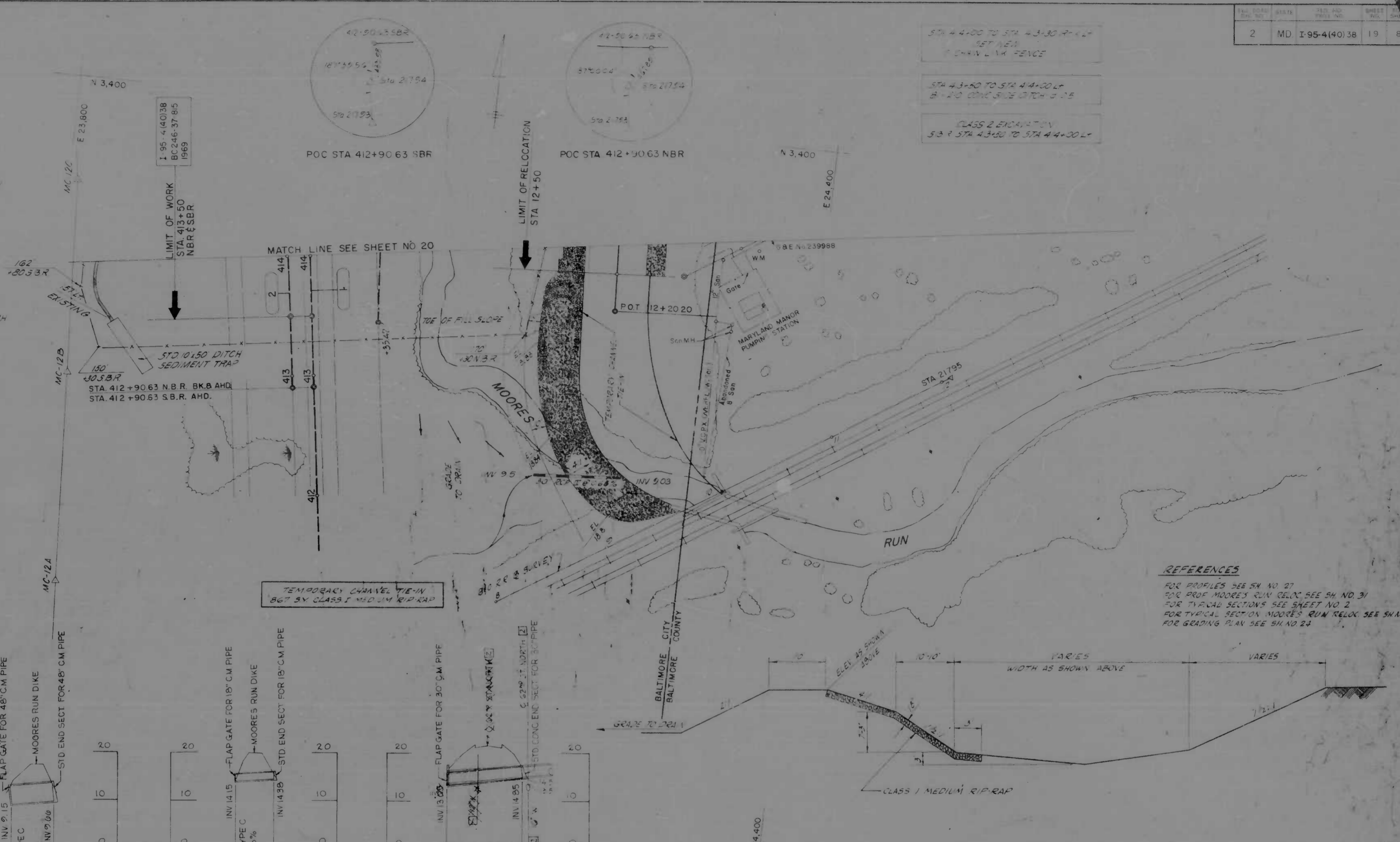


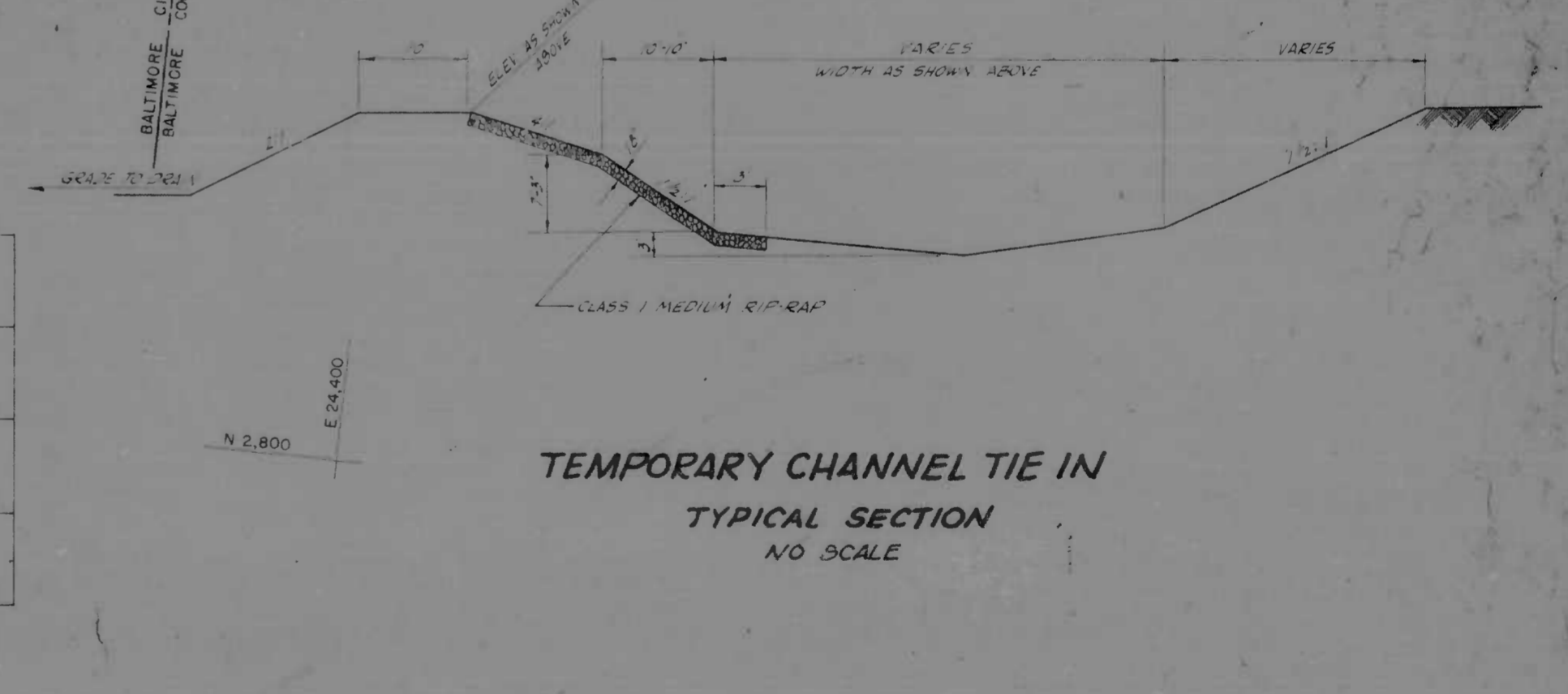
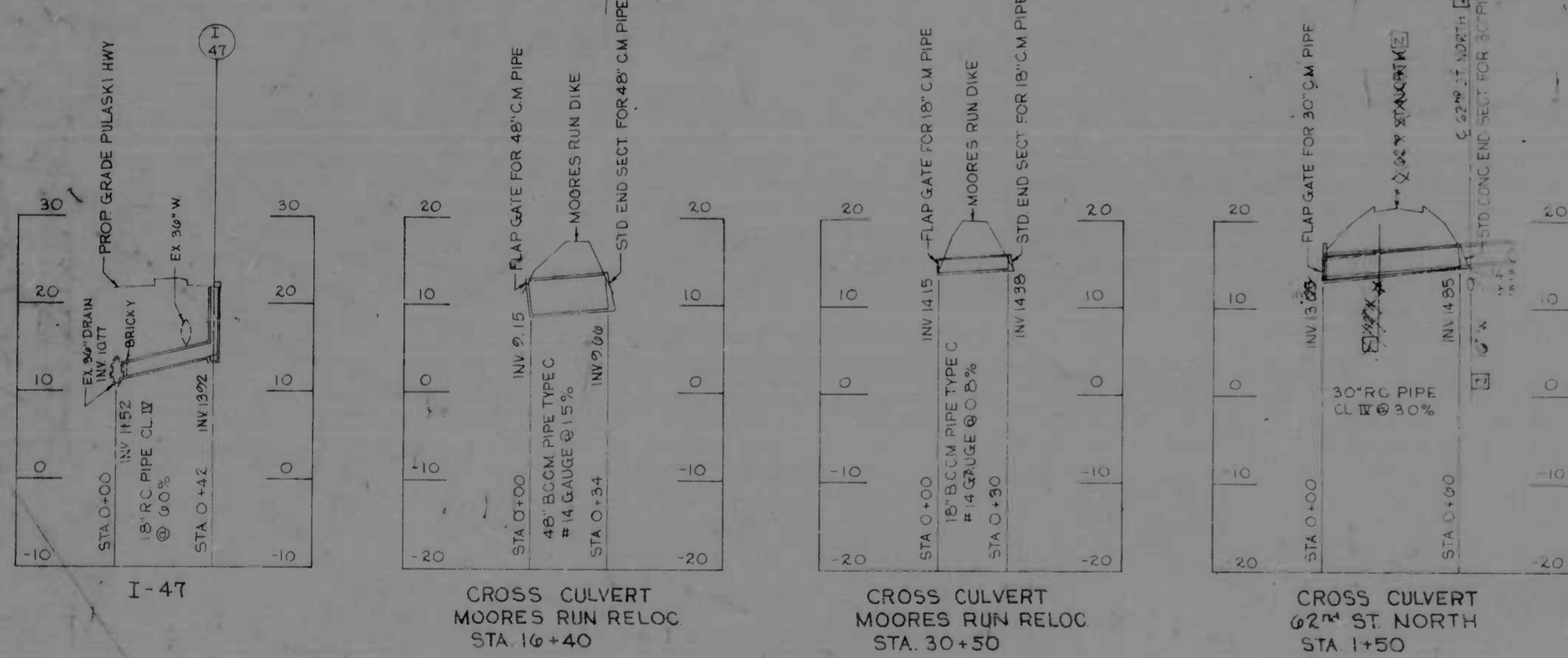
SEDIMENTATION CONTROLS

(FOR DETAILS SEE SHEET 1)

- SHEET 19**
CONSTRUCT STD DITCH SEDIMENT TRAP
STA 413+12
- SHEET 20**
CONSTRUCT STD DITCH SEDIMENT TRAPS
MAINLINE STA 416+12
STREAM STA 16+12
PROVIDE TEMPORARY SLOPE DRAINS
MAINLINE STA 417+12
STREAM STA 16+12
PROVIDE TEMPORARY BERM ALONG STREAM BENCH
STREAM STA 13+12 TO 16+12
STREAM STA 16+12 TO 18+12
- SHEET 21**
CONSTRUCT STD DITCH SEDIMENT TRAPS
RAMP H STA 17+12
RAMP H STA 18+12
RAMP G STA 14+12
RAMP G STA 16+12
PROVIDE TEMPORARY SLOPE DRAINS
MAINLINE STA 421+12
RAMP H STA 16+12
STREAM STA 25+12
PROVIDE TEMPORARY BERM ALONG STREAM BENCH
STREAM STA 23+12 TO 25+12
STREAM STA 25+12 TO 28+12
- SHEET 22**
CONSTRUCT STD DITCH SEDIMENT TRAPS
MAINLINE STA 431+12
RAMP G STA 20+12
RAMP G STA 30+12
PROVIDE TEMPORARY SLOPE DRAINS
MAINLINE STA 432+12
STREAM STA 31+12
PROVIDE TEMPORARY BERM ALONG STREAM BENCH
STREAM STA 28+12 TO 33+12



REFERENCES
FOR PROFILES SEE SH NO 27
FOR PROFILES MOORES RUN RELOC SEE SH NO 31
FOR TYPICAL SECTIONS SEE SHEET NO 2
FOR TYPICAL SECTION MOORES RUN RELOC SEE SH NO 22
FOR GRADING PLAN SEE SH NO 22



REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1) Addendum 1 to 2 - Feb 13, 1971 2) Revision No. 2 - Nov 24, 1972	KNOBLE, BENDER, STONE & ASSOC., INC. 140 MATT, GIBBS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21201	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: P.W.Z. TRACED BY: P.W.Z. F.A.P. NO. I-95-44038 S.R.C. NO. BC 246-37-B15 BALTO. CITY NO. 1969
		SCALE: 1" = 40'	DATE: _____ SHEET NO. 15 of 22

