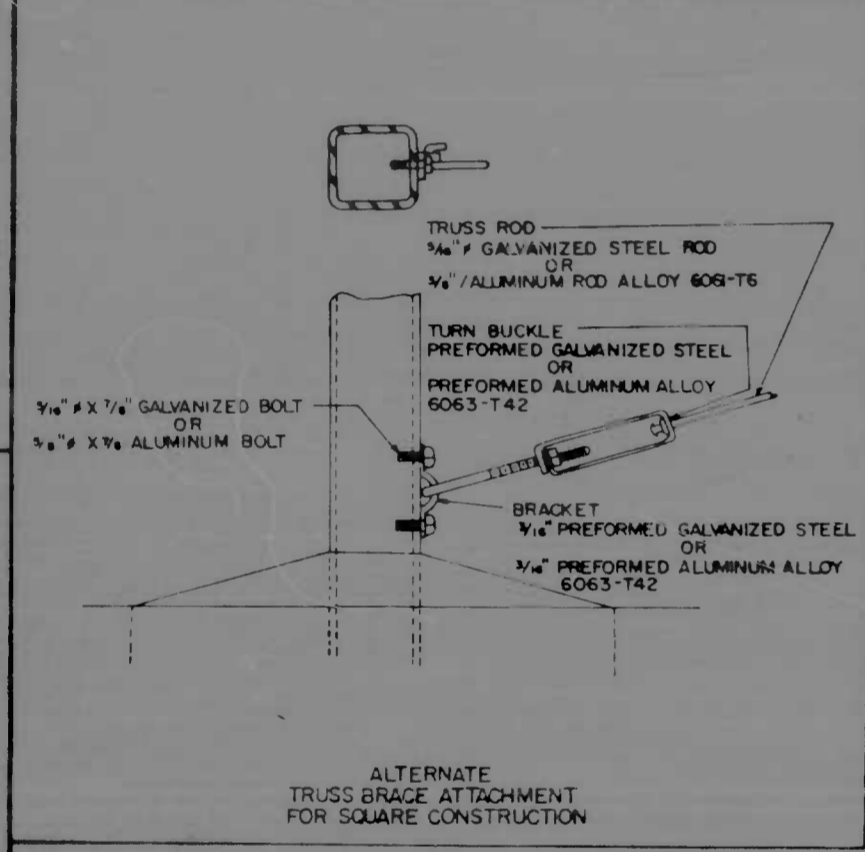
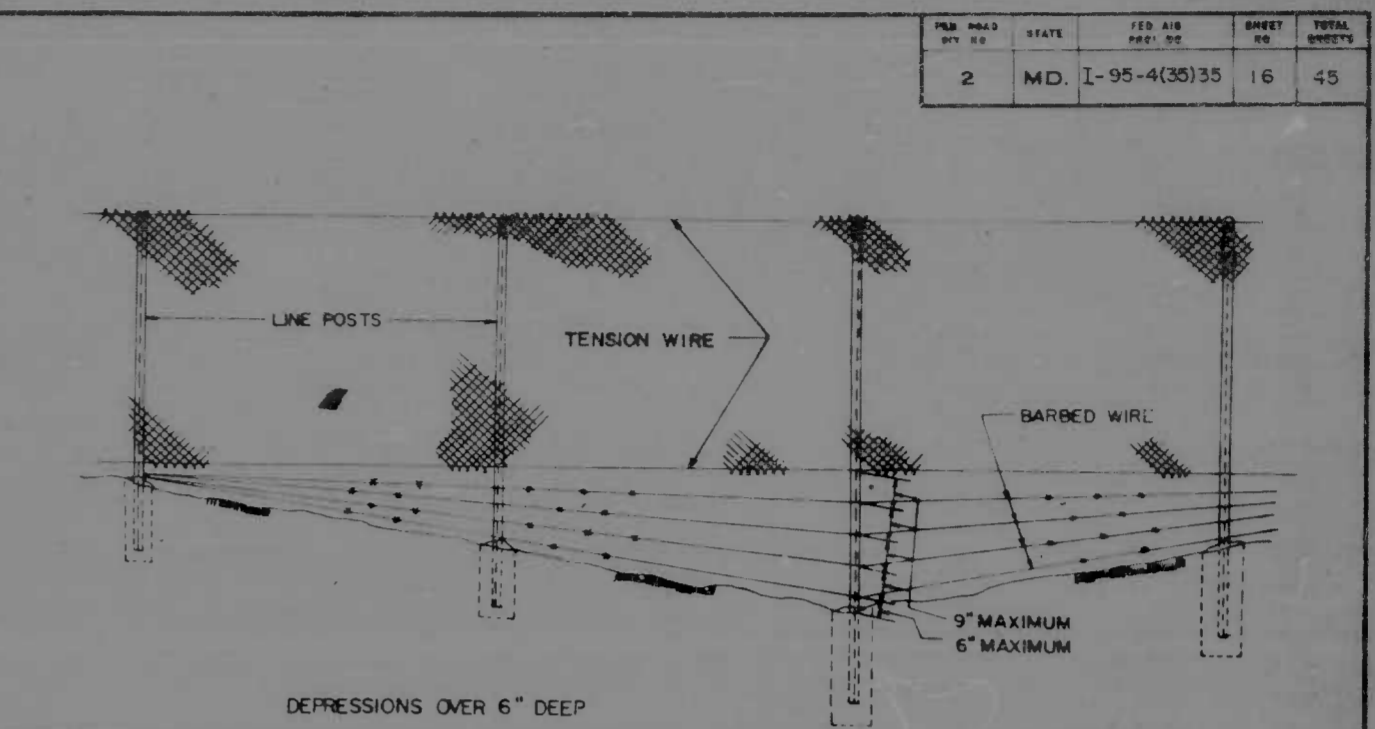
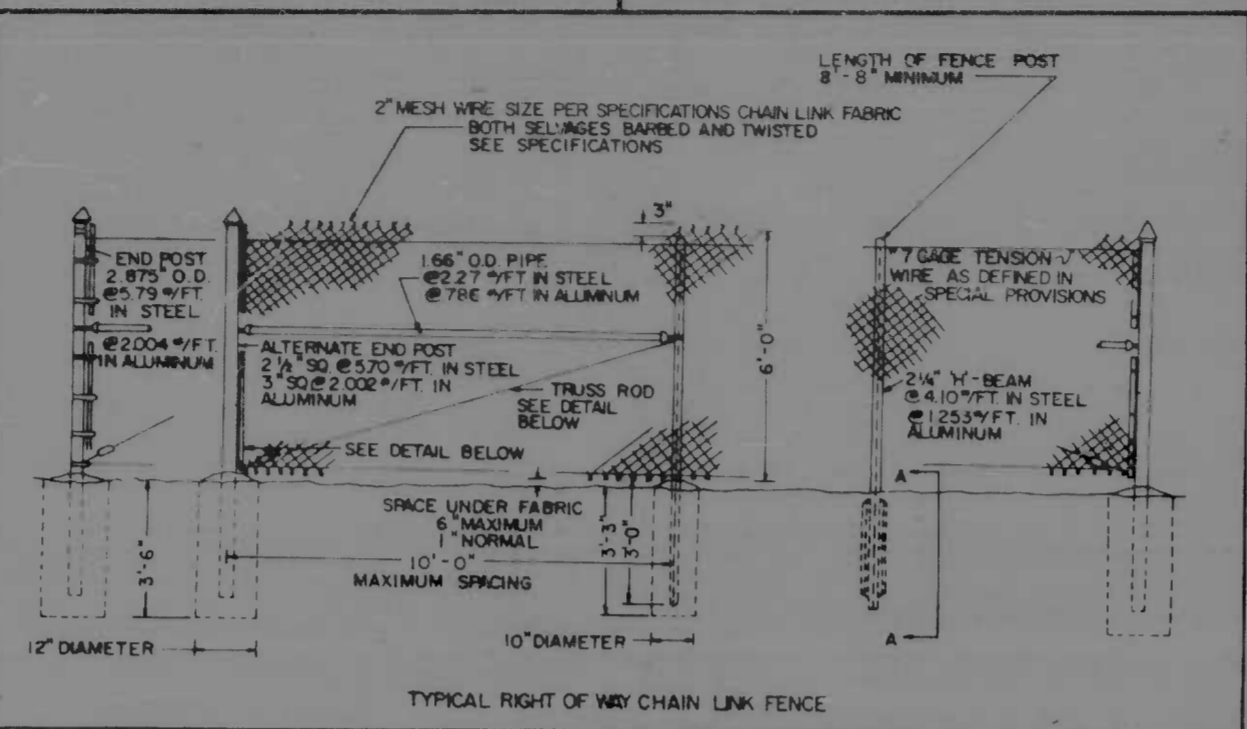
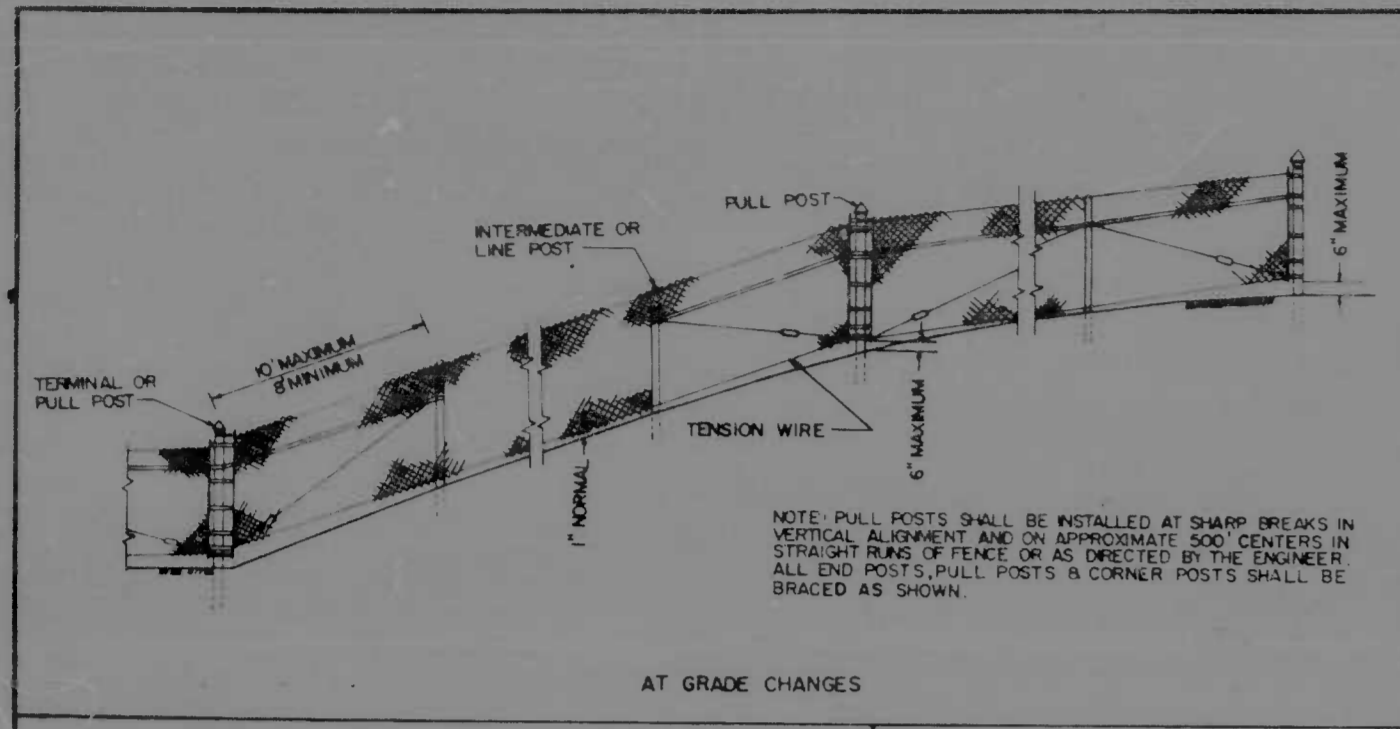


FLAP GATE FOR 36" BCCM PIPE

NO SCALE

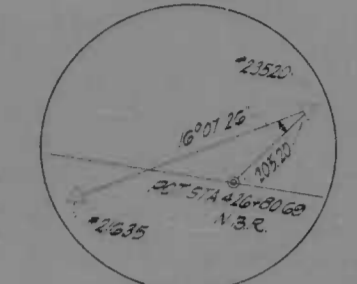
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	CONOILL, DENNER, STONE & ASSOC., INC. AND MATT, DENNER & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY J.E.L. TRACED BY J.E.L. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		SCALE: AS SHOWN	DES. BY J.L.C. CHK. BY B.W.G. SHEET NO. 15 of 45

PROJ. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(38)35	16	45



6' CHAIN LINK FENCE

CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
KNORRLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: S.R.C. DES. BY: S.R.C. TRACED BY: S.R.C. CHK. BY: S.R.C.
SCALE: AS SHOWN	DATE	F.A.P. NO. I-95-4(38)35 S.R.C. NO. BC 246 32 815 BALTO CITY NO. 1983
		SHEET NO. 16 OF 45



ST 426+80.69 NBR A I-95

2 1/4" = 1' ELEV. 17.4
 30M SPIRE SET AT BASE OF MAPLE
 TREE 30.2 FT E IN 2ND ST G25
 FROM CURB LINE PULASKI HWY

RIGHT OF WAY LINE OF THROUGH HIGHWAY
 TO BEGIN 20' LEFT SIDE STA 433+50 AND
 15' RIGHT SIDE STA 433+00

STA 434+50 LT
 1-STD INLET
 1-400 DEPTH
 22-1/2" RCP
 1-STD CONC. END SECTION
 15' CONC. 2 OUTLET DITCH
 TO 6' DEPTH

I-95-4(35)35
 BC 246-32-815
 1983

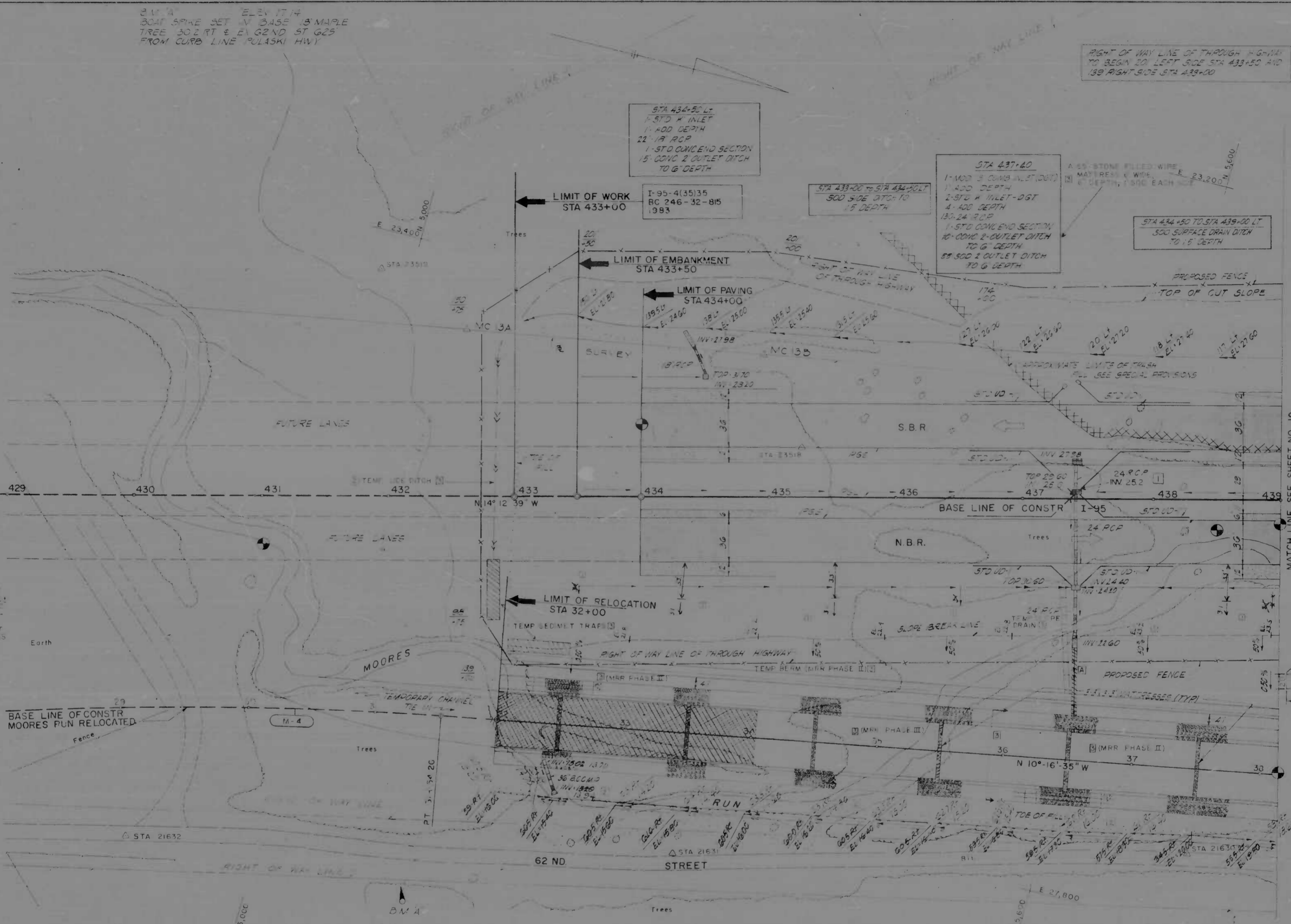
STA 437+40
 1-400 3' CONC. INLET (STD)
 1-400 DEPTH
 2-STD 6" TILET-DGT
 4-400 DEPTH
 130.24 RCP
 1-STD CONC. END SECTION
 10' CONC. 2-OUTLET DITCH
 TO 6' DEPTH
 58-500 2 OUTLET DITCH
 TO 6' DEPTH

STA 434+50 TO STA 439+00 LT
 500 SURFACE DRAIN DITCH
 TO 1.5' DEPTH

GENERAL NOTES (3)
 1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS, EDITION 1983, AS AMENDED BY THE CITY OF BALTIMORE, MARYLAND, AND THE STATE OF MARYLAND, AND THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS, EDITION 1983, AS AMENDED BY THE STATE OF MARYLAND, AND THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS, EDITION 1983, AS AMENDED BY THE STATE OF MARYLAND.
 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE APPROPRIATE AGENCIES.
 3. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.
 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES AND STRUCTURES.
 5. THE CONTRACTOR SHALL MAINTAIN ADEQUATE DRAINAGE THROUGHOUT THE PROJECT.
 6. THE CONTRACTOR SHALL MAINTAIN ADEQUATE EROSION CONTROL MEASURES.
 7. THE CONTRACTOR SHALL MAINTAIN ADEQUATE SAFETY MEASURES.
 8. THE CONTRACTOR SHALL MAINTAIN ADEQUATE RECORDS OF ALL WORK.
 9. THE CONTRACTOR SHALL MAINTAIN ADEQUATE COMMUNICATIONS.
 10. THE CONTRACTOR SHALL MAINTAIN ADEQUATE QUALITY CONTROL MEASURES.
 11. THE CONTRACTOR SHALL MAINTAIN ADEQUATE INSPECTION RECORDS.
 12. THE CONTRACTOR SHALL MAINTAIN ADEQUATE SAFETY RECORDS.
 13. THE CONTRACTOR SHALL MAINTAIN ADEQUATE ENVIRONMENTAL RECORDS.
 14. THE CONTRACTOR SHALL MAINTAIN ADEQUATE SOCIAL RECORDS.
 15. THE CONTRACTOR SHALL MAINTAIN ADEQUATE HISTORICAL RECORDS.
 16. THE CONTRACTOR SHALL MAINTAIN ADEQUATE CULTURAL RECORDS.
 17. THE CONTRACTOR SHALL MAINTAIN ADEQUATE ARCHAEOLOGICAL RECORDS.
 18. THE CONTRACTOR SHALL MAINTAIN ADEQUATE PALEONTOLOGICAL RECORDS.
 19. THE CONTRACTOR SHALL MAINTAIN ADEQUATE BOTANICAL RECORDS.
 20. THE CONTRACTOR SHALL MAINTAIN ADEQUATE ZOOLOGICAL RECORDS.
 21. THE CONTRACTOR SHALL MAINTAIN ADEQUATE GEOLOGICAL RECORDS.
 22. THE CONTRACTOR SHALL MAINTAIN ADEQUATE MINERALOGICAL RECORDS.
 23. THE CONTRACTOR SHALL MAINTAIN ADEQUATE METEOROLOGICAL RECORDS.
 24. THE CONTRACTOR SHALL MAINTAIN ADEQUATE CLIMATOLOGICAL RECORDS.
 25. THE CONTRACTOR SHALL MAINTAIN ADEQUATE ASTRONOMICAL RECORDS.
 26. THE CONTRACTOR SHALL MAINTAIN ADEQUATE PHYSICAL RECORDS.
 27. THE CONTRACTOR SHALL MAINTAIN ADEQUATE CHEMICAL RECORDS.
 28. THE CONTRACTOR SHALL MAINTAIN ADEQUATE BIOLOGICAL RECORDS.
 29. THE CONTRACTOR SHALL MAINTAIN ADEQUATE MEDICAL RECORDS.
 30. THE CONTRACTOR SHALL MAINTAIN ADEQUATE LEGAL RECORDS.
 31. THE CONTRACTOR SHALL MAINTAIN ADEQUATE FINANCIAL RECORDS.
 32. THE CONTRACTOR SHALL MAINTAIN ADEQUATE TAX RECORDS.
 33. THE CONTRACTOR SHALL MAINTAIN ADEQUATE EMPLOYMENT RECORDS.
 34. THE CONTRACTOR SHALL MAINTAIN ADEQUATE LABOR RECORDS.
 35. THE CONTRACTOR SHALL MAINTAIN ADEQUATE UNION RECORDS.
 36. THE CONTRACTOR SHALL MAINTAIN ADEQUATE GOVERNMENT RECORDS.
 37. THE CONTRACTOR SHALL MAINTAIN ADEQUATE MILITARY RECORDS.
 38. THE CONTRACTOR SHALL MAINTAIN ADEQUATE NAVAL RECORDS.
 39. THE CONTRACTOR SHALL MAINTAIN ADEQUATE AIR FORCE RECORDS.
 40. THE CONTRACTOR SHALL MAINTAIN ADEQUATE ARMY RECORDS.
 41. THE CONTRACTOR SHALL MAINTAIN ADEQUATE MARINE RECORDS.
 42. THE CONTRACTOR SHALL MAINTAIN ADEQUATE COAST GUARD RECORDS.
 43. THE CONTRACTOR SHALL MAINTAIN ADEQUATE NATIONAL GUARD RECORDS.
 44. THE CONTRACTOR SHALL MAINTAIN ADEQUATE STATE GUARD RECORDS.
 45. THE CONTRACTOR SHALL MAINTAIN ADEQUATE FEDERAL GUARD RECORDS.

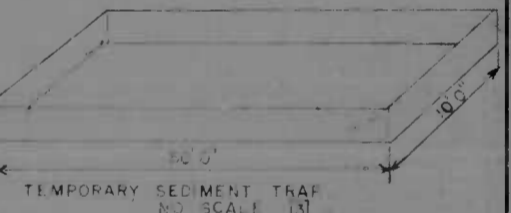
MOORES RUN RELOCATED (MRR)
 PHASE I: CONSTRUCT TEMP SEDIMENT TRAP (SEE DETAILS THIS SHEET)
 PHASE II: CONSTRUCT BOX CULVERT AND THOSE PORTIONS OF MOORES RUN (STA 434+00 TO STA 434+45) WHICH CAN BE CONSTRUCTED IN THE WY.
 PHASE III: NEXT DIVERT WATER AND CONSTRUCT THE REMAINING PORTIONS OF MOORES RUN RELOCATED EXCEPT PHASE V.
 PHASE IV: FINALLY CONSTRUCT FIVE BOX CULVERTS AND GULLIES (SEE SHEETS 17 AND 18).
 PHASE V: WHEN ALL OTHER CONSTRUCTION IS COMPLETED, CONSTRUCT MRR FROM STA 426+00 TO STA 434+00.
 TEMP SEDIMENT TRAP PHASE I LOCATION AND CAPACITY ARE TO BE DETERMINED BY FIELD CONDITIONS BUT SHOULD BE AS FOLLOWS:
 LOCATION: AS SHOWN
 DIMENSIONS: LENGTH 20', WIDTH 4', DEPTH 3'
 CAPACITY: 1000 cu. yds. (APPROX.) (3)

M-4
 CONSTR
 MOORES RUN
 RELOCATED
 Δ = 10°-20'-48"
 D = 02°-07'-19"
 R = 2700.00'
 T = 244.46'
 L = 487.58'



MATCH LINE SEE SHEET NO. 19

STA 434+00 UNDERPASS
 NBR 435.92 436+00 TO 437+00 6" RCP
 OUTLET LOCATIONS AS SHOWN ON PLANS



CLASS 2 EXCAVATION
 MOORES RUN RELOCATED
 STA 32+00 TO STA 32+30 2' TO 6"
 STA 34+00 TO STA 34+20 2' TO 6"

CROSS SECTION FOR STONE FILLED
 WIRE MESH WITH
 NO SCALE (3)

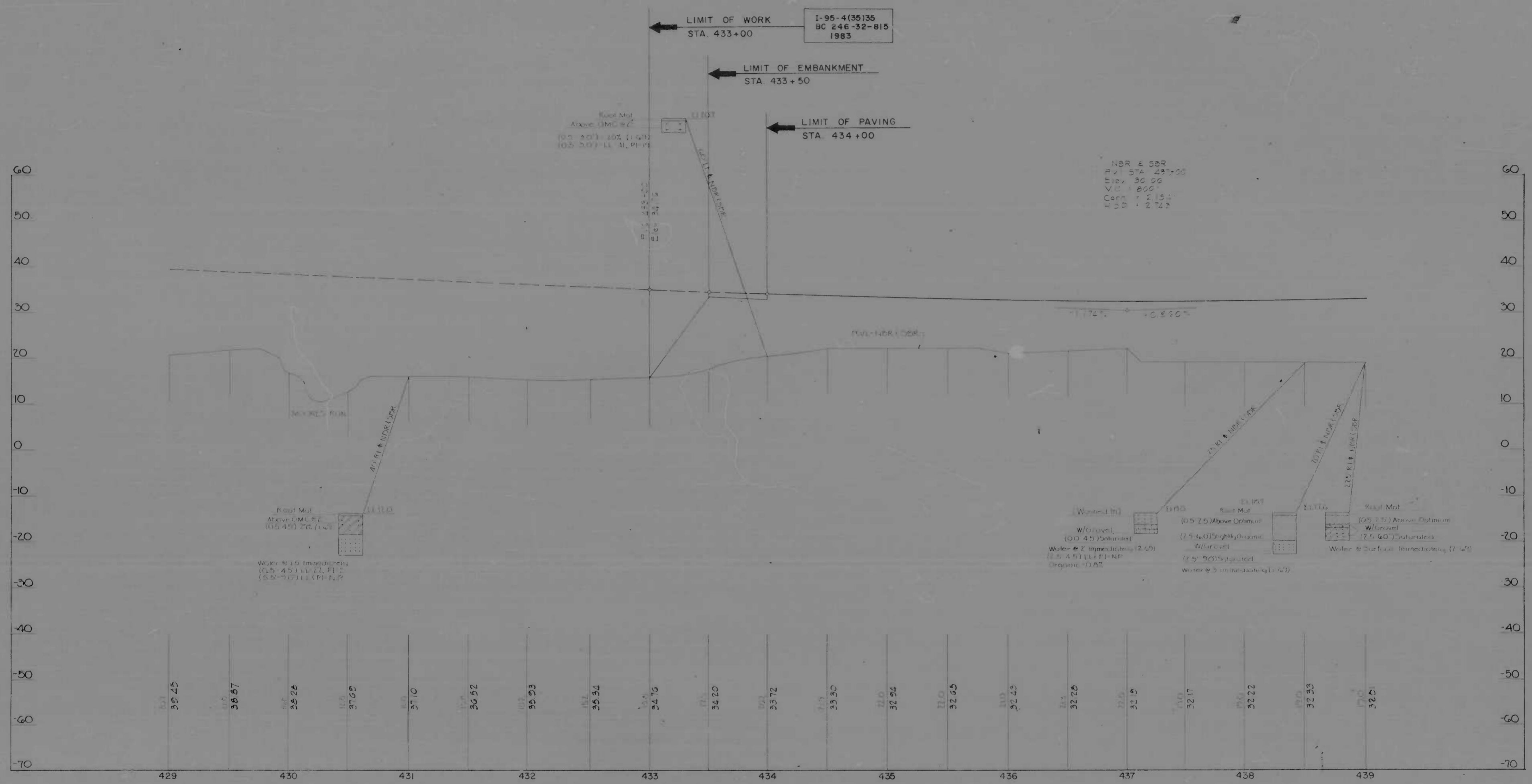
CONTINUOUSLY REINFORCED
 CONCRETE PAVEMENT
 CONVENTIONAL REINFORCED
 CONCRETE PAVEMENT

REFERENCES
 For Profile See Sheet No. 18
 For Typical Section See Sheet No. 2
 For Phase I - MOORES RUN RELOCATED See Sheet No. 21
 For Phase II - MOORES RUN RELOCATED See Sheet No. 21
 For Drainage Details See Sheet No. 18

NOTE
 TEMP CHANNEL IS TO BE
 EXCAVATED AND EXISTING MOORES
 RUN CHANNEL TO BE FILLED AND
 GRADED AS DIRECTED BY THE ENGINEER

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1) ADDENDUM NO. 1 07-09-70 REVISION INDICATE TOP OF FILL AND WIRE MESH EXCAVATION 12/24/70	EMPHRE, BEARD, STONE & ASSOC., INC. AND MATT, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 541 N CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F KENNEDY MEMORIAL HIGHWAY	DRAWN BY: J.E.L. TRACED BY: J.E.L. F.A.P. NO. 1-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
2) 6-7 REVISION: DRAINAGE STA 32+00 TO 33-00 RCP		DATE	CHK. BY: R.W.C. SHEET NO. 17 OF 45

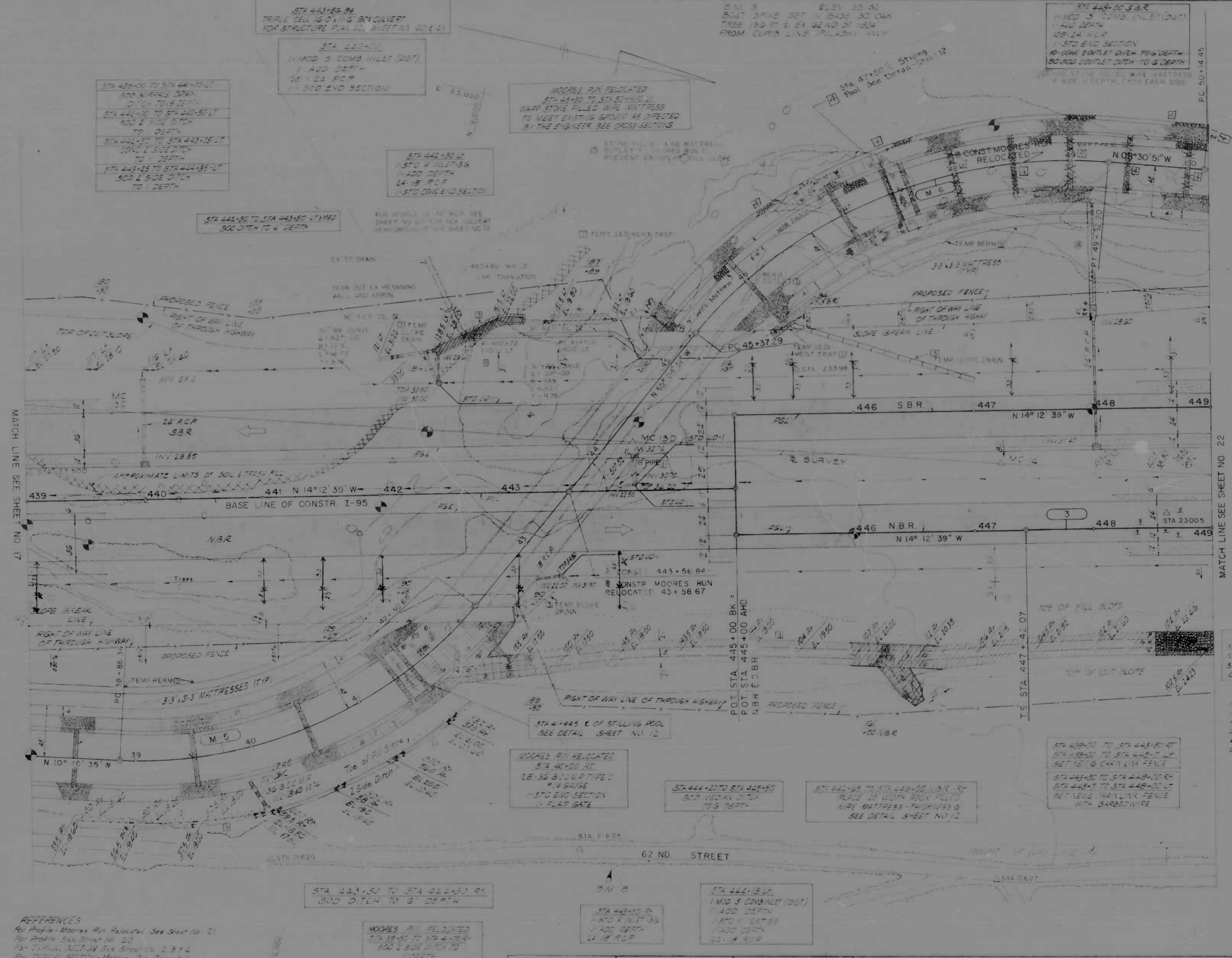
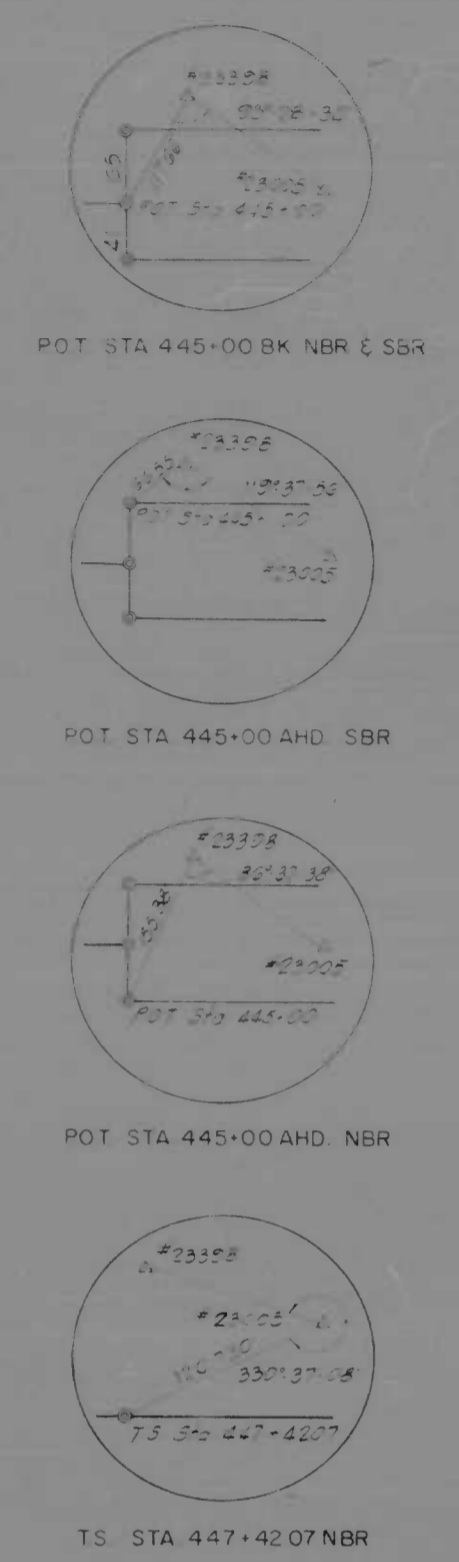
FED. ROAD DIV. NO.	STATE	FISC. YEAR	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	18	45



NORTHBOUND ROADWAY AND SOUTHBOUND ROADWAY STA. 433+00 TO STA. 439+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KIMBLE, BENDER, STONE & ASSOC., INC. 440 MATZ, CHING & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
	SCALE: Hor. 1"=40', Vert. 1"=10'	DATE	DRAWN BY: J.R.W. CHECKED BY: J.R.W. F.A.P. NO.: I-95-4(35)35 S.P.C. NO.: BC 246-32-815 BALTO. CITY NO. 1983

NO.	DATE	BY	CHK.
2	MD I-95-4(15)35	19	45



3
E CONSTR NBR

CURVE DATA

Δ	51°45'08"
Ts	943.12'
Δc	36°45'08"
Dc	3°45'00"
R	1527.89'
T	507.55'
L	980.06'
Os	7°30'00"
Ls	400.00'
LT	266.91'
ST	133.55'

M-5
B CONSTR
MOORE'S RUN
RELOCATED

Δ = 54°48'16"
D = 15°54'56"
R = 360.00'
T = 186.82'
L = 344.35'

M-5
C CONSTR
MOORE'S RUN
RELOCATED

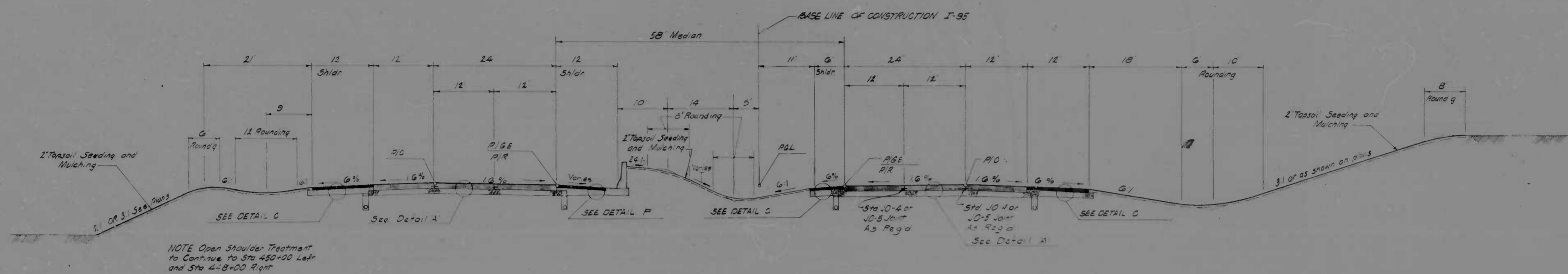
Δ = 56°34'00"
D = 14°19'26"
R = 400.00'
T = 215.23'
L = 394.91'

REFERENCES
For Profile - Moore's Run Relocated See Sheet No. 21
For Profile - See Sheet No. 22
For Typical Section - See Sheet No. 23 & 4
For Typical Section - Moore's Run Relocated See Sheet No. 12
For Drainage Details See Sheet No. 11, 2 & 3