2. Typical Sections

Missing

missing

7 THRU 18 DETAILS

27 PROFILE
Missing

missing

29-31 PROFILE

33 . STORM DRAIN

Missing

Missing

35 36 STORM DRAIN

Missing

44 WATER MAIN

missing

48 - 51

Missing

53 THRU 56

missing

58 THRU 66

Missing

69 - 89

Missing

71 THRU 82

Missing

4 Typical SECTIONS

5 " "

NO.	DATE	BY	REVISION
2	MD	T-95-414038	70 82



1
CONSTR NBR
CURVE DATA
 $\Delta = 25^{\circ}44'30''$
 $T_s = 276.39$
 $D_c = 24^{\circ}44'30''$
 $R = 11,459.16'$
 $L = 4948.33'$
 $E_s = 0^{\circ}30'00''$
 $L_s = 200.00'$
 $L_T = 133.35'$
 $ST = 66.67'$

2
CONSTR SBR
CURVE DATA
 $\Delta = 59^{\circ}42'00''$
 $D_c = 0^{\circ}45'00''$
 $R = 7639.44'$
 $L = 380.33'$
 $E_s = 76.04'$
 $E_s = 0^{\circ}45'00''$
 $L_s = 200.00'$
 $L_T = 133.33'$
 $ST = 66.67'$

STA 414+00 TO STA 414+00.00
20' CONC. SIDE DITCH @ 1:0.5

STA 414+00 TO STA 414+00.00
20' CONC. SIDE DITCH @ 1:0.5

STA 414+00 RT
32" CONC. OUTLET DITCH
PIPED @ 4:1

STA 414+00 RAMP RT
48" CONC. OUTLET DITCH
PIPED @ 1:0.5

STA 414+00 TO STA 414+00.00
20' CONC. SIDE DITCH @ 1:0.5

STA 414+00 TO STA 414+00.00
20' CONC. SIDE DITCH @ 1:0.5

STA 414+00 RT
32" CONC. OUTLET DITCH
PIPED @ 4:1

CLASS & FINISH
STA 414+00 TO STA 414+00.00
CONC. SIDE DITCH @ 1:0.5

CLASS & FINISH
STA 414+00 TO STA 414+00.00
CONC. SIDE DITCH @ 1:0.5

REFERENCES
 FOR PROFILES SEE SHEET 118 11330
 FOR PROFILE MOORES RUN RELOCATED SEE SHEET 118 11331
 FOR TYPICAL SECTIONS SEE SHEET 118 11332
 FOR TYPICAL SECTION MOORES RUN RELOCATED SEE SHEET 118 11333
 FOR DRAINAGE DETAILS SEE SHEET 118 11334
 FOR CURVE DATA SEE SHEET 118 11335
 FOR STORM DRAIN PROFILES SEE SHEET NO. 34 THROUGH 37 AND 38
 FOR BARRELS CURB TRANSITIONS SEE SHEET NO. 15
 FOR SURVEY CONTROL NOTES SEE SHEET NO. 18

STD. NO. 3870 UNDERDRY
 4" x 4" STA 414+00 - STA 414+00.00
 1/2" x 1/2" STA 414+00 - STA 414+00.00
 2" x 2" STA 414+00 - STA 414+00.00
 OUTLET LOCATIONS AS SHOWN

- BITUMINOUS SURFACE MATERIAL
- BITUMINOUS OVERLAY ON CONVENTIONAL REINFORCED CONCRETE (CONTRASTING PAVEMENT)
- CONVENTIONAL REINFORCED CONCRETE
- CONTINUOUSLY REINFORCED CONCRETE

REVISIONS 1) Alteration to 2/1/77	CONSULTANT ANDRELL BENDER, STINE & ASSOC., INC.	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
MATZ CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202		INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	
SCALE: 1" = 40'	DATE	DRAWN BY: PMZ CHECKED BY: EWS	DESIGNED BY: JLC CHECKED BY: RKS
		PROJECT NO.: T-95-414038	SHEET NO.: 20 OF 82

HE BALTIMORE 17989

B.M. 1777 ELEV 9.13
BRASS SCREW IN A W END OF SEWER
INLET AT NE CORNER PULASKI HWY
AND G3RD ST

B.M. 1782 ELEV 33.20
BRASS DISK IN CONC MONUMENT FLUSH
WITH SURFACE OF PULASKI HWY MEDIAN,
30' N E OF 64TH ST

FED. ROAD DIST. NO.	STATE	F.P. NO.	PROJ. NO.
2	MD.	I-95-4(40)38	24

STA 19+20 TO STA 20+50 RT RAMP 4
TO 2" CONC SIDE DITCH 6" O.S.

STA 19+40 TO STA 19+60 RT RAMP 4
35'-20" SOD SIDE DITCH 6" O.S.

STA 20+50 TO STA 20+80 RT RAMP 4
36'-20" SOD SIDE DITCH 6" O.S.

STA 19+20 RT RAMP 4
5'-0" CURB OPENING
26'-30" CONC BUTTER 6" O.S.

STA 4+00 TO STA 4+00 RT RAMP 5
3'-0" SOD SIDE DITCH 6" O.S.

STA 4+00 TO STA 5+00 RT RAMP 5
3'-0" SOD SURFACE DRAIN DITCH 6" O.S.

CLASS 2 EXCAVATION
62ND STREET NORTH
STA 1+50 TO STA 4+00 RT

STA 11+00 TO 62ND ST CONN
36'-20" SOD OUTLET DITCH 6" O.S.

STA 12+71 RT PULASKI HWY
REBUILD INLET AND RESET SALAGED
GRATE CURB RADIUS TO BE VARIED
TO MEET INLET LOCATION

STA 12+71 TO STA 13+18
REBUILD MEDIAN TYPE 4 CURB

CLASS 2 EXCAVATION
RAMP 4
STA 13+40 TO STA 10+80 RT
STA 13+59 RT

CLASS 2 EXCAVATION
62ND & 63RD ST CONN RD
STA 2+80 LT

CLASS 2 EXCAVATION
PULASKI HWY
STA 108+80 TO STA 111+50 LT

NOTES

1. AT 5' CABLE IN PLACE CABLE SET 5' LEFT OF PULASKI HWY
LEFT SIDE MEDIAN AND 12' LEFT OF 64TH ST

2. SET 12" METAL 5" x 5" LEFT OF PULASKI HWY STA 109+25
REFLECTOR BEING THE WORK UNDER THE CITY OF BALTIMORE'S JURISDICTION.

- REFERENCES
- FOR PROFILES SEE SHEET NO. 28, 30, 32
 - FOR DRAINAGE DETAILS SEE SHEET NO. 15, 16, 17
 - FOR TYPICAL SECTIONS SEE SHEET NO. 34, 35
 - FOR GRADING PLAN & CONT. LAIDOUT SEE SHEET NO. 24, 25
 - FOR STORM DRAIN SCHEDULE SEE SHEET NO. 33
 - FOR T-TURN AROUND DETAIL SEE SHEET NO. 14
 - FOR CURVE DATA SEE SHEET NO. 38-40
 - FOR UTILITY RELOCATION SEE SHEET NO. 38-40
 - FOR MAINTENANCE OF TRAFFIC SEE SHEET NO. 46-48
 - FOR DRIVEWAY DETAILS SEE SHEET NO. 14
 - FOR STORM DRAIN PROFILES SEE SHEET NO. 34, 37, 19

- 3 CONVENTIONAL REINFORCED CONCRETE
- BITUMINOUS PAVEMENT OR CONCRETE SIDEWALK (SEE TYPICAL SECTIONS)

REVISIONS (1) Added to 10-2-1997 (2) Revised	CONSULTANT CROSSLER, BERNER, STONE & ASSOC., INC. AND W&Z SCHUBB & ASSOC. INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: P.W.Z. TRACED BY: P.W.Z. F.A.P. NO. I-95-4(40)38 S.R.C. NO. BC 256.37-818 BALTO. CITY NO. 1989
SCALE	DATE	DES. BY: J.L.C. CHK. BY: B.W.C.	SHEET NO. 22 OF 82

DATE	NO.	BY	REVISION
2	MD	195-4-40138	23 82

CLASS 2 EXCAVATION
 STA 429+00 TO STA 430+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 430+00 TO STA 431+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 431+00 TO STA 432+00
 24" CONC. OUTLET DITCH 24" DIA.

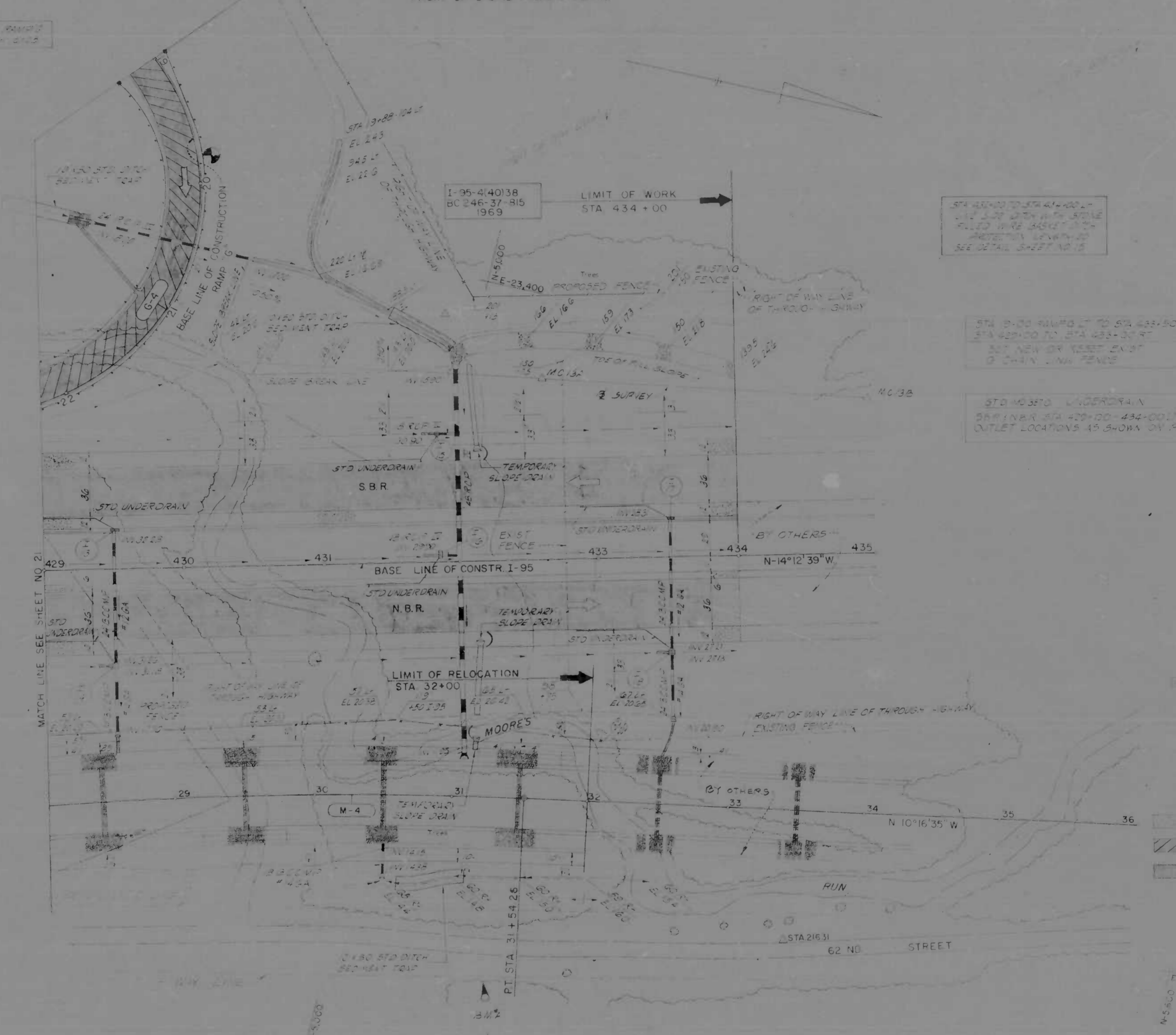
CLASS 2 EXCAVATION
 STA 432+00 TO STA 433+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 433+00 TO STA 434+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 434+00 TO STA 435+00
 24" CONC. OUTLET DITCH 24" DIA.

CLASS 2 EXCAVATION
 STA 435+00 TO STA 436+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 436+00 TO STA 437+00
 24" CONC. OUTLET DITCH 24" DIA.

CLASS 2 EXCAVATION
 STA 437+00 TO STA 438+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 438+00 TO STA 439+00
 24" CONC. OUTLET DITCH 24" DIA.

CLASS 2 EXCAVATION
 STA 439+00 TO STA 440+00
 24" CONC. OUTLET DITCH 24" DIA.
 STA 440+00 TO STA 441+00
 24" CONC. OUTLET DITCH 24" DIA.

B.M. #1 ELEV 171.4
 BOAT SPIKE SET IN BASE 18 MAPLE
 TREE 30' W OF 62 NO ST G28
 FROM CURB LINE PULASKI HIGHWAY



STA 434+00 TO STA 435+00
 24" CONC. OUTLET DITCH 24" DIA.
 SEE DETAIL SHEET NO. 15

STA 435+00 RAMP TO STA 436+00
 STA 436+00 TO STA 437+00
 STA 437+00 TO STA 438+00
 STA 438+00 TO STA 439+00
 STA 439+00 TO STA 440+00
 STA 440+00 TO STA 441+00

CONCRETE AND REBAR
 STA 432+00 TO STA 433+00
 STA 433+00 TO STA 434+00

STA 435+00 UNDERDRAIN
 S.B.R. STA 435+00 - 436+00
 N.B.R. STA 436+00 - 437+00
 OUTLET LOCATIONS 15' FROM CURB

CONSTRUCTION RAMP G
 STA 435+00 TO STA 436+00
 STA 436+00 TO STA 437+00
 STA 437+00 TO STA 438+00
 STA 438+00 TO STA 439+00
 STA 439+00 TO STA 440+00
 STA 440+00 TO STA 441+00

CONSTRUCTION RAMP H
 STA 435+00 TO STA 436+00
 STA 436+00 TO STA 437+00
 STA 437+00 TO STA 438+00
 STA 438+00 TO STA 439+00
 STA 439+00 TO STA 440+00
 STA 440+00 TO STA 441+00

REFERENCES
 FOR PROFILES SEE SHEET NO. 12
 FOR PROFILE MOORES RUN RELOCATED SEE SHEET NO. 32
 FOR TYPICAL SECTIONS SEE SHEET NO. 24
 FOR TYPICAL SECTION MOORES RUN RELOCATED SEE SHEET NO. 34
 FOR DRAINAGE DETAILS SEE SHEET NO. 14
 FOR ROAD PLAN AND ELEVATION SEE SHEET NO. 15
 FOR CURVE DATA SEE SHEET NO. 8
 FOR STORM DRAIN SCHEDULE SEE SHEET NO. 33
 FOR STORM DRAIN PROFILES SEE SHEET NO. 34
 AND SEE SHEET NO. 11 FOR OTHER SHEET NO. 15

- 3" THICKNESS SAND OR MATERIAL
- 8" CONVENTIONAL REINFORCED CONCRETE
- 4" CONTINUOUS REINFORCED CONCRETE

REVISIONS Addendum No. 1 (1/18/79)	CONSULTANT KANDARI, BENDER, STONE & ASSOC., INC. AND MATL. CHILDS & ASSOC., INC. CONSULTING ENGINEERS 943 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
DRAWN BY JEL CHECKED BY G.S. DATE 1-95-440138 SHEET NO. 23 OF 82	SHEET NO. 23 OF 82	SCALE: 1" = 40' DATE:	BALTO. CITY NO. 1959

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	1-95-44038	24	82



LEGEND

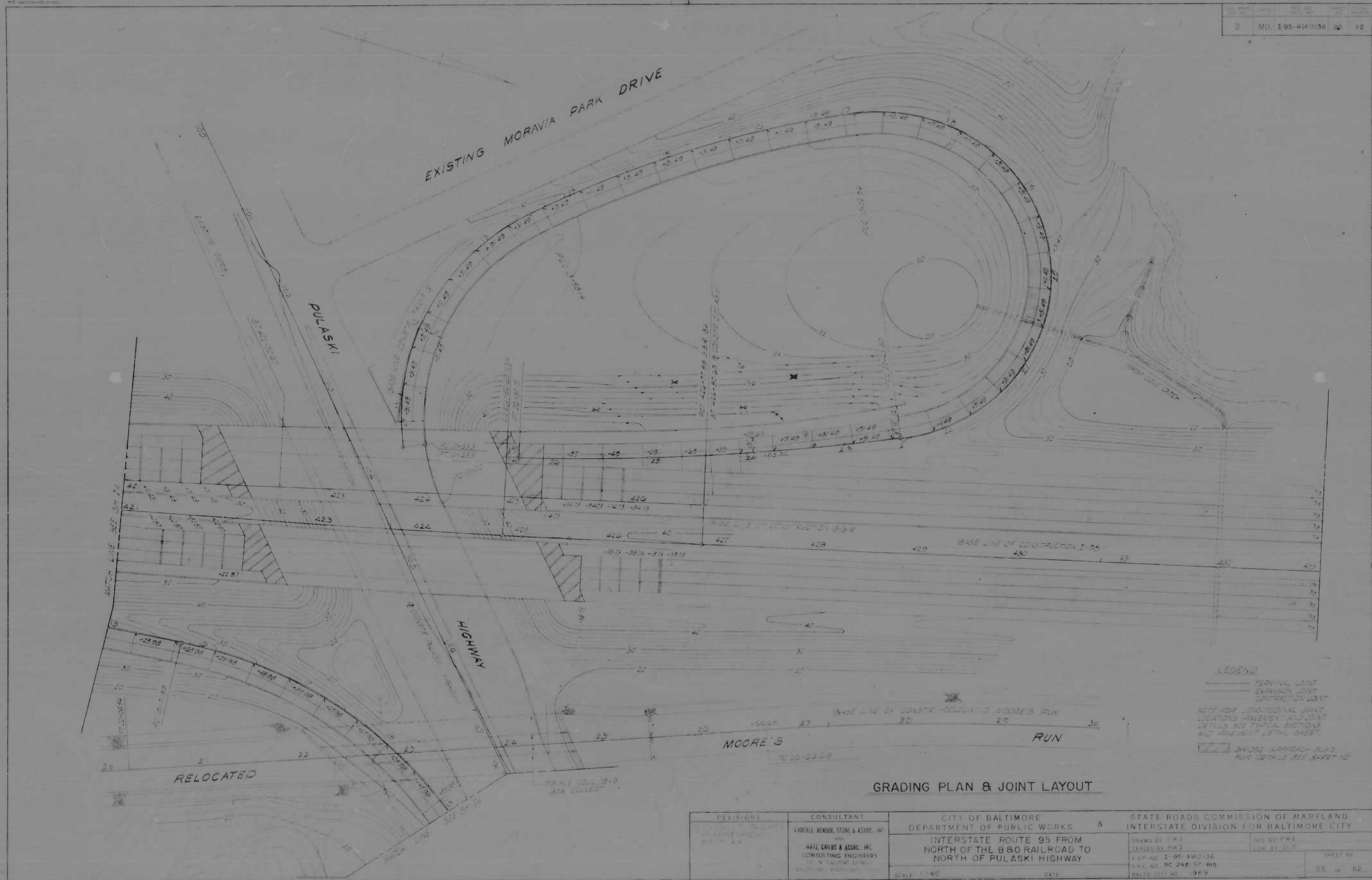
TERMINAL JOINT
 EXPANSION JOINT
 CONSTRUCTION JOINT
 FUTURE JOINTS

NOTE FOR LONGITUDINAL JOINT LOCATIONS
 PAVEMENT AND JOINT DETAILS SEE TYPICAL
 SECTIONS AND PAVEMENT DETAIL SHEET

GRADING PLAN & JOINT LAYOUT

REVISIONS (None listed)	CONSULTANT WHEELER, BERKELEY, STONE & ASSOC., INC. MATZ, GORRIS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: PWZ TRACED BY: PWZ DES. BY: PWZ CHK. BY: JLC F.A.P. NO. 1-95-44038 S.R.C. NO. BC 14E 37-915 BALTO. DIST. NO. 1569
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PROJECT NO.	DATE	SHEET NO.	TOTAL SHEETS
2	MO. I-95-4(4)138	25	62



LEGEND

TERMINAL JOINT
 EXPANSION JOINT
 CONSTRUCTION JOINT

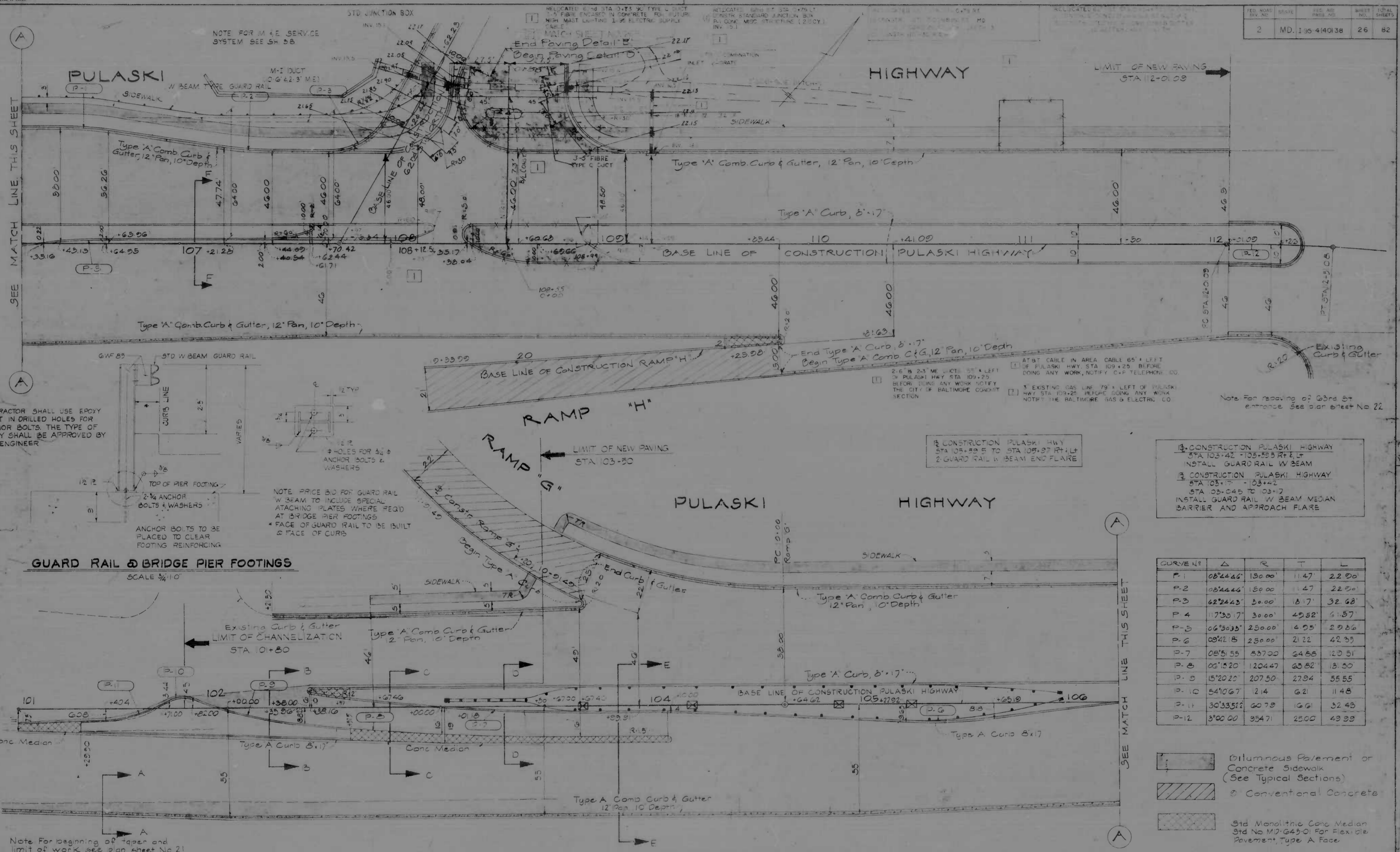
NOTE FOR LONGITUDINAL JOINT
 LOCATIONS, PAVEMENT AND JOINT
 DETAILS SEE TYPICAL SECTIONS
 AND PAVEMENT DETAIL SHEET