NO.	REVISIONS	CONSULTANT
1	ADDITIONAL NGI 07-09-70	EMWELL, RENDLER, STONE & ASSOC., INC. AND NATZ, CHARLES & ASSOC., INC. CONSULTING ENGINEERS 141 N. CALVERT STREET BALTIMORE, MARYLAND 21202
2	REVISIONS TO NGI 07-09-70	

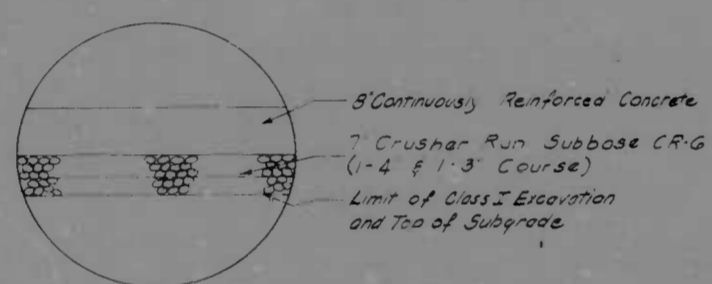
CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: J.E.L. TRACED BY: J.E.L.	DES. BY: J.E.L. CHK. BY: R.W.C.
SCALE: 1"=40'	DATE:	F.P. NO. I-95-4(15)35 S.P.C. NO. DC 286-12-B15 BALTO. CITY NO. 1983	SHEET NO. 19 of 45

SHEET NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4 (35)35	2	45

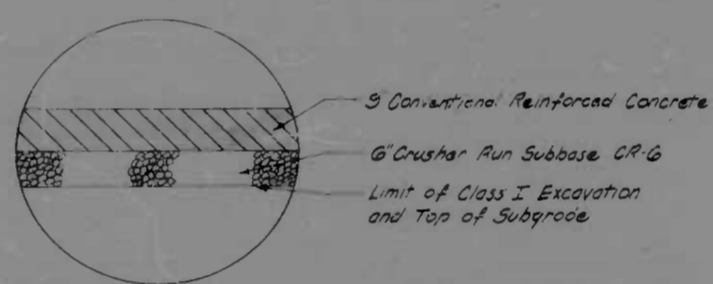


INTERSTATE ROUTE 95
STA 434+00 TO STA 445+00

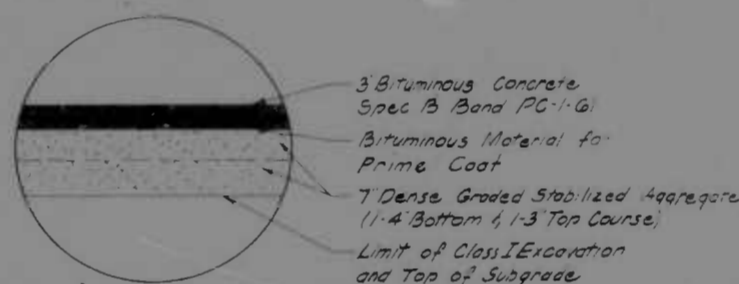
SCALE 1" = 1'-0"



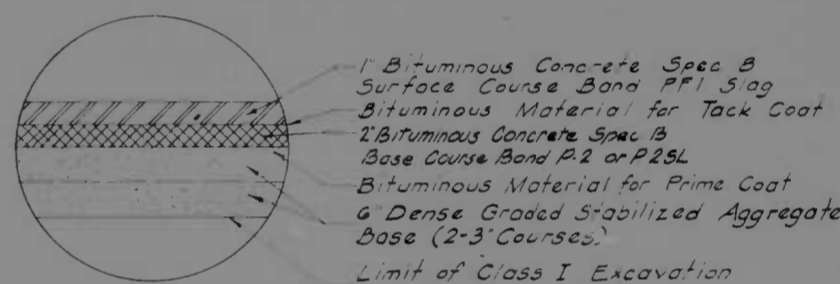
DETAIL-A



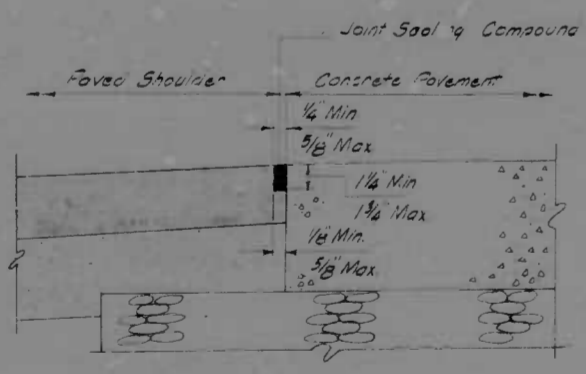
DETAIL-B



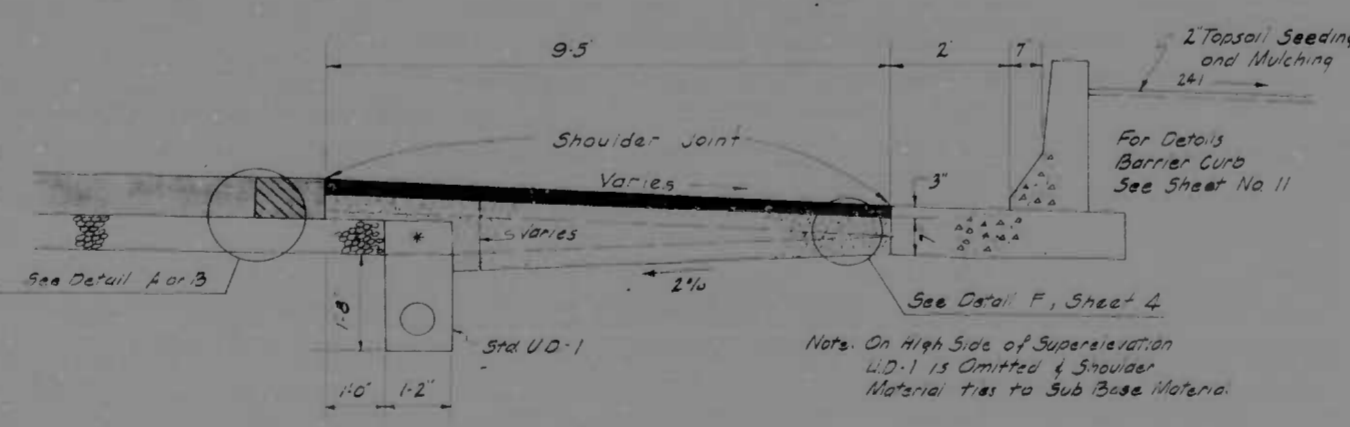
DETAIL-C



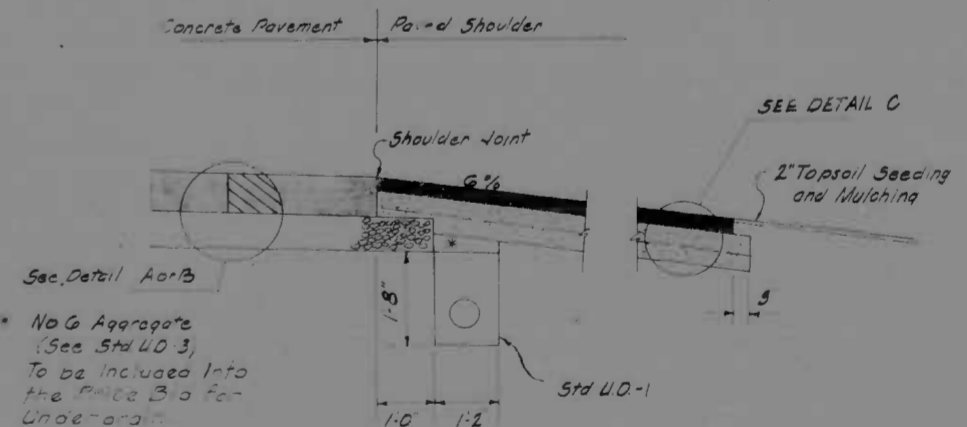
DETAIL-D



SHOULDER JOINT DETAIL
 NO SCALE

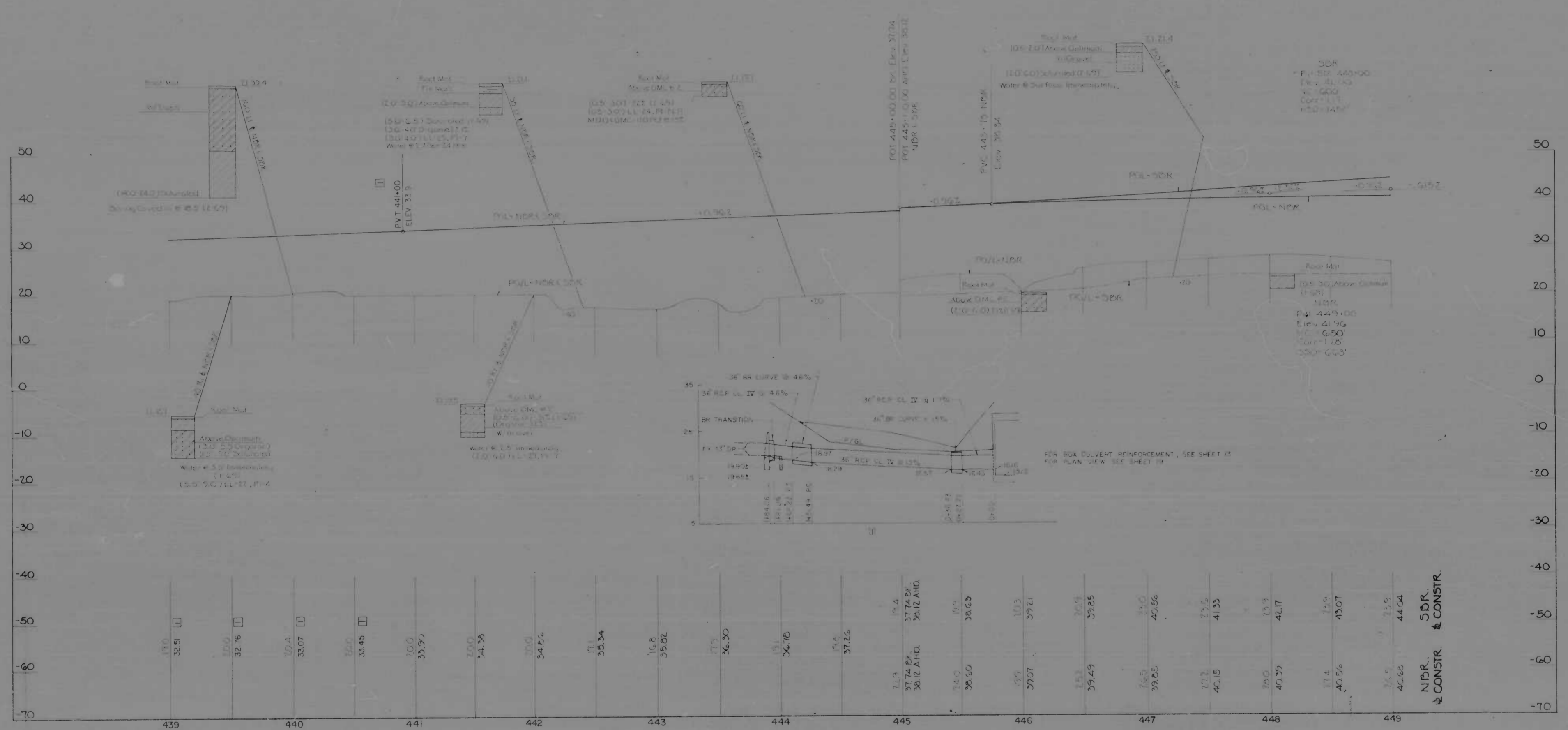


SHOULDER TREATMENT WITH BARRIER CURB
 SCALE 1" = 2'-0"



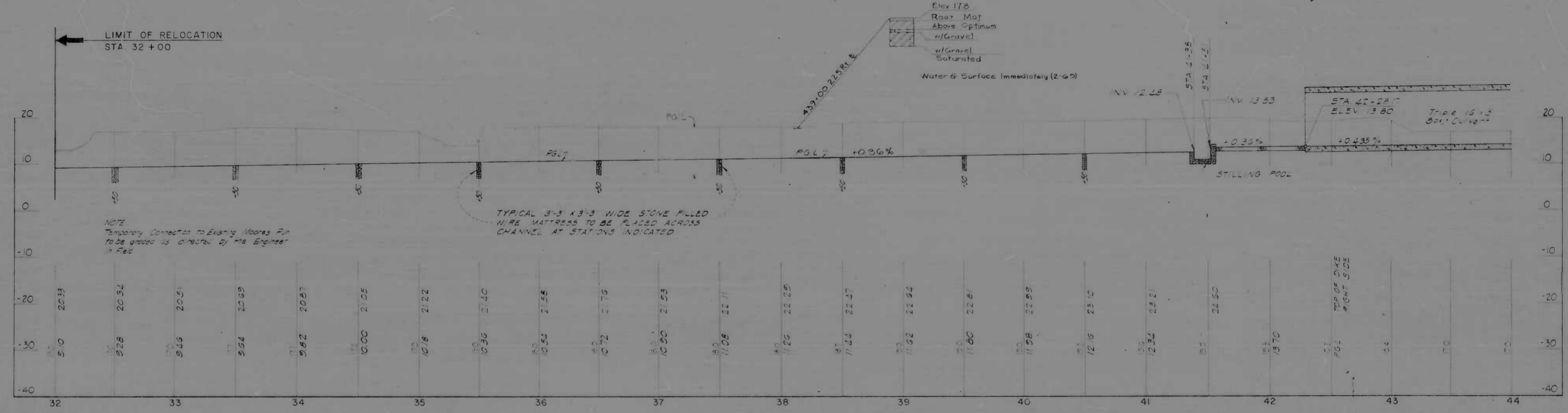
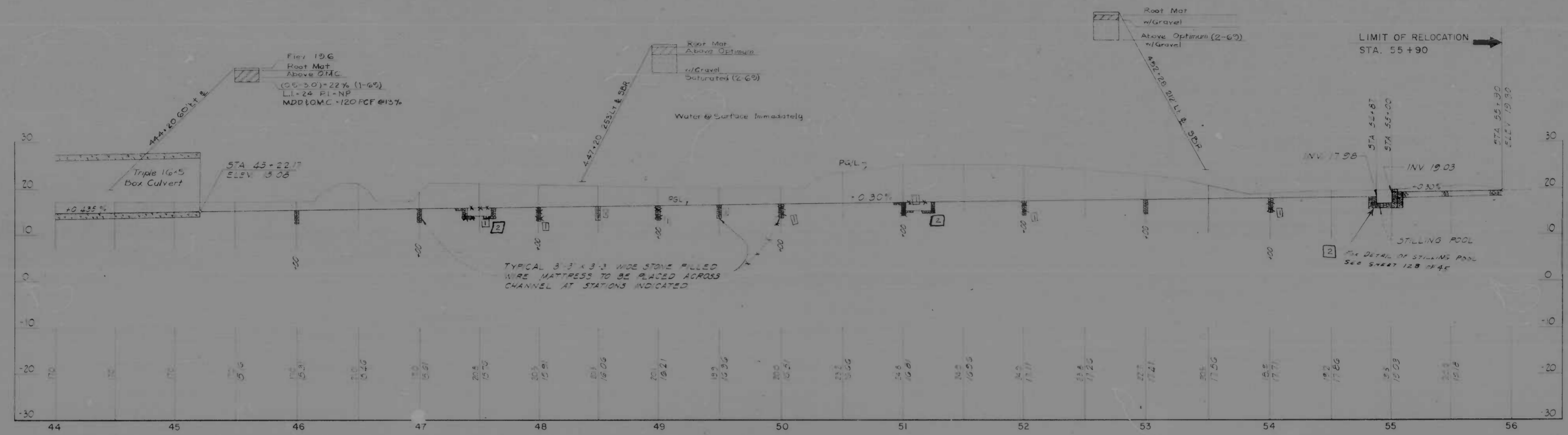
SHOULDER TREATMENT WITHOUT BARRIER CURB
 SCALE 1" = 2'-0"

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNORRLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: P.W.Z. TRACED BY: P.W.Z. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
			DES. BY: J.L.C. CHK. BY: R.W.C. SHEET NO. 2 OF 45



REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. ADDENDUM NO. 1 07-09-70 PROVIDE 36" PEE INTO BOX CULVERT 12/29/70	KIMBLE, BENDER, STONE & ASSOC., INC. AND MATZ, CARDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: J.R.W. TRACED BY: J.R.W. DES. BY: J.L.C. CHK. BY: R.W.C.
		SCALE: Hor. 1"=40', Vert. 1"=10'	SHEET NO. 20 OF 45

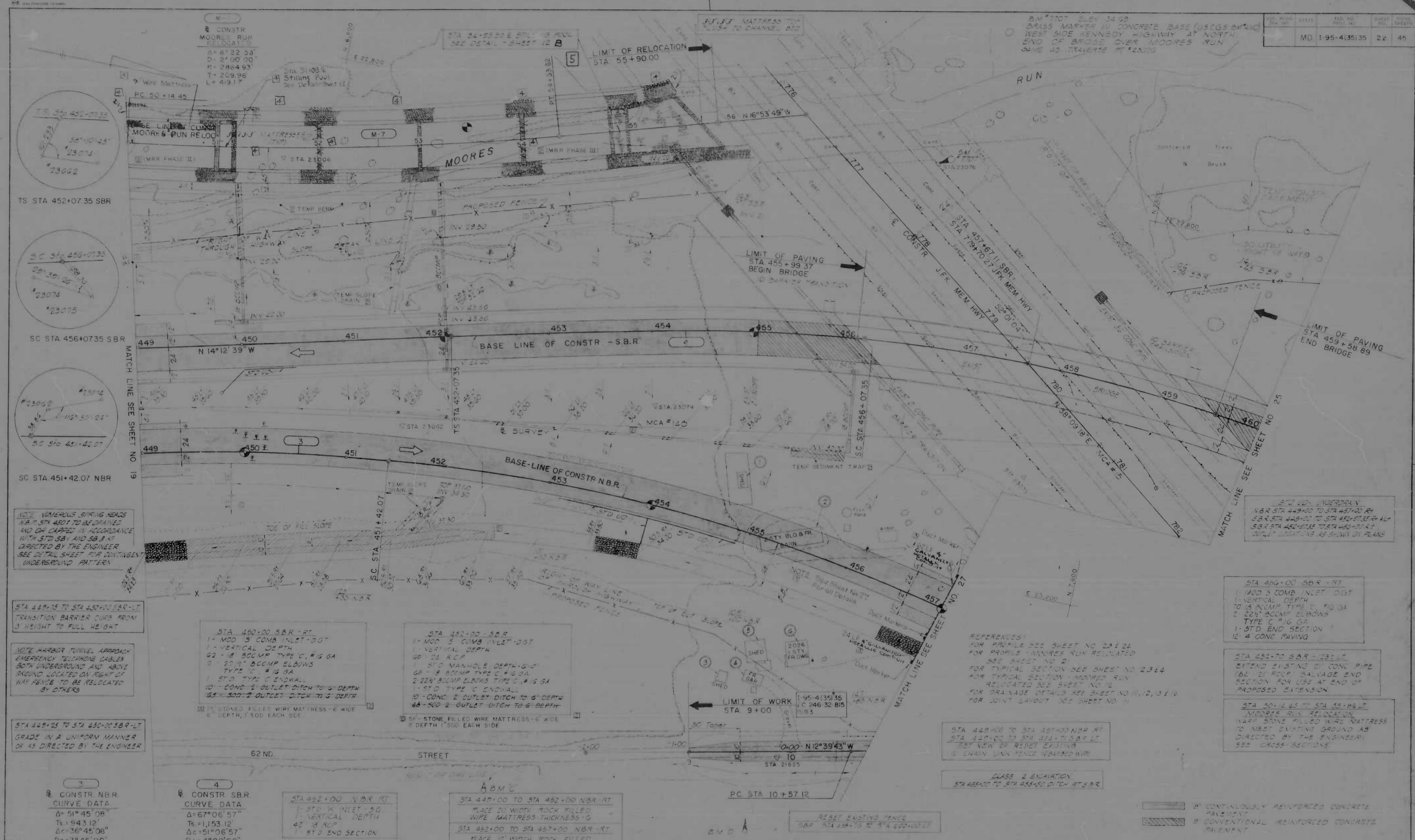
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4 (35)35	21	45



MOORES RUN RELOCATED STA. 32+00 TO STA. 55+90

REVISIONS 11 Revised Wire Basket 9/27/83 12 Revised Stilling Pool	CONSULTANT EMPRIE, HENDER, STONE & ASSOC., INC. AND MATZ, CHURCH & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
		DRAWN BY: J.E.L. CHECKED BY: J.E.L. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-22-815 BALTO. CITY NO. 1983	DESIGNED BY: J.L.C. CHECKED BY: R.W.C. SHEET NO. 21 of 45

NO.	DATE	BY	REVISION
1	MD 1-95-4135135	22	45



NOTE: NUMEROUS SPRING HEADS N.B.R. STA 450+ TO BE DEFINED AND CHAINED IN ACCORDANCE WITH STA 50+ AND S.B.R. AS DIRECTED BY THE ENGINEER. SEE DETAIL SHEET FOR CONTINGENT UNDERGROUND PATTERN.

STA 449+25 TO STA 450+00 S.B.R.-LT TRANSITION BARRIER CURB FROM 3' HEIGHT TO FULL HEIGHT.

NOTE: HARBOR TUNNEL APPROACH EMERGENCY TELEPHONE CABLES BOTH UNDERGROUND AND ABOVE GROUND LOCATED ON RIGHT OF WAY FENCE TO BE RELOCATED BY OTHERS.

STA 449+25 TO STA 450+00 S.B.R.-LT GRADE IN A UNIFORM MANNER OR AS DIRECTED BY THE ENGINEER.

STA 450+00 S.B.R.-RT
 1- MOD 5 COMB INLET-DGT
 1- VERTICAL DEPTH
 02- 8" OCCMP TYPE C, #16 GA
 2- 20" 2" OCCMP ELBOWS TYPE C, #16 GA
 1- STD TYPE C ENDWALL
 10- CONC 2" OUTLET DITCH TO 6" DEPTH
 05- 500-2 OUTLET DITCH TO 6" DEPTH
 1" STONE FILLED WIRE MATTRESS-6" WIDE DEPTH, 1' 50" EACH SIDE

STA 452+00 S.B.R.
 1- MOD 5 COMB INLET-DGT
 1- VERTICAL DEPTH
 00- 24" RCP
 1- STD MANHOLE DEPTH 6' 0"
 02- 8" OCCMP TYPE C, #16 GA
 2- 22" 2" OCCMP ELBOWS TYPE C, #16 GA
 1- STD TYPE C ENDWALL
 10- CONC 2" OUTLET DITCH TO 6" DEPTH
 05- 500-2 OUTLET DITCH TO 6" DEPTH
 1" STONE FILLED WIRE MATTRESS-6" WIDE DEPTH, 1' 50" EACH SIDE

STA 452+00 N.B.R.-RT
 1- STD INLET 5' 0"
 1- VERTICAL DEPTH
 48" RCP
 1- STD END SECTION

STA 449+00 TO STA 452+00 N.B.R.-RT
 PLACE 20" WIDTH ROCK FILLER WIRE MATTRESS THICKNESS 6"
 STA 452+00 TO STA 457+00 N.B.R.-RT
 PLACE 12" WIDTH ROCK FILLER WIRE MATTRESS THICKNESS 6"

3
 CONSTR NBR
 CURVE DATA
 Δ=51°45'08"
 Ts=943.12'
 Δc=369'45.06"
 Dc=3'45'00"
 R=1152'89"
 T=507.55'
 L=980.06'
 9s=7°30'00"
 Ls=400.00'
 LT=265.94'
 ST=133.55'
 SE=0.058'/FT

4
 CONSTR SBR
 CURVE DATA
 Δ=67°06'57"
 Ts=1153.12'
 Δc=519'06.57"
 Dc=49'00'00"
 R=1432.39"
 T=684.99'
 L=1277.90'
 9s=8°00'00"
 Ls=400.00'
 LT=265.94'
 ST=133.55'
 SE=0.06'/FT

STA 453+00 TO STA 455+25 N.B.R.-LT
 500' MEDIAN DITCH TO 1' DEPTH
 B.M. D. ELEV 30.48
 D.O.A.T. SPIKE SET IN BASE 24" OAK TREE
 27' RT & EX 6" END ST 26' 8" FROM CURB
 LINE PULASKI HWY.

B.M. D. ELEV 30.48
 R.R. SPIKE SET IN BASE 24" OAK TREE
 27' RT & EX 6" END ST 26' 8" FROM CURB
 LINE PULASKI HWY.

RELOCATE EXISTING FENCE
 SBR STA 449+25 TO STA 450+00

REFERENCES:
 FOR PROFILES SEE SHEET NO. 22422L
 FOR PROFILES - MOORES RUN RELOCATED SEE SHEET NO. 2
 FOR TYPICAL SECTION SEE SHEET NO. 2364
 FOR TYPICAL SECTION - MOORES RUN RELOCATED SEE SHEET NO. 2
 FOR DRAINAGE DETAILS SEE SHEET NO. 2, 3 & 4
 FOR JOINT LAYOUT SEE SHEET NO. 11

STA 450+00 S.B.R.-RT
 1- MOD 5 COMB INLET-DGT
 1- VERTICAL DEPTH
 70" 8" ACCUM TYPE C, #16 GA
 2- 20" 2" OCCMP ELBOWS TYPE C, #16 GA
 1- STD END SECTION
 12- 4" CONC PAVING

STA 451+00 S.B.R.-RT
 EXTEND EXISTING 24" CONC PIPE 164' 21" RCP SALVAGE END SECTION FOR USE AT END OF PROPOSED EXTENSION

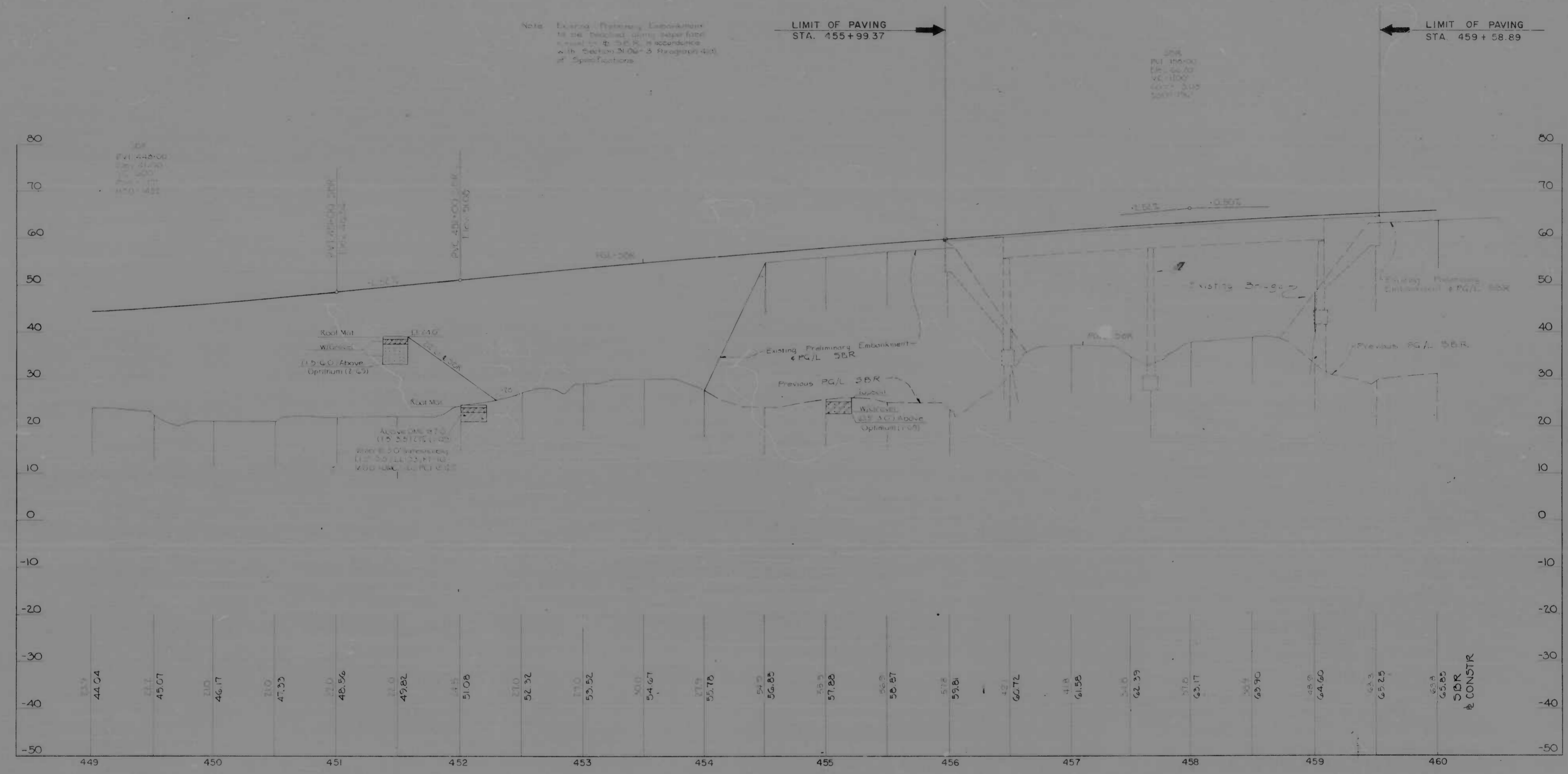
STA 452+00 TO STA 455+25 N.B.R.-LT
 MODIFIED SBR RELOCATION WARP STONE FILLED WIRE MATTRESS TO MEET EXISTING GROUND AS DIRECTED BY THE ENGINEER. SEE CROSS SECTIONS.

REVISIONS	CONSULTANT
1. REVISION: INCREASE TOP OF FILL AND SLOPE (WARP) ELEVATION TO 34.75	ANDERIL BENDER, STONE & ASSOC. INC.
2. REVISION: CORRECT MESSUP MEASUREMENTS AND CHANGES TO	MATZ, CHAPIN & ASSOC. INC.
3. DUCT ENCLOSURES 7-28-71	CONSULTING ENGINEERS
4. REVISION: WIRE MATTRESS TO 12"	381 N. CALVERT STREET
5. REVISION: WIRE MATTRESS TO 12"	BALTIMORE, MARYLAND 21202

CITY OF BALTIMORE	STATE ROADS COMMISSION OF MARYLAND
DEPARTMENT OF PUBLIC WORKS	INTERSTATE DIVISION FOR BALTIMORE CITY
INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
SCALE: 1"=40'	DATE:

DRAWN BY: J.E.L.	DES BY: J.L.C.	SHEET NO.
TRACED BY: J.E.L.	CHK BY: R.W.C.	22 OF 45
F.P. NO. 1-95-4135135	S.R.C. NO. E-235-22-815	
BALTO. CITY NO. 1283		

SHEET NO.	TOTAL SHEETS
2	45



SOUTHBOUND ROADWAY STA. 449+00 TO STA. 460+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KNOERLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: J.R.W.	DES. BY: J.L.C.
		SCALE: Hor 1"=40', Vert 1"=10'		DATE:	F.A.P. NO. I-95-2 (35)35
				5.R.C. NO. BC 246-32-615	23 of 45

2	MD	I-95-4(35)35	24	45
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NORTHBOUND ROADWAY STA. 449+00 TO STA. 457+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE & STATE ROADS COMMISSION OF MARYLAND	
	KIMBLE, WENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	DEPARTMENT OF PUBLIC WORKS	INTERSTATE DIVISION FOR BALTIMORE CITY
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
		DRAWN BY J.R.W. TRACED BY J.R.W.	DES. BY J.L.C. CHK. BY R.W.C.
		F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1083	SHEET NO. 24 OF 45
		SCALE: Hor. 1"=40', Vert. 1"=10'	DATE

PROJECT NO.	DATE	ISS. NO.	SCALE	SHEET NO.	TOTAL SHEETS
2	MD 195-4(35)35	25	25	45	

STA 40+00 SBR RT
 1.50' CONC. TYPICAL
 2.00' CONC. TYPICAL
 3.00' CONC. TYPICAL
 4.00' CONC. TYPICAL
 5.00' CONC. TYPICAL
 6.00' CONC. TYPICAL
 7.00' CONC. TYPICAL
 8.00' CONC. TYPICAL
 9.00' CONC. TYPICAL
 10.00' CONC. TYPICAL

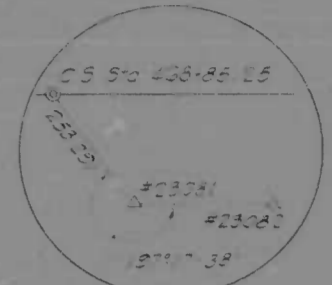
4
 CONST. SBR
 CURVE DATA
 $\Delta = 67^{\circ}06'57''$ RT
 $T_s = 1,153.12'$
 $\Delta c = 51^{\circ}06'57''$
 $D_c = 4^{\circ}00'00''$
 $R = 1,432.39'$
 $T = 684.99'$
 $L = 1,277.90'$
 $OS = 8^{\circ}00'00''$
 $LS = 400.00'$
 $LT = 266.99'$
 $ST = 133.58'$
 $SE = 0.06$ FT

STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT

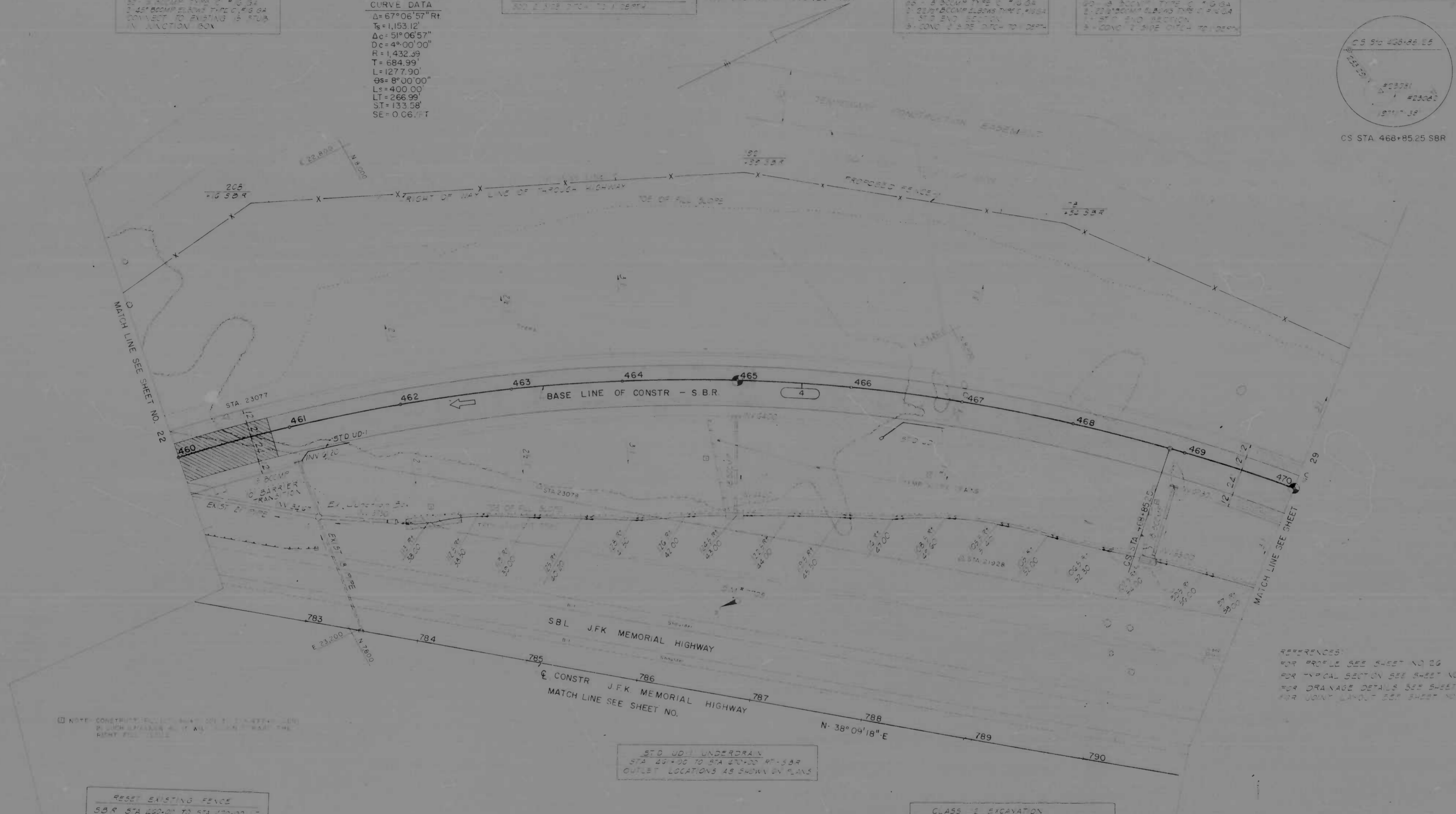
STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT
 STA 40+00 TO STA 40+00 SBR RT

STA 40+00 SBR RT
 1.50' CONC. TYPICAL
 2.00' CONC. TYPICAL
 3.00' CONC. TYPICAL
 4.00' CONC. TYPICAL
 5.00' CONC. TYPICAL
 6.00' CONC. TYPICAL
 7.00' CONC. TYPICAL
 8.00' CONC. TYPICAL
 9.00' CONC. TYPICAL
 10.00' CONC. TYPICAL

STA 40+00 SBR RT
 1.50' CONC. TYPICAL
 2.00' CONC. TYPICAL
 3.00' CONC. TYPICAL
 4.00' CONC. TYPICAL
 5.00' CONC. TYPICAL
 6.00' CONC. TYPICAL
 7.00' CONC. TYPICAL
 8.00' CONC. TYPICAL
 9.00' CONC. TYPICAL
 10.00' CONC. TYPICAL



CS STA 468+85.25 SBR



NOTE: CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE MARYLAND CONSTRUCTION SPECIFICATIONS AND THE MARYLAND ROAD AND BRIDGE SPECIFICATIONS.