BRIDGE APPROACH SLAB
 SEE DETAIL SHEET 25-62

GRADING PLAN & JOINT LAYOUT

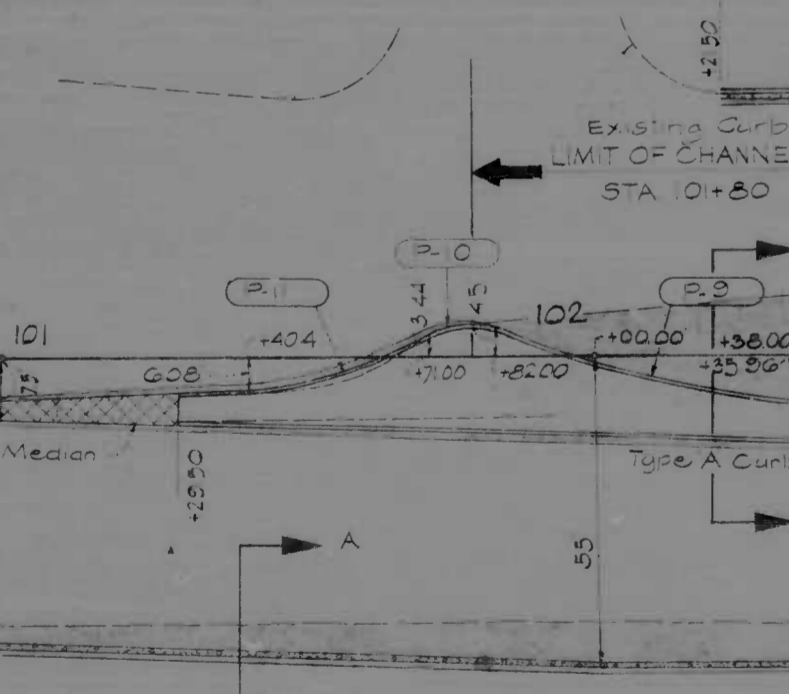
REVISIONS NO. DATE BY 1 10/15/69 JLC	CONSULTANT KNOWLE, BENDER, STONE & ASSOC. INC. 1110 N. CALVERT STREET BALTIMORE, MARYLAND 21202 HALL, CHUBB & ASSOC. INC. CONSULTING ENGINEERS 111 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
			DRAWN BY P.W.Z. CHECKED BY J.L.C.	DATE 10/15/69

FEED NO.	REV. NO.	DATE	BY	TOTAL SHEETS
2	MD 135 4(40)38	26	82	



CONTRACTOR SHALL USE EPOXY GROUT IN DRILLED HOLES FOR ANCHOR BOLTS. THE TYPE OF EPOXY SHALL BE APPROVED BY THE ENGINEER.

GUARD RAIL & BRIDGE PIER FOOTINGS
SCALE 3/4"=1'-0"



NOTE: PRICE SO FOR GUARD RAIL W BEAM TO INCLUDE SPECIAL ATTACHING PLATES WHERE REQ'D AT BRIDGE PIER FOOTINGS. FACE OF GUARD RAIL TO BE BUILT @ FACE OF CURB.

1. CONSTRUCTION PULASKI HWY STA 105+00 TO STA 108+27 RT+LT 2. GUARD RAIL W BEAM END FLARE

13. CONSTRUCTION PULASKI HIGHWAY STA 103+42 - 105+88 RT+LT INSTALL GUARD RAIL W BEAM
14. CONSTRUCTION PULASKI HIGHWAY STA 103+17 - 103+42 STA 03+045 TO 03+7 INSTALL GUARD RAIL W BEAM MEDIAN BARRIER AND APPROACH FLARE

CURVE NO.	Δ	R	T	L
P-1	08°44'46"	130.00'	11.47'	22.20'
P-2	08°44'46"	150.00'	13.47'	22.20'
P-3	62°24'43"	30.00'	8.17'	32.68'
P-4	17°35'17"	30.00'	49.52'	61.57'
P-5	06°39'35"	250.00'	14.95'	29.26'
P-6	08°42'18"	250.00'	21.22'	42.35'
P-7	08°15'55"	837.00'	64.66'	129.51'
P-8	06°18'20"	1204.47'	63.52'	131.50'
P-9	5°20'20"	207.50'	27.94'	55.55'
P-10	54°10'67"	12.14'	6.21'	11.48'
P-11	30°33'52"	60.79'	10.61'	32.43'
P-12	3°00'00"	354.71'	25.00'	49.33'

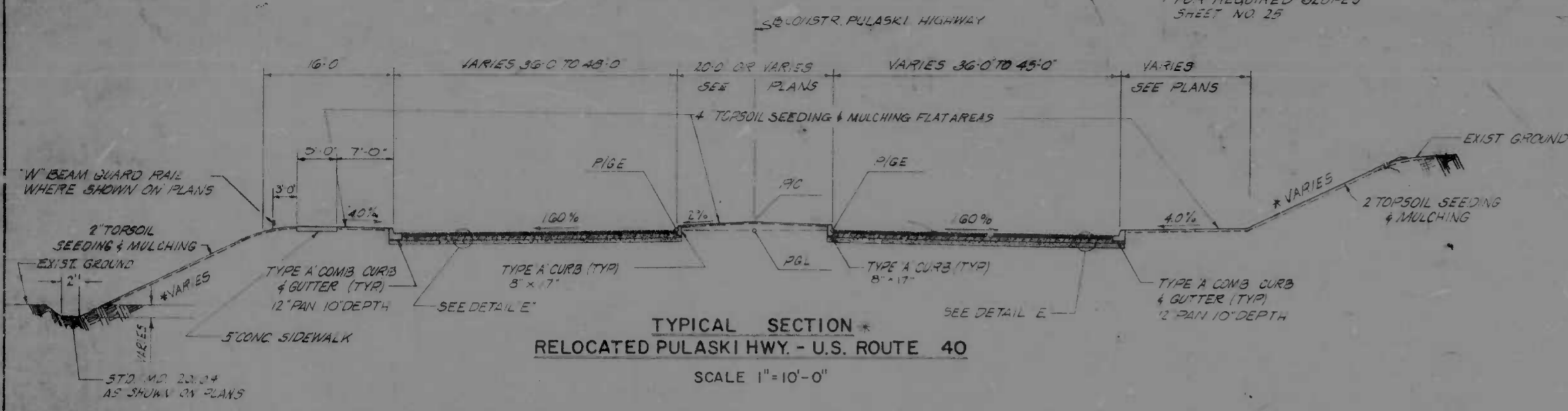
- Bituminous Pavement or Concrete Sidewalk (See Typical Sections)
- Conventional Concrete
- Std Monolithic Conc Median Std No. MD-G45-O1 For Flexible Pavement, Type A Face

Note for beginning of taper and limit of work see plan sheet No 21

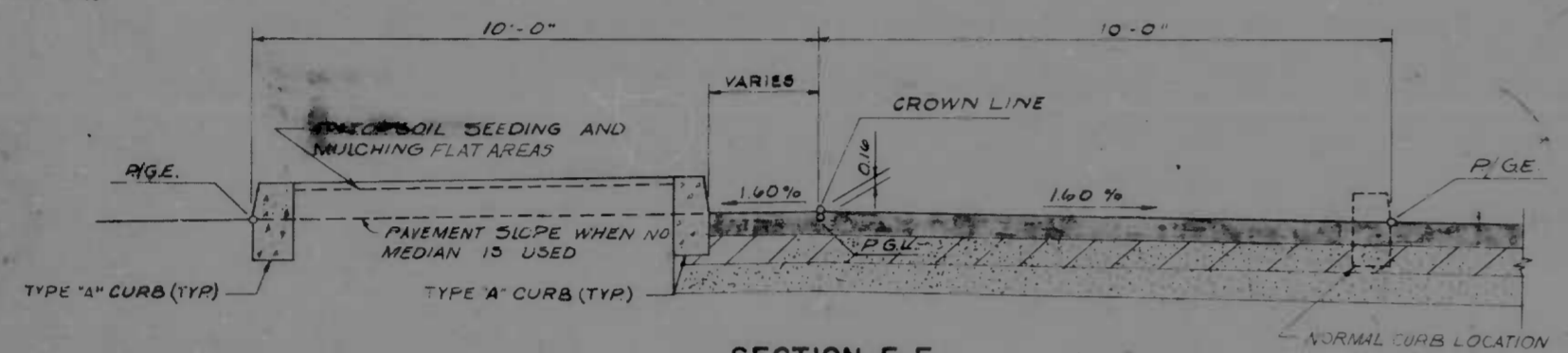
REVISIONS 1. REVISED 10/21/76 2. REVISED FEB 7, 77	CONSULTANT ENDRELL, BENDER, STONE & ASSOC., INC. AND MATZ, CURBS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: C.E.U. TRACED BY: F.W.Z. F.P. NO. 1-95-4(40)38 S.R.C. NO. BC 246-37-815 BALTO. CITY NO. 1969	DES. BY: K.H. CHK. BY: J.L.C. SHEET NO. 26 OF 82
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FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(40)38	3	82

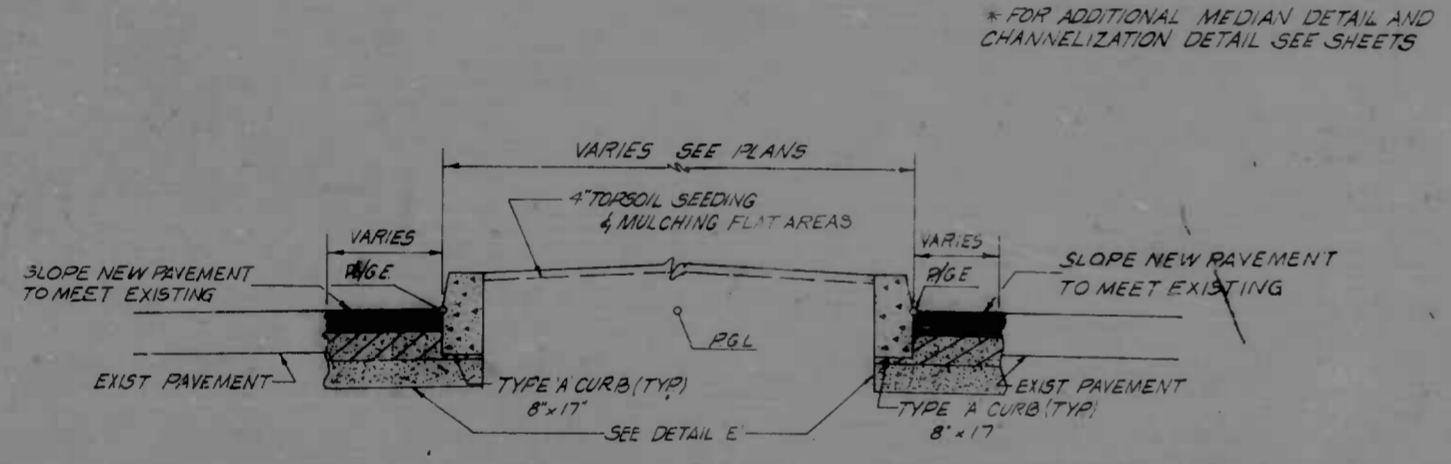
* NOTE SEE GRADING PLANS FOR REQUIRED SLOPES SHEET NO. 29



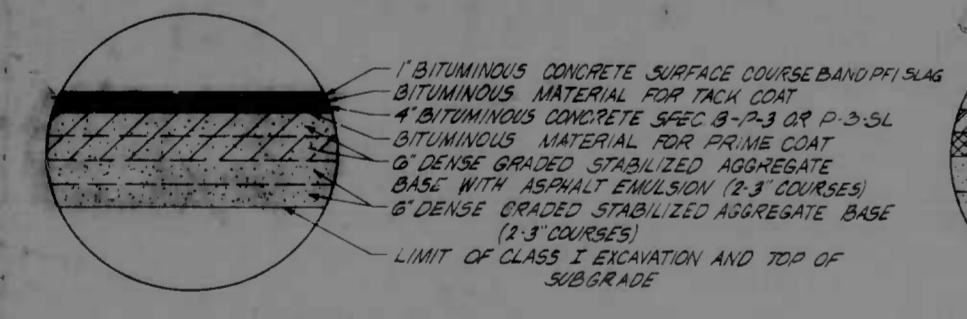
TYPICAL SECTION
RELOCATED PULASKI HWY - U.S. ROUTE 40
SCALE 1" = 10'-0"



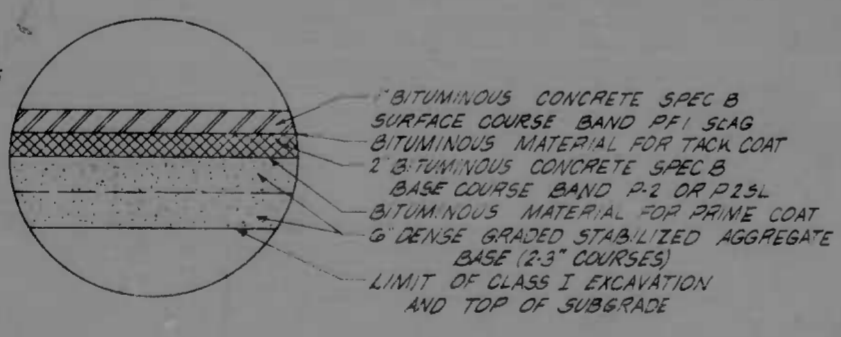
SECTION F-F
TYPICAL SECTION
VARIABLE WIDTH MEDIAN
SCALE 1" = 2'-0"



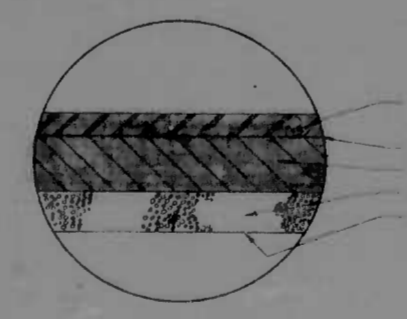
TYPICAL SECTION
NEW MEDIAN
SCALE 1" = 2'-0"



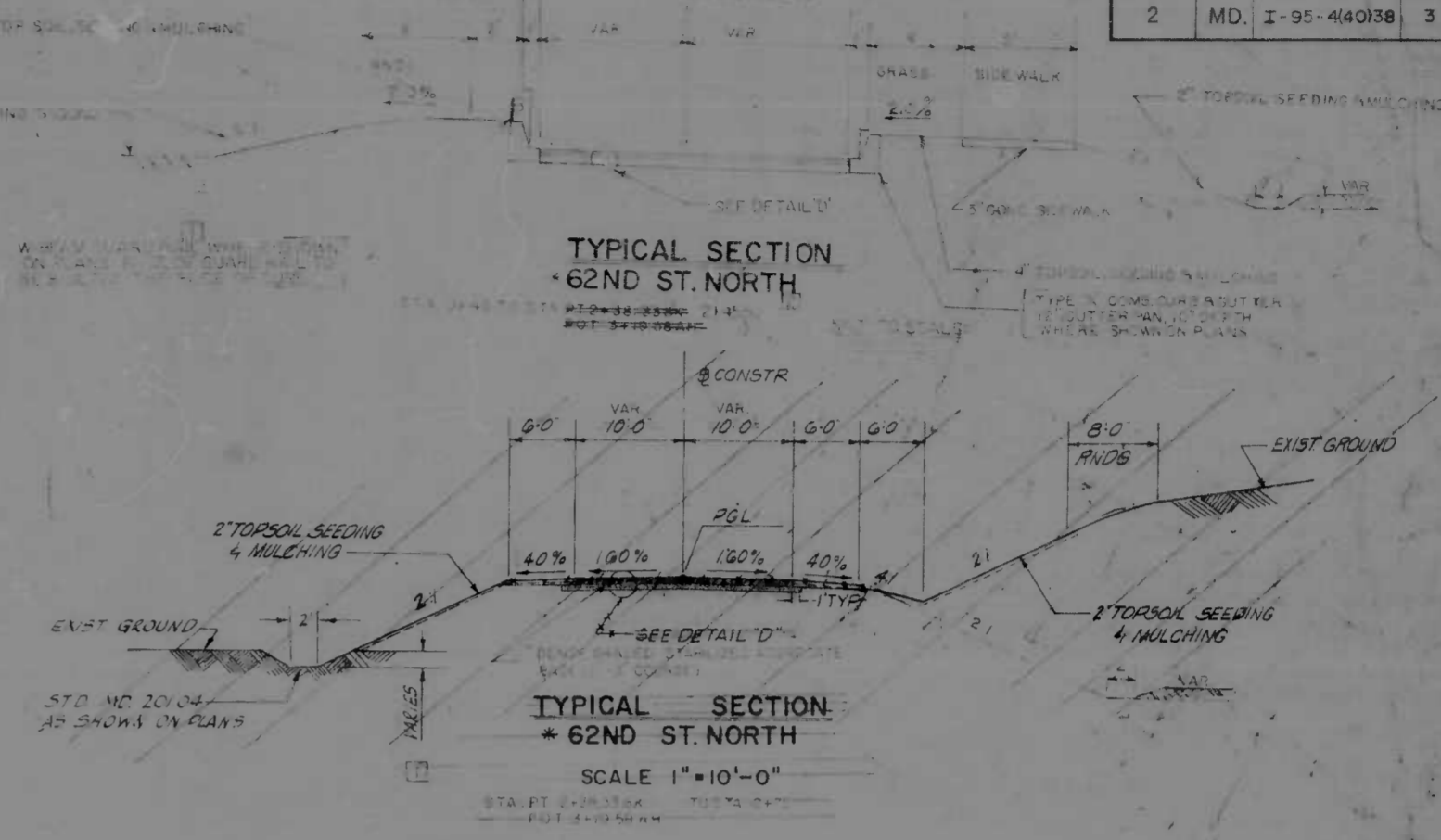
TYPICAL PAVING DETAIL "E"



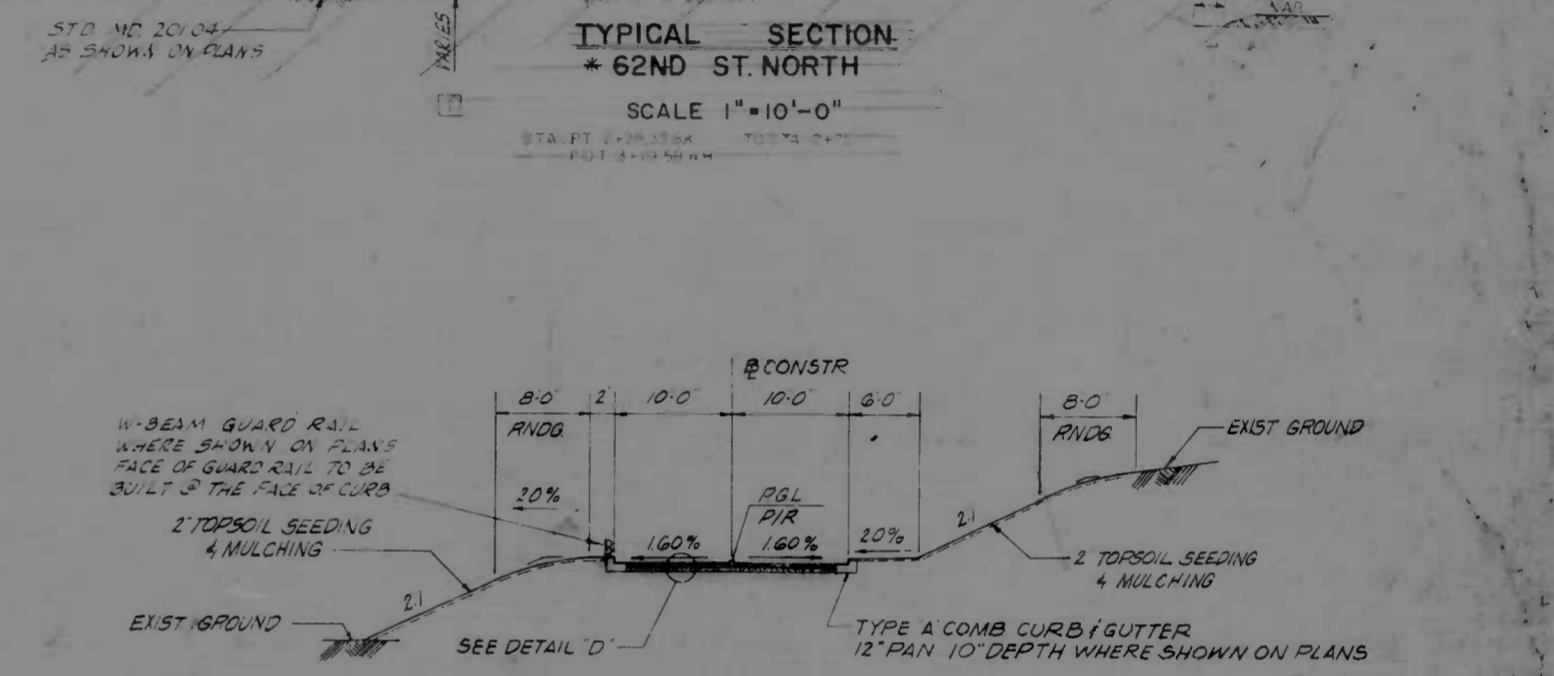
TYPICAL PAVING DETAIL "D"



TYPICAL PAVING DETAIL "G"

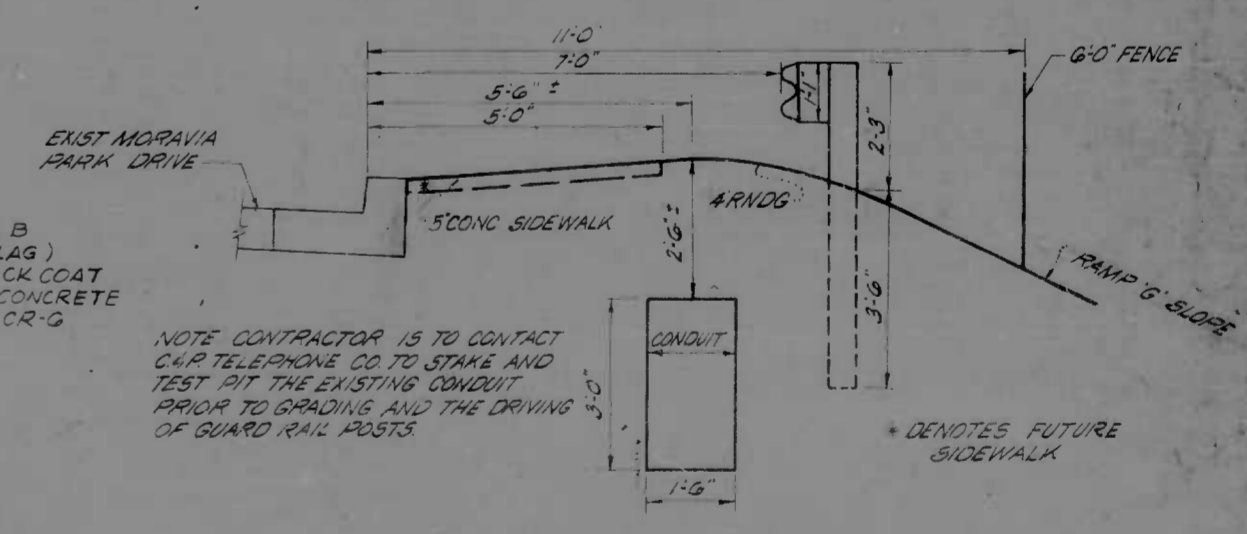


TYPICAL SECTION
62ND ST. NORTH
SCALE 1" = 10'-0"



TYPICAL SECTION
62ND ST. NORTH
62ND ST. & 63RD ST. CONNECTION ROAD
SCALE 1" = 10'-0"