RESET EXISTING FENCE
 SBR STA 460+00 TO STA 470+00 L

STD. U.D. UNDERDRAIN
 STA 40+00 TO STA 470+00 SBR
 OUTLET LOCATIONS AS SHOWN BY PLANS

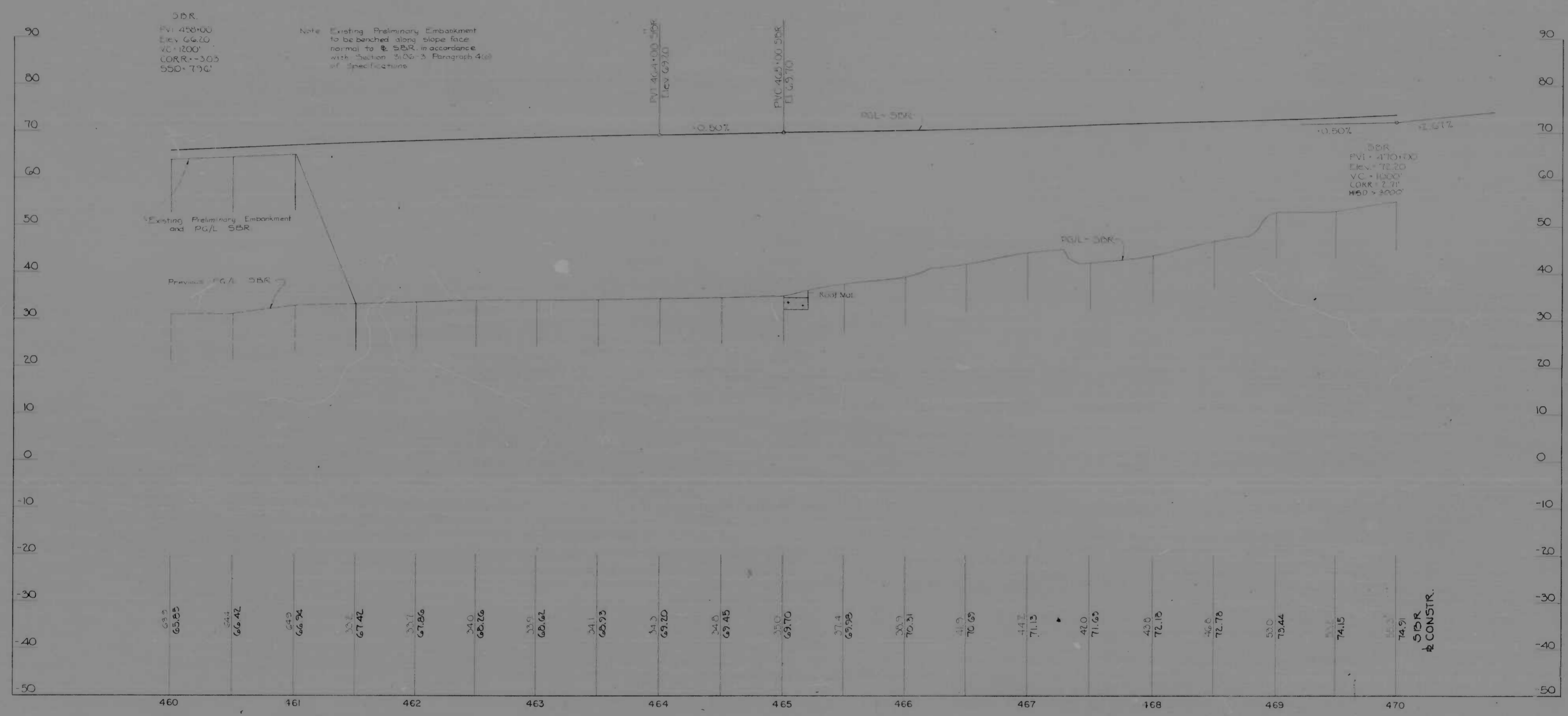
CLASS II EXCAVATION
 STA 40+00 TO STA 470+00 DITCH RT-SBR

REFERENCES:
 FOR PROFILES SEE SHEET NO. 26
 FOR TYPICAL SECTION SEE SHEET NO. 24B
 FOR DRAINAGE DETAILS SEE SHEET NO. 13
 FOR UNIT LAYOUT SEE SHEET NO. 11

CONTINUOUSLY REINFORCED CONCRETE PAVEMENT
 CONVENTIONAL REINFORCED CONCRETE PAVEMENT

REVISIONS	CONSULTANT	CITY OF BALTIMORE		STATE ROADS COMMISSION OF MARYLAND	
		DEPARTMENT OF PUBLIC WORKS		INTERSTATE DIVISION FOR BALTIMORE CITY	
MATZ, CHARLES & ASSOC., INC. CONSULTING ENGINEERS 441 N. CALVERT STREET BALTIMORE, MARYLAND 21201		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: J.E.L. TRACED BY: J.E.L.	DES. BY: J.L.C. CHK. BY: R.W.C.
SCALE: 1" = 40'		DATE: _____		F.A.P. NO. 195-4(35)35 S.B. NO. BC 246-32-315 BALTO. CITY NO. 1983	SHEET NO. 25 OF 45

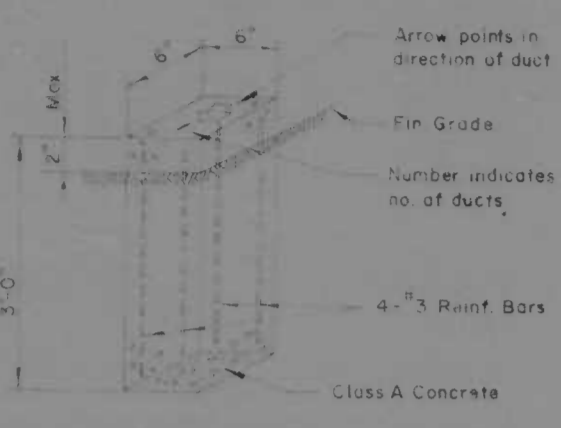
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35)35	26	45



SOUTHBOUND ROADWAY STA. 460+00 TO STA. 470+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KIMBLE, BENNER, STONE & ASSOC., INC. AND MATZ, CHILES & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
		SCALE: Hor. 1" = 40', Vert. 1" = 10'	DATE
		DRAWN BY: J.R.W. TRACED BY: J.R.W.	DES. BY: J.L.C. CHK. BY: R.W.C.
		F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815	SHEET NO. 26 of 45
		BALTO. CITY NO. 1983	

1" = 77.05' ELEV 62.47
 GRASS TYPEN IN W CONC FOOTING FOR
 STEEL H-COLUMN SUPPORTING TUNNEL
 THRUWAY. ETC SIGN W SIDE KENNEDY
 HWY 300' N OF MOORE'S RUN



DUCT MARKER DETAIL
 NO SCALE

NOTE: DUCT MARKER FLUSH WITH
 END AND LEFT SIDE OF 4" GALVANIZED
 METALLIC CONDUIT

NOTE: MIN COVER 1'-6" BELOW
 TOP OF ROADWAY

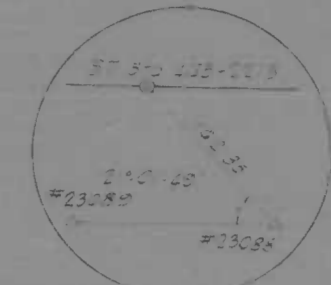
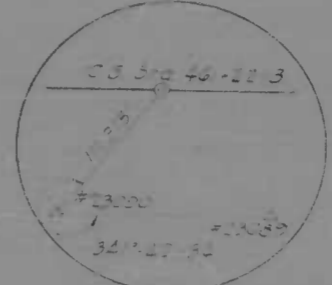
NOTE:
 HAFODR TUNNEL APPROACH
 EMERGENCY TELEPHONE CABLE
 BOTH UNDERGROUND AND ABOVE
 GROUND LOCATED ON RIGHT OF
 WAY FENCE TO BE RELOCATED
 BY OTHERS

STA 460+00 TO STA 461+50 NBR
 2" CONCRETE SURFACE DRAIN DITCH TO 1' DEPTH

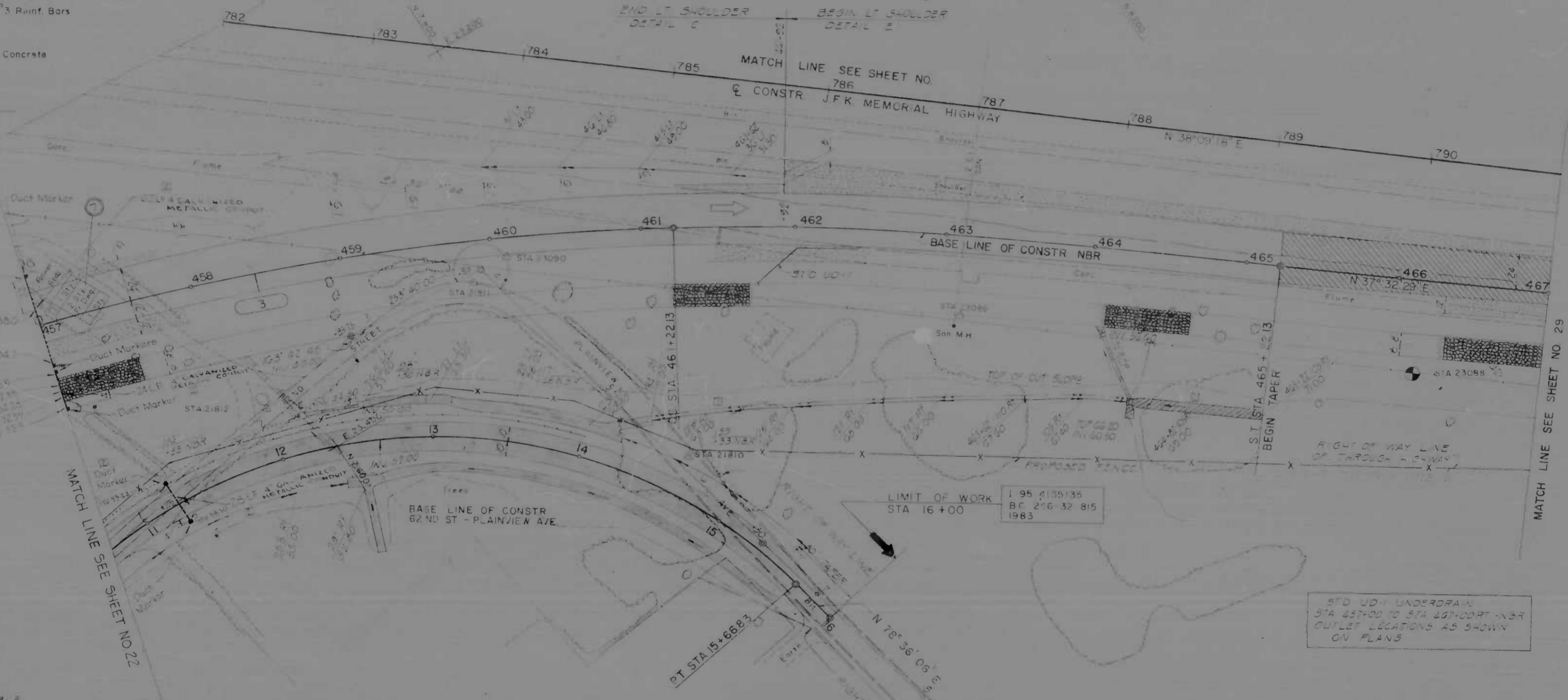
STA 461+50 TO STA 462+00 NBR
 1" SPECIAL X INLET NON-FRAME
 3" CONCRETE END SECTION
 6" CONC 2" SIDE DITCH TO 1' DEPTH

STA 462+00 TO STA 463+00 NBR
 2" CONCRETE SURFACE DRAIN DITCH TO 1' DEPTH

STA 463+00 TO STA 464+00 NBR
 2" CONCRETE SURFACE DRAIN DITCH TO 1' DEPTH



DATE	BY	CHKD	APP'D
27	MD	1-95-4135135	27



CONSTR
 62'ND PLAINVIEW
 $\Delta = 91^{\circ}15'49''$
 $Dc = 17^{\circ}54'18''$
 $R = 320.00'$
 $T = 327.14'$
 $L = 509.71'$
 $SE = 0.0201/FT$

62'ND ST - PLAINVIEW AVE
 STA 12+35
 1" STD 10" HALL
 7" STD CONC END SECTION
 100" 30" RCP
 23" CONC 2" INLET DITCH TO 1' DEPTH
 10" CONC 3" OUTLET DITCH TO 1' DEPTH

62'ND ST - PLAINVIEW AVE - RT
 STA 12+50 TO STA 13+20
 CONC 2" SIDE DITCH TO 1' DEPTH
 STA 13+20 TO STA 14+25
 CONC 2" SIDE DITCH IN CUT TO 1' DEPTH
 STA 14+25
 1" SPECIAL X INLET NON-FRAME
 3" CONC
 10" 15" RCP
 1" 35" X 15" BRICK WYE
 STA 14+25 TO STA 16+00
 300" SURFACE DRAIN DITCH TO 1' DEPTH

RESET EXISTING FENCE
 NBR 457+00 TO 458+00
 CLASS 2 EXCAVATION
 STA 461+50 TO STA 462+00 NBR
 62'ND ST - PLAINVIEW AVE
 STA 12+50 TO STA 13+5 DITCHLET

62'ND ST - PLAINVIEW AVE - RT
 STA 14+50 TO STA 15+20
 300" SURFACE DRAIN DITCH TO 1' DEPTH
 STA 15+20 TO STA 16+20
 300" 2" SIDE DITCH IN CUT TO 1' DEPTH
 STA 16+20 TO STA 17+00
 CONC 2" SIDE DITCH IN CUT TO 1' DEPTH
 STA 17+00 TO STA 18+00
 CONC SURFACE DRAIN DITCH TO 1' DEPTH

3
 CONSTR
 CURVE DATA
 $\Delta = 51^{\circ}45'08''$
 $Ts = 943.12'$
 $\Delta C = 36^{\circ}45'08''$
 $Dc = 3^{\circ}45'00''$
 $R = 1527.89'$
 $T = 507.55'$
 $L = 980.08'$
 $Qs = 7^{\circ}30'00''$
 $LS = 4000.00'$
 $LT = 266.91'$
 $ST = 133.55'$
 $SE = 0.038/FT$

REFERENCES
 FOR PROFILE SEE SHEET NO 26
 FOR TYPICAL SECTION SEE
 SHEET NO 244
 FOR PROFILE 62'ND ST - PLAINVIEW
 AVE SEE SHEET NO 35
 FOR DRAINAGE DETAILS SEE
 SHEET NO 12, 13 & 14
 FOR JOINT LAYOUT SEE
 SHEET NO 10

CONTINUOUSLY REINFORCED
 CONCRETE PAVEMENT
 CONVENTIONAL REINFORCED
 CONCRETE PAVEMENT

CONSTR 62'ND ST - PLAINVIEW AVE
 STA 10+50 TO 14+00 LT
 INSTALL GUARD RAIL W BEAM

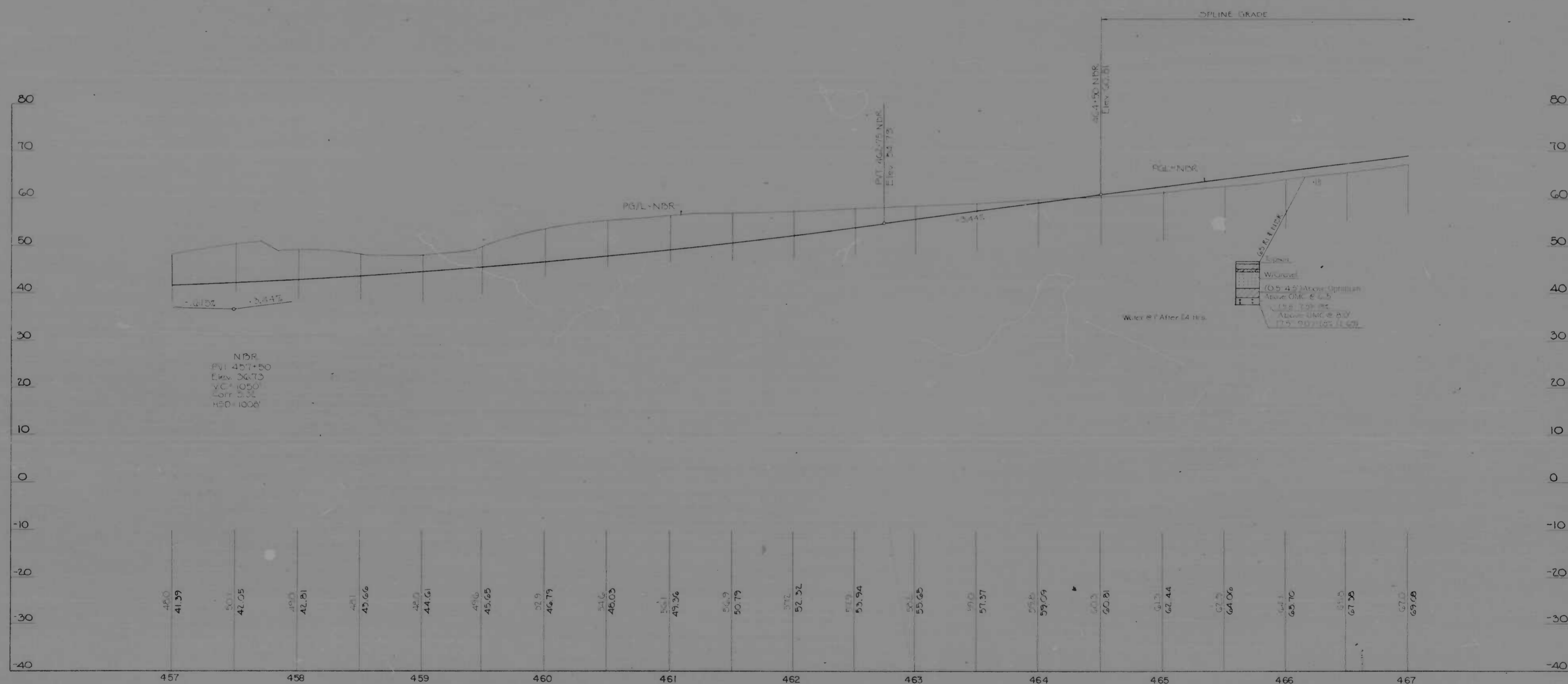
STA 463+05 NBR G3 RT
 ABANDON SANITARY SEWER MANHOLE
 EXISTING PIPE TO BE SEALED, MANHOLE
 TO BE DISMANTLED AND FILLED IN, AND
 SLOPE GRADED TO CONFORM TO TYPICAL
 SECTION

REVISIONS	CONSULTANT
1. PRELIMINARY DESIGN MOVED AND CHANGED 4-8-71 (PROJECT DISCONTINUED 7-23-71)	EMORLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHANDS & ASSOC., INC. CONSULTING ENGINEERS 341 N CALVEY STREET BALTIMORE, MARYLAND 21202

CITY OF BALTIMORE
 DEPARTMENT OF PUBLIC WORKS &
 INTERSTATE ROUTE 95 FROM
 NORTH OF PULASKI HIGHWAY
 TO JOHN F KENNEDY MEMORIAL HIGHWAY

STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	DATE	SHEET NO
DRAWN BY: JEL TRACED BY: JEL F.A.P. NO. 1-95-4135135 SRC NO. 3C 146 32 B 5 BALTO CITY NO. 1993	DES BY: J.L.C. CHK BY: R.W.C.	27 OF 45

FED. ROAD DIV. NO.	STATE	F.P. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I 95-4(35)35	28	45

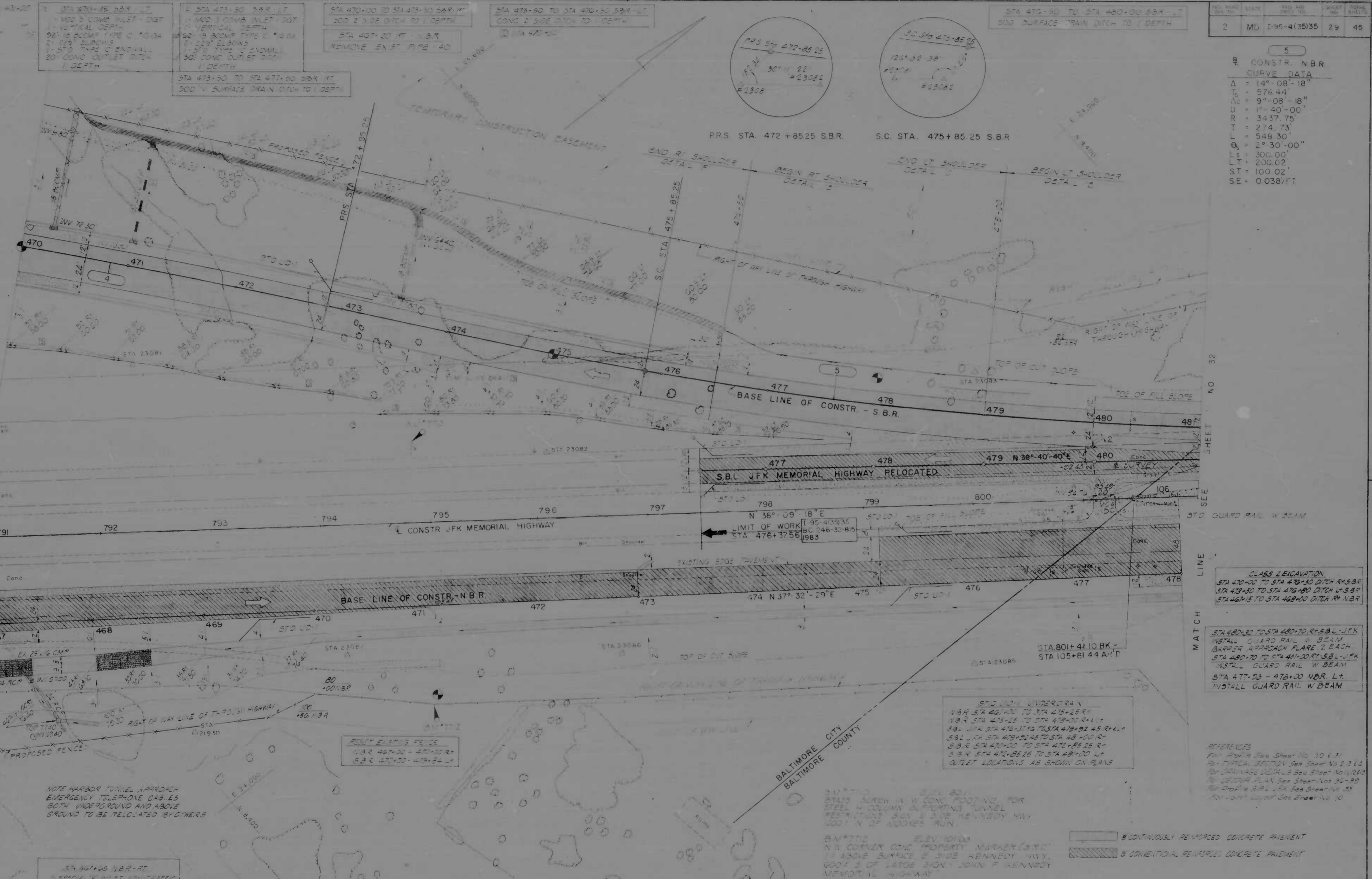


NORTHBOUND ROADWAY STA. 457+00 TO STA. 467+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE & STATE ROADS COMMISSION OF MARYLAND	
	KROCKLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	DEPARTMENT OF PUBLIC WORKS	INTERSTATE DIVISION FOR BALTIMORE CITY
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
SCALE: Hor. 1"=40', Vert. 1"=10'		DATE	SHEET NO. 28 of 45

MD BALTIMORE COUNTY

4
CONSTR - SBR
CURVE DATA
 $\Delta = 67^{\circ}06'57''$
 $T_s = 1153.12'$
 $\Delta_s = 5^{\circ}06'57''$
 $D = 4^{\circ}00'00''$
 $R = 1432.39'$
 $T = 684.99'$
 $L = 1277.90'$
 $\Delta_s = 8^{\circ}00'00''$
 $L_s = 400.00'$
 $LT = 266.94'$
 $ST = 133.58'$
 $SE = 0.06/FT$



NOTE: HARBOR TOWER APPROACH EMERGENCY TELEPHONE CABLES BOTH UNDERGROUND AND ABOVE GROUND TO BE RELOCATED BY OTHERS

STA 467+00 TO STA 470+00 NBR RT
 CONC 2 SIDE DITCH TO 1 DEPTH

STA 470+00 TO STA 473+00 NBR RT
 PLACE 16" WITH ROCK FILLED 12" MATRESS THICKNESS 6"

STA 473+00 TO STA 477+00 NBR RT
 CONC 2 BEAM DITCH TO 1 DEPTH

REVISIONS	CONSULTANT
1. CORRECT STATION 470+00 TO 473+00	KNOERLE, BENDER, STONE & ASSOC., INC.
2. SBR DRAINAGE DETAILS	MATT, CHILDS & ASSOC., INC.
3. STA 477+00 TO STA 480+00 NBR RT	CONSULTING ENGINEERS
4. CONC 2 BEAM DITCH TO 1 DEPTH	341 N. CALVERT STREET
	BALTIMORE, MARYLAND 21202

CITY OF BALTIMORE
 DEPARTMENT OF PUBLIC WORKS
 &
 STATE ROADS COMMISSION OF MARYLAND
 INTERSTATE ROUTE 95 FROM
 NORTH OF PULASKI HIGHWAY
 TO JOHN F. KENNEDY MEMORIAL HIGHWAY

INTERSTATE DIVISION FOR BALTIMORE CITY
 DRAWN BY: J.E.L.
 CHECKED BY: J.E.L.
 DATE: 1-95-4(15)35
 SHEET NO. 29 OF 45

SHEET NO.	TOTAL SHEETS
2	29

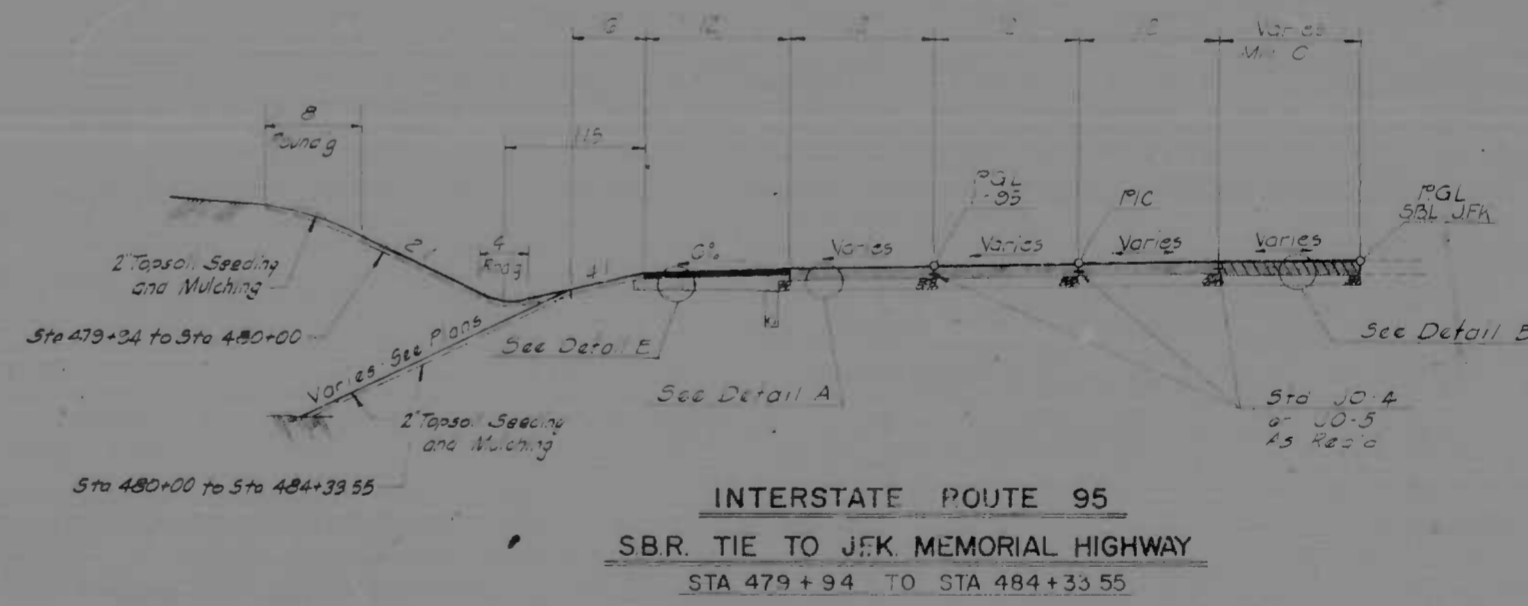
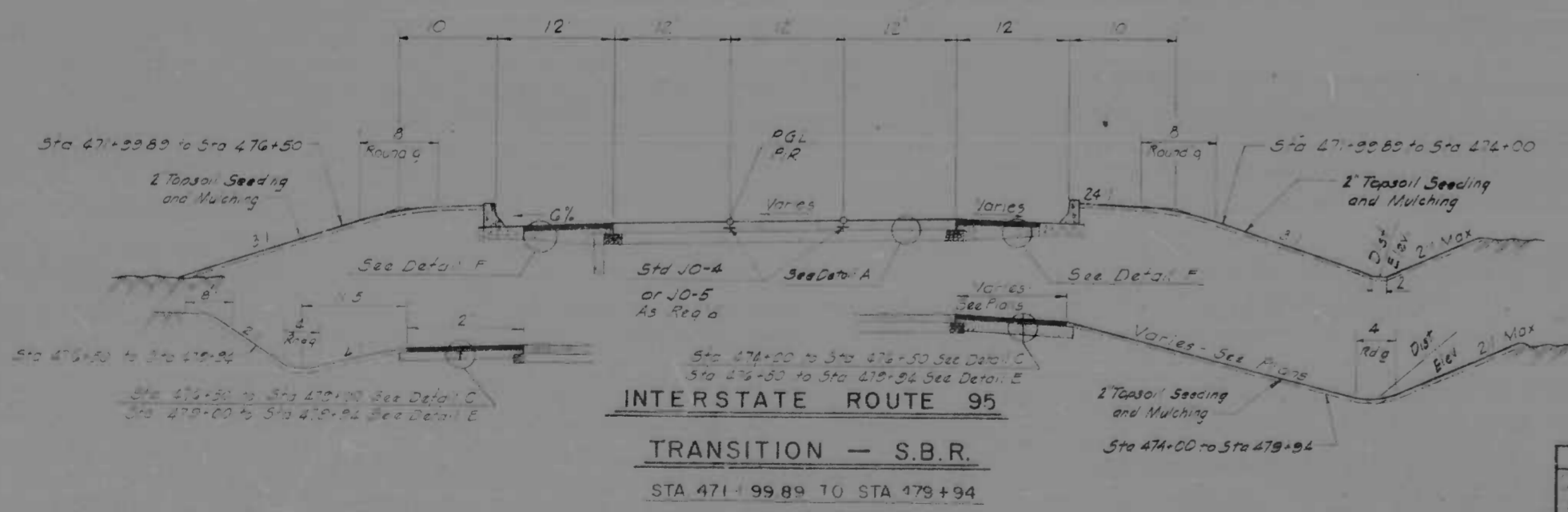
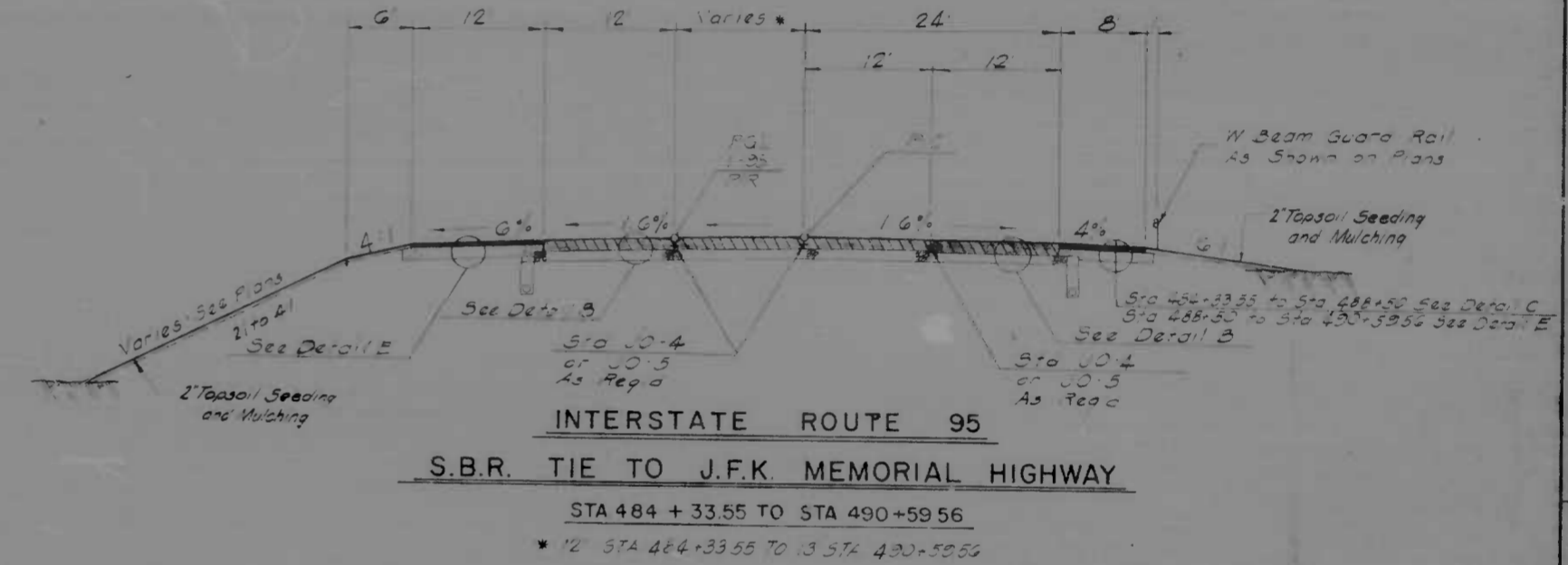
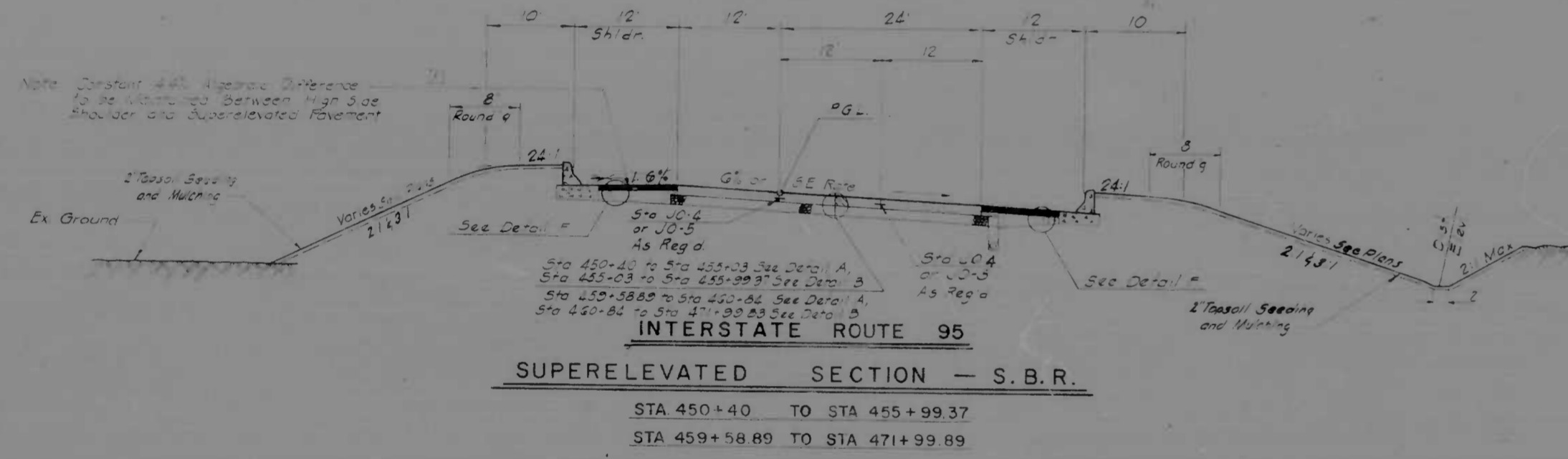
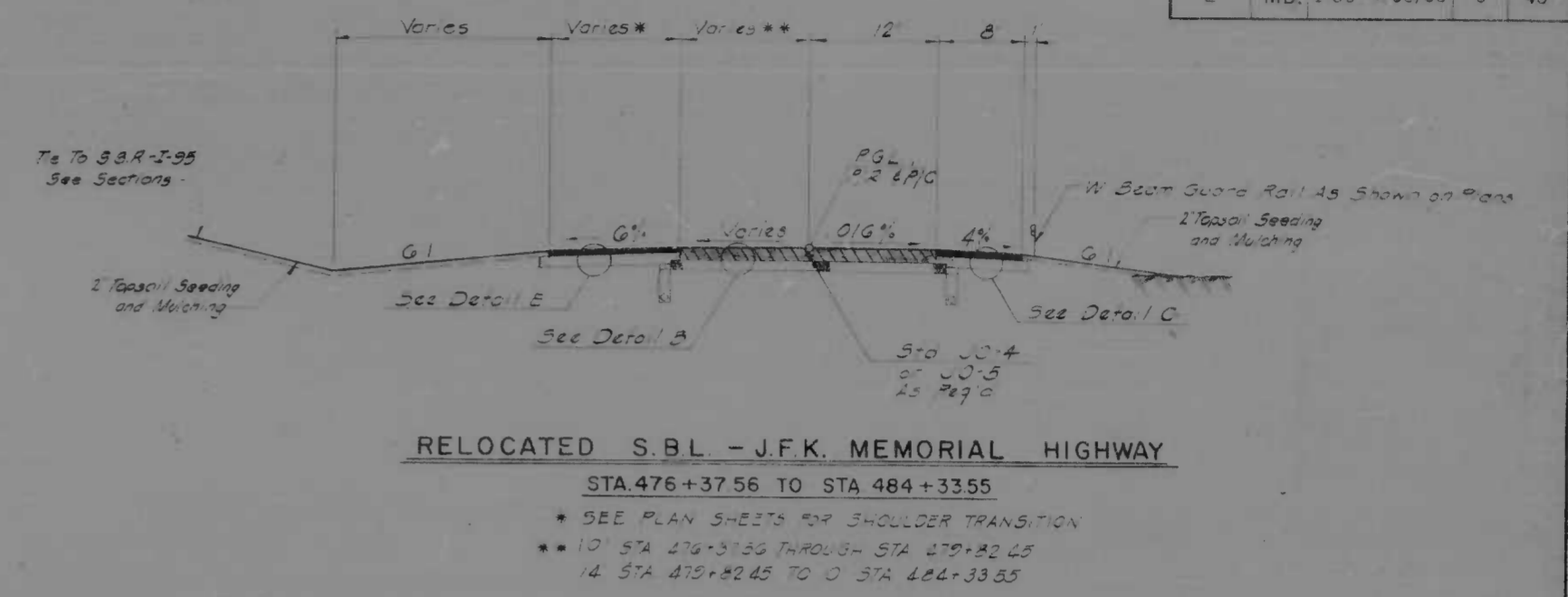
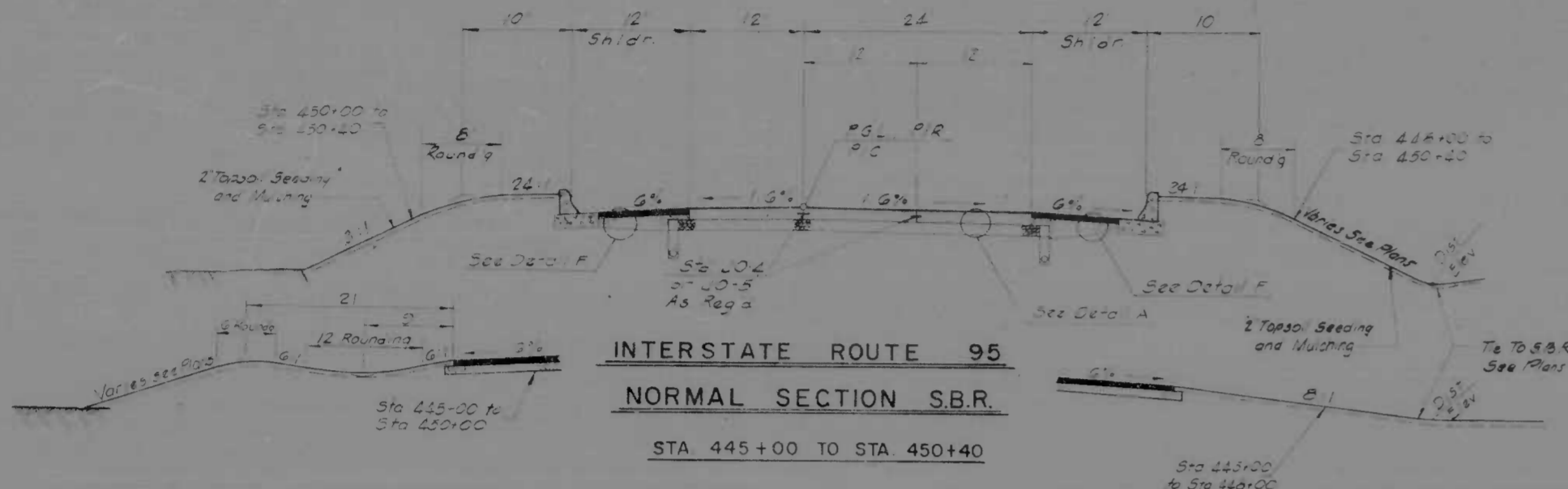
5
CONSTR. NBR
CURVE DATA
 $\Delta = 14^{\circ}08'18''$
 $T_s = 576.44'$
 $\Delta_s = 9^{\circ}08'18''$
 $D = 1^{\circ}40'00''$
 $R = 3437.75'$
 $T = 274.73'$
 $L = 548.30'$
 $\Delta_s = 2^{\circ}30'00''$
 $L_s = 300.00'$
 $LT = 200.02'$
 $ST = 100.02'$
 $SE = 0.038/FT$