* COMB CURB & GUTTER TO BE USED AS INDICATED ON PLANS



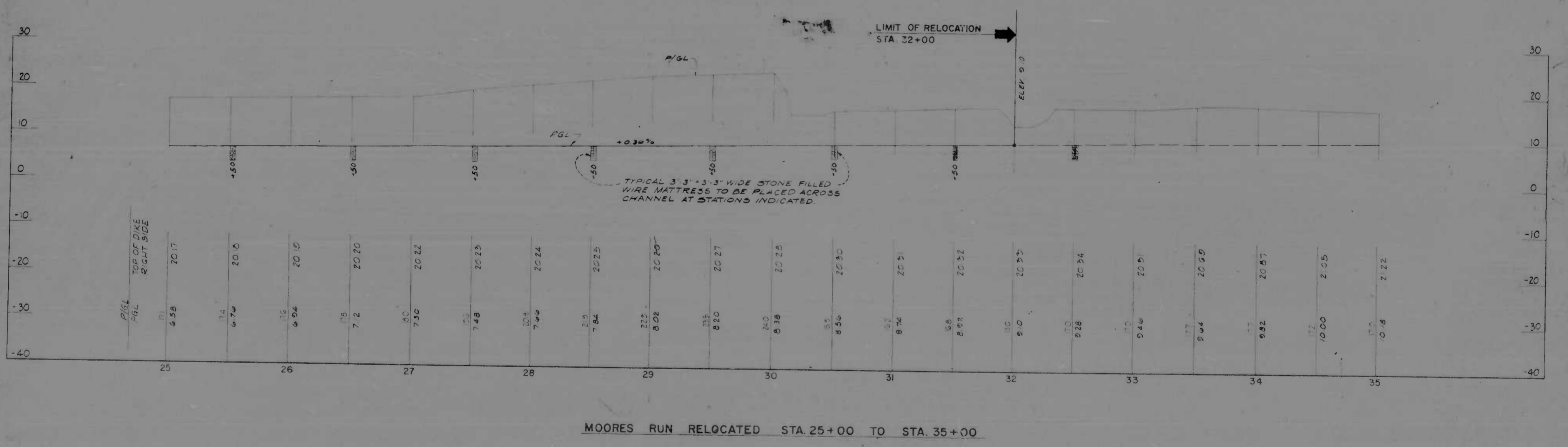
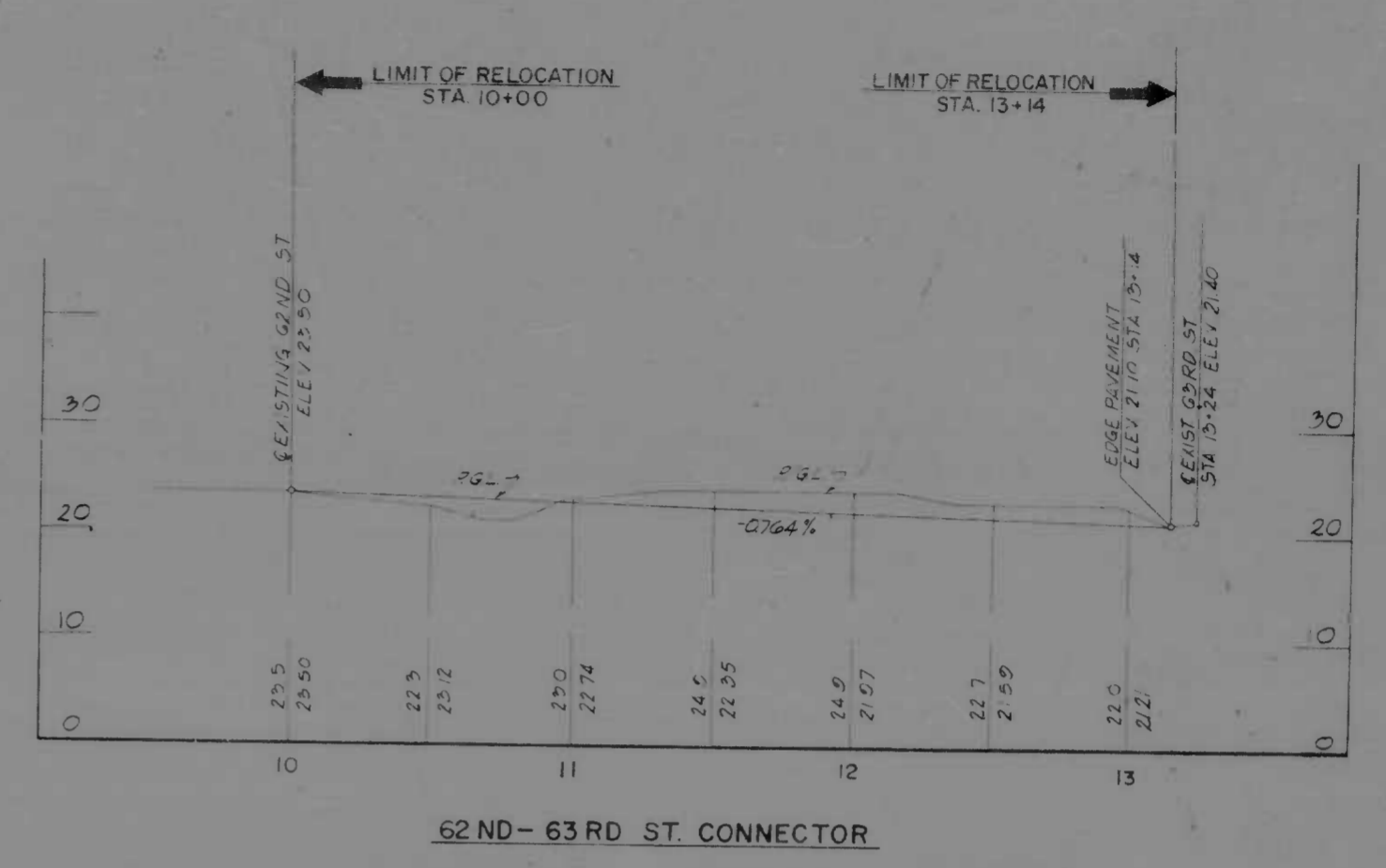
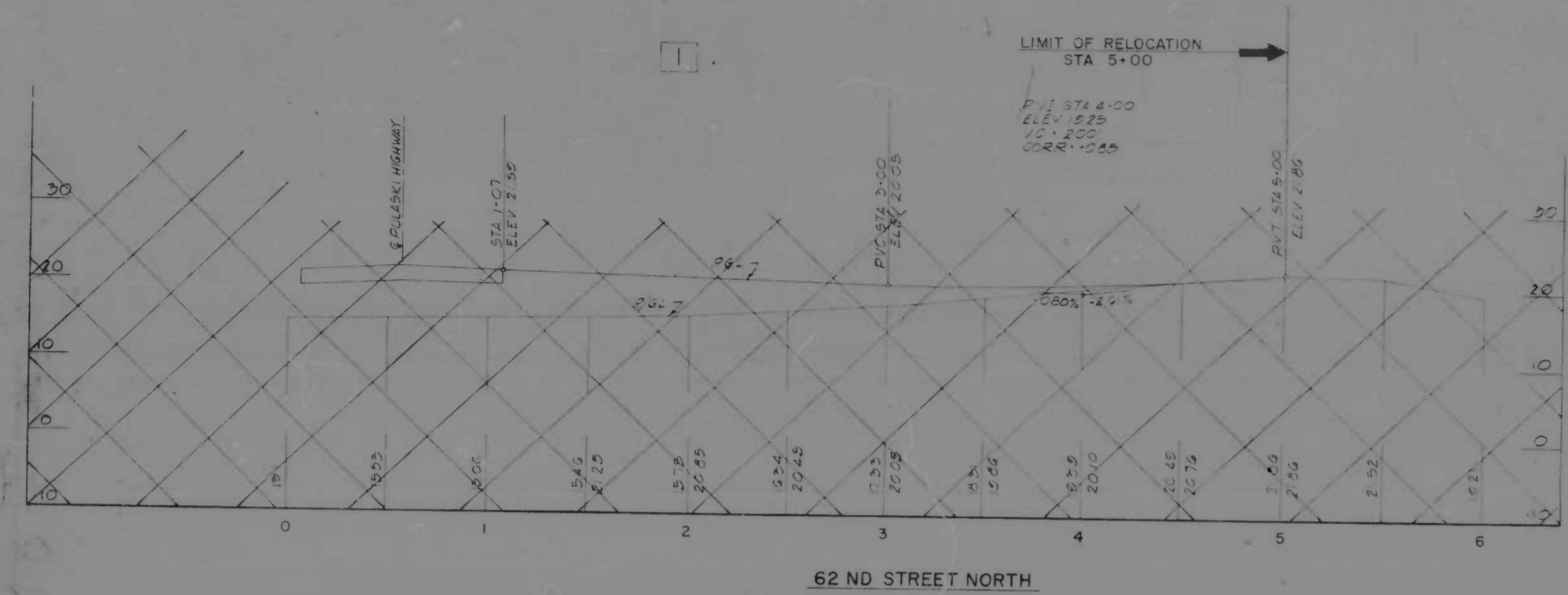
TYPICAL SECTION
MORAVIA PARK RD. & RAMP "G" GRADING
SCALE 1" = 2'-0"

NOTE CONTRACTOR IS TO CONTACT C&P TELEPHONE CO. TO STAKE AND TEST PIT THE EXISTING CONDUIT PRIOR TO GRADING AND THE DRIVING OF GUARD RAIL POSTS.

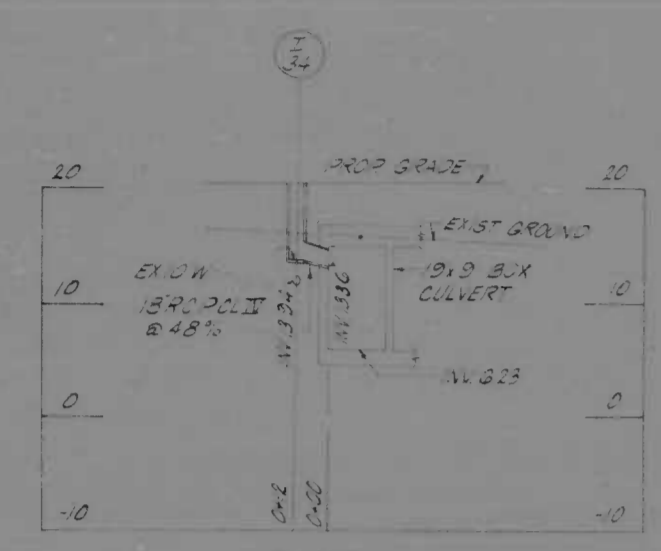
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. REVISED TO SHOW GRADING PLANS	KOZAR, NEMER, STONE & ASSOC., INC. AND HAZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY P.W.Z. TRACED BY P.W.Z. F.A.P. NO. I-95-4(40)38 S.R.C. NO. BS 245-37-815 BALTO. CITY NO. 1969
		SCALE: As Shown	DES. BY J.L.C. CHK. BY R.W.C. SHEET NO. 3 OF 92

FOR PROPOSED RELOCATED 62nd STREET
 PROFILE SEE SHEET 26A.

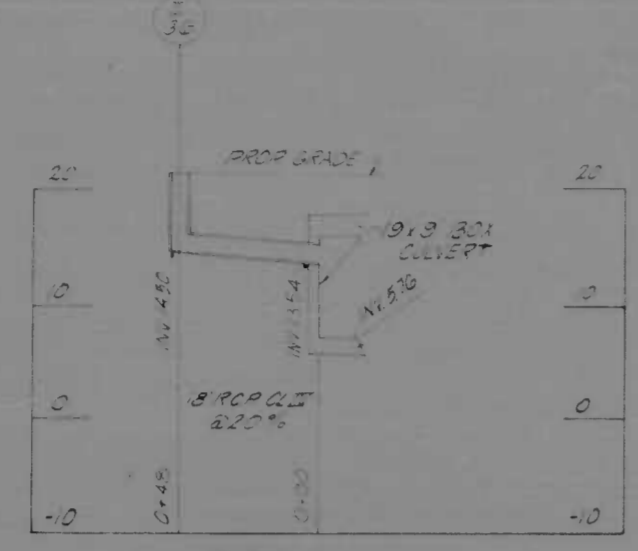
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-440(38)	32	82



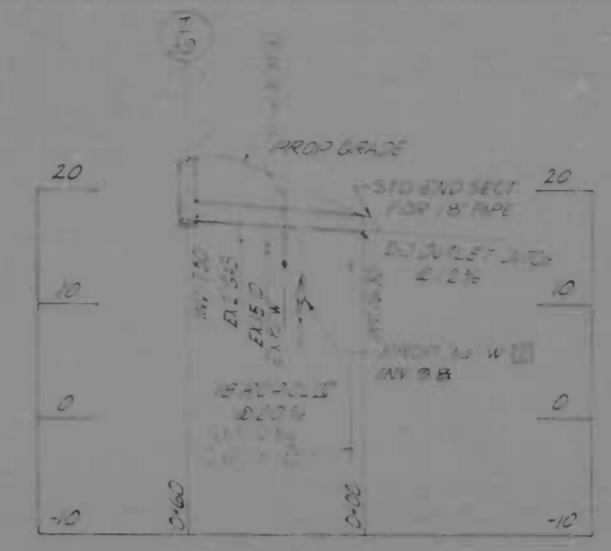
REVISIONS 1. AS SHOWN NOV 24 1972 2. REV. FEB 1975	CONSULTANT KIMBLE, BENFORD, STONE & ASSOC., INC. AND MATZ, GARDNER & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS &		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY		DRAWN BY J.W.W. TRACED BY J.W.W.	DES. BY T.E.L. CHK. BY J.L.C.
SCALE: HORIZ. 1"=40' VERT. 1"=10' DATE:		F.A.P. NO. I-95-440(38) S.R.C. NO. 246-37-815 BALTO. CITY NO. 1969			



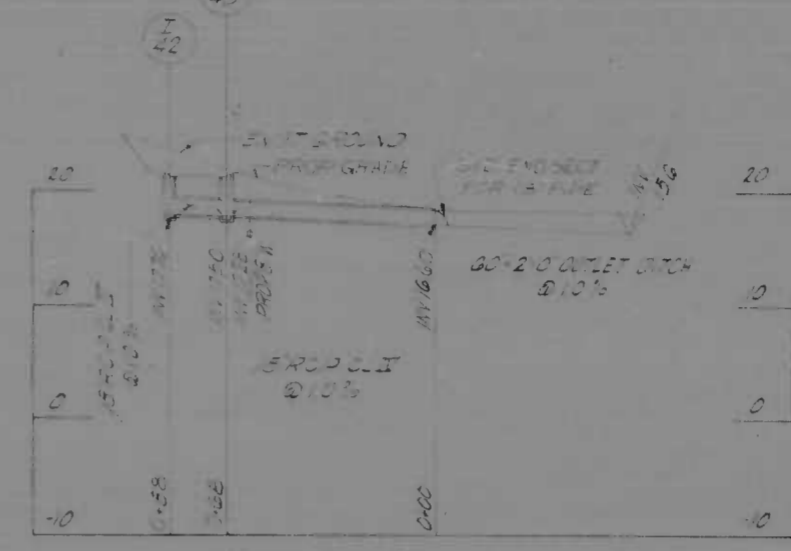
I-34



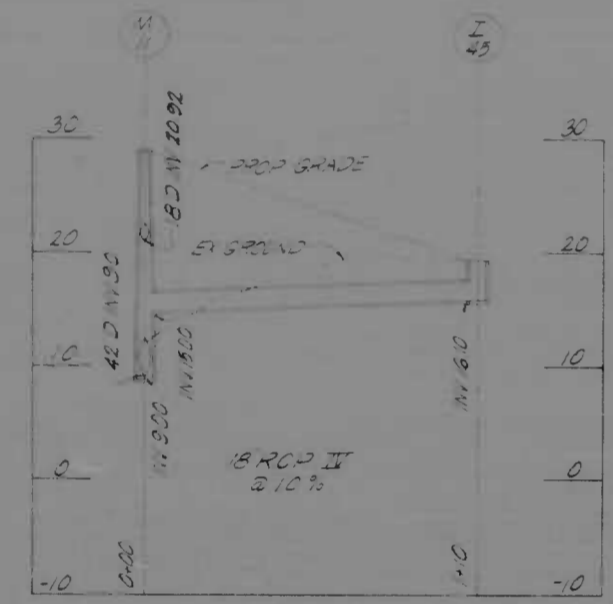
I-36



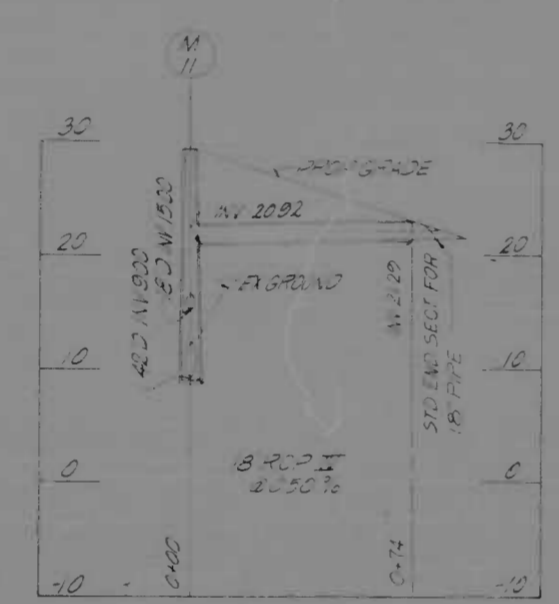
I-39



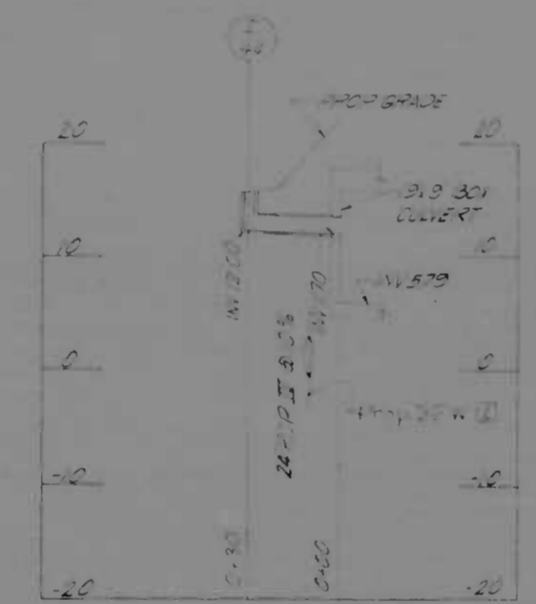
I-42 & I-43



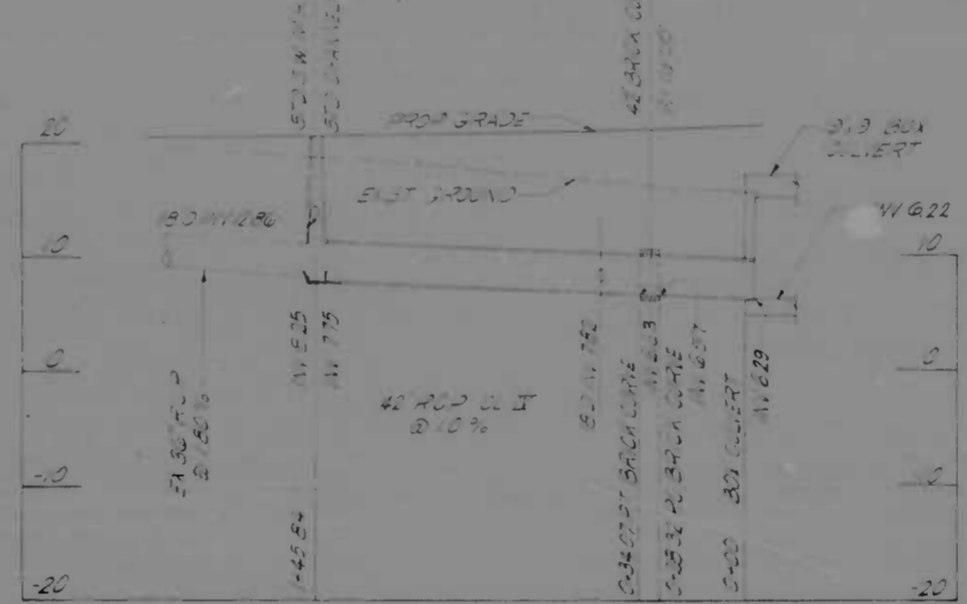
M-11-S



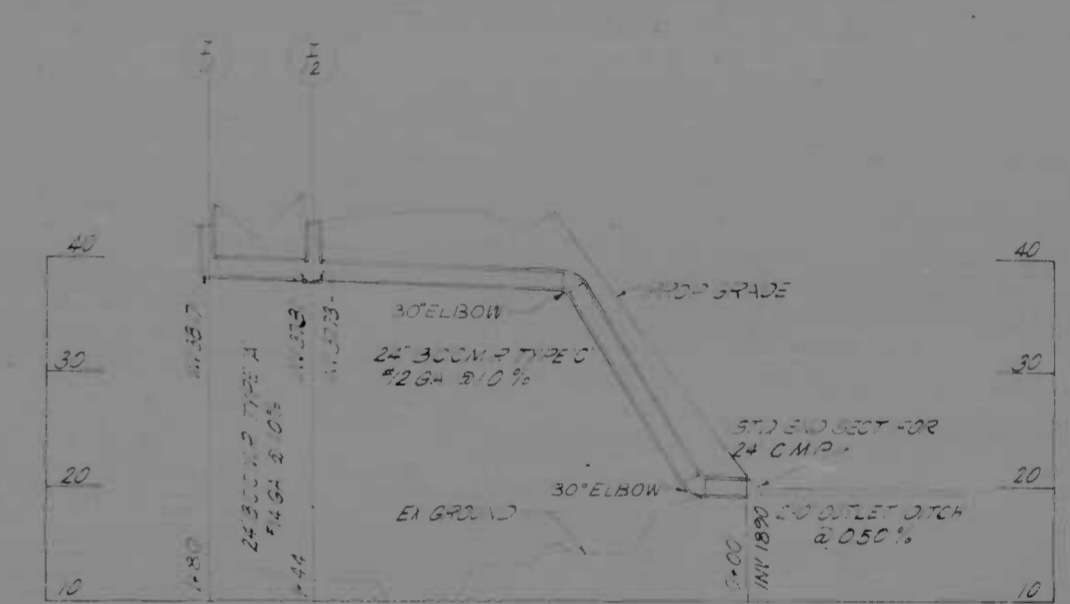
M-11-N



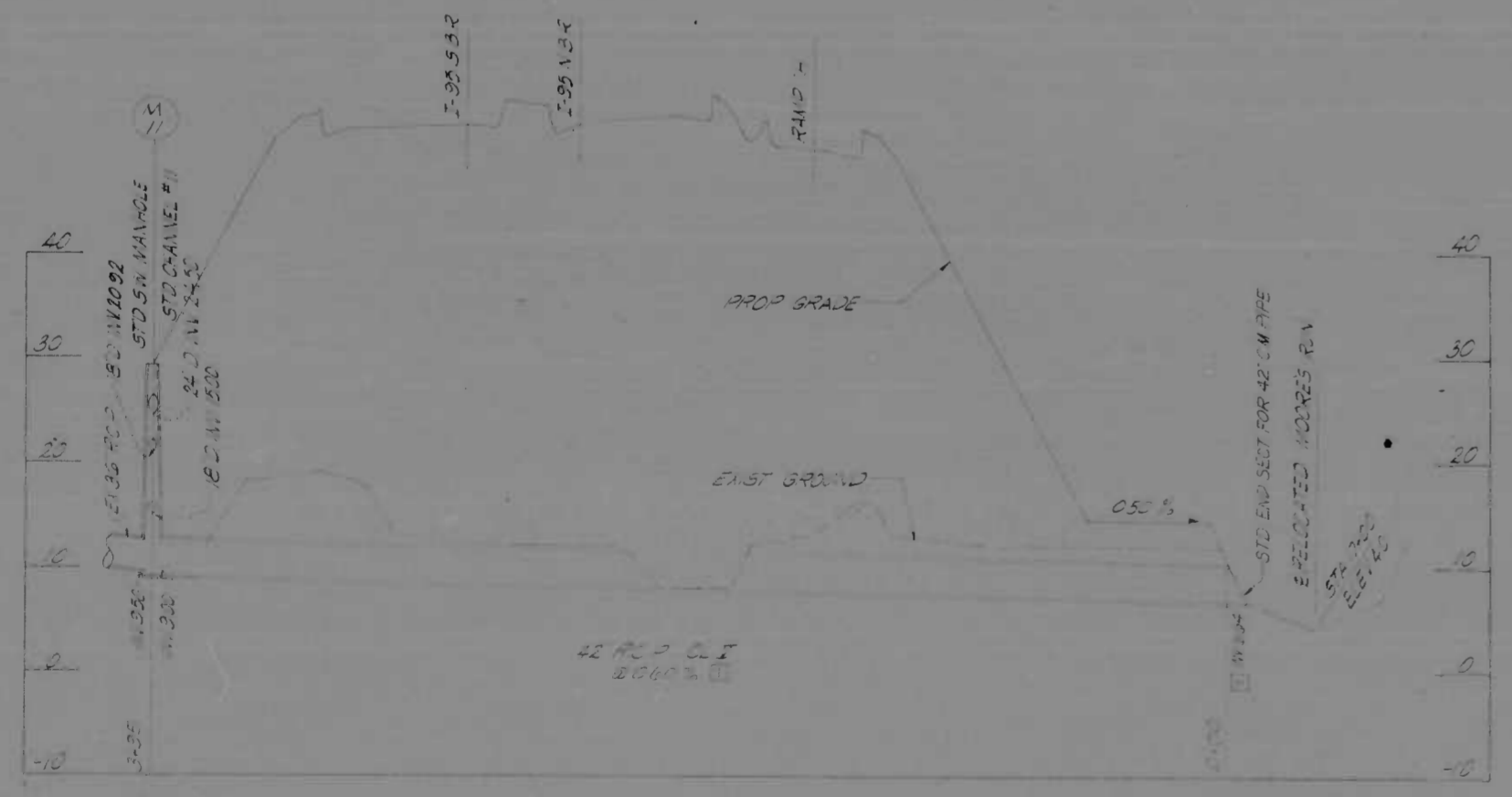
I-44



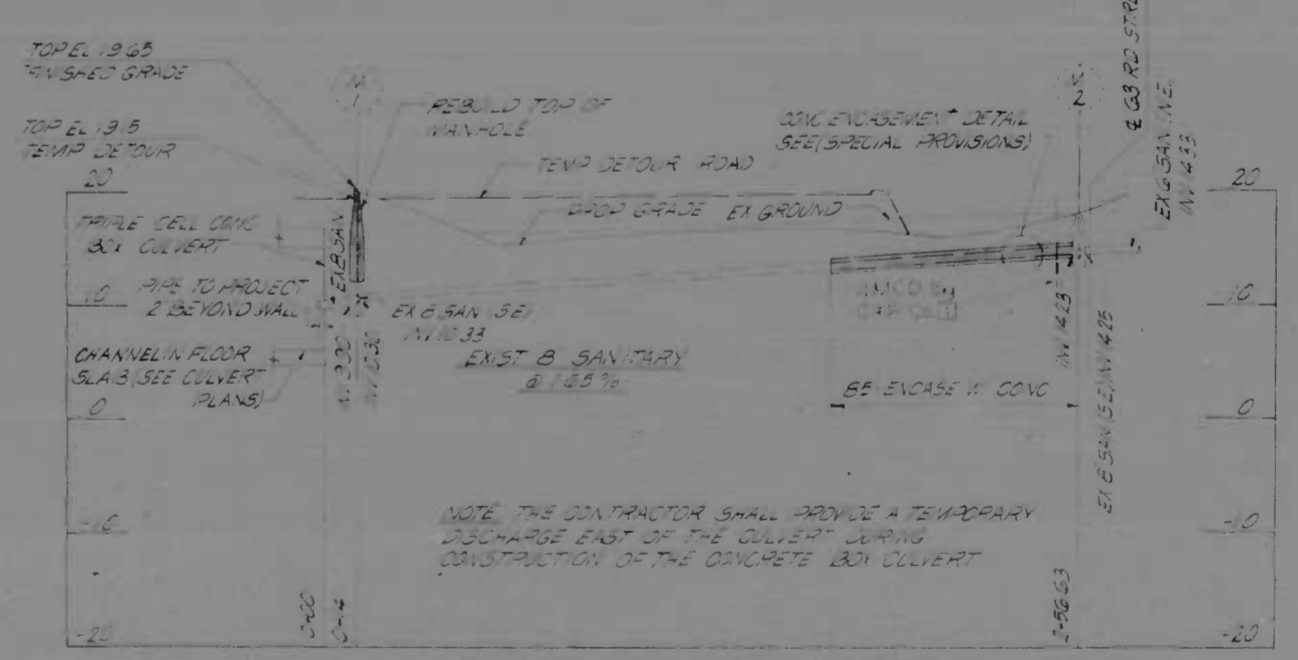
M-12



I-11 & I-12



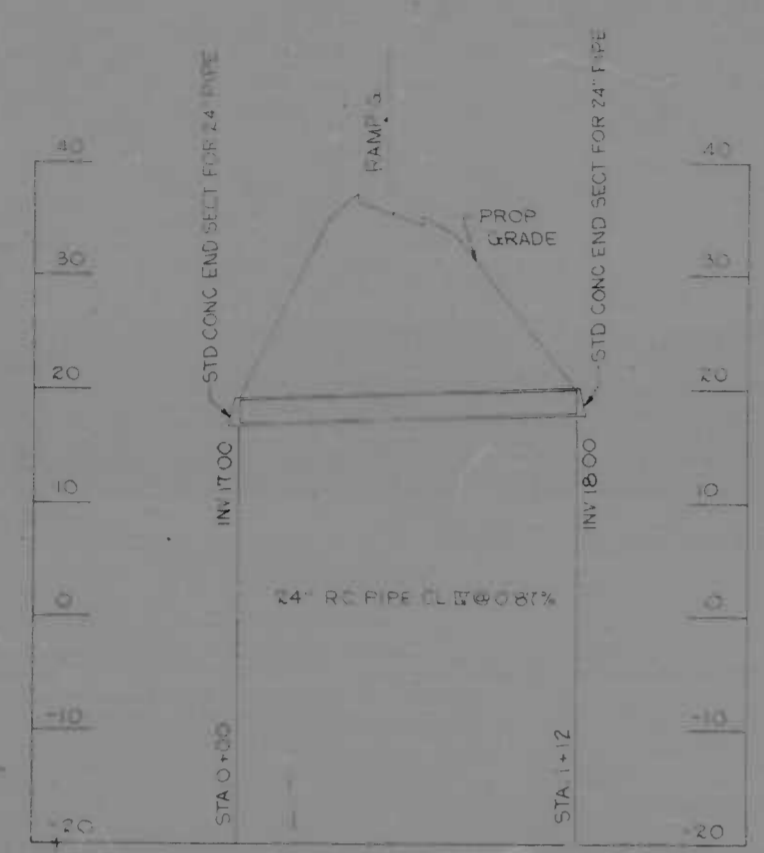
M-11



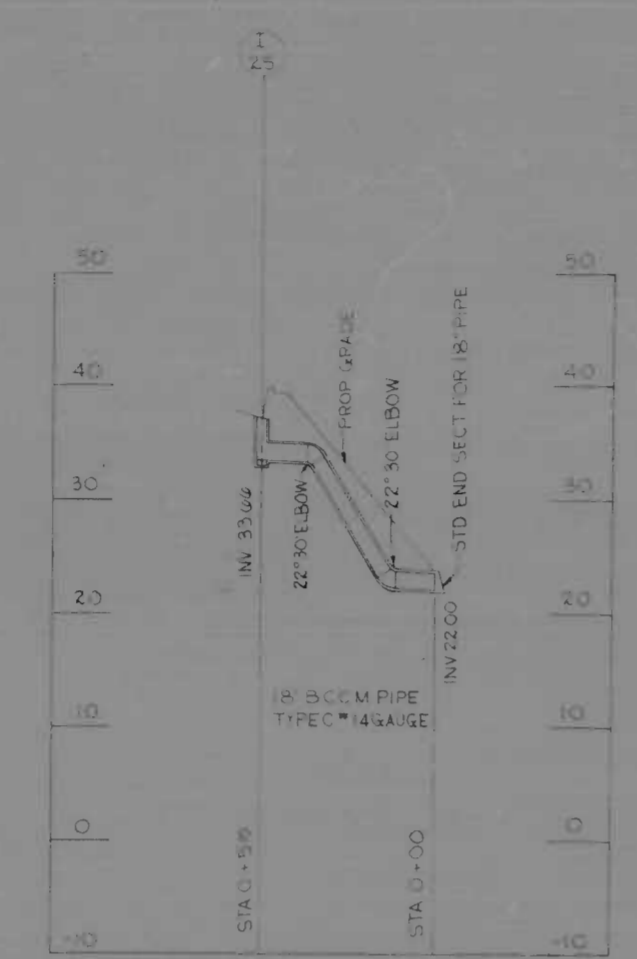
EXISTING 8" SANITARY SEWER

REVISIONS 1) Amend Item 2 Feb 1982 2) Revisions from 10/1/82 3) Revisions from 10/1/82	CONSULTANT