CLASS RECAVATION
 STA 470+00 TO STA 473+00 DITCH R-SBR
 STA 473+00 TO STA 476+00 DITCH L-SBR
 STA 476+00 TO STA 480+00 DITCH R-NBR

STANDARD UNDERPASS
 VBR STA 476+00 TO STA 478+00 RT
 VBR STA 478+00 TO STA 480+00 RT
 SBR STA 476+00 TO STA 478+00 RT
 SBR STA 478+00 TO STA 480+00 RT
 SBR STA 476+00 TO STA 480+00 LT
 DITCH LOCATIONS AS SHOWN ON PLANS

CONTINUOUSLY REINFORCED CONCRETE PAVEMENT
 CONVENTIONAL REINFORCED CONCRETE PAVEMENT

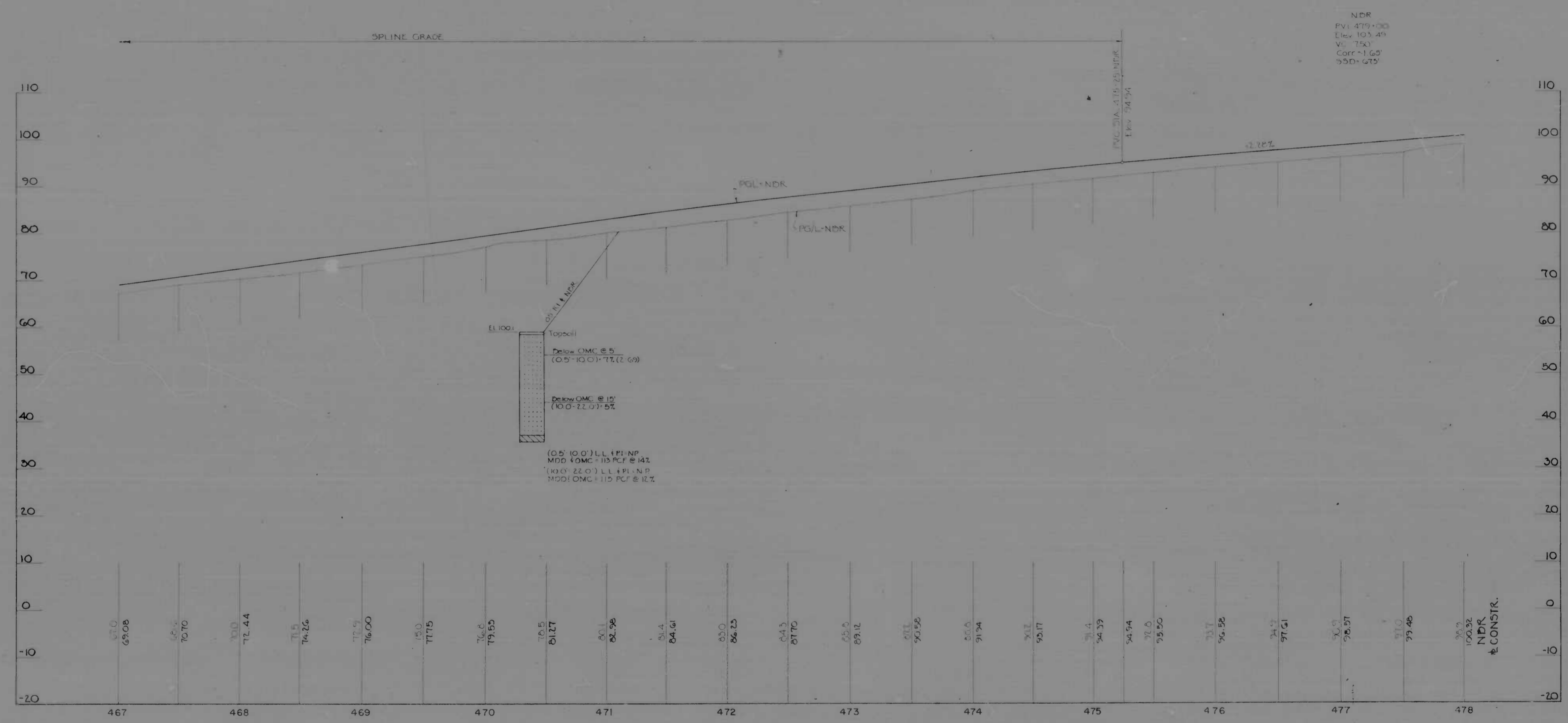
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	1-95-4(35)35	3	45



TYPICAL SECTION

REVISIONS 1/ REVISION TO TYPICAL SECTION RW 2 3-10-71	CONSULTANT KIMMEL, DENVER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 841 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY			

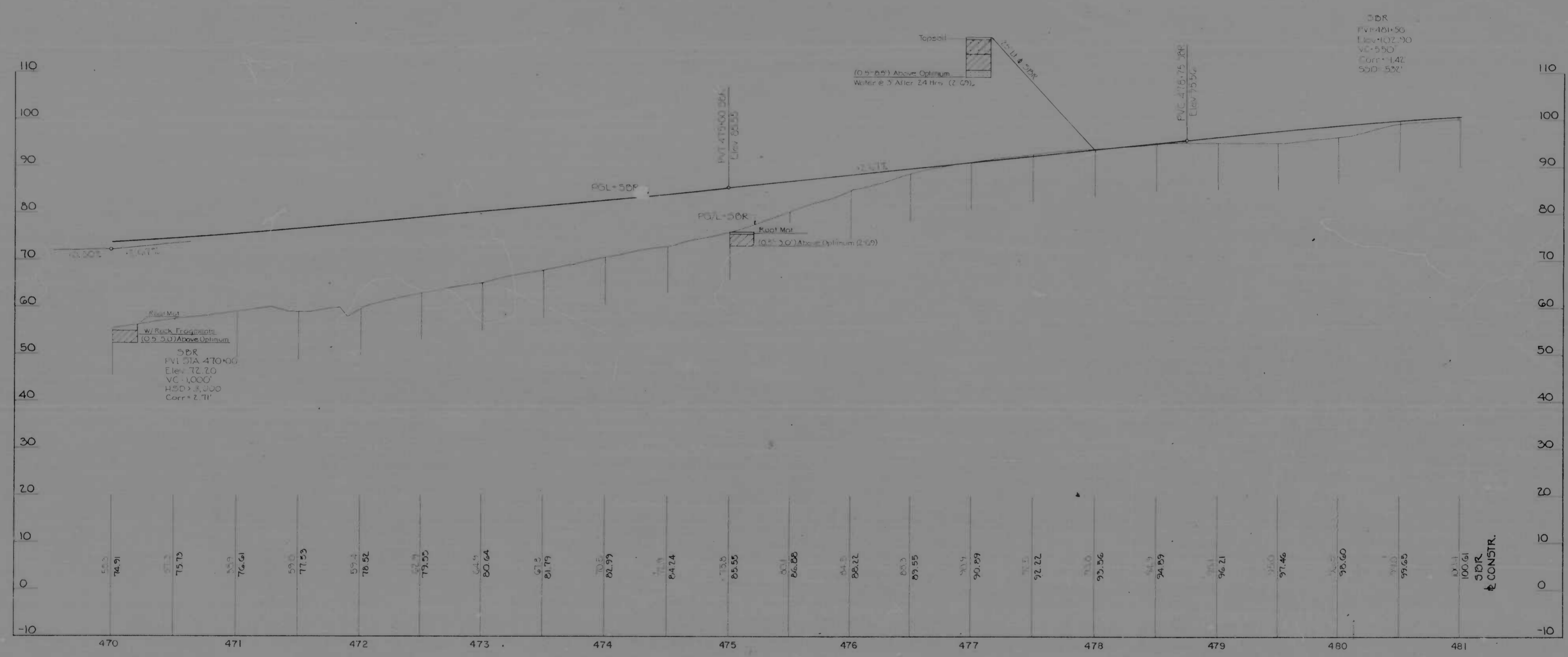
FED. PROJ. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35) 35	30	45



NORTHBOUND ROADWAY STA. 467+00 TO STA. 478+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KIMBERLIE, BEYMER, STONE & ASSOC., INC. 140 MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	DRAWN BY: JRW TRACED BY: JRW DES BY: JLC CHK BY: RWC	
SCALE: Hor. 1"=40', Vert. 1"=10'		DATE: _____	
		F.A.P. NO. I-95-4(35) 35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983	
		SHEET NO. 30 of 45	

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	31	45



SOUTHBOUND ROADWAY STA. 470+00 TO STA. 481+00

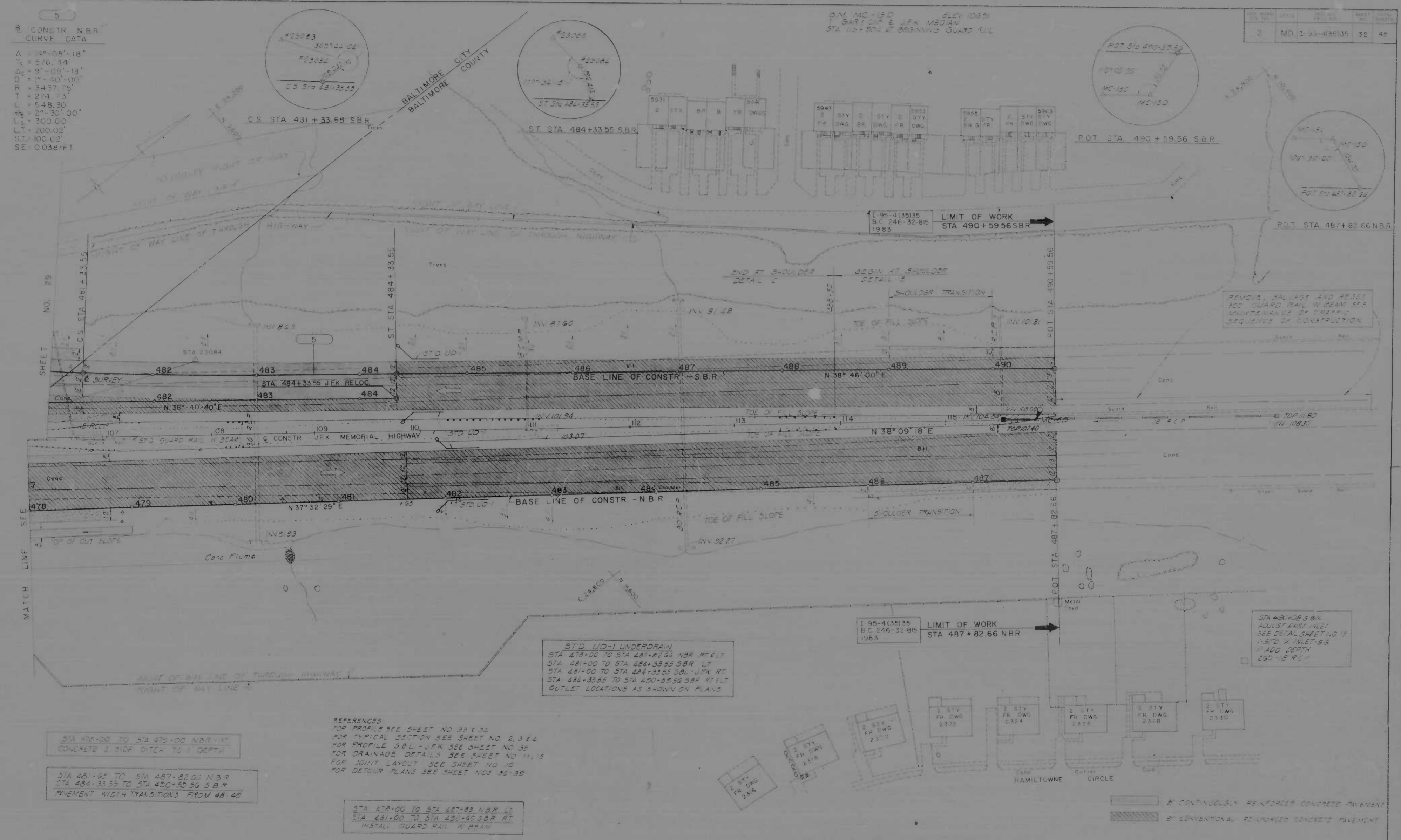
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNOCKE, BENDER, STONE & ASSOC., INC. AND MATT, CHURCH & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY
	DRAWN BY: J.R.W. TRACED BY: J.R.W.	DES. BY: J.L.C. CHK. BY: R.W.C.
	SCALE: Hor. 1"=40', Vert. 1"=10' DATE:	F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		SHEET NO. 31 of 45

CONSTR. NBR
CURVE DATA

Δ = 14° 08' 18"
T₁ = 576.44
Δ₂ = 9° 09' 19"
D = 3° 40' 00"
R = 3437.75'
T = 274.73'
E = 548.30'
C₁ = 2° 30' 00"
L₁ = 300.00'
L₂ = 200.00'
S₁ = 100.02'
S₂ = 0038/FT

B.M. MC-50 ELEV 108.91
BARI CAP & JFK MEDIAN
STA 15+00 AT BEGINNING GUARD RAIL

NO.	DATE	BY	REVISION
2	MD 1-25-1983	32	45



STA 478+00 TO STA 479+00 NBR RT
CONCRETE & SIDE DITCH TO 1' DEPTH

STA 481+05 TO STA 487+82.66 NBR
STA 484+33.55 TO STA 490+50 SBR
PAVEMENT WIDTH TRANSITIONS FROM 48' 4"

REFERENCES
FOR PROFILE SEE SHEET NO 33 E 32
FOR TYPICAL SECTION SEE SHEET NO 2, 3 & 4
FOR PROFILE SBL - JFK SEE SHEET NO 35
FOR DRAINAGE DETAILS SEE SHEET NO 11, 15
FOR JOINT LAYOUT SEE SHEET NO 10
FOR DETOUR PLANS SEE SHEET NOS 35-39

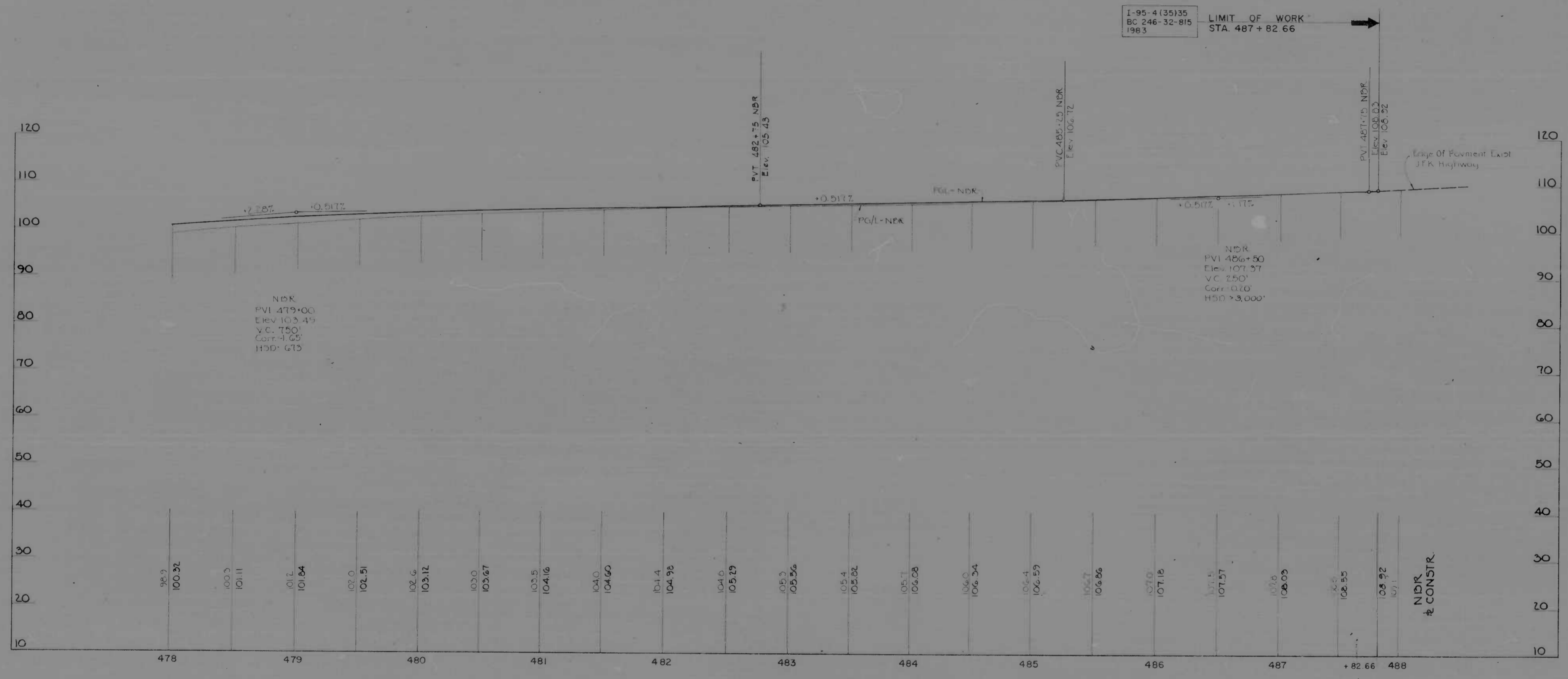
STA 478+00 TO STA 487+83 NBR LT
STA 481+00 TO STA 490+40 SBR RT
INSTALL GUARD RAIL W BEAM

STA 478+00 TO STA 479+00 NBR RT
REMOVE EXISTING GUARD RAIL
AS CLEARING & GRUBBING

STD UD-1 UNDERDRAIN
STA 478+00 TO STA 481+82.66 NBR RT LT
STA 481+00 TO STA 484+33.55 SBR LT
STA 481+00 TO STA 484+33.55 SBL - JFK RT
STA 484+33.55 TO STA 490+50 SBR RT LT
OUTLET LOCATIONS AS SHOWN ON PLANS

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNOCHE, BENDER, STONE & ASSOC. INC. MATT, CHILDS & ASSOC. INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: M.S.F. CHECKED BY: M.S.F. DATE: 1-25-1983 SHEET NO: 32 OF 45

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35)35	33	45



NORTHBOUND ROADWAY STA. 478+00 TO STA. 488+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KNOERLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHURS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: J.R.W.	DES. BY: J.L.C.
		SCALE: Hor. 1"=40', Vert. 1"=10'		DATE:	TRACED BY: J.R.W.
			F.A.P. NO. I-95-4(35)35	SHEET NO. 33 OF 45	
			S.A.C. NO. BC 246-32-815	BALTO. CITY NO. 1983	

ME BALTIMORE 17 0471

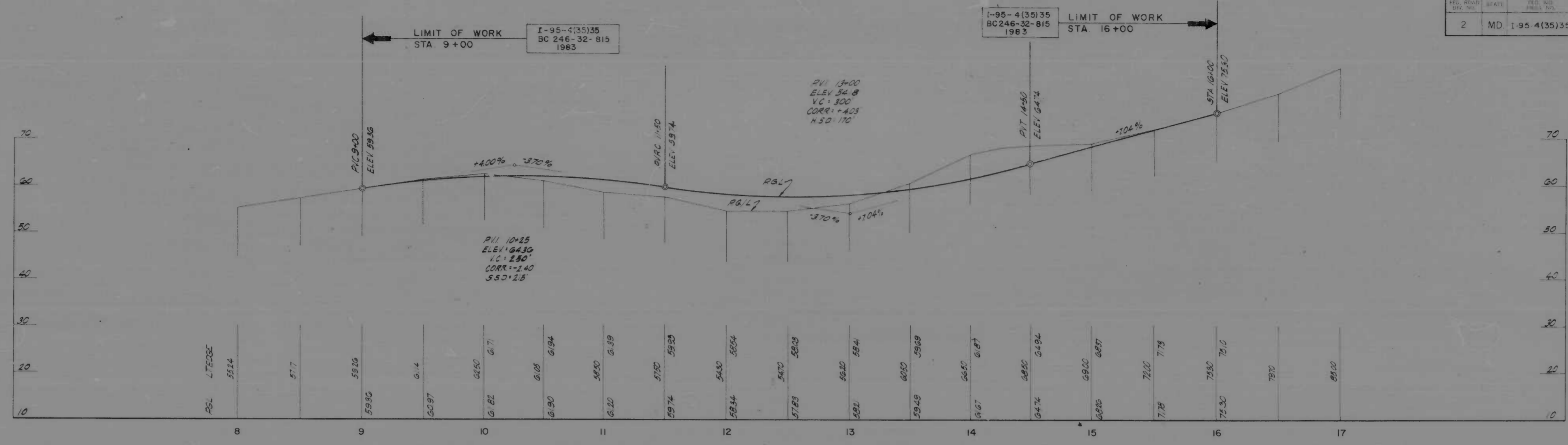
FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	34	45



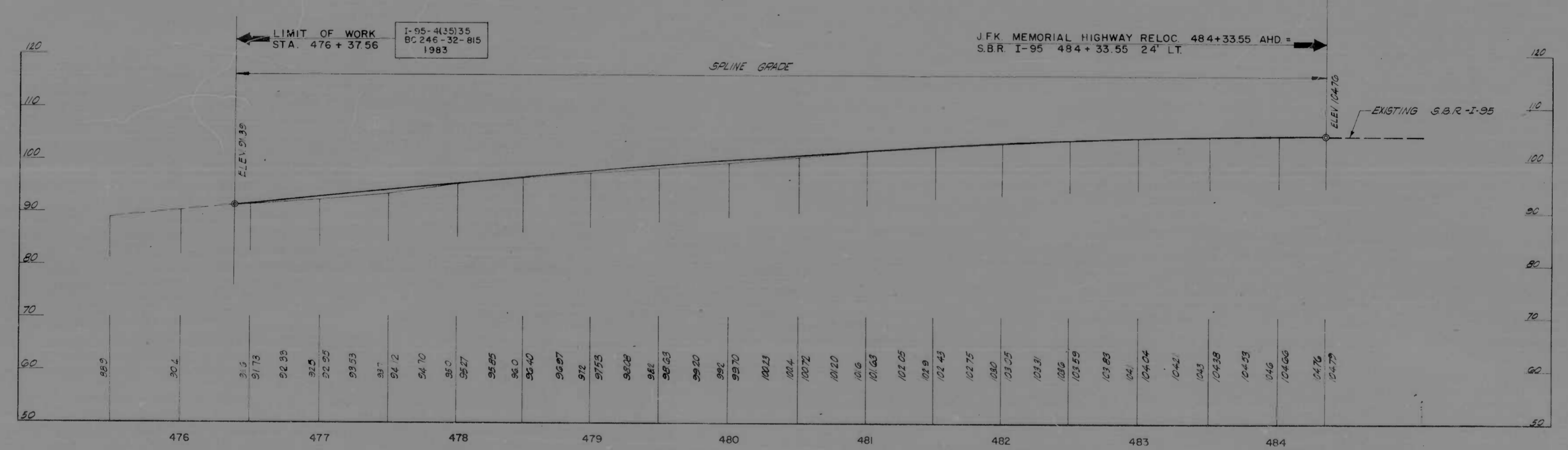
SOUTHBOUND ROADWAY STA. 481+00 TO STA. 491+00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	KNORRE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	
	SCALE: Hor 1"=40', Vert 1"=10'	DATE	DRAWN BY: J.R.W. TRACED BY: J.R.W. F.A.P. NO.: I-95-4(35)35 S.R.C. NO.: BC 246-32-815 BALTO. CITY NO.: 1983

FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	35	45



RELOCATED 62ND. ST. & PLAINVIEW AVE. STA.9+00 TO STA.16+00

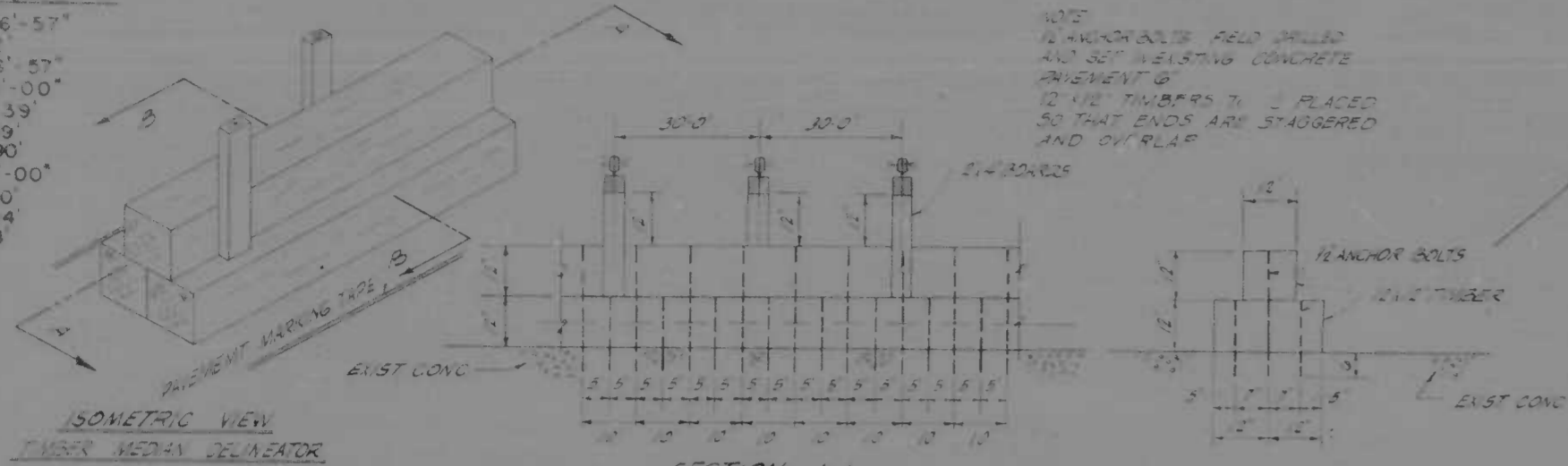


S.P.L. J.F.K. MEMORIAL HIGHWAY RELOCATED

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNORRLE, BERGER, STONE & ASSOC., INC. AND MATZ, CHUNG & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: P.W.Z. CHECKED BY: P.W.Z. F.A.P. NO.: I-95-4(35)35 S.R.C. NO.: BC 246-32-815 BALTO. CITY NO.: 1983
		SCALE: Hor. 1"=40', Vert. 1"=10'	DES. BY: J.L.C. CHK BY: R.W.C. SHEET NO.: 35 OF 45

4
CONSTR - SBR
CURVE DATA

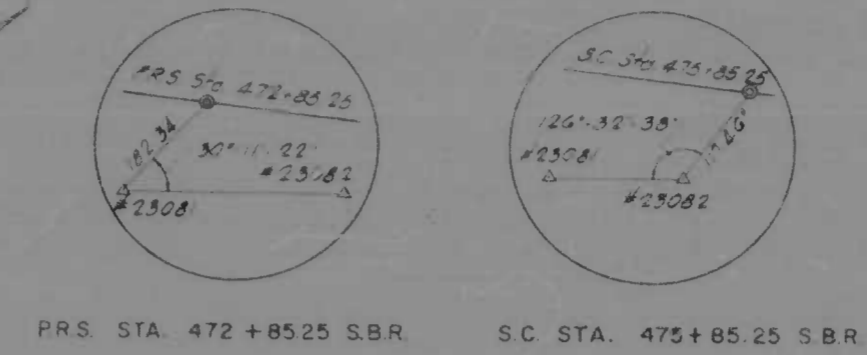
Δ = 67°-06'-57"
 T = 1153.12'
 D = 51°-08'-57"
 D = 47°-00'-00"
 R = 1432.39'
 T = 684.99'
 L = 1277.90'
 Δ = 8°-00'-00"
 T = 400.00'
 L = 266.94'
 ST = 133.58'



ISOMETRIC VIEW
TIMBER MEDIAN DELINEATOR

SECTION A-A
TIMBER MEDIAN DELINEATOR

SECTION B-B

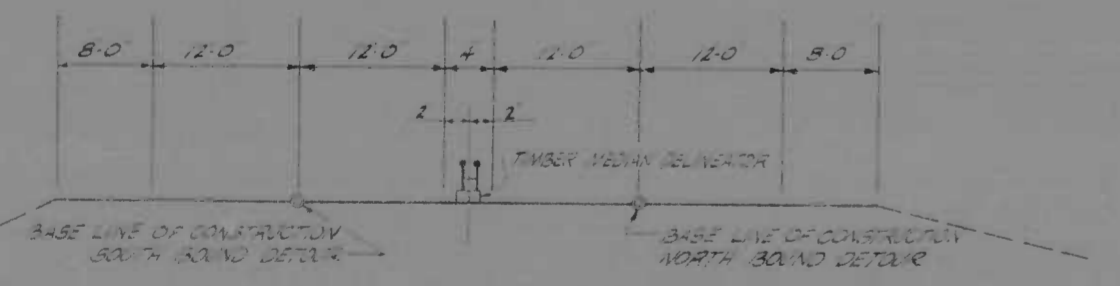


PRS STA. 472+85.25 S.B.R. SC STA. 473+85.25 S.B.R.

REV. NO.	DATE	BY	CHKD.	SHEET NO.	TOTAL SHEETS
2	MD. 1-95-815535			36	45

5
CONSTR. NBR
CURVE DATA

Δ = 14°-08'-18"
 T = 576.44'
 ΔC = 9°-08'-18"
 D = 1°-40'-00"
 R = 3437.75'
 T = 274.73'
 L = 548.30'
 Δ = 2°-30'-00"
 T = 300.00'
 L = 200.02'
 ST = 100.02'



LEGEND

[Symbol]	LOCATION OF DETOUR ON EXISTING PAVEMENT
[Symbol]	LOCATION OF DETOUR ON PROPOSED FINAL CONC PAVT
[Symbol]	LOCATION OF DETOUR ON TEMP PAVEMENT

MAINTENANCE OF TRAFFIC PHASE I

<p>REVISIONS</p> <p>1 CORRECT STATION 12/24/70</p>	<p>CONSULTANT</p> <p>KNOBLE, BROWN, STONE & ASSOC., INC.</p> <p>MATZ, CHILDS & ASSOC., INC.</p> <p>CONSULTING ENGINEERS</p> <p>341 N. CALVERT STREET</p> <p>BALTIMORE, MARYLAND 21209</p>	<p>CITY OF BALTIMORE</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>INTERSTATE ROUTE 95 FROM</p> <p>NORTH OF PULASKI HIGHWAY</p> <p>TO JOHN F. KENNEDY MEMORIAL HIGHWAY</p> <p>SCALE: 1"=40'</p>	<p>STATE ROADS COMMISSION OF MARYLAND</p> <p>INTERSTATE DIVISION FOR BALTIMORE CITY</p> <p>DRAWN BY: PWZ</p> <p>TRACED BY: PWZ</p> <p>FAB NO. 1-95-815535</p> <p>SRC NO. BC 24632-B18</p> <p>BALTD. CITY NO. 1983</p>
		DATE	DES. BY: J.L.G.
			CHK. BY: R.W.G.
			SHEET NO. 36 of 45

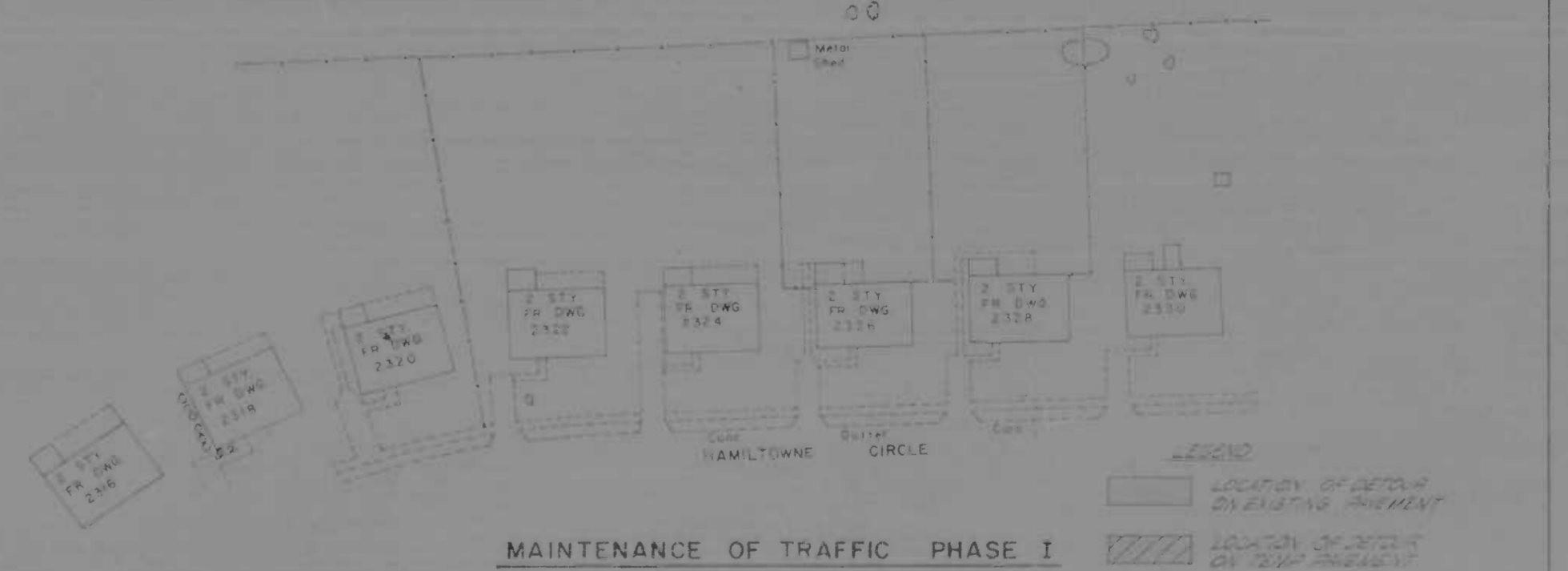
CONSTR N&R
CURVE DATA

Δ	14° 08' 18"
D	576.44'
Δ	3° 08' 18"
D	174.40'
R	3437.75'
T	214.73'
L	548.30'
Δ	2° 30' 00"
L	300.00'
L	200.00'
L	100.00'

REV. NO.	DATE	BY	CHKD.	DATE
2	MO. 3-95	4/15/95	37	45



- SEQUENCE OF CONSTRUCTION PHASE I
- 1) INSTALL TEMPORARY CHAINAGE STRUCTURE STA 483+50 TO STA 484+50 & CONSTRUCTION EXISTING J.F.K. AND PLACE PILE FOR CROSS OVERS
 - 2) REMOVE EXISTING GUARD RAIL MEDIAN BARRIER NORTH OF DETOUR AND SALVAGE FOR RESETTING DURING PHASE II
 - 3) MODIFY INLET AT STA 490+00 AT BASE LINE OF CONSTRUCTION SBR AND INSTALL NEW PIPE AND INLET AS SHOWN ON CONSTRUCTION PLANS AND DETAIL SHEET
 - 4) PLACE ALL TEMPORARY PAVEMENT IN AREAS BETWEEN N.B. & S.B.L. JFK. ALL PAVEMENT NECESSARY FOR BOTH DETOUR PHASES SHALL BE PLACED AT THIS TIME TO THE DIMENSIONS AND SECTIONS SHOWN
 - 5) CONSTRUCT PROPOSED FINAL CONCRETE PAVEMENT AND CONC. PAVEMENT ON I-95 FROM AT LEAST STA 469+00 TO STA 475+75 BASE LINE OF CONSTRUCTION N.B.R. SEE CONSTRUCTION PLANS
 - 6) PLACE REMAINING TEMPORARY PAVEMENT ON EAST SIDE OF N.B.L. TO CONFORM TO TYPICAL SECTION DIMENSIONS
 - 7) PLACE TEMPORARY PAVEMENT MARKING TAPE TO DELINEATE LANE LINES AS NOTED ON PLANS AND AS FOLLOWS: PUT TO SHOULDER WHITE-SOLID-BETWEEN LANES WHITE-DASHED-ON MEDIAN SIDE YELLOW-SOLID
 - 8) ERECT TEMPORARY TIMBER MEDIAN DELINEATOR TO THE LIMITS AS SHOWN ON PLANS
 - 9) RELOCATE EXISTING STATE ROAD COMMISSION BARRICADES ON THE PRESENT S.B.L. OF JFK TO CHANNEL TRAFFIC INTO THE DETOUR AND OPEN LANES TO TRAFFIC
 - 10) BUILD S.B.R. I-95 FROM AT LEAST STA 475+00 TO STA 490+39.50 AND ALL OF S.B.L. JFK. RELOCATED PLACE TEMPORARY PAVEMENT BETWEEN S.B.L. JFK AND S.B.R. AT THIS TIME



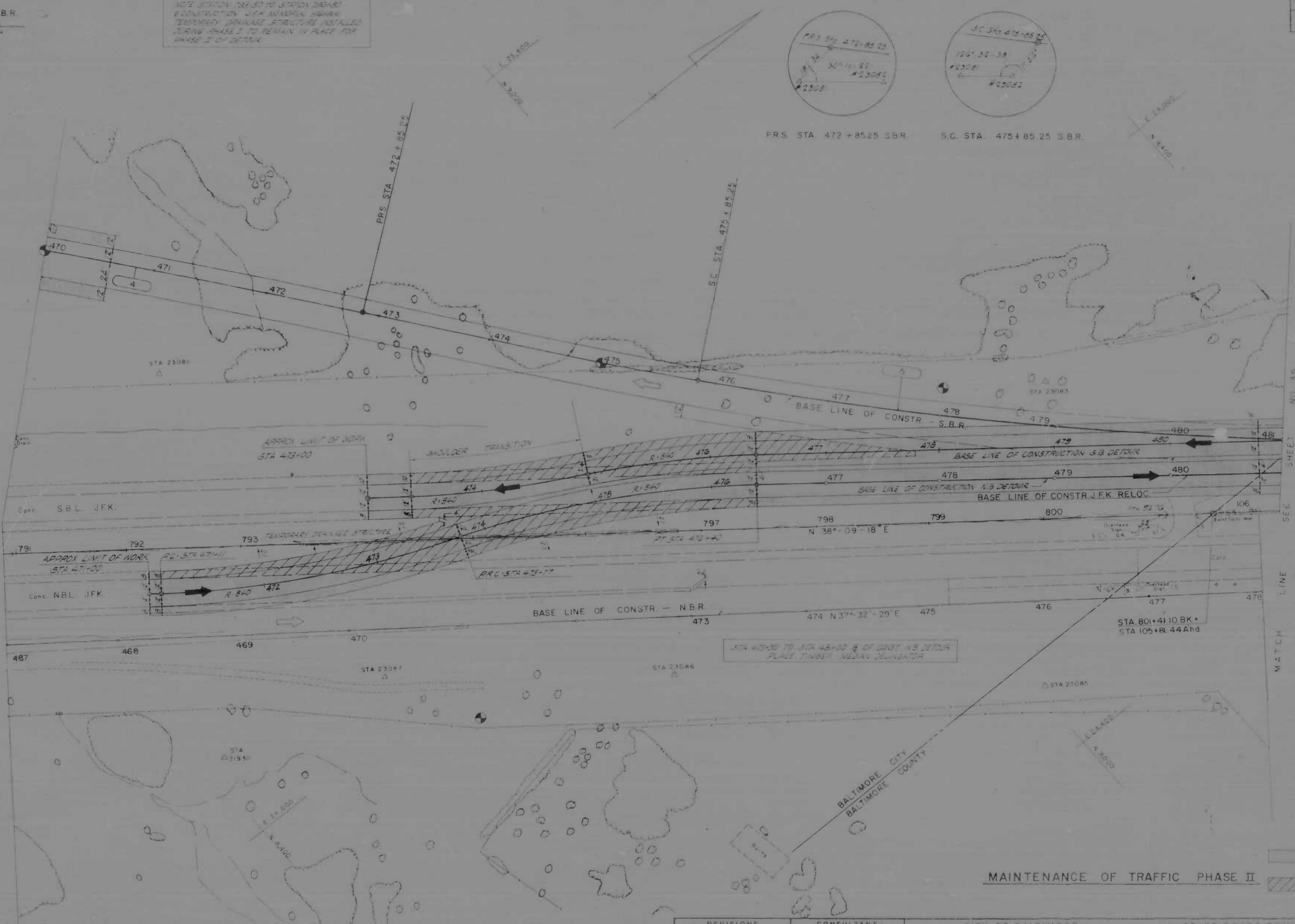
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KIMBLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHURCH & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: E.W.Z. CHECKED BY: E.W.Z. DATE: 3-95 SHEET NO. 37 OF 45

4
 R CONSTR - SBR
 CURVE DATA
 $\Delta = 67^{\circ}06'57''$
 $\Delta_s = 153.12'$
 $\Delta_c = 51^{\circ}06'57''$
 $D = 41^{\circ}00'00''$
 $R = 1432.39'$
 $T = 584.99'$
 $L = 1277.90'$
 $\theta_s = 8^{\circ}00'00''$
 $L_s = 400.00'$
 $LT = 266.94'$
 $ST = 133.58'$

NO. 2 STATION MARKED TO STATION 474.00
 P. CONSTRUCTION WITH REMAINING NUMBER
 TEMPORARY SIGNAGE STRUCTURE INSTALLED
 DURING PHASE I TO REMAIN IN PLACE FOR
 PHASE II OF DETOUR

NO.	STATE	FILE NO.	SHEET NO.	TOTAL SHEETS
2	MD	1-95-435135	38	45

5
 R CONSTR NBR
 CURVE DATA
 $\Delta = 14^{\circ}08'18''$
 $\Delta_s = 576.44'$
 $\Delta_c = 3^{\circ}08'18''$
 $D = 1^{\circ}40'00''$
 $R = 3437.75'$
 $T = 274.73'$
 $L = 548.30'$
 $\theta_s = 2^{\circ}30'00''$
 $L_s = 203.80'$
 $LT = 200.02'$
 $ST = 100.02'$

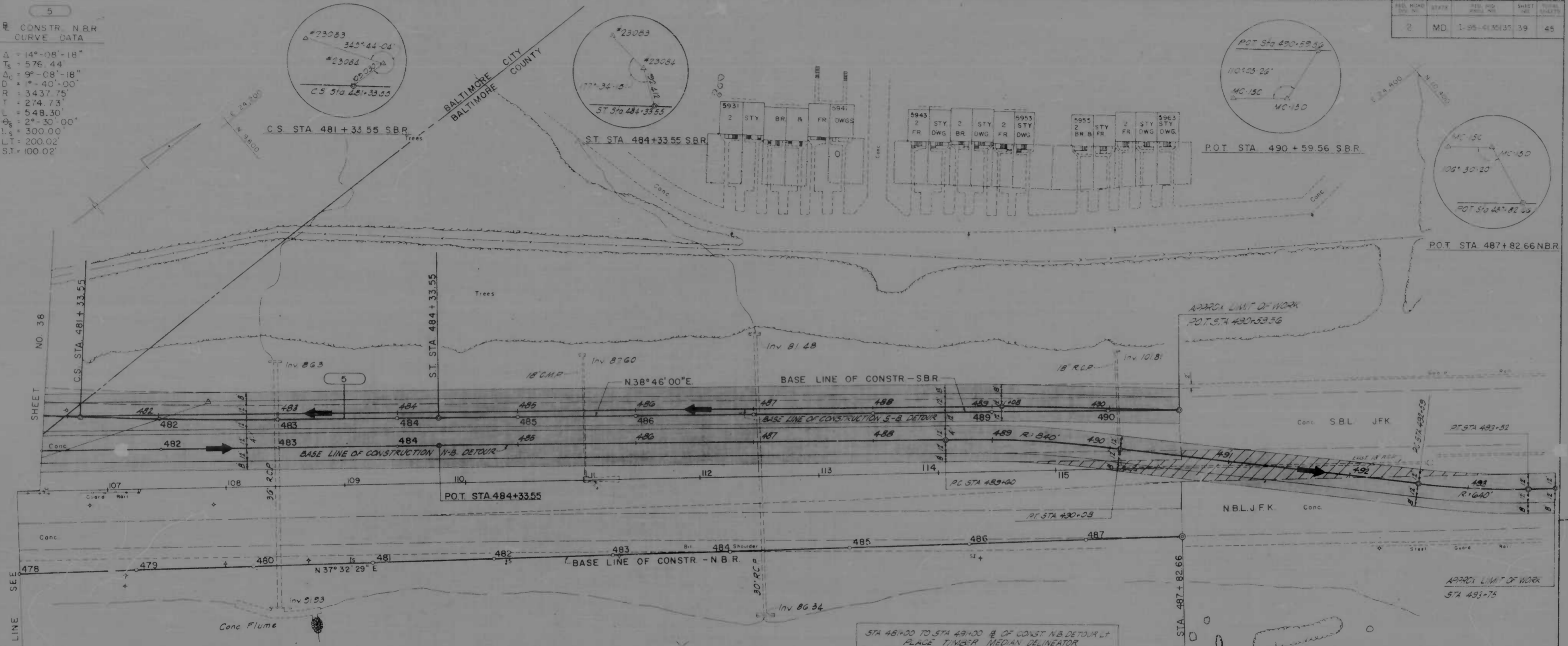


REVISIONS NO. DESCRIPTION DATE 1 CORRECT STATION 12/26/70	CONSULTANT KIMBLE, HENDER, STONE & ASSOC., INC. AND NATL. BRIDGES & ASSOC., INC. CONSULTING ENGINEERS 241 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY SCALE: 1"=40' DATE:	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: P.W.2 TRACED BY: P.W.2 F.A.P. NO.: 1-95-435135 S.R.C. NO.: BC 246-32-818 BALTO. CITY NO.: 1587 SHEET NO.: 38 OF 45
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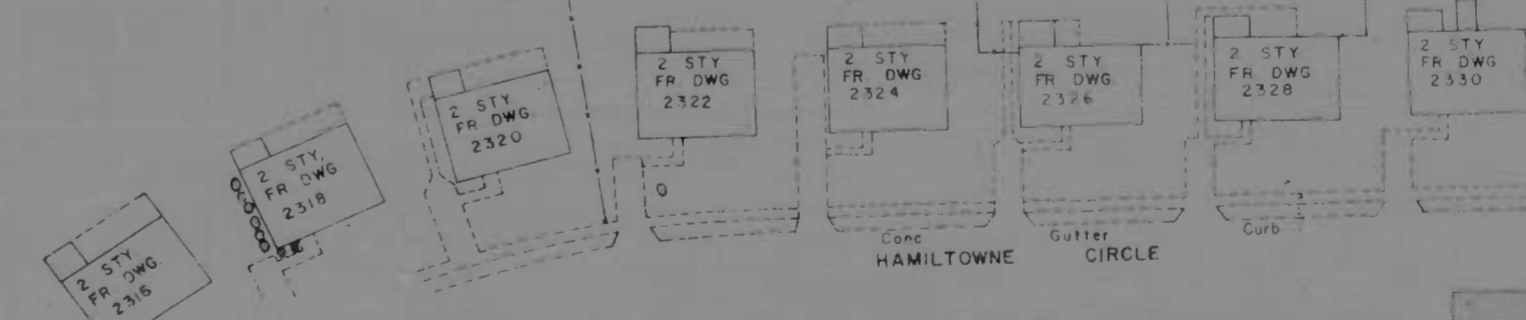
LOCATION OF UTILITY STRUCTURE ON EXISTING ALIGNMENT
 LOCATION OF DETOUR ON TEMP. ALIGNMENT

5
 CONSTR. N.B.R.
 CURVE DATA
 $\Delta = 147^{\circ}08'18''$
 $T_s = 576.44'$
 $D_L = 9^{\circ}08'18''$
 $R = 3437.75'$
 $T = 274.73'$
 $L = 548.30'$
 $e_s = 2^{\circ}30'00''$
 $L_s = 300.00'$
 $LT = 200.02'$
 $ST = 100.02'$

SHEET NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	1-95-4(35)35	39	45

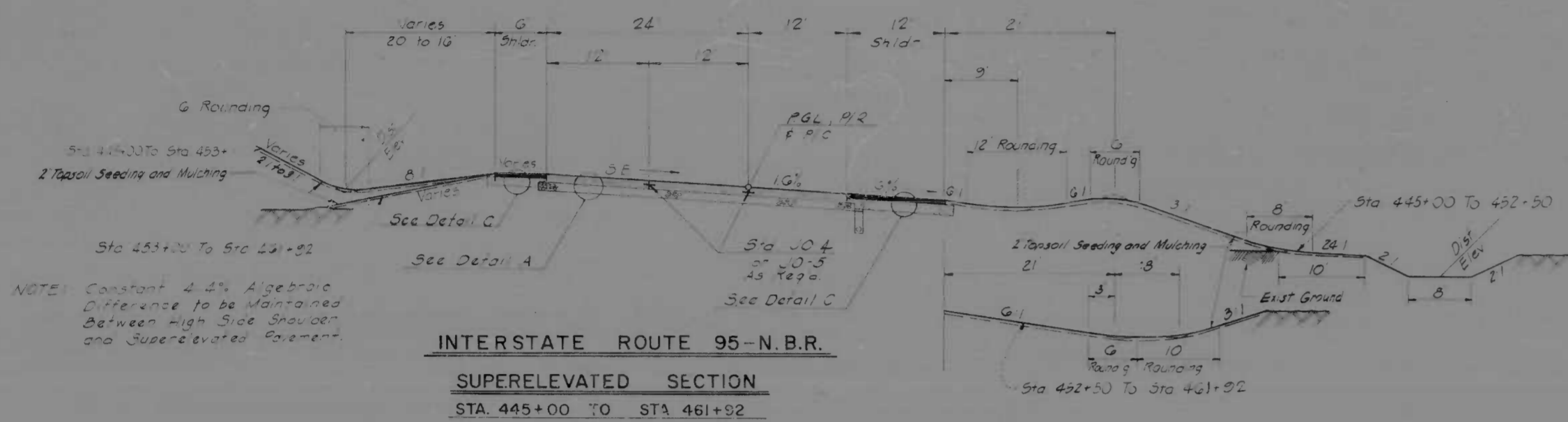


- SEQUENCE OF CONSTRUCTION PHASE II**
- 1) PLACE TEMPORARY PAVEMENT MARKING TAPE TO DELINEATE LANE LINES AS NOTED NOTED ON PLANS PNT TO SHOULDER WHITE-SOLID BETWEEN LANES WHITE-DASHED ON MEDIAN SIDE YELLOW-SOLID
 - 2) ERECT TEMPORARY TIMBER MEDIAN DELINEATOR TO THE LIMITS AS SHOWN ON PLANS
 - 3) RELOCATE EXISTING STATE ROADS COMMISSION BARRICADES ON THE PRESENT S.B.L. OF JFK TO CHANNEL TRAFFIC INTO THE DETOUR AND OPEN LANES TO TRAFFIC
 - 4) BUILD N.B.R. I-95 IN ACCORDANCE WITH CONSTRUCTION PLANS
 - 5) REMOVE TEMPORARY PAVEMENT MARKING TAPE AND BARRICADES FROM S.B.R. I-95 AND S.B.L. JFK PLACE EXISTING STATE ROADS COMMISSION BARRICADE TO BLOCK 2 WESTERLY LANES OF THE JFK AND DIRECT ALL TRAFFIC INTO THE 2 LANES OF S.B.L. JFK RELOCATED REMOVE ALL OTHER BARRICADES AND LAPE TAPE AND OPEN N.B.R. TO TRAFFIC
 - 6) REMOVE TEMPORARY PAVEMENT, TEMPORARY EMBANKMENT AND TEMPORARY DRAINAGE STRUCTURE BETWEEN NBL AND SBL. TEMPORARY PAVEMENT LOCATED IN NORMAL PAVED SHOULDER AREAS OF EXISTING JFK SHALL REMAIN IN PLACE. GUARD RAIL MEDIAN BARRIER SHALL BE RESET AND CONNECTED TO THE NEW PROPOSED GUARD RAIL. ALL DISTURBED SLOPE AREAS OF THE EXISTING JFK SHALL BE RETURNED TO THEIR ORIGINAL CONDITION.
 - 7) REMOVE TEMPORARY PAVEMENT BY SAWING BETWEEN S.B.L. JFK AND S.B.R. I-95 AND GRADE ACCORDING TO CONSTRUCTION PLANS.



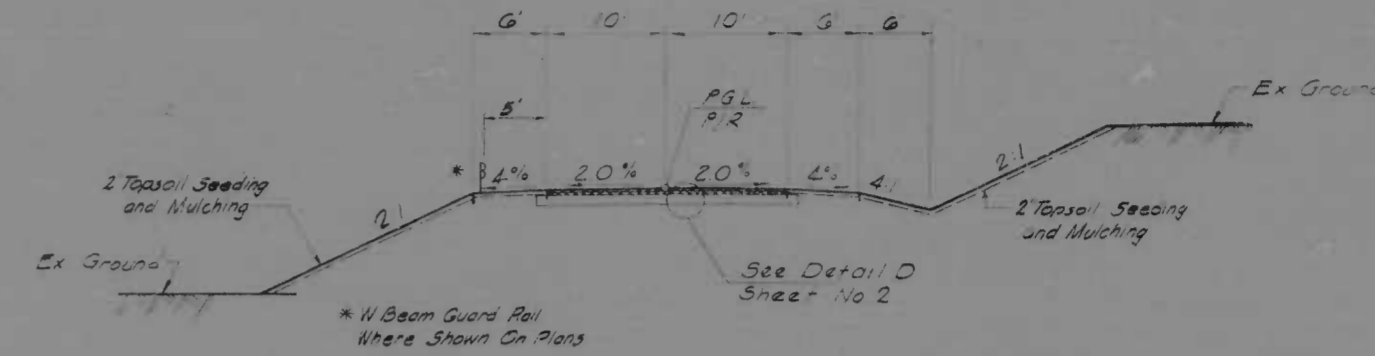
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNOXLE, BUNDA, STONE & ASSOC., INC. AND KATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: P.W.Z. TRACED BY: S.R.C. F.A.P. NO. 1-95-4(35)35 S.B.C. NO. BL 246-32-B15 BALTO. CITY NO. 1985
		SCALE: 1"=40'	DES. BY: J.L.C. CHK. BY: R.W.G. SHEET NO. 39 OF 45

FED. ROAD DIV. NO.	STATE	FIG. AND SHEET NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(35)35	4	45

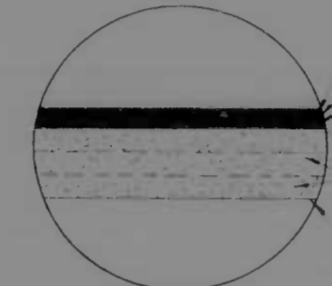


INTERSTATE ROUTE 95-N.B.R.
SUPERELEVATED SECTION
 STA. 445+00 TO STA. 461+92

NOTE: Constant 4.4% Algebraic Difference to be Maintained Between High Side Shoulder and Super-elevated Pavement.

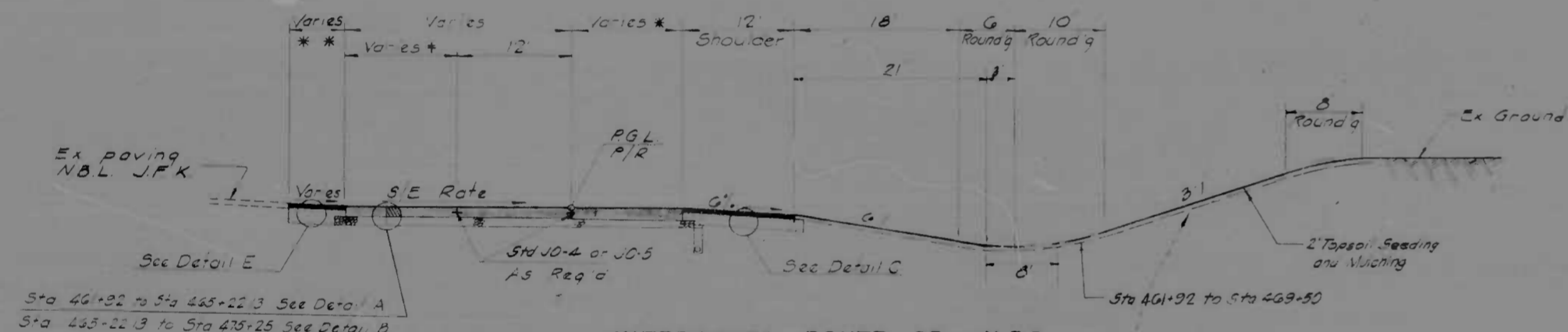


62ND STREET TO PLAINVIEW AVE.
 STA. 9+00 TO STA. 16+00



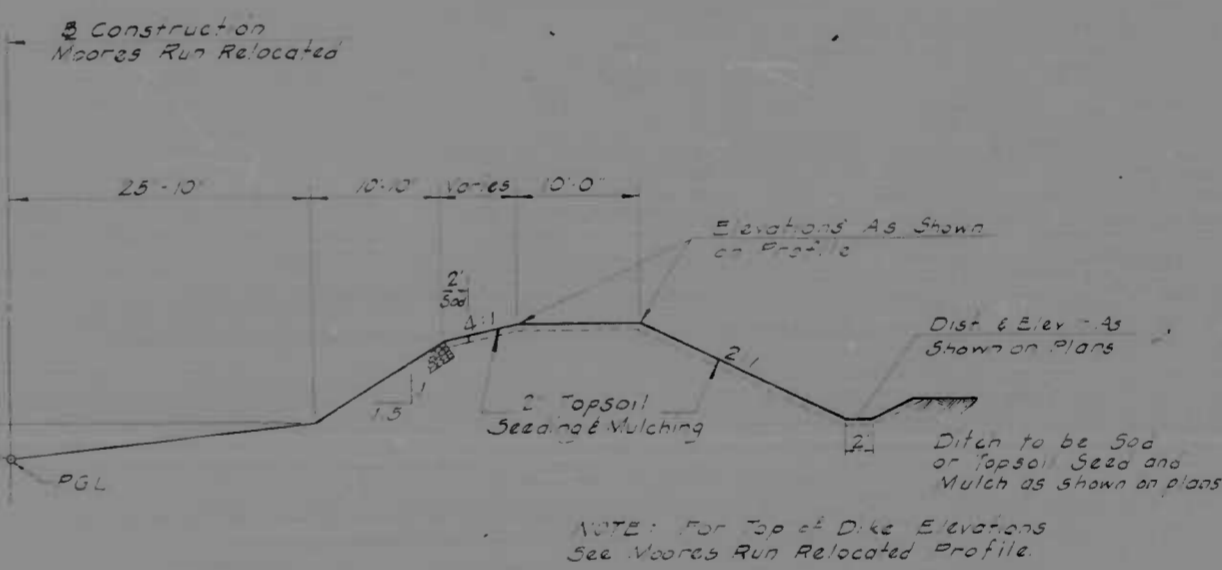
1 Bituminous Concrete Spec B-PC-16
 Bituminous Material for Tack Coat
 3 Bituminous Concrete Spec B-PC-2
 Bituminous Material for Prime Coat
 2 Dense Graded Stabilized Aggregate Base (3' 4' Courses)
 Limit of Class I Excavation and Top of Subgrade

DETAIL E



INTERSTATE ROUTE 95-N.B.R.
TRANSITIONAL SECTION
 STA. 461+92 TO STA. 475+25

NOTE: For Additional Details on Cut, Ditch Slopes, Weirns, Back Slopes, Transition Locations and Lengths See Plan Sheets.

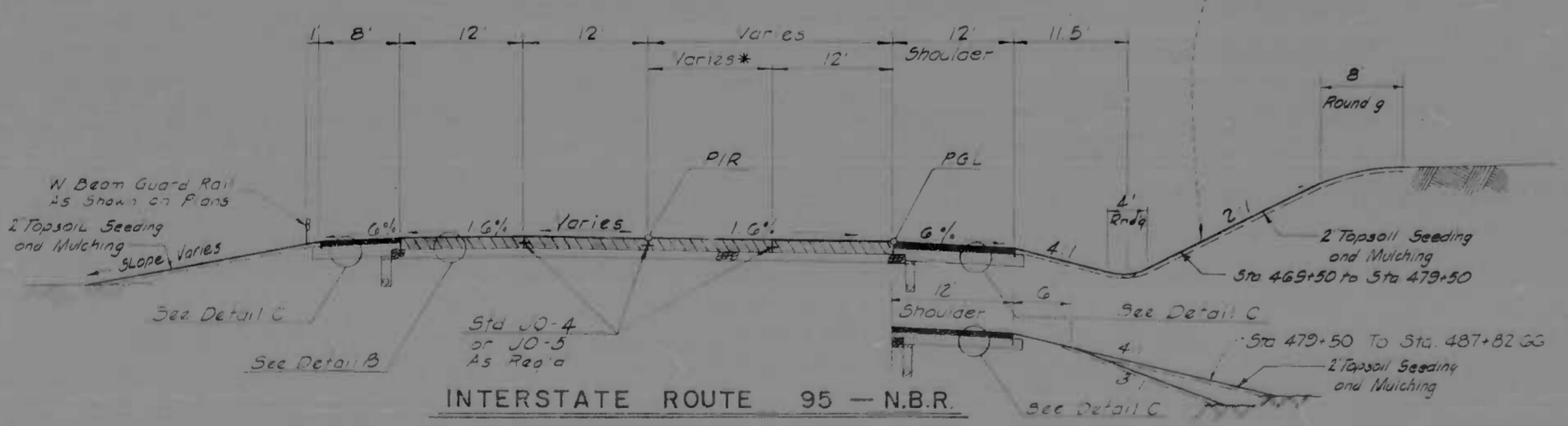


MOORES RUN RELOCATED W/DIKE
 STA. 32+00 TO STA. 42+00



3 Bituminous Concrete Spec B-Band PC-16
 Bituminous Material for Prime Coat
 Variable Depth Dense Graded Stabilized Aggregate (1-3 Course, 1 Variable Depth Course, 1-4 Course)
 Limit of Class I Excavation & Top of Subgrade

DETAIL F

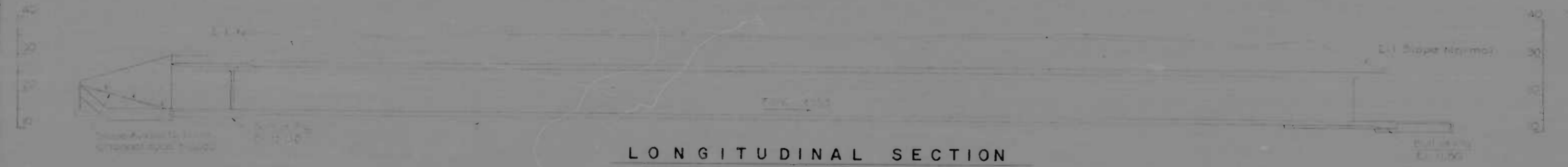
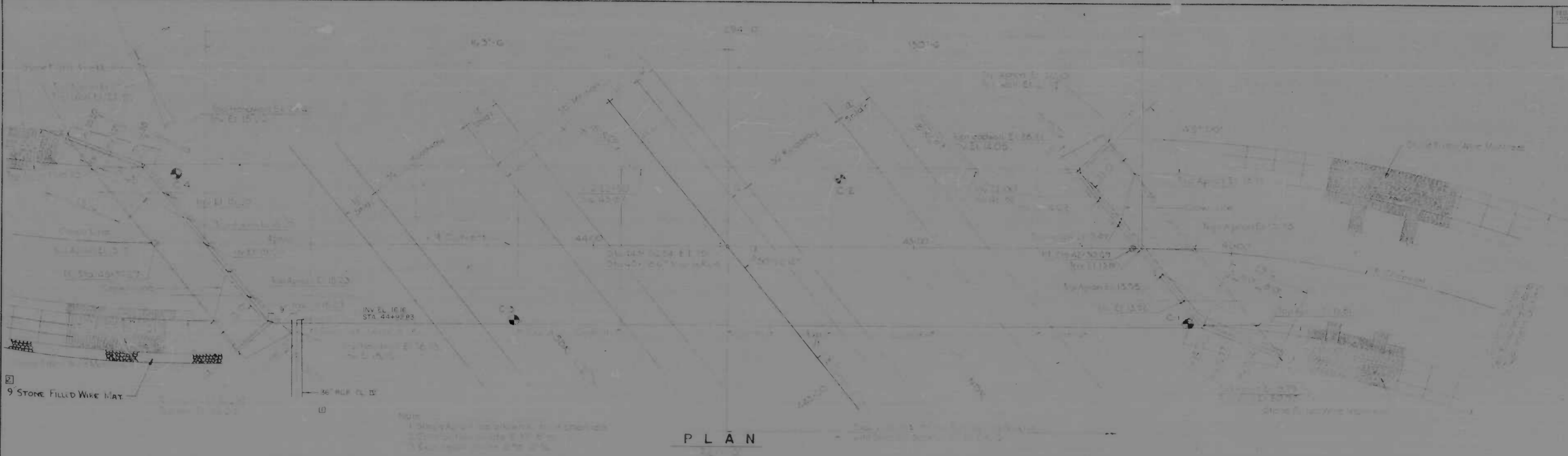


INTERSTATE ROUTE 95-N.B.R.
TIE TO JOHN F. KENNEDY MEMORIAL HIGHWAY
 STA. 475+25 TO STA. 487+82.66

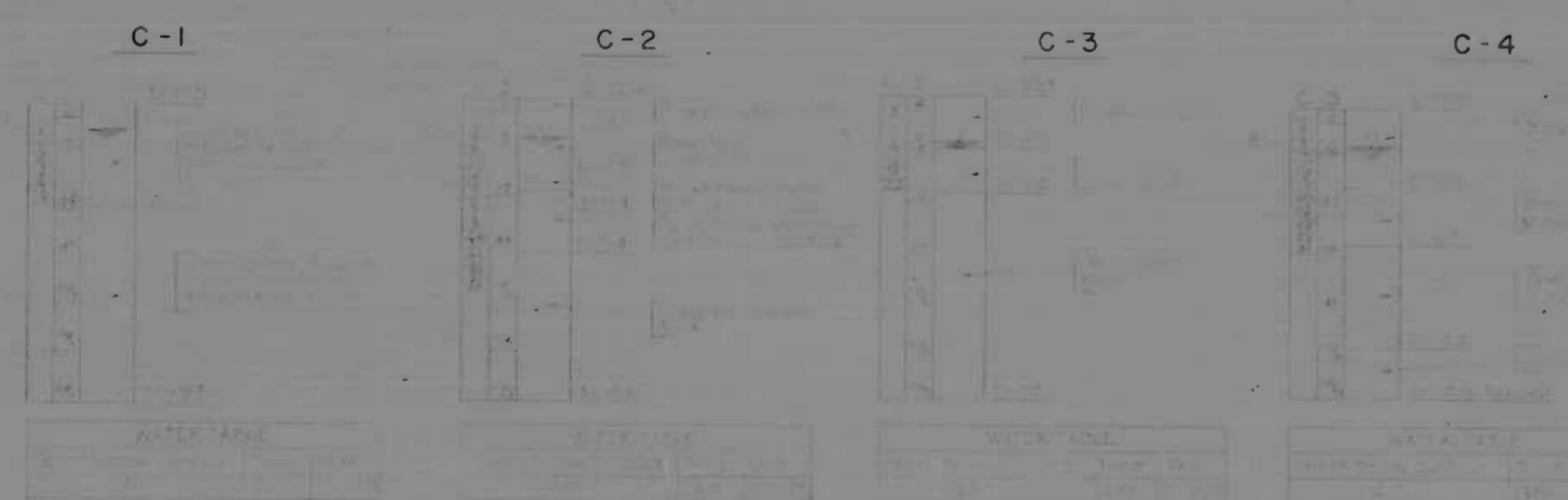
VARIES 12' STA. 481+05 TO 13' STA. 487+82.66

TYPICAL SECTIONS

REVISIONS CONSULTANT KNOXLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY			
	DRAWN BY: J.E.L. TRACED BY: J.E.L.	DES. BY: J.L.C. CHK. BY: R.W.C.	F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983	SHEET NO. 4 of 45

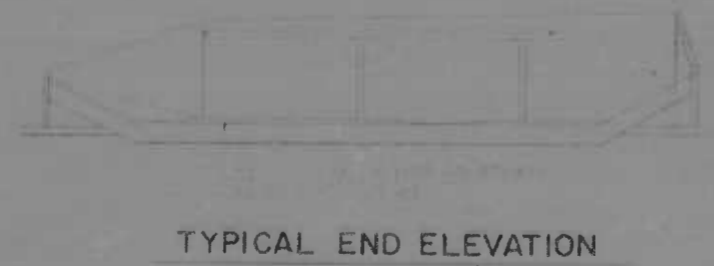


SUMMARY OF QUANTITIES			
CATEGORY	ITEM	MEASUREMENT	QUANTITY
SPACING
...
...
...
...
...
...
...
...



HYDROLOGIC AND HYDRAULIC DATA

DRAINAGE STATION: NONE
 DRAINAGE AREA: NONE SQ. MI. = 0.00 ACRES
 MAXIMUM DISCHARGE: NO RECORD cfs.
 MAXIMUM DISCHARGE OF RECORD: NO RECORD cfs.
 PROPOSED STRUCTURE
 DRAINAGE AREA: 4.28 SQ. MI. = 2740 ACRES
 STEADY STATE DISCHARGE: 1.5 cfs @ 5800
 TIME FLOW: NONE
 TOTAL MAXIMUM DISCHARGE: 5.5 cfs @ 5800
 MAXIMUM FLOW DEPTH AT H.W.: FEET = 12.1
 OPENINGS: 80 FT. = 552.0
 VELOCITY AT OUTLET: FT. PER SEC. = 15.4

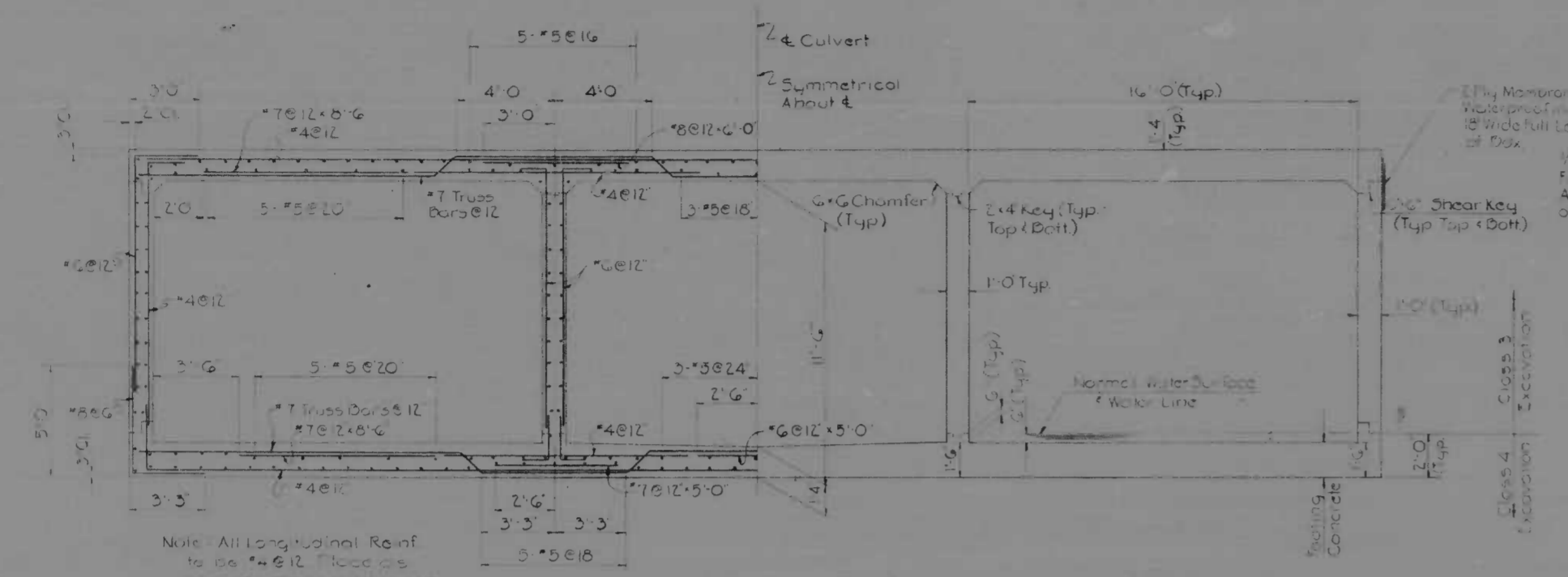


GENERAL NOTES

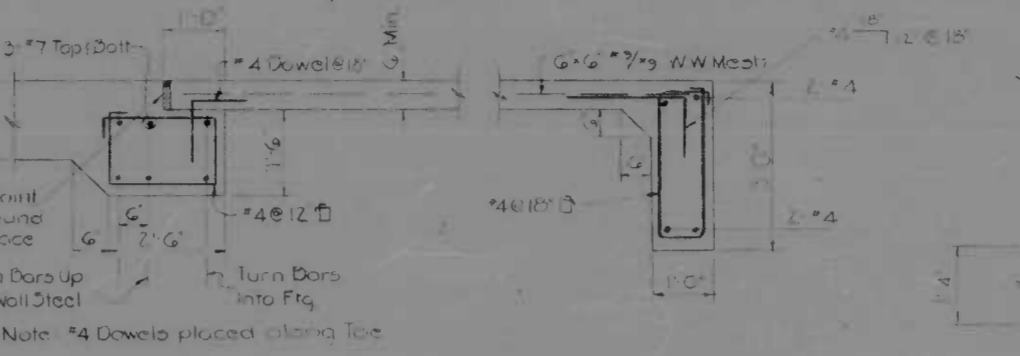
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.
2. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES AND STRUCTURES.
4. THE CONTRACTOR SHALL MAINTAIN THE PROPOSED STRUCTURE AND SURROUNDING AREAS IN GOOD CONDITION.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL DEBRIS AND WASTE MATERIALS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ADJACENT PROPERTIES AND UTILITIES.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES AND STRUCTURES.
8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.
10. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. PIPE AT STA 44+ 12/24/70		INTERSTATE ROUTE 95 I-95 OVER MOORES RUN CULVERT GENERAL PLAN AND ELEVATION	DRAWN BY: J.R.W. DES. BY: J.J.M. TRACED BY: J.R.W. CHK. BY: F.F.M. F.A.P. NO.: 95-4(35)35 S.P.C. NO.: BC 216-32-915 BALTO. CITY NO.: 1993
2. Revisions Wire Basket 11/8/71		SCALE: As Shown	SHEET NO.: 40 of 45

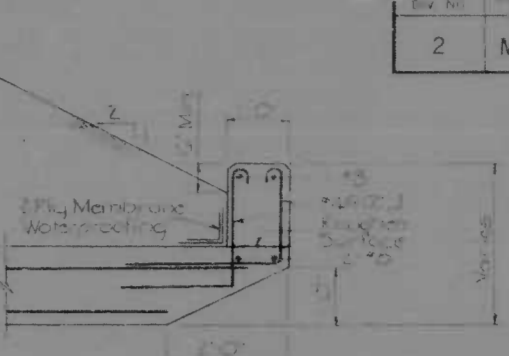
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	1-95-4(35)35	41	45



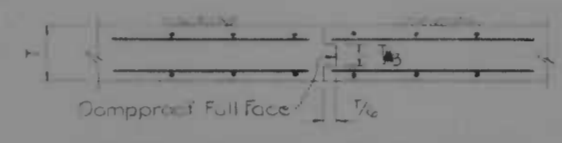
TYPICAL CROSS SECTION THRU BOX CULVERT
1/4" = 1'-0"



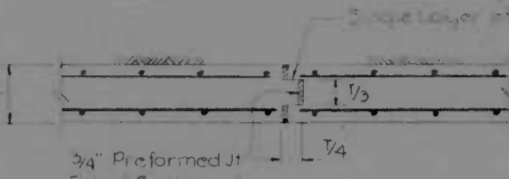
TOE FOOTING AND APRON DETAIL
1/2" = 1'-0"



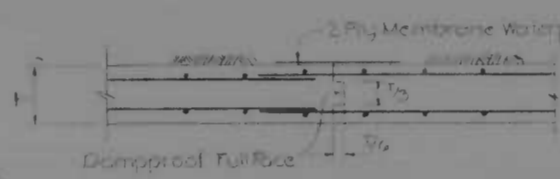
HEADWALL DETAIL
1/2" = 1'-0"



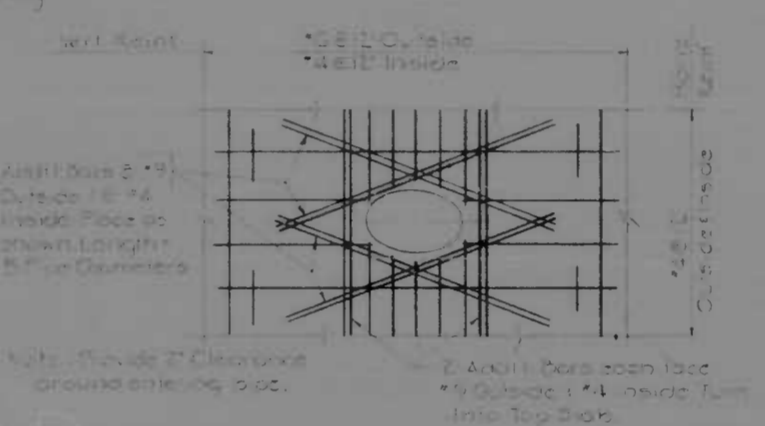
CONTRACTION JOINT DETAIL
1/2" = 1'-0"



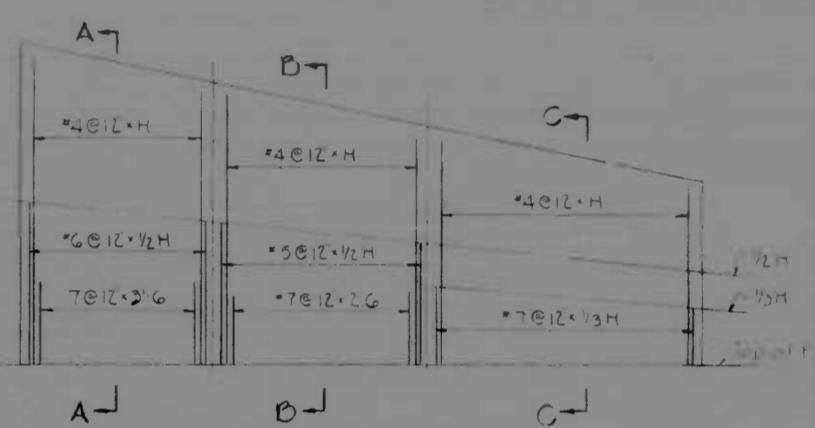
EXPANSION JOINT DETAIL
1/2" = 1'-0"



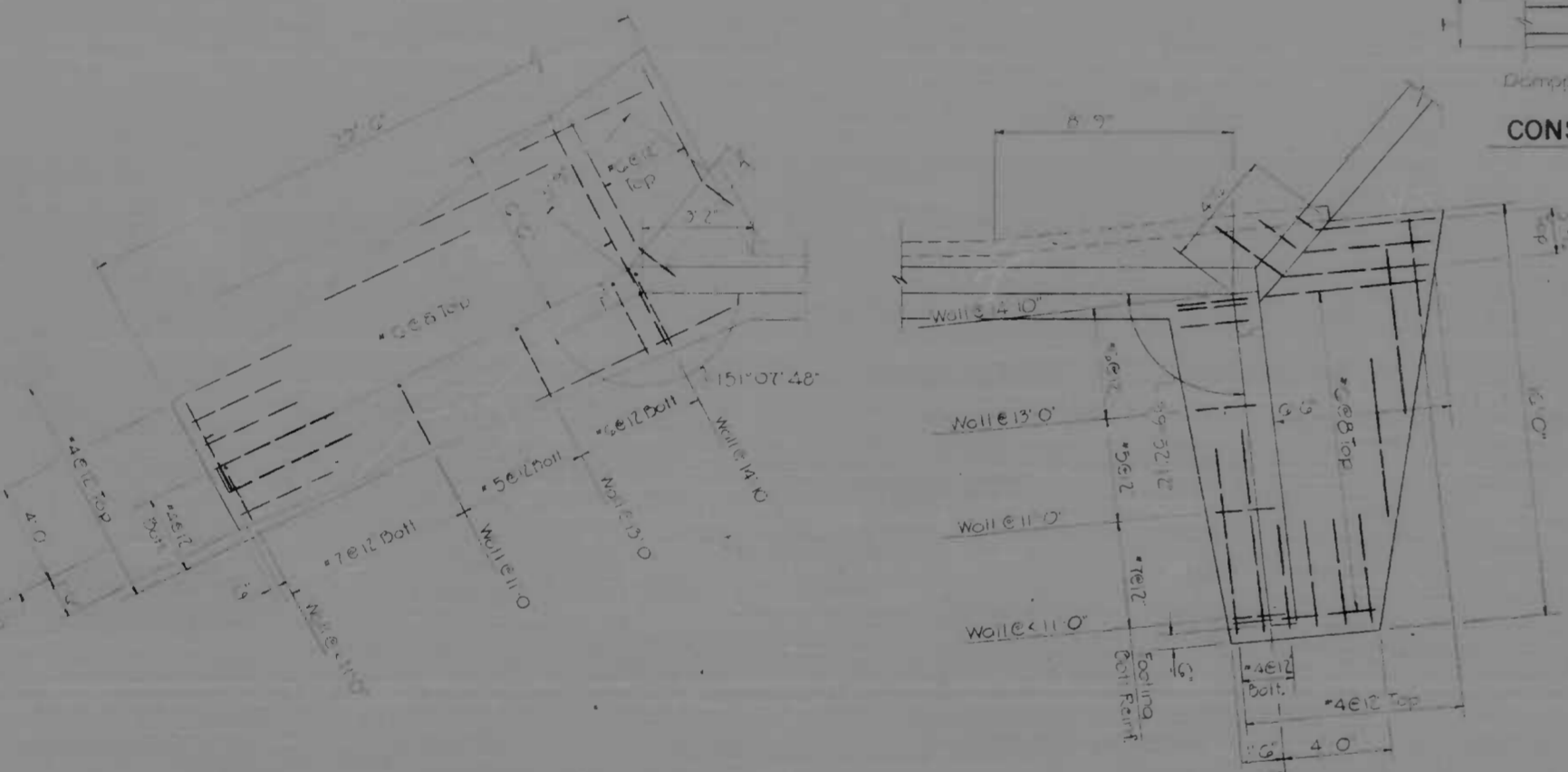
CONSTRUCTION JOINT DETAIL
1/2" = 1'-0"



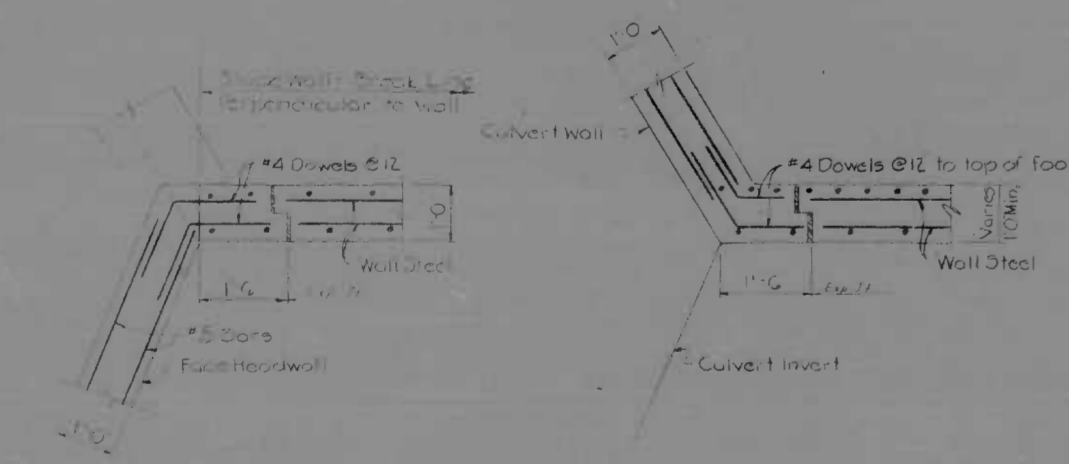
REINFORCING AT PIPE ENTRANCE INTO CULVERT WALLS
1/4" = 1'-0"



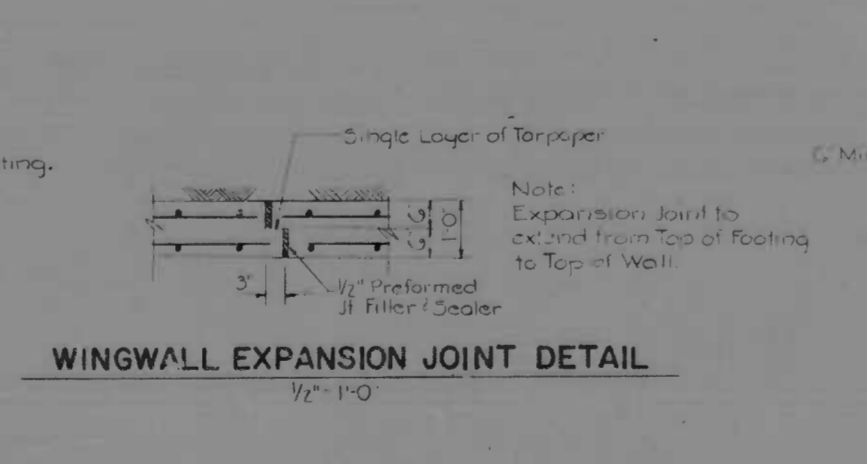
TYPICAL WINGWALL ELEVATION
3/8" = 1'-0"



WINGWALL FOOTING REINFORCEMENT
1/4" = 1'-0"



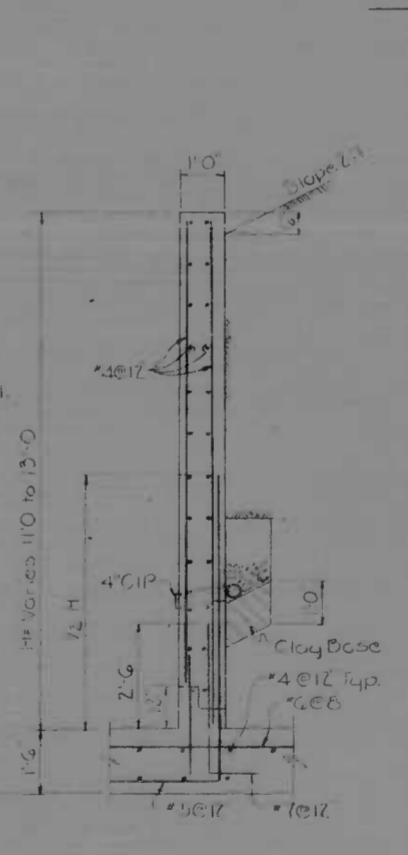
WINGWALL CONNECTION ABOVE TOP SLAB
1/2" = 1'-0"



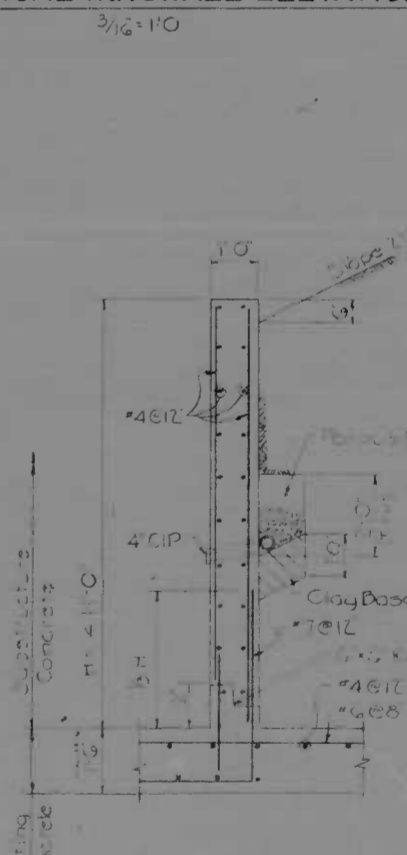
WINGWALL CONNECTION BELOW TOP SLAB
1/2" = 1'-0"



SECTION A-A
3/8" = 1'-0"



SECTION B-B
3/8" = 1'-0"



SECTION C-C
3/8" = 1'-0"

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		INTERSTATE ROUTE 95 1-95 OVER MOORES RUN CULVERT SECTIONS AND DETAILS	DRAWN BY: J.R.W. TRACED BY: J.R.W. F.A.P. NO.: I-95-4(35)35 S.R.C. NO.: BC 246-32-815 BALTO. CITY NO.: 1983
		SCALE: As Shown	DATE: Jan 15, 1970
			DES. BY: J.J.M. CHK. BY: F.E.M. SHEET NO.: 41 of 45

EARTHWORK

SUMMARY

EARTHWORK

ANALYSIS

STATION FROM	STATION TO	CUT	EMBANKMENT	TOPSOIL		UNSUITABLE MATERIAL		CLASS 3	CLASS 4	CUT ADJUSTED	CUT DENSIFIED	REMARKS
				CUT	FILL	CUT	FILL					
Base Line 1-95												
433+00	437+00	5373	41,463			39	550			3251	4236	Note All SF = 24 Unless Noted
437+00	437+00						5076*					6" Root Mat
437+00	445+00	557	62,140			265	1962			3272	4936	See Special Provisions
437+00	442+30						14,486*					6" Root Mat
Surrounding Roadway												
445+00	449+00	521	21,055			592	232			2579	2649	6" Root Mat
445+00	457+00	2,044	18,633			892	1,004			11,52	108.7	6" Root Mat
457+00	467+00	29,000	581	2006	261					27,035	27,066	6" Topsoil SF = 0.97
467+00	475+00	13,283	865	1755	385					11,528	11,82	6" Topsoil SF = 0.97
475+00	487+82	2,364	224	110	262					1184	1140	6" Topsoil * SRC Item SF = 0.97
Surrounding Roadway												
445+00	449+00	654	95409				200			454	427	6" Root Mat
449+00	456+00	65	71,509				1740			165	95	6" Root Mat
456+00	470+00		163856				3971					6" Root Mat
470+00	481+00	1483	40,927	334	435	100	1647			840	792	6" Root Mat (6" Topsoil)
481+00	490+50	2364	1020	155	36					1169	1099	6" Topsoil * SRC Item
5.5' JFC Relief												
476+37	480+00	694	146	31	111					463	435	
62' Planview												
0+00	16+00	2297	704			446	282			1851	1795	6" Root Mat SF = 0.97
100' Run												
32+00	38+00		7282				722					6" Root Mat
38+00	42+00		5995				470					6" Root Mat
42+00	45+50							2525	2025			
45+50	50+00		6363				485					6" Root Mat
50+00	55+00		7682				605					6" Root Mat
Want of Traffic												
		500	500							500	500	See Special Provisions
TOTALS		79645	486397	6691	1951	2394	17060	2523	2023	70560	67963	

CHANNEL EXCAVATION (CLASS 5)

STATION FROM	STATION TO	CUT	EMBANKMENT	CLASS 5	REMARKS
32+00	38+00			7245	6" Root Mat SF = 0.90
38+00	42+00			495	6" Root Mat SF = 0.90
42+00	45+50			2025	6" Root Mat SF = 0.90
45+50	50+00			4323	6" Root Mat SF = 0.90
50+00	55+00			605	6" Root Mat SF = 0.90
TOTALS				2237	18,767

Note: Items marked with * and the SRC Item note are to have State Participation.
 † Approximate Quantities Unsuitable Trash Fill.

CLASS 3 EXCAVATION

FROM GRADING TABLE	
TOTAL CLASS 3 EXCAVATION	2525 C.Y.
LOSS DUE TO HANDLING & DENSIFICATION (10%)	253 C.Y.
TOTAL CLASS 3 AVAILABLE FOR EMBANKMENT	2272 C.Y.

CLASS 4 EXCAVATION

FROM GRADING TABLE	
TOTAL CLASS 4 EXCAVATION	2025 C.Y.
LOSS DUE TO HANDLING & DENSIFICATION (10%)	203 C.Y.
TOTAL CLASS 4 AVAILABLE FOR EMBANKMENT	1822 C.Y.

CLASS "1" EXCAVATION

CUT	79645 C.Y.
PLUS TOPSOIL REMOVED UNDER FILL	1051
PLUS ROOT MAT REMOVED UNDER FILL	17060
PLUS UNSUITABLE TRASH FILL	19562
TOTAL CLASS 1 EXCAVATION	118218 C.Y.
EXCAVATION AVAILABLE FOR EMBANKMENT	
TOTAL CLASS 1 EXCAVATION	118218 C.Y.
MINUS	
TOP SOIL REMOVED IN CUT	6691
TOP SOIL REMOVED UNDER FILL	1051
ROOTMAT REMOVED IN CUT	2096
ROOTMAT REMOVED UNDER FILL	17060
UNSUITABLE TRASH FILL	19562
CUT ADJUSTED	70560 C.Y.
CUT DENSIFIED	67963
PLUS CLASS 2 EXCAVATION AVAILABLE FOR EMBANKMENT	868
CLASS 3 EXCAVATION AVAILABLE FOR EMBANKMENT	2272
CLASS 4 EXCAVATION AVAILABLE FOR EMBANKMENT	1822
CLASS 5 EXCAVATION AVAILABLE FOR EMBANKMENT	14877
TOTAL EXCAVATION AVAILABLE FOR EMBANKMENT	87802 C.Y.

CLASS "2" EXCAVATION

FROM PIPE SHEET	
TOTAL CLASS 2 EXCAVATION	964 C.Y.
LOSS DUE TO HANDLING AND DENSIFICATION (10%)	96
TOTAL CLASS 2 EXCAVATION AVAILABLE FOR EMBANKMENT	868 C.Y.

CLASS "5" EXCAVATION

FROM GRADING TABLE - TOTAL CLASS 5 EXCAVATION	18767 C.Y.
LOSS DUE TO ROOT MAT REMOVED	2237
	16530
LOSS DUE TO HANDLING AND DENSIFICATION (10%)	1653
TOTAL CLASS 5 AVAILABLE FOR EMBANKMENT	14877 C.Y.

EMBANKMENT REQUIRED

EMBANKMENT	486397 C.Y.
REFILL FOR TOPSOIL REMOVED UNDER FILL	1051
REFILL FOR ROOT MAT REMOVED UNDER FILL	17060
REFILL FOR UNSUITABLE TRASH FILL	19562
EMBANKMENT REQUIRED	524070
EXCAVATION AVAILABLE FOR EMBANKMENT	87802
BORROW REQUIRED	43717
BORROW DENSIFICATION (10%)	43717
TOTAL BORROW	480885 C.Y.

PROPOSAL QUANTITIES

ITEM	PROPOSAL QUANTITY
CLASS 1 EXCAVATION	118300 C.Y.
CLASS 1A EXCAVATION	4000 C.Y.
CLASS 2 EXCAVATION	1000 C.Y.
CLASS 3 EXCAVATION	19000 C.Y.
BORROW EXCAVATION	481000 C.Y.
CONTINGENT BORROW EXCAVATION	25000 C.Y.

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KIMBLE, FENDER, STONE & ASSOC., INC. AND WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: D.L.T. TRACED BY: D.L.T. DES. BY: J.L.C. CHK. BY: R.W.C.
		SCALE: NONE	F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		DATE	SHEET NO. 42 OF 45

TABULATION OF PAVING & SHOULDER QUANTITIES

SHEET NO.	ITEM NO.	LOCATION	PAVING COURSES											TRAFFIC MAINTENANCE										REMARKS												
			501	502	503	504	505	506	507	508	510	511	512	513	514	516	600	517	518	601	602	603	604		605	607	608	609	610	611	612	613	614	617	618	
			3" SUB-BASE USING CRUSHER RUN	4" SUB-BASE USING CRUSHER RUN	6" SUB-BASE USING CRUSHER RUN	3" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	4" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	CALCIUM CHLORIDE FOR 3" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	CALCIUM CHLORIDE FOR 4" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	CALCIUM CHLORIDE FOR VARIABLE DEPTH DENSE GRADED STABILIZED AGGREGATE BASE COURSE	BITUMINOUS MATERIAL FOR PRIME COAT	BITUMINOUS CONCRETE SPEC B' USING BAND PF-1, SLAG	BITUMINOUS CONCRETE SPEC B' USING BAND PC-1-6, STONE	BITUMINOUS CONCRETE SPEC B' USING BAND P-2 OR P-2 SL-7	5" REINFORCED CEMENT CONCRETE PAVEMENT, C.L.P.	10" PLAIN CEMENT CONCRETE SHOULDER C.L.P.	8" CONTINUOUS REINFORCED CEMENT CONCRETE PAVEMENT, C.L.P.	TERMINAL JOINTS	3" STABILIZED SHOULDERS, DENSE GRADED STABILIZED AGGREGATE	4" STABILIZED SHOULDERS, DENSE GRADED STABILIZED AGGREGATE	VARIABLE DEPTH STABILIZED SHOULDER, DENSE GRADED STABILIZED AGGREGATE	BITUMINOUS CONCRETE SHOULDERS SPEC B' BAND PC-1-6	BITUMINOUS CONCRETE SHOULDERS SPEC B' BAND P-2		CONCRETE BARRIER CLASS A-1 CONCRETE	GUARD RAIL W BEAM	GUARD RAIL W BEAM APPROACH FLARE	REMOVE AND RESET EXISTING GUARD RAIL W BEAM	6" CHAIN LINK FENCE	6" CHAIN LINK FENCE WITH BARBED WIRE	TERMINAL POSTS FOR 6" CHAIN LINK FENCE	TERMINAL POSTS FOR 6" CHAIN LINK FENCE WITH BARBED WIRE	RESET EXISTING FENCE	REMOVE EXISTING FENCE		
SY	SY	SY	SY	SY	SY	SY	SY	SY	TON	TON	TON	SY	SY	SY	LF	SY	SY	SY	TON	TON	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	
		STA 434+00 TO STA 439+00	4222	4222				2754	2634	523	669			223	4000	72		2754	2634	523	669			500					1542		7			0		
		STA 439+00 TO STA 449+00 NBR & SBR	8444	8444				4464	4223	1047	1333			444	8000	0		444	4223	1047	730			1000					1260	684	8	3		0		
		STA 449+00 TO STA 457+00 NBR	3380	3380				1645	1645		494				3200			1645	1645		267									860		4		66	66	
		STA 449+00 TO STA 460+00 SBR 62 nd ST - PLAINVIEW AVE	2549	2549	369			1323	1111	1323	479			592	636	2412	36		1323	1111	1323	256			1430							5				
		STA 460+00 TO STA 470+00 SBR	3868	3868	354			2093	1754	2093	620			386	889	3660	36		2093	1754	2093	349			2000					178		3		960	960	
		STA 457+00 TO STA 467+00 NBR 62 nd ST - PLAINVIEW AVE	3471	3471	751			1702	4927		773			712		3288	36		1702	4927		326	146			350				374		5		1020	1020	
		STA 467+00 TO STA 478+00 NBR			3208			1749	2649		615			3918					1749	2649		302	50							304		2		1263	1263	
		STA 470+00 TO STA 481+00 SBR	4633	4633				1779	4656	1099	775				493	4400			1779	4656	1099	338	193	1110								3				
		STA 476+37.56 TO STA 481+00 JFK			1773			423	1005		185			1685					423	1005		80	32			60	2									
		STA 478+00 TO STA 487+00 NBR STA 481+00 TO STA 490+39.56 SBR						5494	2239		672				5276				2239	2239		364				970										
			1371	1371	4106			688	5113		649			3923		1334	48		688	5113		191	240			960										
		TOTAL	32,334	32,334	16,055	3328		24,257	31,356	6155	7773	84		170				16,426	26,655	30,294								2802	3200	15	25		3859	3859		
		MAINTENANCE OF TRAFFIC PHASE I																																		
		STA 792+00 TO STA 106+ JFK						1016		1012		184	552																							
		STA 106+ TO STA 119+ JFK *						4184		1418		740	780																							
		PHASE II																																		
		STA 792+00 TO STA 106+ JFK						7182		7182		192	335																							
		STA 106+ TO STA 119+ JFK *						1629		1629		30	90																							
		TOTAL						33,111		33,111		321	1817																							
		SRC Participation	1371	1371	9600			21,266	2927	28718		3457	392	1175	9209		1334		2927	7352		555	240		1930											
		Baltimore City Participation	30,963	30,963	6,455	3328		11,749	21,329	35,749		6155	7627	84	214	812	228		18,002	24,004	6193	3013	361	6040	410	2										
		GRAND TOTAL	32,334	32,334	16,055	3328		24,257	64,467	61,55	11,084	84		606	1987			16,426	26,885	30,294								2802	3200	15	25		3859	3859		

* Quantities Computed Based on Stone For Slag Quantities See Alternate
* Denotes SRC Participation

LANDSCAPING QUANTITIES	
LOCATION	2" TOPSOIL SEEDING AND MULCHING
MAINLINE	
Sta 434+00 to Sta 439+00	9330 SY
Sta 439+00 to Sta 445+00	9330 SY
Sta 445+00 to Sta 449+00 SBR	5093 SY
Sta 449+00 to Sta 449+00 NBR	3703 SY
Sta 449+00 to Sta 457+00 NBR	9 96 SY
Sta 449+00 to Sta 456+00 SBR	10931 SY
Sta 459+50 to Sta 470+00 SBR	22273 SY
Sta 457+00 to Sta 467+00 NBR	10031 SY
Sta 467+00 to Sta 478+00 NBR	6690 SY
Sta 470+00 to Sta 481+00 SBR	9085 SY
* Sta 478+00 to Sta 487+00 NBR	1456 SY
* Sta 481+00 to Sta 490+50 SBR	2790 SY
Sta 476+50 to Sta 480+00 SBL-JFK	428 SY
MOURES RUN	
Sta 33+00 to Sta 38+00	4081 SY
Sta 38+00 to Sta 42+00	2541 SY
Sta 43+50 to Sta 50+00	2163 SY
Sta 50+00 to Sta 55+50	2719 SY
62 nd St - Plainview Ave	2302 SY
SUB-TOTAL	115,320 SY
Less Rock Filled Wire Mattress Ditches	- 4995 SY
Less Concrete Ditches	- 1998 SY
Less Sod Ditches	- 3791 SY
SUB-TOTAL	98,536 SY
Plus 10%	9853 SY
TOTAL =	108,434 SY
PROPOSAL QUANTITY	109,000 SY

* Denotes SRC Participation

TOPSOIL ANALYSIS	
TOPSOIL REQUIRED	2" Topsoil (109,000 x 1/8) 6056 CY
TOPSOIL AVAILABLE	
Salvaged Topsoil from Cut	6691 CY
Salvaged Topsoil from Fill	1951 CY
Total Topsoil Available	8642 CY
Less 10% Shrinkage	864 CY
Total Topsoil Available for Placement	7778 CY
APPARENT ADDITIONAL TOPSOIL REQUIRED	0 CY
PLACING SALVAGED TOPSOIL	2" Depth = (6056 x 18) 109,000 SY

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNOXELL, WENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21201	& INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: D.L.T. TRACED BY: D.L.T. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		DES BY: J.L.C. CHK. BY: R.W.C.	SHEET NO. 44 OF 45

SUMMARY OF QUANTITIES

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	E-95-4(35)35	45	45

IDEN/NO.	PAY ITEMS	UNIT	BALTO CITY QUANT.	S.R.C. QUANT.	CONT. QUANT.	PROP. QUANT.	FINAL QUANT.
PRELIMINARY ITEMS							
101	CLEARING AND GRUBBING	LS				LUMP SUM	
102	REMOVAL AND DISPOSAL OF EXISTING BUILDINGS	LS				LUMP SUM	
103	ENGINEER'S FACILITY - F.O.S.	LS				LUMP SUM	
104	MAINTENANCE OF TRAFFIC	LS				LUMP SUM	
105	CONSTRUCTION STAKEOUT	LS				LUMP SUM	
106	MOBILIZATION	LS				LUMP SUM	
GRADING ITEMS							
20	CLASS I EXCAVATION	CY	112,792	5,508		118,300	
*201	CLASS 1A EXCAVATION	CY			4,000	4,000	
203	CLASS 2 EXCAVATION	CY	1,000			1,000	
204	CLASS 3 EXCAVATION	CY	18,800			18,800	
205	BORROW EXCAVATION	CY	481,000			481,000	
*206	CONTINGENT BORROW EXCAVATION	CY			25,000	25,000	
*207	TEST PIT EXCAVATION	CY			50	50	
208	REMOVAL OF EXISTING PAVEMENT	SY	5,683	5,217		10,900	
DRAINAGE ITEMS							
*301	CLASS 3 EXCAVATION FOR INCIDENTAL CONSTRUCTION	CY			50	50	
302	SELECTED BACKFILL USING #6 AGGREGATE	CY	550		1,100	1,100	
303	SELECTED BACKFILL USING CR-G	CY	550		1,100	1,100	
304	5" RCC PIPE CLASS II	LF	38			38	
305	18" RCC PIPE CLASS II	LF	154	260		414	
306	21" RCC PIPE CLASS II	LF	84			84	
307	24" RCC PIPE CLASS II	LF	454			454	
308	36" RCC PIPE CLASS II	LF	100			100	
309	18" B.C.C.M. PIPE, TYPE C, #16 GA	LF	52			52	
310	18" B.C.C.M. PIPE, TYPE C, #16 GA	LF	502			502	
311	36" B.C.C.M. PIPE TYPE C, #14 GA	LF	56			56	
312	36"x1.5" PIPE CONNECTION BRICK V	EA	1			1	
313	18" B.C.C.M. PIPE, 15° ELBOW, TYPE C, 16 GA	EA	2			2	
314	18" B.C.C.M. PIPE, 22 1/2° ELBOW, TYPE C, 16 GA	EA	14			14	
315	FLAP GATE FOR 36" B.C.C.M. PIPE	EA	2			2	
316	REMOVAL OF OLD PIPE CULVERTS ANY SIZE, MIN. 12"	LF	338			338	
317	STD TYPE C ENDWALL FOR 18" PIPE	EA	4			4	
318	STD TYPE C ENDWALL FOR 36" PIPE	EA	1			1	
319	STD CONCRETE END SECTION FOR 18" RCC PIPE	EA	3			3	
320	STD CONCRETE END SECTION FOR 24" RCC PIPE	EA	4			4	
321	STD CONCRETE END SECTION FOR 36" RCC PIPE	EA	1			1	
322	STD METAL END SECTION FOR 18" B.C.C.M. PIPE	EA	3			3	
323	STD METAL END SECTION FOR 36" B.C.C.M. PIPE	EA	2			2	
324	STD TYPE K INLET SINGLE GRATE - MINIMUM DEPTH	EA	5	1		6	
325	STD TYPE K INLET DOUBLE GRATE TANDEM, MINIMUM DEPTH	EA	2			2	
326	SPECIAL TYPE K INLET, NON-TRAFFIC, MINIMUM DEPTH	EA	3			3	
327	STD TYPE K INLET SINGLE GRATE, VERTICAL DEPTH	LF	5	1		6	
328	STD TYPE K INLET DOUBLE GRATE TANDEM, VERTICAL DEPTH	LF	4			4	
329	SPECIAL TYPE K INLET, NON-TRAFFIC, VERTICAL DEPTH	LF	9			9	
330	MODIFIED TYPE S COMB INLET, DOUBLE GRATE TANDEM, MINIMUM DEPTH	EA	12			12	
331	MODIFIED TYPE S COMB INLET, DOUBLE GRATE TANDEM, VERTICAL DEPTH	LF	12			12	
332	ADJUST EXISTING INLET	EA				1	
333	STD 4" CIRCULAR MANHOLE - MINIMUM DEPTH	EA	1			1	
334	STD MANHOLE - VERTICAL DEPTH	LF	3			3	
335	ABANDON EXISTING MANHOLE	EA	1			1	
*336	CLASS P-1 CONCRETE FOR MISCELLANEOUS STRUCTURES	CY			25	25	
*337	ORDINARY BRICK MASONRY FOR MISCELLANEOUS STRUCTURES	CY			5	5	

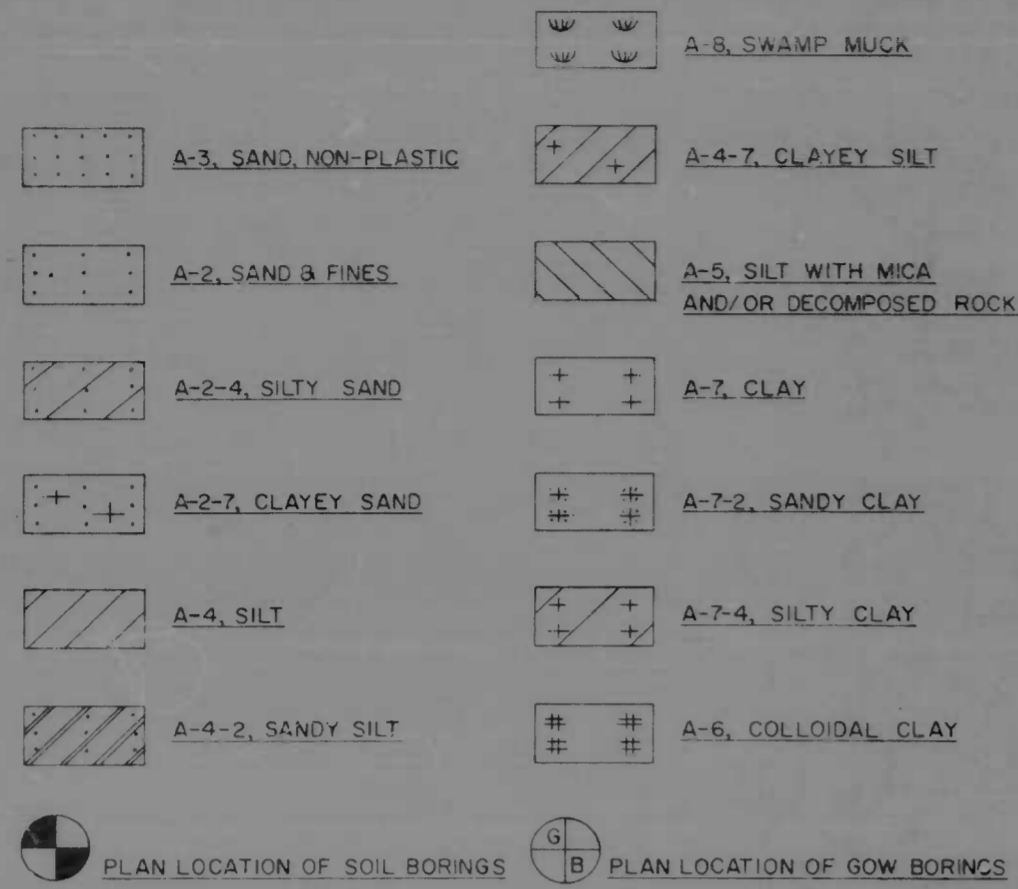
* CONTINGENT ITEM

IDEN/NO.	PAY ITEMS	UNIT	BALTO CITY QUANT.	S.R.C. QUANT.	CONT. QUANT.	PROP. QUANT.	FINAL QUANT.
DRAINAGE ITEMS (cont.)							
338	3" PERFORATED CIRCULAR PIPE LONGITUDINAL UNDERDRAIN	LF	12,885	3,885		16,850	
339	6" PERFORATED CIRCULAR PIPE UNDERDRAIN	LF			500	500	
340	8" PERFORATED CIRCULAR PIPE UNDERDRAIN	LF			300	300	
341	6" CIRCULAR PIPE UNDERDRAIN OUTLETS	LF	495	80		575	
342	8" CIRCULAR PIPE UNDERDRAIN OUTLETS	LF			100	100	
343	AGGREGATE BACKFILL FOR UNDERDRAIN	CY			25	25	
344	5" CONCRETE GUTTER	SY	1,865	95		1,960	
345	STONE FILLED WIRE-MATRESS DITCH PROTECTION	SY	4,995			4,995	
346	STONE FILLED WIRE-MATRESS CHANNEL PROTECTION	CY	4,270			4,270	
STRUCTURE ITEMS							
401	CLASS #3 EXCAVATION FOR STRUCTURES	CY	2,600			2,600	
402	CLASS #4 EXCAVATION	CY	2,100			2,100	
*403	SUBFOUNDATION DRILLING	LF			10	10	
*404	CLASS C CONCRETE FOR SUBFOUNDATION	CY			20	20	
*405	CONTINGENT CONCRETE FOR BOX CULVERT	CY			10	10	
408	TRIPLE REINFORCED CONCRETE BOX CULVERT W/APRON @ STA. 443+50.84	LS	LUMP SUM			LUMP SUM	
PAVING ITEMS							
501	3" SUB-BASE USING CRUSHER RUN	SY	30,970	1,380		32,350	
502	4" SUB-BASE USING CRUSHER RUN	SY	30,970	1,380		32,350	
503	6" SUB-BASE USING CRUSHER RUN	SY	6,500	9,600		16,100	
504	3" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	SY	3,350			3,350	
505	4" DENSE GRADED STABILIZED AGGREGATE BASE COURSE	SY	11,765	21,385		33,150	
506	CALCIUM CHLORIDE FOR 3" DENSE GRADED STABILIZED AGGREGATE	SY	21,400	2,950		24,350	
507	CALCIUM CHLORIDE FOR 4" DENSE GRADED STABILIZED AGGREGATE	SY	35,805	28,745		64,550	
508	CALCIUM CHLORIDE FOR VARIABLE DEPTH DENSE GRADED STABILIZED AGGREGATE	SY	6,160			6,160	
509	CRUSHER RUN AGGREGATE FOR MAINTENANCE OF TRAFFIC	TON			100	100	
510	BITUMINOUS MATERIAL FOR PRIME COAT	GAL	7,630	3,470		11,100	
511	BITUMINOUS CONCRETE, SPEC B, USING BAND P-1 (SLAG)	TON	100			100	
512	BITUMINOUS CONCRETE, SPEC B, USING BAND P-1-G1 (STONE)	TON	214	396		610	
513	BITUMINOUS CONCRETE, SPEC B, USING BAND P-2	TON	815	1,185		2,000	
514	BITUMINOUS CONCRETE, SPEC B, USING BAND P-2.5L	TON	755	1,085		1,840	
515	BITUMINOUS CONCRETE, SPEC B, FOR MAINTENANCE OF TRAFFIC (STONE OR SLAB)	TON			50	50	
516	3" REINFORCED CEMENT CONCRETE PAVEMENT, CLASS-P	SY	7,223	9,227		16,450	
517	3" REINFORCED CEMENT CONCRETE PAVEMENT, CLASS-P	SY	28,960	1,340		30,300	
518	TERMINAL JOINTS	LF	228			228	
*519	CALCIUM CHLORIDE	TON			10	10	
SHOULDER ITEMS							
601	3" STABILIZED SHOULDER, DENSE GRADED STAB AGGREGATE	SY	18,050	2,850		21,000	
602	4" STABILIZED SHOULDER, DENSE GRADED STAB AGGREGATE	SY	24,040	7,960		31,400	
603	VARIABLE DEPTH STABILIZED SHOULDER DENSE GRADED STAB AGGREGATE	SY	6,160			6,160	
604	BITUMINOUS CONCRETE SHOULDERS, SPEC B, BAND P-1-G1	TON	3,040	560		3,600	
605	BITUMINOUS CONCRETE SHOULDERS, SPEC B, BAND P-2	TON	360	240		600	
606	10" PLAIN CEMENT CONCRETE SHOULDER, CLASS P	SY	2,700			2,700	
607	CONCRETE BARRIER CLASS A-1 CONCRETE	LF	6,040			6,040	
608	GUARD RAIL W BEAM	LF	420	1,930		2,350	
609	GUARD RAIL W BEAM APPROACH FLARE	EA	2			2	
610	REMOVE AND RESET EXISTING GUARD RAIL W BEAM	LF		300		300	
611	G CHAIN LINK FENCE	LF	2,850			2,850	
612	G CHAIN LINK FENCE WITH BARBED WIRE	LF	3,200		300	3,500	
613	TERMINAL POST FOR G CHAIN LINK FENCE	EA	15			15	
614	TERMINAL POST FOR G CHAIN LINK FENCE WITH BARBED WIRE	EA	25		10	35	
*615	CORNER POST G CHAIN LINK FENCE WITH BARBED WIRE	EA			10	10	
*616	LINE POST FOR G CHAIN LINK FENCE WITH BARBED WIRE	EA			10	10	
617	RESET EXISTING FENCE	LF	3,860			3,860	
618	REMOVE EXISTING FENCE	LF	3,860			3,860	
LANDSCAPING ITEMS							
701	PLACING SALVAGED TOPSOIL 2' DEPTH	SY	102,754	4,246		107,000	
702	SEEDING AND MULCHING	SY	102,754	4,246		107,000	
703	SOLID SODDING	SY	9,900	30		9,930	
704	TEMPORARY SEEDING	SY	20,250	4,750		25,000	

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KNORRLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: P.W.Z. TRACED BY: P.W.Z. F.A.P. NO. E-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
		SCALE: _____ DATE: _____	DES. BY: J.L.C. CHK. BY: P.W.C. SHEET NO. 45 of 45

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	5	45

SOIL LEGEND



□ IN-PLACE DRY DENSITY (DEPTH)
 _____ PCF % MOISTURE (DATE)
 PROFILE VERTICAL SCALE: 1"=10'

GENERAL LOCATION OF REQUIRED MATERIAL REMOVAL

LL - LIQUID LIMIT PL - PLASTICITY INDEX NP - NON PLASTIC
 M.D.D. B.O.M.C. - MAXIMUM DRY DENSITY AND OPTIMUM MOISTURE CONTENT AS DETERMINED BY AASHO. DESIGNATION T-99 METHOD 'C'

UNLESS OTHERWISE NOTED ON PLANS ALL SOIL SURVEY BORINGS FOR ROADWAY CONSTRUCTION WERE LEFT OPEN FOR 24 HOURS WITH NO EXCESS MOISTURE OR FREE WATER ENCOUNTERED DURING TIME OF SOIL SURVEY (10/68 & 1/69-2/69)
 BORING DATA FOR STRUCTURES IS INDICATED ON RESPECTIVE PLAN SHEETS.

ABBREVIATIONS

- B.C.C.M.P. - Bituminous Coated Corrugated Metal Pipe
- B.M. - Bench Mark
- B.S. - Base Line Survey
- ℓ Constr. - Base Line Construction
- Elev. - Elevation
- P/G.L. - Profile Ground Line
- P.G.L. - Profile Grade Line
- P/C - Point of Crown
- P/R - Point of Rotation
- P/G.E. - Profile Grade Elevation
- R.C.C.P. - Reinforced Cement Concrete Pipe
- S.S.D. - Stopping Sight Distance
- H.S.D. - Headlight Sight Distance
- S.E. - Superelevation
- S.D.D. - Surface Drain Ditch
- S.D. - Side Ditch
- STD. PL. - Standard Plate
- Ⓜ - Remove Existing Culvert Pipe
- V.C.P. - Vitrified Clay Pipe
- V.C.P.X. - Vitrified Clay Pipe (Extra Strength)
- M.H. - Manhole
- H.W. - Headwall
- U.D. - Underdrain
- N.B.R. - Northbound Roadway
- S.B.R. - Southbound Roadway
- G.R. - Guard Rail
- S.B.M. - Seed and Mulch

EXPLANATORY NOTES AND REFERENCES

VERTICAL CONTROL
 The Location and Elevations of Bench Marks are Shown on The Plans. All Elevations Shown are Based on Baltimore City Datum.

DRAINAGE
 All Concrete Pipe to be Class IV unless Otherwise Noted on Plans.

INVERT ELEVATIONS
 All Invert Elevations are Approximate and May be Modified to Meet Conditions Encountered During Installation of Drainage Structures.

SIGHT DISTANCE
 Stopping Sight Distance For Crest Vertical Curves is Based on a Height of Object = 0.33' and a Height of Eye = 3.0'. Headlight Sight Distance For Sag Vertical Curves is Based on a Height of Headlight = 2.0' and an Upward Divergence of Beam = 1".

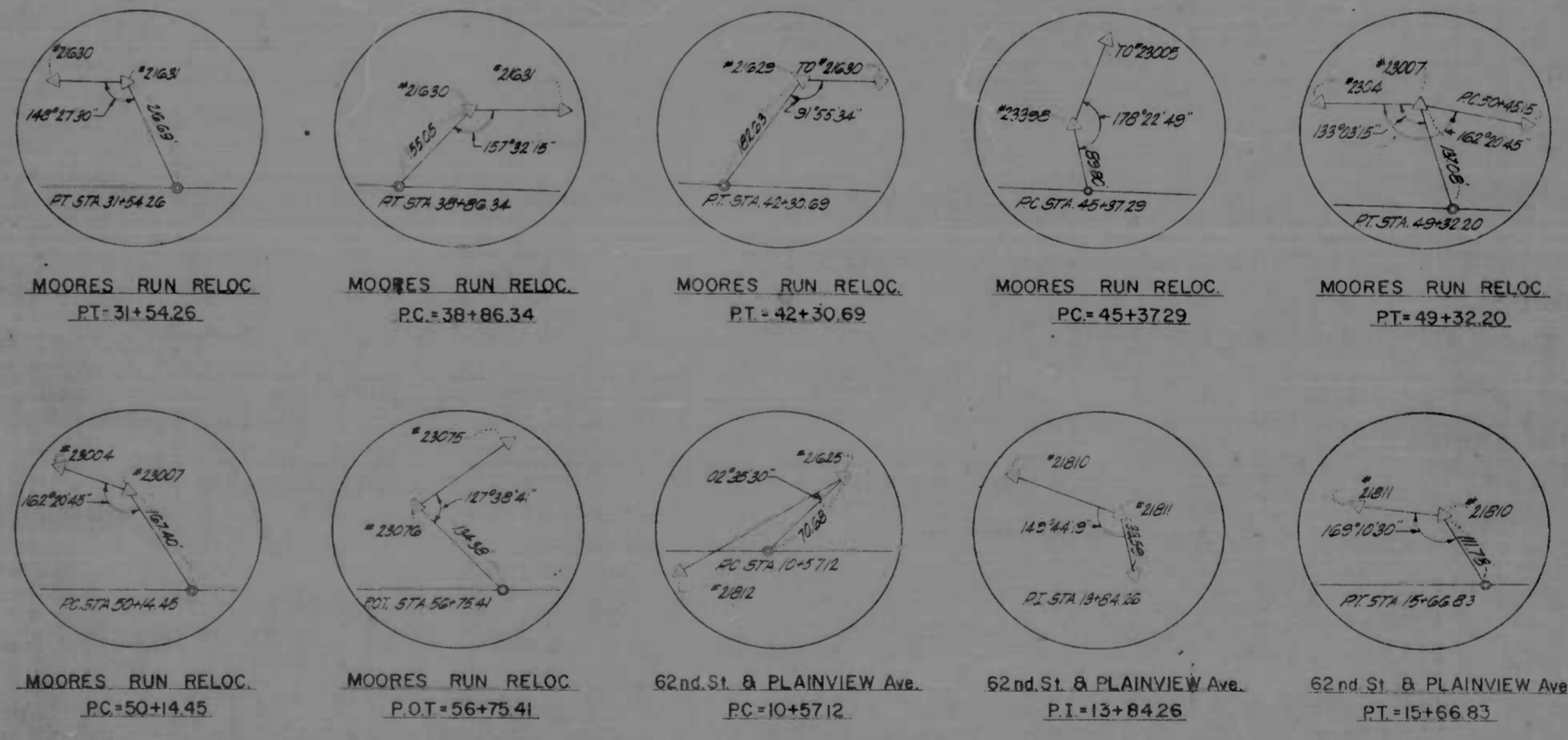
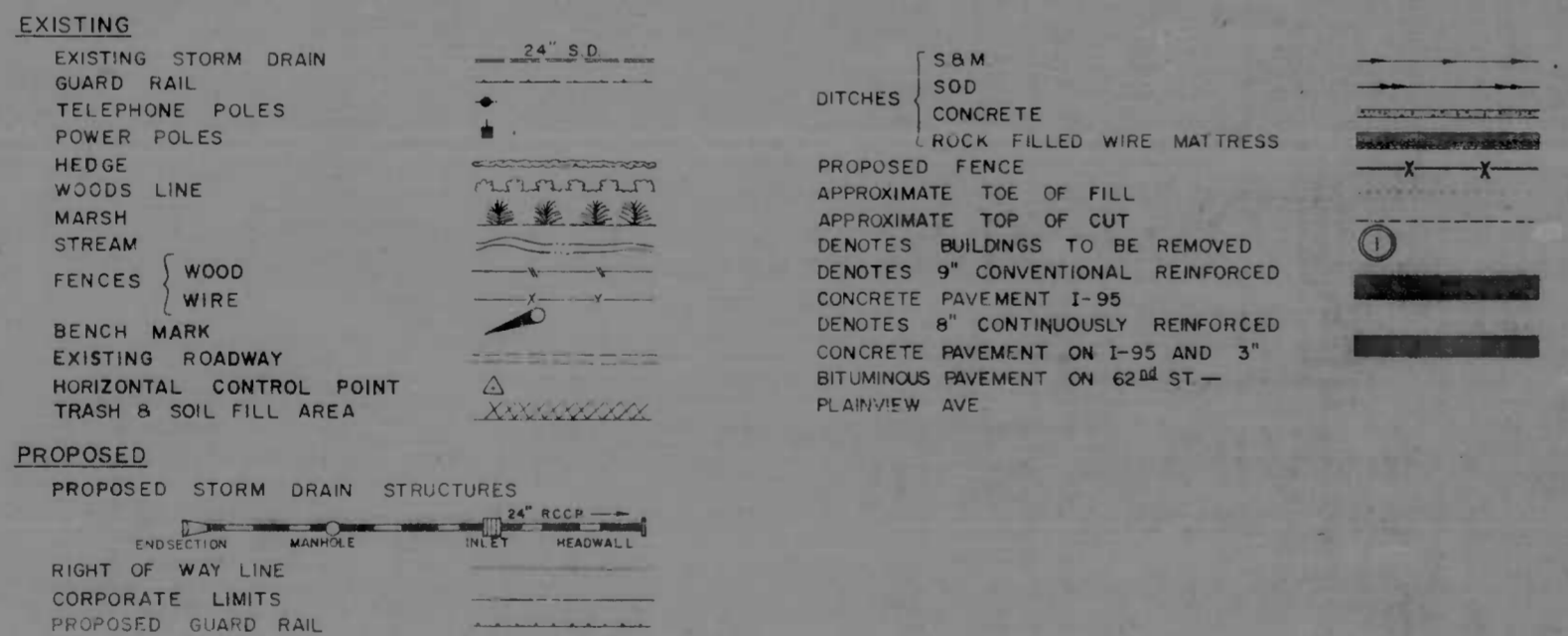
MATERIALS SALVAGED IN CONSTRUCTION
 These Materials Shall Become The Property of The Contractor and Shall be Removed From The Site of Construction Except For Those Items Provided For Salvage as S.R.C. Property in The Special Provisions.

HORIZONTAL CONTROL
 The Project is Oriented to Conform With The Baltimore City Grid System.

SPECIFICATIONS
 Md State Roads Commission Specifications for Materials, Highways, Bridges and Incidental Structures, Dated March, 1968 and Revisions Thereof or Additions Thereto Included in The Proposal and Special Provisions.

CLEARING AND GRUBBING
 Special Attention is Directed to the Fact that no Clearing and Grubbing or Grading May Commence for this Project Until the Clearing Limits are Staked in the Field and Approved by the Engineer. No Clearing Will be Allowed Beyond Construction Limits.

CONVENTIONAL SIGNS



GENERAL NOTES & REFERENCES

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	KIMBLE, DENNER, STONE & ASSOC., INC. AND WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	DRAWN BY: J.R.W. DES. BY: J.L.C. TRACED BY: J.R.W. CHK. BY: R.W.C. F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-815 BALTO. CITY NO. 1983
	SCALE: As Shown	DATE:	SHEET NO. 5 of 45

SUPERELEVATION DATA

FED. ROAD DIST. NO.	STATE	FISC. YEAR	SHEET NO.	TOTAL SHEETS
2	MU	I-95-4(5)35	6	45

N.B.R.						
NORMAL SECTION						
STATION	24'LT	12'LT	PGL & P/R	12'RT	24'RT	REMARKS
445+02.07	38.53	38.72	38.91	38.72	38.53	BEGIN TRANSITION
+25	38.73	38.90	39.07	38.88		
+50	39.01	39.16	39.29	39.10		
+75	39.27	39.39	39.49	39.30		C+0.00010'/FT
+100	39.52	39.60	39.68	39.49		
+125	39.75	39.80	39.85	39.68		
+150	39.97	39.99	40.0	39.80		
+175	40.0	40.0	40.0	39.9		12' RT LEVEL
+200	40.19	40.16	40.1	39.94		
+225	40.20	40.34	40.28	40.09		C+0.00015'/FT
+250	40.60	40.49	40.39	40.20		
+275	40.78	40.64	40.49	40.30		
+300	40.88	40.55	40.42	40.36		PLANE INCLINE
+325	40.95	40.45	40.50	40.37		
+350	41.11	40.67	40.63	40.65		
+375	41.25	40.86	40.68	40.60		
+400	41.38	41.05	40.70	40.39		
+425	41.49	41.11	40.74	40.37		C+0.00015'/FT
+450	41.58	41.6	40.74	40.32		
+475	41.55	41.19	40.73	40.27		
+500	41.73	41.22	40.71	40.20		
+525	41.78	41.22	40.67	40.12		
+550	41.81	41.21	40.61	40.01		
+575	41.83	41.18	40.54	39.90		
+600	41.83	41.14	40.45	39.76		
+625	41.83	41.14	40.44	39.74		BEGIN FULL SUPER

FULL SUPERELEVATION = 0.058'/FT.						
461+22.13	51.37	50.68	49.98	49.28		END FULL SUPER
+25	51.44	50.75	50.05	49.37		
+50	52.08	51.44	50.79	50.14		
+75	52.74	52.12	51.54	50.94		
+100	53.43	52.83	52.32	51.76		
+125	54.14	53.53	53.12	52.61		C+0.00015'/FT
+150	54.87	54.21	53.94	53.47		
+175	55.63	55.21	54.79	54.37		
+200	56.40	56.03	55.65	55.27		
+225	57.17	56.84	56.5	56.18		
+250	57.94	57.66	57.37	57.08		
+275	58.71	58.47	58.23	57.99		
+300	59.48	59.29	59.09	58.89		
+325	60.23	59.94	59.6	59.67		PLANE INCLINE

FULL SUPERELEVATION = 0.016'/FT.							
STATION	PGL	12'LT	24'LT	P/R	36'LT	48'LT	REMARKS
475+50	55.50	55.89	56.28	56.68	57.08	57.48	PLANE INCLINE
+75	56.08	56.24	56.43	56.64	56.85	57.06	
+100	56.58	56.77	56.95	57.07	57.27	57.47	C+0.00015'/FT
+125	57.10	57.29	57.48	57.64	57.83	58.03	
+150	57.61	57.80	57.99	58.01	58.01	58.01	
+175	58.13	58.33	58.52	58.52	58.52	58.52	24' RT LEVEL
+200	58.65	58.85	59.04	59.04	59.04	59.04	C+0.00015'/FT
+225	59.17	59.22	59.24	59.25	59.25	59.25	
+250	59.68	59.67	59.66	59.70	59.70	59.70	
+275	60.17	60.09	60.09	60.09	60.09	60.09	END TRANS

NORMAL SECTION

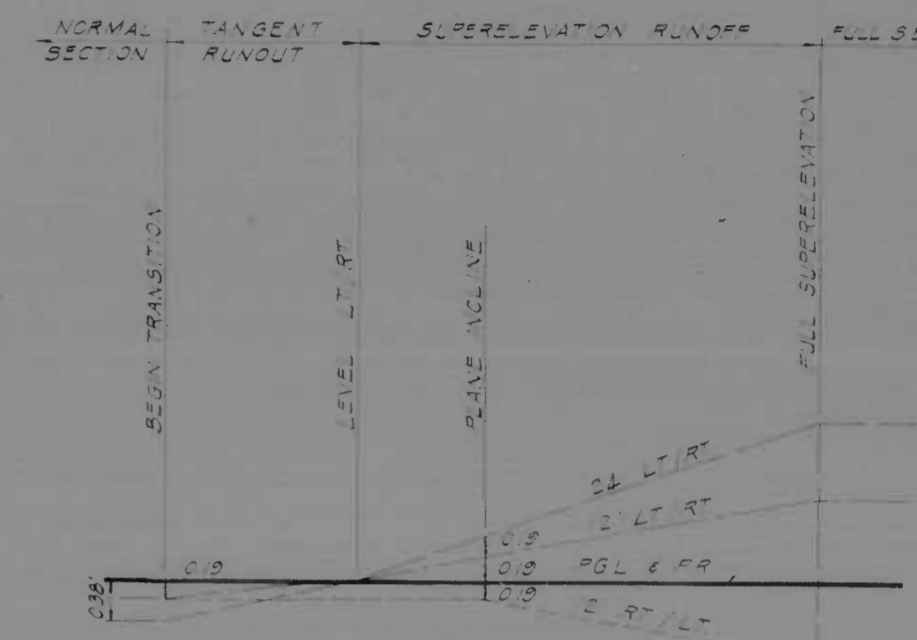
S.B.R.					
NORMAL SECTION					
STATION	12'LT	PGL & P/R	12'RT	24'RT	REMARKS
450+40	46.91	47.10	47.31	47.52	BEGIN TRANSITION
+50	47.15	47.33	47.54	47.75	
+75	47.78	47.94	48.14	48.34	
+100	48.44	48.59	48.87	49.14	C+0.00010'/FT
+125	49.10	49.18	49.30	49.41	
+150	49.76	49.82	49.89	49.94	
+175	50.42	50.45	50.46	50.47	
+200	51.08	51.08	51.08	51.08	12' LT LEVEL
+225	51.75	51.70	51.51	51.32	
+250	52.42	52.31	52.13	51.94	C+0.00015'/FT
+275	53.09	52.92	52.73	52.54	
+300	53.77	53.52	53.33	53.14	PLANE INCLINE
+325	54.44	54.10	53.86	53.62	
+350	55.12	54.67	54.38	54.09	
+375	55.87	55.23	54.85	54.56	
+400	56.6	55.78	55.40	55.01	
+425	57.31	56.30	55.89	55.56	C+0.00015'/FT
+450	58.03	56.85	56.37	56.04	
+475	58.76	57.37	56.84	56.51	
+500	59.48	57.88	57.30	57.03	
+525	60.20	58.36	57.76	57.53	
+550	60.94	58.87	58.20	57.83	
+575	61.67	59.35	58.63	58.21	BEGIN FULL SUPER

FULL SUPERELEVATION = 0.060'/FT.						
468+65.61	73.55	73.23	72.51	71.79		END FULL SUPER
+100	74.14	73.44	72.74	72.05		
+125	74.43	73.78	73.13	72.47		
+150	74.76	74.15	73.54	72.93		
+175	75.10	74.53	73.99	73.39		
+200	75.44	74.91	74.38	73.85		
+225	75.81	75.32	74.83	74.35		C+0.00015'/FT
+250	76.17	75.73	75.29	74.84		
+275	76.55	76.13	75.75	75.33		
+300	76.97	76.61	76.25	75.89		
+325	77.38	77.08	76.74	76.43		
+350	77.81	77.53	77.25	76.98		
+375	78.25	78.02	77.79	77.55		
+400	78.71	78.52	78.33	78.14		PLANE INCLINE
+425	79.17	78.92	78.63	78.44		
+450	79.68	79.55	79.44	79.33		C+0.00015'/FT
+475	80.16	80.09	80.02	79.96		
+500	80.66	80.64	80.62	80.59		LEVEL
+525	81.15	81.21	81.23	81.25		
+550	81.73	81.79	81.85	81.91		
+575	82.28	82.36	82.48	82.58		C+0.00015'/FT
+600	82.85	82.99	83.13	83.28		
+625	83.42	83.57	83.60	83.68		
+650	84.07	84.27	84.36	84.45		PLANE INCLINE
+675	84.71	84.89	85.0	85.13		
+700	85.34	85.55	85.69	85.87		C+0.00015'/FT
+725	85.97	86.22	86.37	86.53		
+750	86.6	86.86	87.08	87.27		
+775	87.21	87.55	87.89	88.23		
+800	87.8	88.24	88.30	88.75		BEGIN FULL SUPER

FULL SUPERELEVATION = 0.038'/FT.

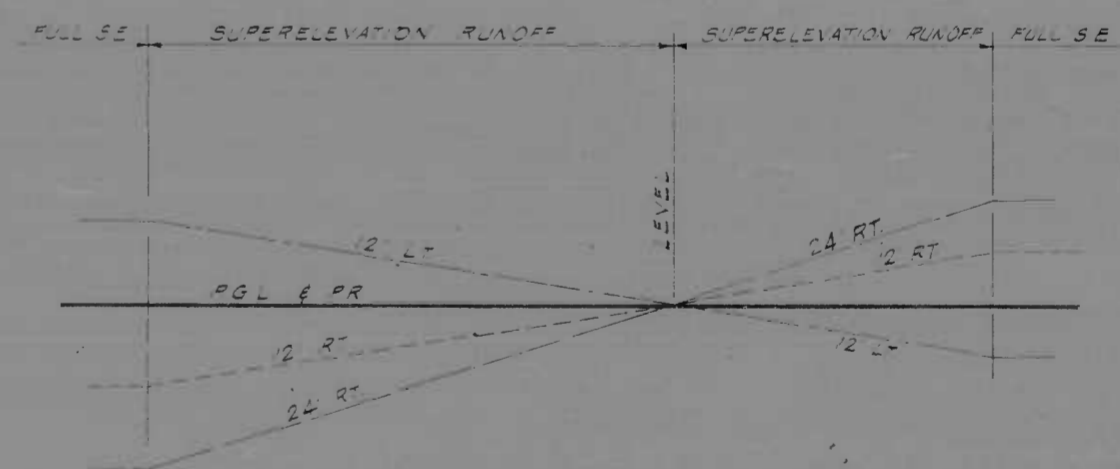
S.B.R.					
FULL SUPERELEVATION = 0.038'/FT.					
STATION	12'LT	PGL & P/R	12'RT	24'RT	REMARKS
481+33.55	100.74	101.20	101.66	102.11	END FULL SUPER
+50	101.04	101.48	101.92	102.35	
+75	101.56	102.07	102.58	102.98	
+100	101.87	102.25	102.68	103.00	
+125	102.25	102.33	102.85	103.29	
+150	102.60	102.92	103.24	103.55	C+0.000125'/FT
+175	102.90	103.23	103.51	103.80	
+200	103.25	103.57	103.77	104.02	
+225	103.53	103.76	103.99	104.27	
+250	103.79	103.99	104.19	104.38	
+275	103.82	104.03	104.22	104.31	PLANE INCLINE
+300	104.0	104.20	104.39	104.53	
+325	104.100	104.39	104.58	104.66	C+0.000125'/FT
+350	104.66	104.68	104.74	104.76	
+375	104.41	104.60	104.78	104.79	24' RT LEVEL
+400	104.51	104.70	104.89	104.87	
+425	104.66	104.85	105.04	105.09	
+450	104.81	105.00	105.19	105.11	C+0.00010'/FT
+475	104.95	105.15	105.34	105.23	
+500	105.11	105.30	105.49	105.35	
+525	105.25	105.45	105.64	105.47	
+550	105.37	105.56	105.75	105.59	END TRANSITION

NORMAL SECTION



METHOD OF SUPERELEVATION TRANSITION
CURVE TO THE RIGHT / LEFT

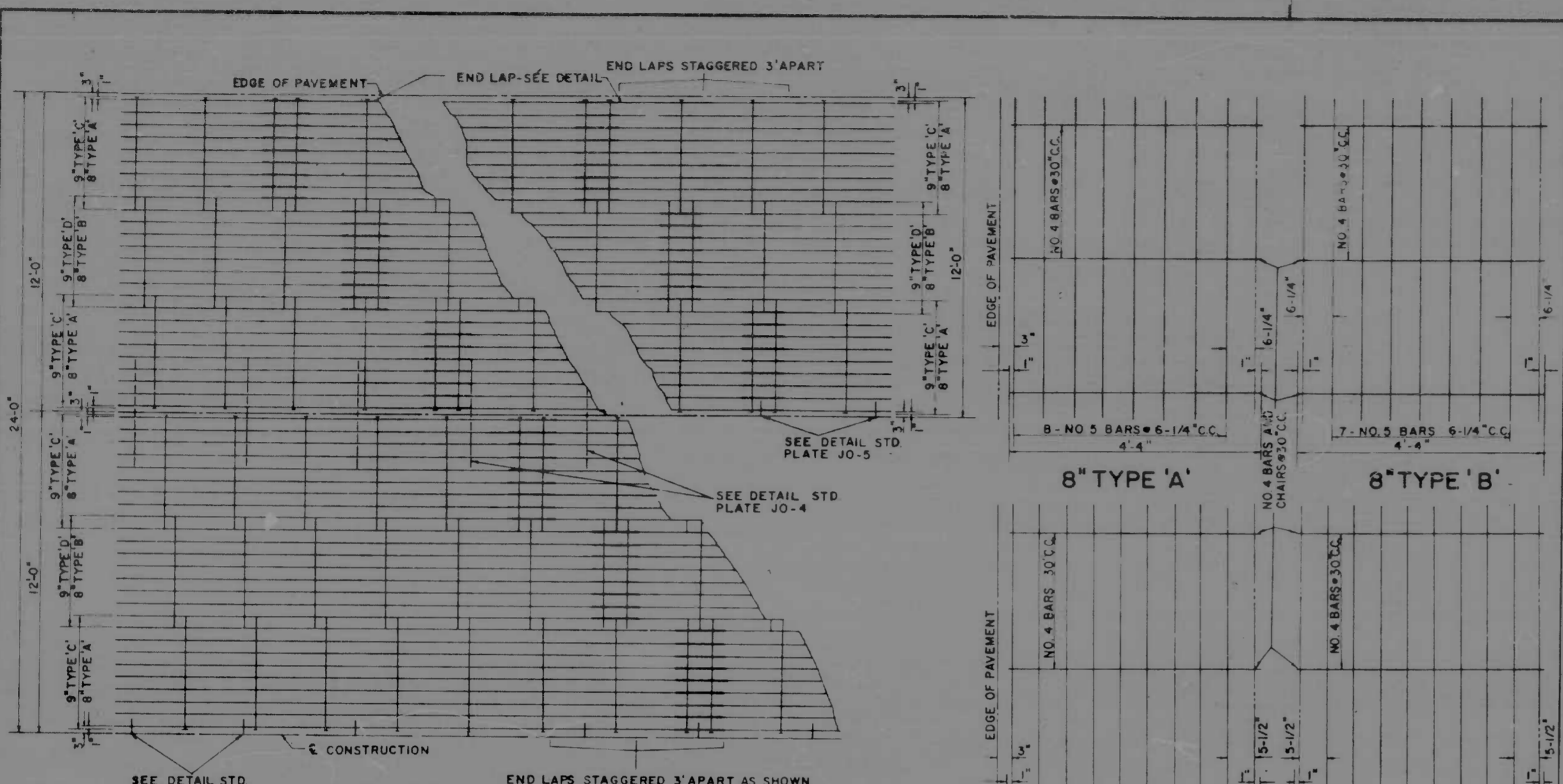
STA 445+32.07 TO STA 441+28.74
 STA 461+22.13 TO STA 461+28.13
 STA 475+50 TO STA 477+63.33
 STA 450+40 TO STA 435+75
 STA 481+33.55 TO STA 485+33.55



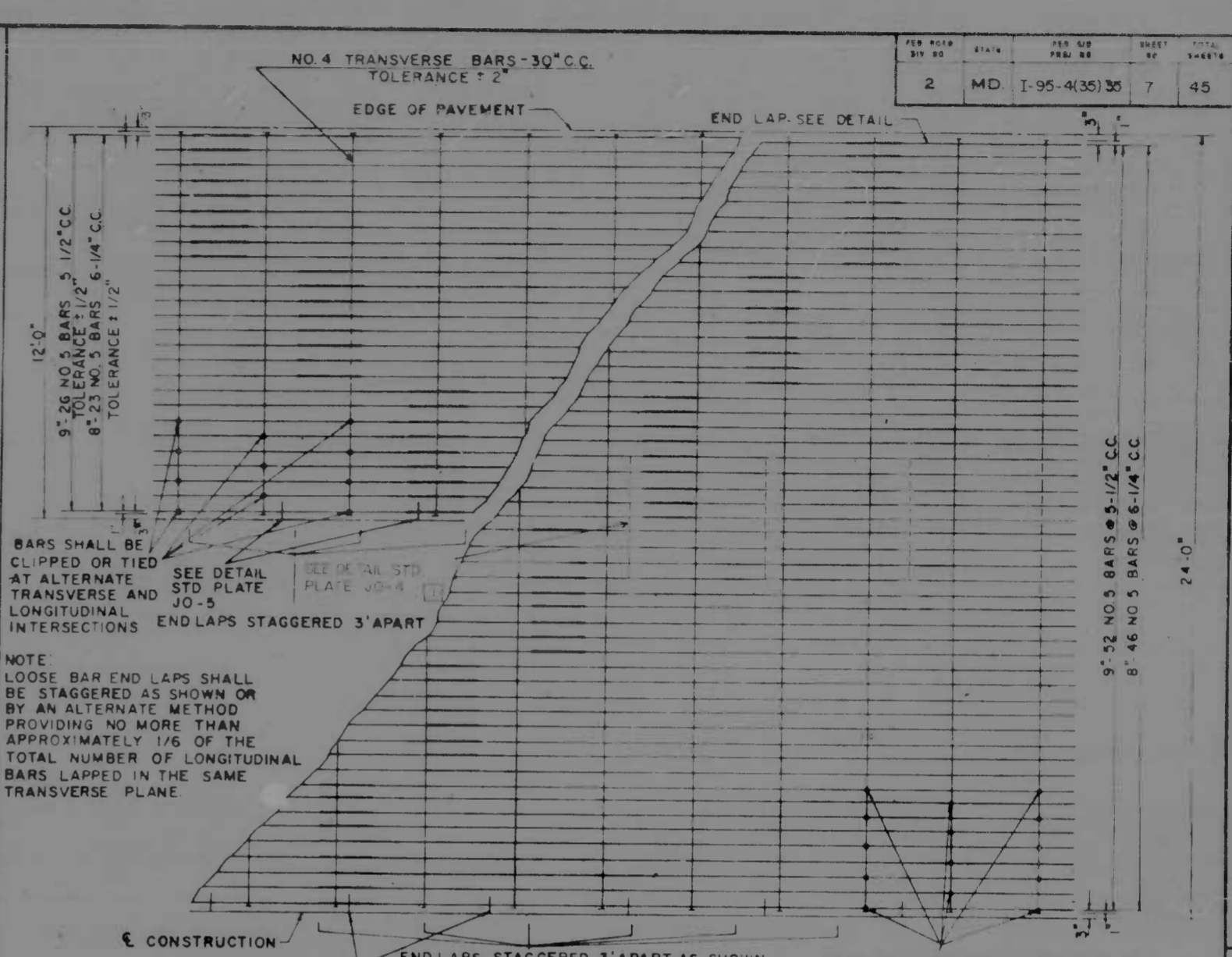
METHOD OF SUPERELEVATION TRANSITION
REVERSE SPIRALED CURVE
STA 468+65.61 TO STA 475+65.61

REVISIONS	CONSULTANT KNORR, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		DRAWN BY: P.W.Z. TRACED BY: P.W.Z. P.A.P. NO. 1-95-A(5)35 S.R.C. NO. BC 246-32-B15 BALTO. CITY NO. 1183	DES. BY: J.L.C. CHK. BY: B.V.C. SHEET NO. 6 OF 22

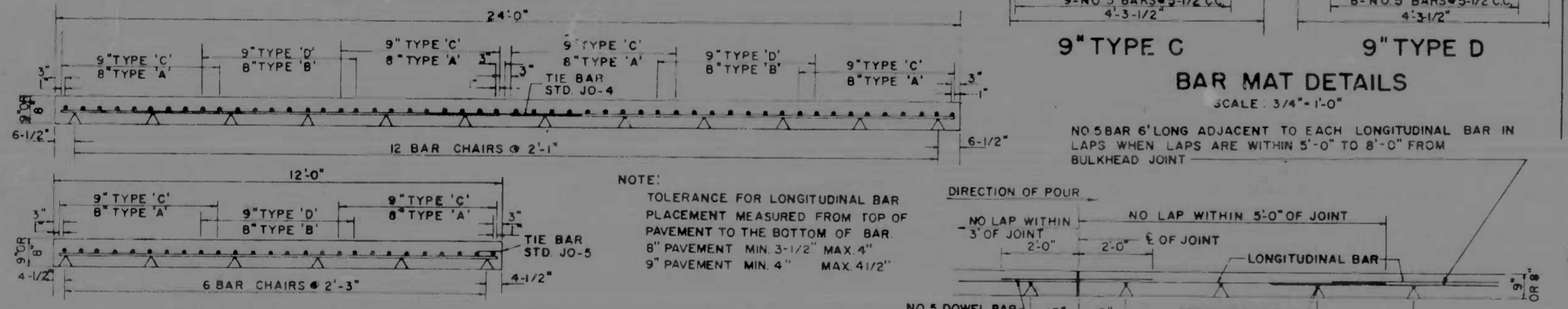
DESIGN	DATE	REV UP	BY	DATE
2	MD	I-95-4(35)30	7	45



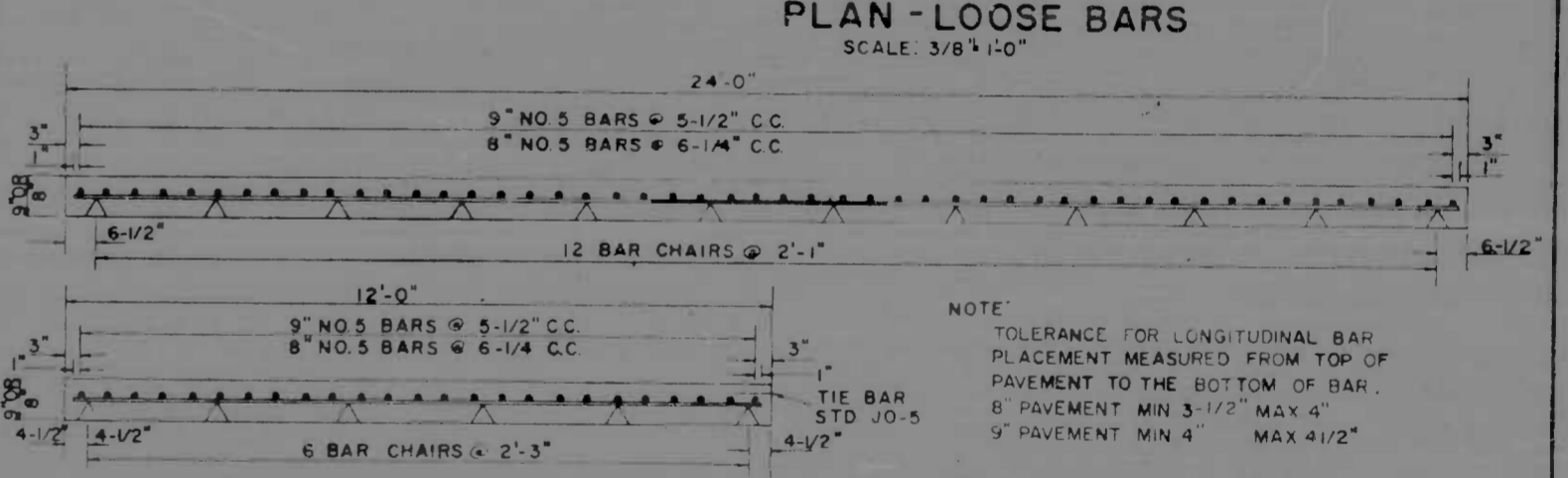
PLAN-BAR MATS
SCALE: 3/8"=1'-0"
LENGTH OPTIONAL 35' MINIMUM



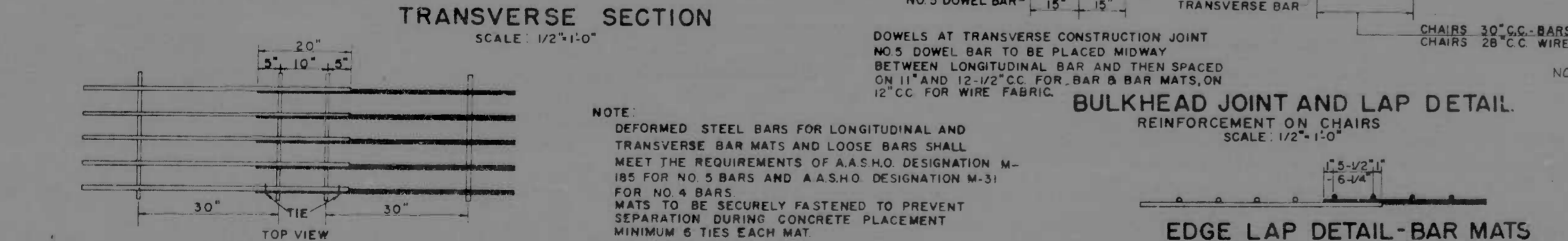
PLAN-LOOSE BARS
SCALE: 3/8"=1'-0"



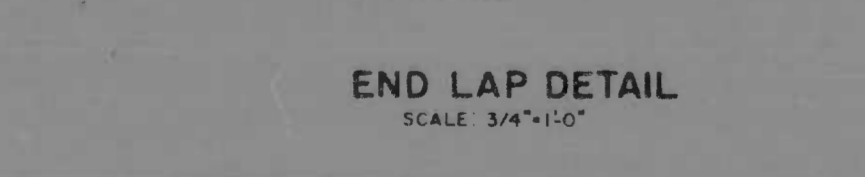
BAR MAT DETAILS
SCALE: 3/4"=1'-0"



TRANSVERSE SECTION
SCALE: 1/2"=1'-0"



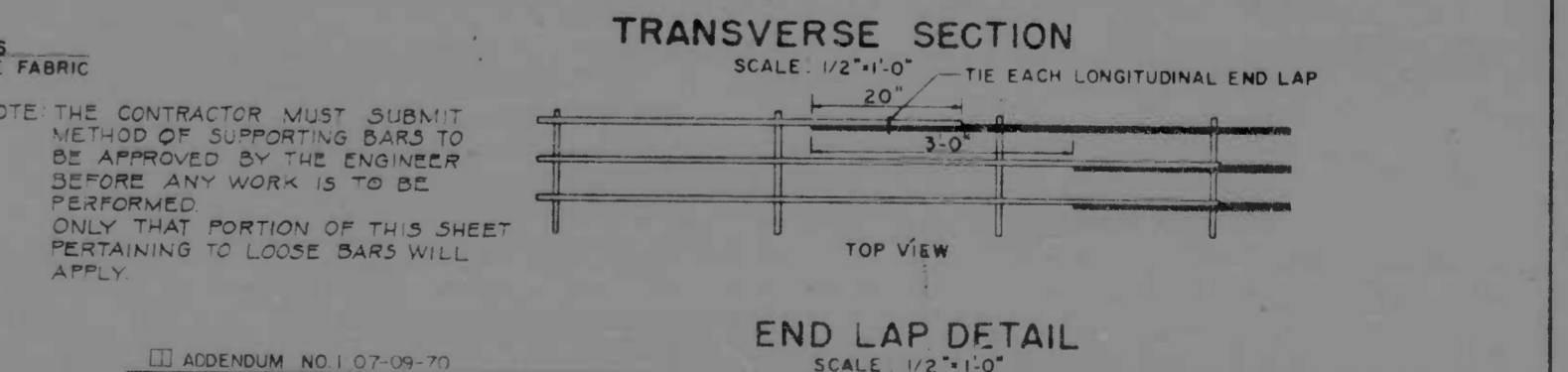
TRANSVERSE SECTION
SCALE: 1/2"=1'-0"



END LAP DETAIL
SCALE: 3/4"=1'-0"

CONTINUOUSLY REINFORCED CONCRETE PAVT
BAR STEEL REINFORCEMENT

BULKHEAD JOINT AND LAP DETAIL
REINFORCEMENT ON CHAIRS
SCALE: 1/2"=1'-0"



END LAP DETAIL
SCALE: 1/2"=1'-0"

CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE	
INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: S.R.C.	DATE: 7/83
SCALE: AS SHOWN		TRACED BY: S.R.C.	SHEET NO: 7 OF 45
DATE: 7/83		F.A.P. NO: I-95-4(35)30	S.R.C. NO: BC 246-32.3.5
		BALTO. CITY NO: 1983	

NOTE:
TOLERANCE FOR LONGITUDINAL BAR PLACEMENT MEASURED FROM TOP OF PAVEMENT TO THE BOTTOM OF BAR.
8" PAVEMENT MIN 3-1/2" MAX 4"
9" PAVEMENT MIN 4" MAX 4-1/2"

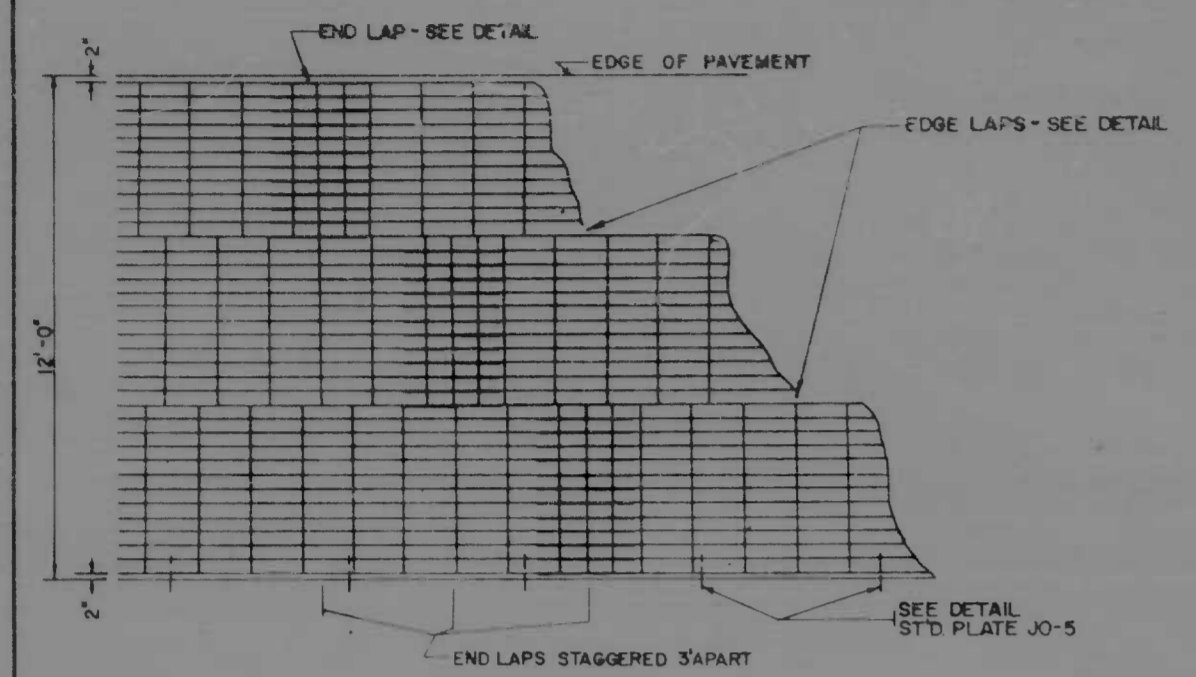
NOTE:
TOLERANCE FOR LONGITUDINAL BAR PLACEMENT MEASURED FROM TOP OF PAVEMENT TO THE BOTTOM OF BAR.
8" PAVEMENT MIN 3-1/2" MAX 4"
9" PAVEMENT MIN 4" MAX 4-1/2"

NOTE:
THE CONTRACTOR MUST SUBMIT METHOD OF SUPPORTING BARS TO BE APPROVED BY THE ENGINEER BEFORE ANY WORK IS TO BE PERFORMED.
ONLY THAT PORTION OF THIS SHEET PERTAINING TO LOOSE BARS WILL APPLY.

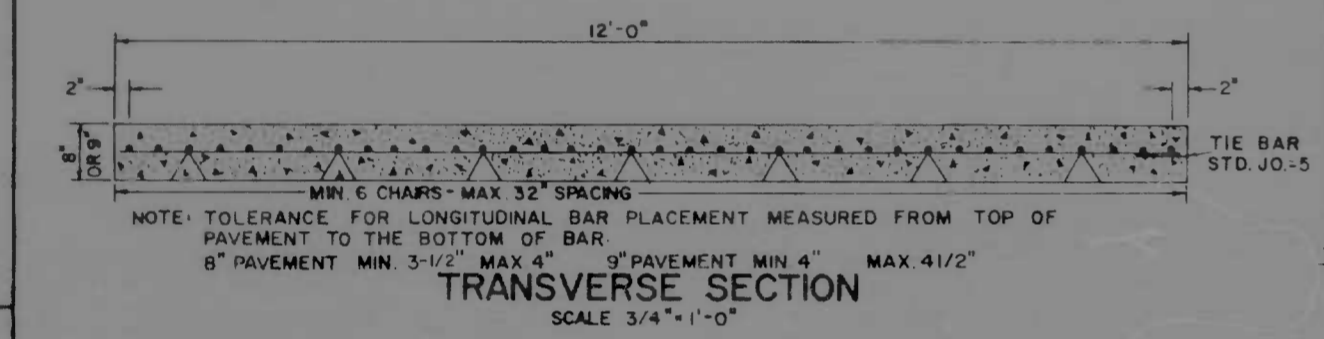
NOTE:
DEFORMED STEEL BARS FOR LONGITUDINAL AND TRANSVERSE BAR MATS AND LOOSE BARS SHALL MEET THE REQUIREMENTS OF A.A.S.H.O. DESIGNATION M-185 FOR NO. 5 BARS AND A.A.S.H.O. DESIGNATION M-31 FOR NO. 4 BARS.
MATS TO BE SECURELY FASTENED TO PREVENT SEPARATION DURING CONCRETE PLACEMENT.
MINIMUM 6 TIES EACH MAT.

NOTE:
DOWELS AT TRANSVERSE CONSTRUCTION JOINT
NO. 5 DOWEL BAR TO BE PLACED MIDWAY BETWEEN LONGITUDINAL BAR AND THEN SPACED ON 11" AND 12-1/2" C.C. FOR BAR & MATS, ON 12" C.C. FOR WIRE FABRIC.

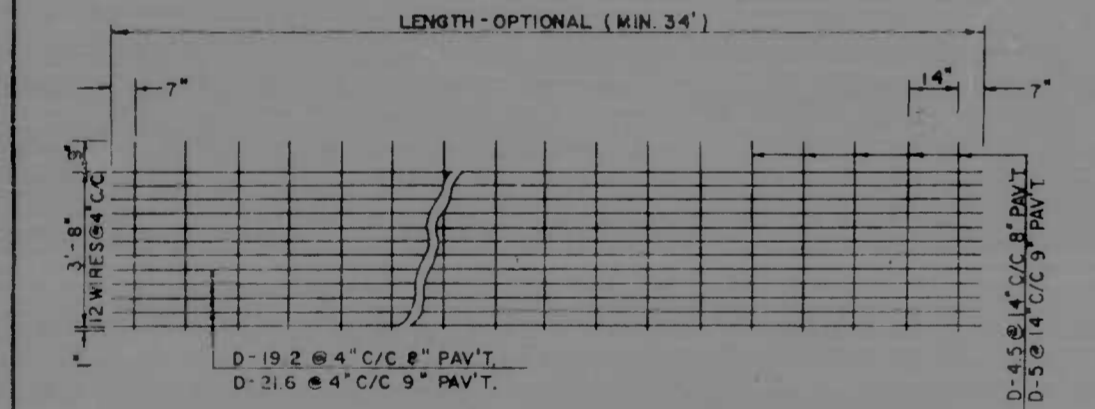
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(36)35	8	45



PLAN
SCALE 3/8" = 1'-0"

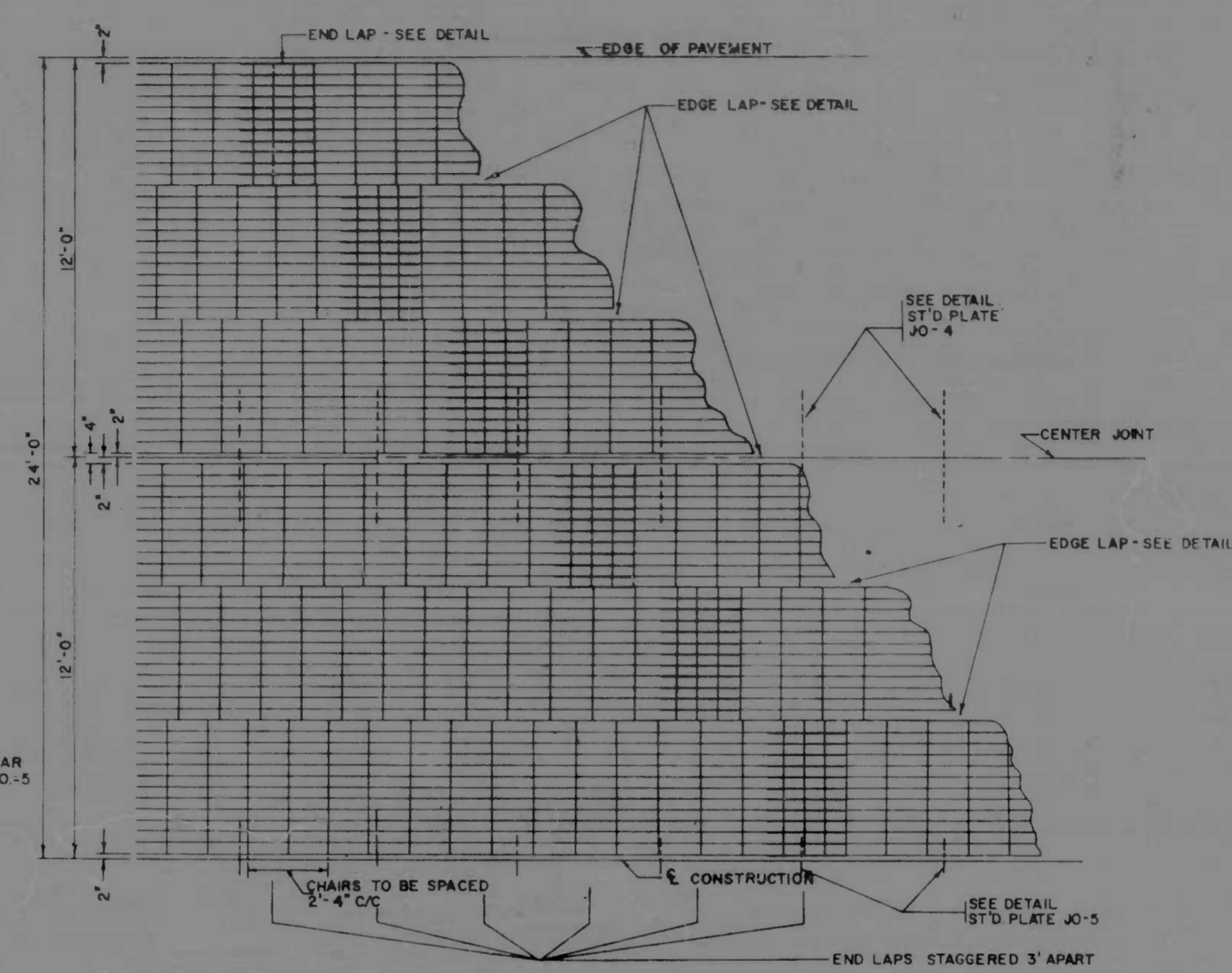


TRANSVERSE SECTION
SCALE 3/4" = 1'-0"

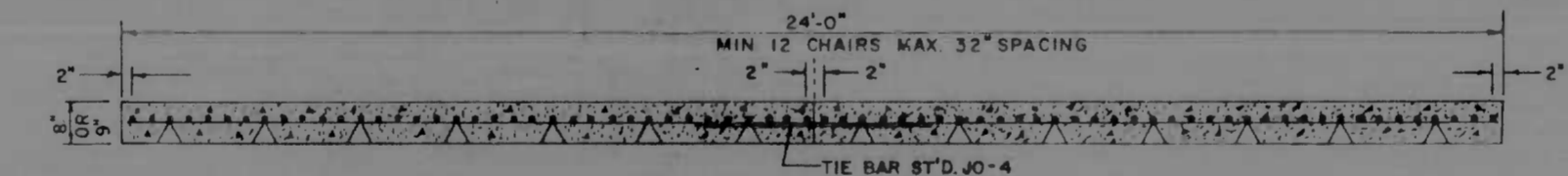


TYPICAL FABRIC SHEET
SCALE 3/8" = 1'-0"

NOTE: THE CONTRACTOR MUST SUBMIT METHOD OF SUPPORTING BARS TO BE APPROVED BY THE ENGINEER BEFORE ANY WORK IS TO BE PERFORMED. ONLY THAT PORTION OF THIS SHEET PERTAINING TO LOOSE BARS WILL APPLY.

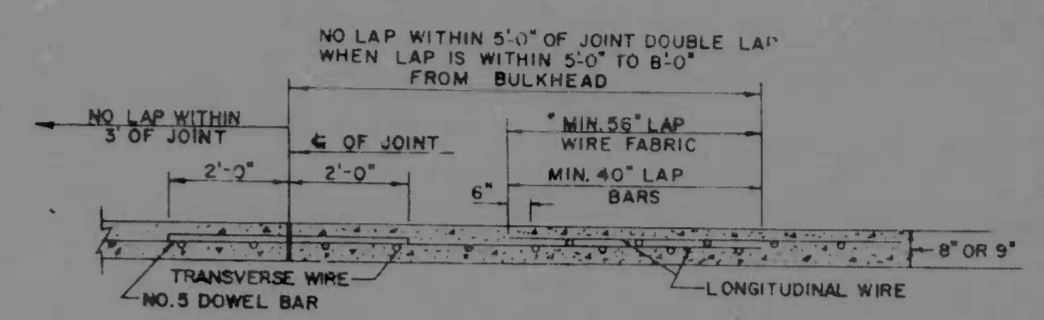


PLAN
SCALE 3/8" = 1'-0"



TRANSVERSE SECTION
SCALE 1/2" = 1'-0"

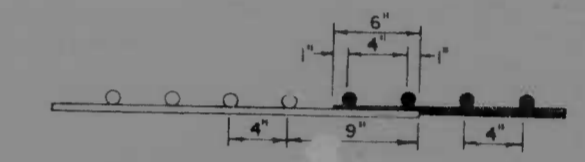
NOTE: THE SHEET OF WELDED DEFORMED STEEL WIRE FABRIC WHEN PREPLACED SHALL BE SUPPORTED IN POSITION BY CHAIR SUPPORTS OF ANY TYPE SATISFACTORY AND CAPABLE OF PROVIDING ADEQUATE SUPPORT, AND THE CONCRETE PLACED THROUGH THE OPENINGS. SHEETS OF WELDED DEFORMED STEEL WIRE FABRIC MAY BE PLACED WITH THE TRANSVERSE WIRES UP OR DOWN.



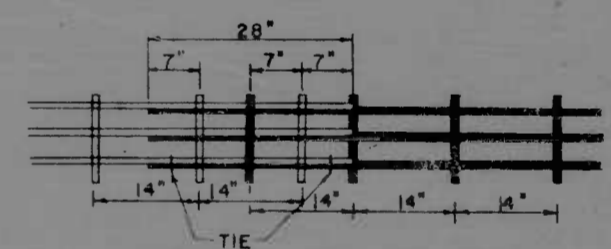
DOWELS AT TRANSVERSE CONSTRUCTION JOINTS
NO. 5 DOWEL BAR TO BE PLACED MIDWAY BETWEEN LONGITUDINAL BAR AND THEN SPACED ON 12" C/C FOR WIRE FABRIC ON 11" AND 12-1/2" C/C FOR BAR AND BAR MATS.

BULKHEAD JOINT AND LAP DETAIL
REINFORCEMENT BY MECHANICAL MEANS
SCALE 1/2" = 1'-0"

WHEN THE OPTION OF PLACING REINFORCEMENT BY MECHANICAL MEANS IS USED THE BAR OR WIRE FABRIC MATS SHALL BE DOUBLE LAPPED AS SHOWN ABOVE.
WHEN THE OPTION OF PLACING REINFORCEMENT ON CHAIRS OR CHAIR BARS IS USED THE BAR OR WIRE FABRIC MATS SHALL BE LAPPED AS SHOWN ON SHEET NO. 7 OF 45.



EDGE LAP DETAIL
SCALE 1/2" = 1'-0"



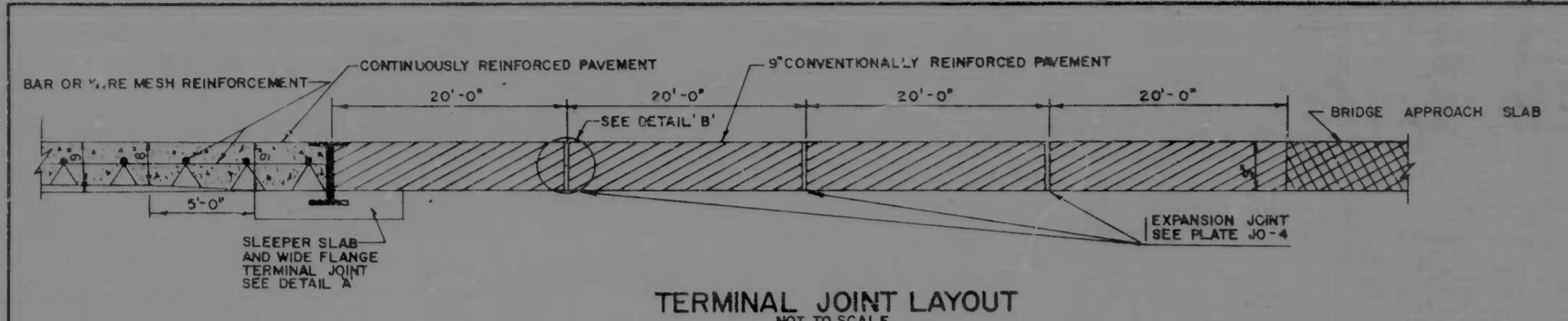
NOTE: MATS TO BE SECURELY FASTENED TO PREVENT SEPARATION DURING CONCRETE PLACEMENT, MINIMUM 6 TIES EACH MAT.

END LAP DETAIL
SCALE 3/4" = 1'-0"

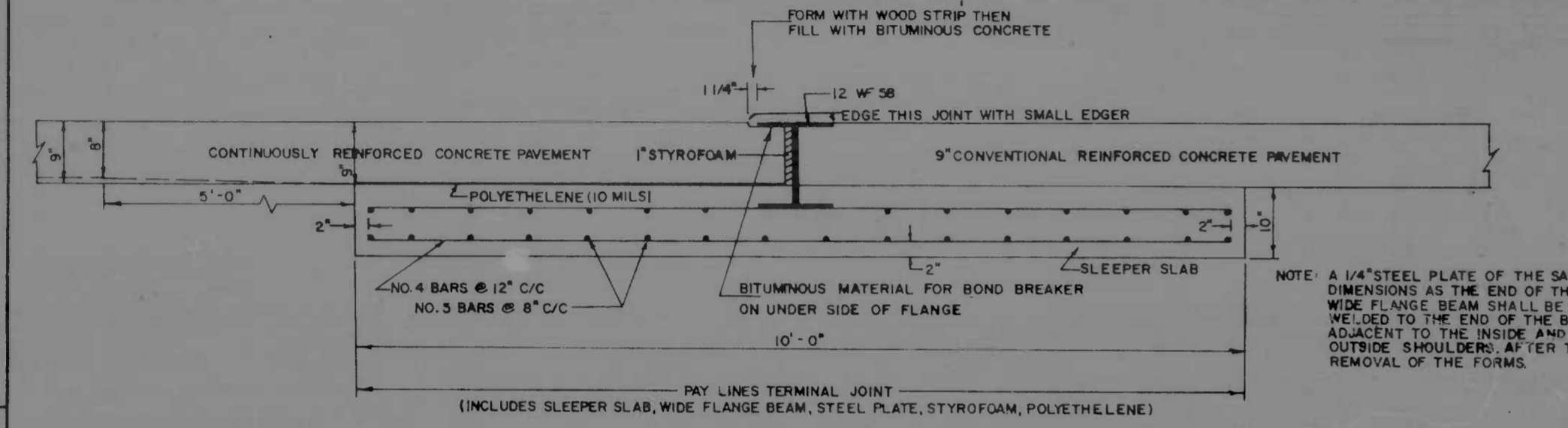
NOTE FOR JOINT DETAILS SEE SHEET NO. 9 OF 45

CONTINUOUSLY REINFORCED CONCRETE PAVEMENT
WELDED DEFORMED STEEL WIRE FABRIC REINFORCEMENT

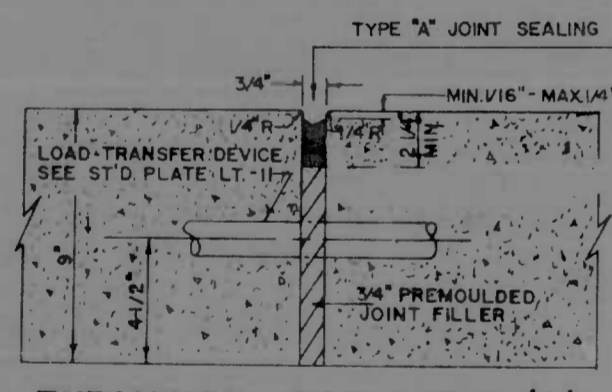
CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE	
INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY			
DRAWN BY: SRC	TRACED BY: SRC	DES BY: SRC	CHK BY: SRC
SCALE: AS SHOWN		DATE:	
PROJECT NO. I-95-4(36)35		SHEET NO. 8 OF 45	
S.R.C. NO. BG. 246-32-813		BALTO. CITY NO. 1983	



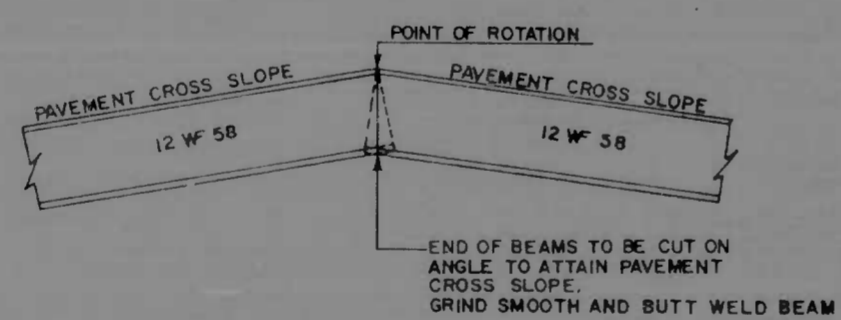
TERMINAL JOINT LAYOUT
NOT TO SCALE



DETAIL 'A'
SLEEPER SLAB AND WIDE FLANGE TERMINAL JOINT
SCALE: 1" = 1'-0"



EXPANSION JOINT DETAIL 'B'
SCALE: 3" = 1'-0"



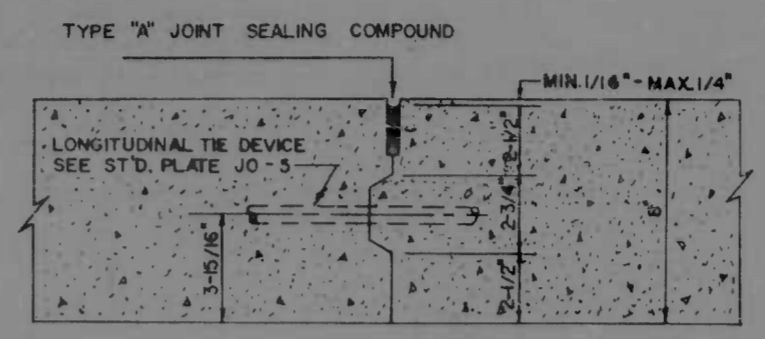
WIDE FLANGE BEAM ANGLE TIE
NOT TO SCALE

TERMINAL JOINT
BAR MATS, BARS, WIRE FABRIC

NOTE: ONLY THAT PORTION OF THIS SHEET PERTAINING TO LOOSE BARS WILL APPLY.

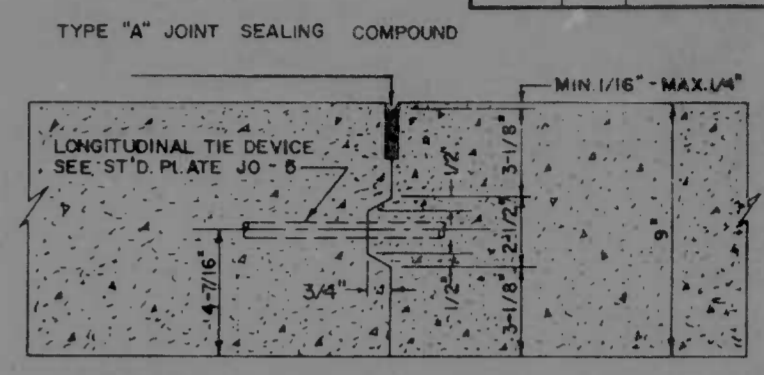
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(32)32	9	45

SAWED JOINT



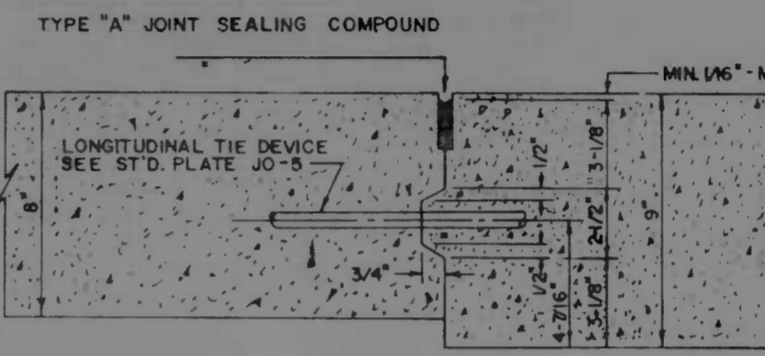
LONGITUDINAL CONSTRUCTION JOINT FOR 12' WIDTH PAVING WITH 8" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT.

SAWED JOINT



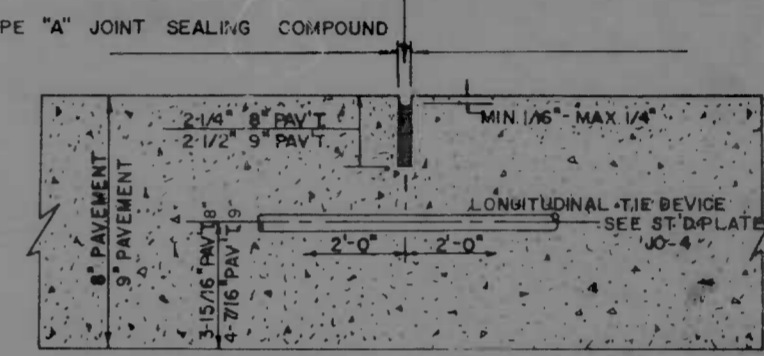
LONGITUDINAL CONSTRUCTION JOINT FOR 12' WIDTH PAVING WITH 9" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT.

SAWED JOINT



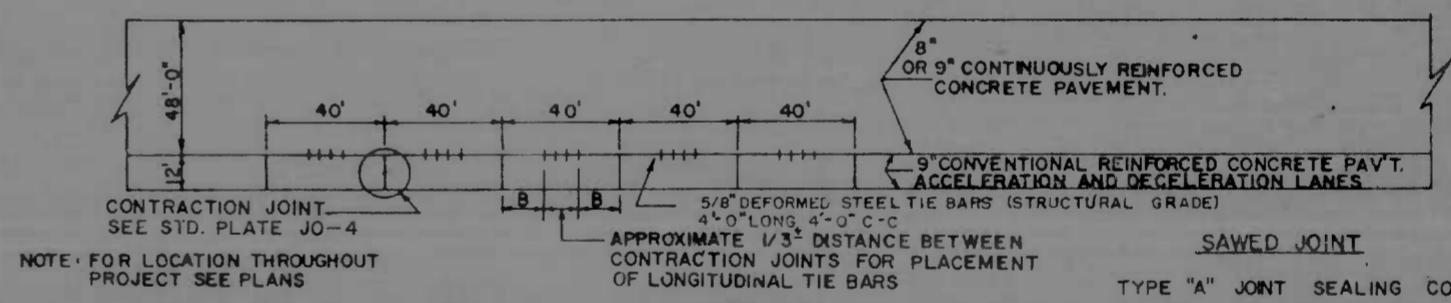
LONGITUDINAL CONSTRUCTION JOINT FOR 24' WIDTH PAVING WITH 8" AND 9" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT.

SAWED JOINT DUMMY JOINT TOOLED JOINT



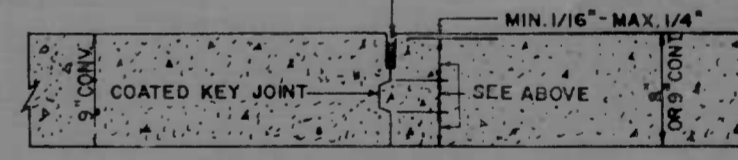
LONGITUDINAL CONSTRUCTION JOINT FOR 24' WIDTH PAVING WITH 8" OR 9" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT AND/OR 9" CONVENTIONAL REINFORCED CONCRETE PAVEMENT. SAW JOINT IN 72 HOURS.

DETAILS OF LONGITUDINAL CONSTRUCTION JOINTS
SCALE: 3" = 1'-0"



PLAN
SCALE: 1" = 40'

NOTE FOR ALL LONGITUDINAL TIE DEVICES, SEE SPECIAL PROVISIONS.



LONGITUDINAL KEY JOINT ('B' DISTANCE)
SCALE: 1 1/2" = 1'-0"

NOTE: 'B' DISTANCE COAT KEY JOINT WITH BITUMINOUS MATERIAL AS DIRECTED IN APPROXIMATE 2/3 DISTANCE BETWEEN CONTRACTION JOINTS NO TIE BARS WILL BE PLACED WITHIN THIS DISTANCE.

9" CONVENTIONAL REINFORCED CONCRETE PAVEMENT
ADJACENT TO 8" OR 9" CONTINUOUSLY REINFORCED CONCRETE PAVEMENT

CONTINUOUSLY REINFORCED CONCRETE PAVEMENT JOINT DETAILS

CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE	
INTERSTATE ROUTE 95 FROM NORTH OF PULASKI HIGHWAY TO JOHN F. KENNEDY MEMORIAL HIGHWAY		DRAWN BY: S.R.C. TRACED BY: S.R.C.	DES. BY: S.R.C. CHK. BY: S.R.C.
SCALE: AS SHOWN		F.A.P. NO. I-95-4(35)35 S.R.C. NO. BC 246-32-B15	SHEET NO. 9 of 45
DATE:		BALTO. CITY NO. 1983	

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(35)35	1	45

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- 5 GENERAL NOTES
- 6 SUPERELEVATION TABLES
- 7-16 DETAILS
- 17 PLAN SHEET
- 18 PROFILE SHEET
- 19 PLAN SHEET
- 20 PROFILE SHEET
- 21 PROFILE SHEET MOORES RUN
- 22 PLAN SHEET
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- 25 PLAN SHEET
- 26 PROFILE SHEET
- 27 PLAN SHEET
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- 33-35 PROFILE SHEETS
- 36-39 MAINTENANCE OF TRAFFIC
- 40-41 BOX CULVERT
- 42-45 SUMMARY OF QUANTITIES
- 12A-12B TYPICAL AND SPECIFIC SECTIONS
- 12C

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
AND
STATE ROADS COMMISSION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

FEDERAL AID PROJECT NO. I-95-4(35)35
STATE ROADS COMMISSION PROJECT NO. BC 246-32-815
CITY OF BALTIMORE BUREAU OF ENGINEERING,
HIGHWAY ENGINEERING DIVISION CONTRACT NO. 1983

INTERSTATE ROUTE 95

I-95 FROM NORTH OF PULASKI HIGHWAY
TO JOHN F. KENNEDY MEMORIAL HIGHWAY

LIMIT OF WORK
NBR & SBR 433+00
I-95-4(35)35
BC 246-32-815
1983



LIMIT OF WORK
NBR 487+82.66
SBR 490+59.56
I-95-4(35)35
BC 246-32-815
1983

FIELD BOOKS	
BOOK NO.	DESCRIPTION
I-95-009	CROSS SECTIONS
I-95-011	
I-95-001	
I-95-002	TRAVERSE
I-95-003	
I-95-009	CENTERLINE SURVEY
I-95-010	

TRAFFIC DATA		
	1970	1990
ADT	26,800	53,800
DHV		4,300
D		60%
% Trucks ADT		13%
% Trucks DHV		7%
MAX CURVE Dc = 4°-00'-00"		
MAX GRADE = 2.67%		

X INDICATES APPROXIMATE LOCATION OF INFORMATIONAL SIGN

LOCATION PLAN
SCALE 1"=1000'

DESIGN SPEED - 60 MPH
LENGTH OF PROJECT - 4.022 miles

RIGHT OF WAY LINES SHOWN ON THESE PLANS DO NOT INCLUDE EASEMENT. THEY ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THESE LINES DO NOT REPRESENT THE OFFICIAL PROPERTY ACQUISITION LINES. FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION, SEE THE APPROPRIATE RIGHT OF WAY PLAT OR PLATS.

CHECKED BY BUREAU OF ENGINEERING WATER DIVISION WASTE WATER DIVISION HIGHWAY ENGINEERING DIVISION SURVEY AND RECORDS DIVISION BUREAU OF UTILITY OPERATIONS LIGHTING SECTION - HIGHWAY MAINTENANCE DIV. CONDUIT SECTION - HIGHWAY MAINTENANCE DIV.	<table border="0"> <tr> <td>INITIALS</td> <td>DATE</td> </tr> <tr> <td><i>RJR</i></td> <td>5-19-70</td> </tr> <tr> <td><i>WAB</i></td> <td>5-19-70</td> </tr> <tr> <td><i>AS</i></td> <td>4-15-70</td> </tr> <tr> <td><i>WAB</i></td> <td>5-19-70</td> </tr> <tr> <td><i>WAB</i></td> <td>5-19-70</td> </tr> </table>	INITIALS	DATE	<i>RJR</i>	5-19-70	<i>WAB</i>	5-19-70	<i>AS</i>	4-15-70	<i>WAB</i>	5-19-70	<i>WAB</i>	5-19-70	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS APPROVAL RECOMMENDED <i>C. J. ...</i> HEAD, BUREAU OF ENGINEERING APPROVED <i>P. ...</i> DIRECTOR OF PUBLIC WORKS	PREPARED BY KNOERLE, BENDER, STONE & ASSOCIATES, INC. AND MATZ, CHILDS, & ASSOCIATES, INC. CONSULTING ENGINEERS BALTIMORE, MARYLAND <i>John P. ...</i>	STATE ROADS COMMISSION OF MARYLAND REVIEWED AND APPROVAL RECOMMENDED <i>...</i> CHIEF, BUREAU OF ENGINEERING APPROVAL RECOMMENDED <i>...</i> CHIEF, INTERSTATE DIVISION FOR BALTIMORE CITY	APPROVAL RECOMMENDED <i>...</i> DEPUTY CHIEF ENGINEER - DEVELOPMENT APPROVED <i>...</i> CHIEF ENGINEER	U. S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION - BUREAU OF PUBLIC ROADS APPROVED DIVISION ENGINEER _____ DATE _____
INITIALS	DATE																	
<i>RJR</i>	5-19-70																	
<i>WAB</i>	5-19-70																	
<i>AS</i>	4-15-70																	
<i>WAB</i>	5-19-70																	
<i>WAB</i>	5-19-70																	