KINDER, BENDER, STONE & ASSOC., INC. WATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 141 N. CALVERT STREET BALTIMORE, MARYLAND	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: P.M.Z. TRACED BY: P.M.Z. P.A.P. NO. I 95 440156 S.R.C. NO. BC 246-37 B-5 BALTO. CITY NO. 1982
SCALE: HORIZ. 1" = 40' VERT. 1" = 10' DATE:		SHEET NO. 34 OF 82	

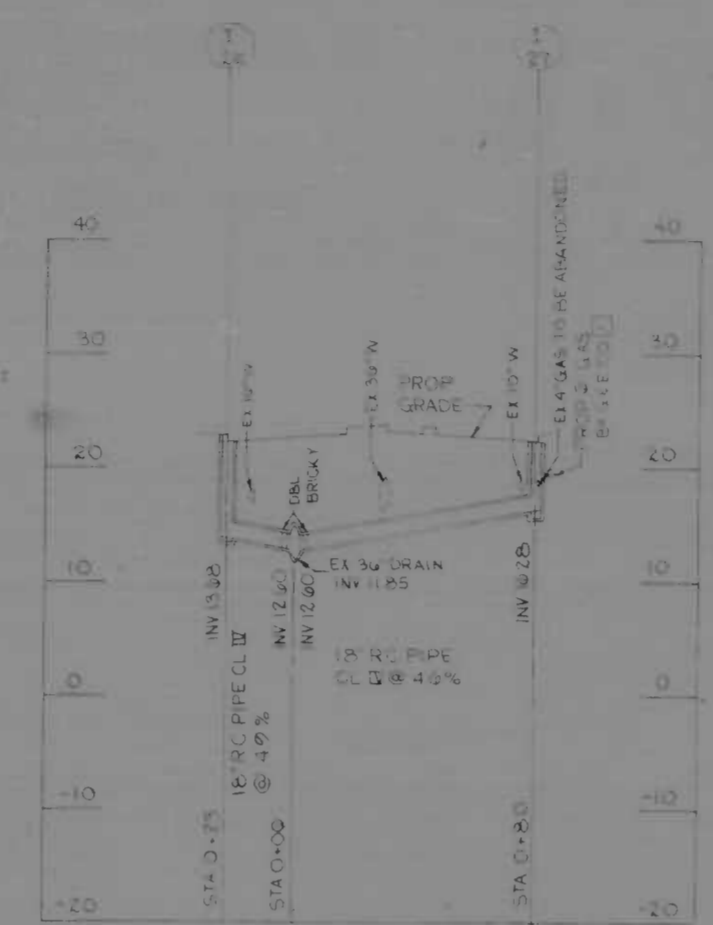
2	MU, I-95-44(3)R	37	82
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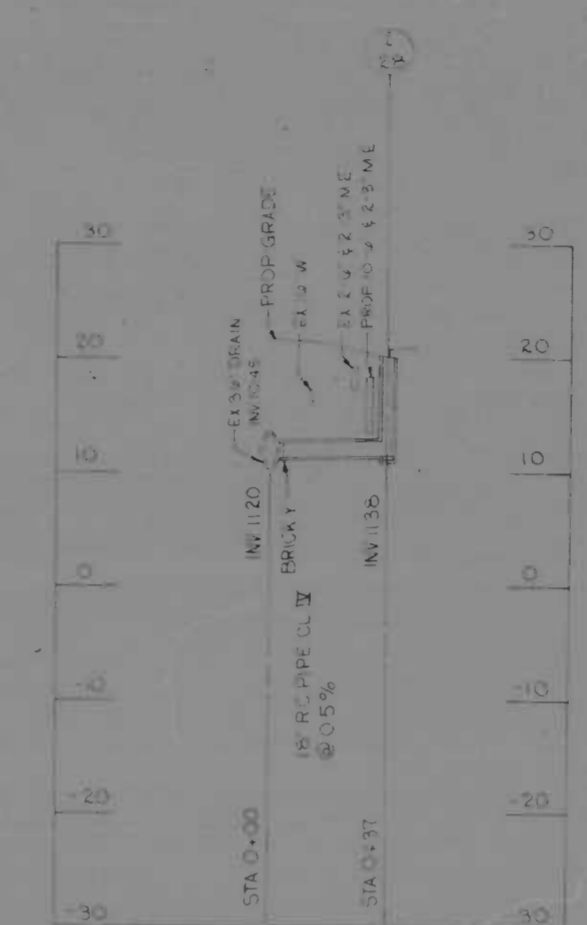
CROSS CULVERT @ STA 20+50



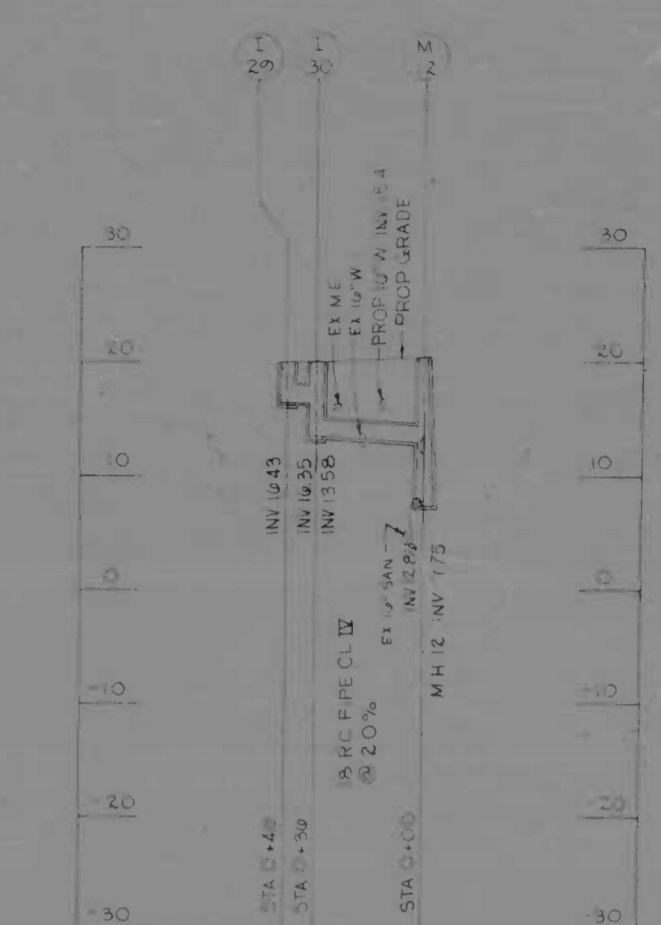
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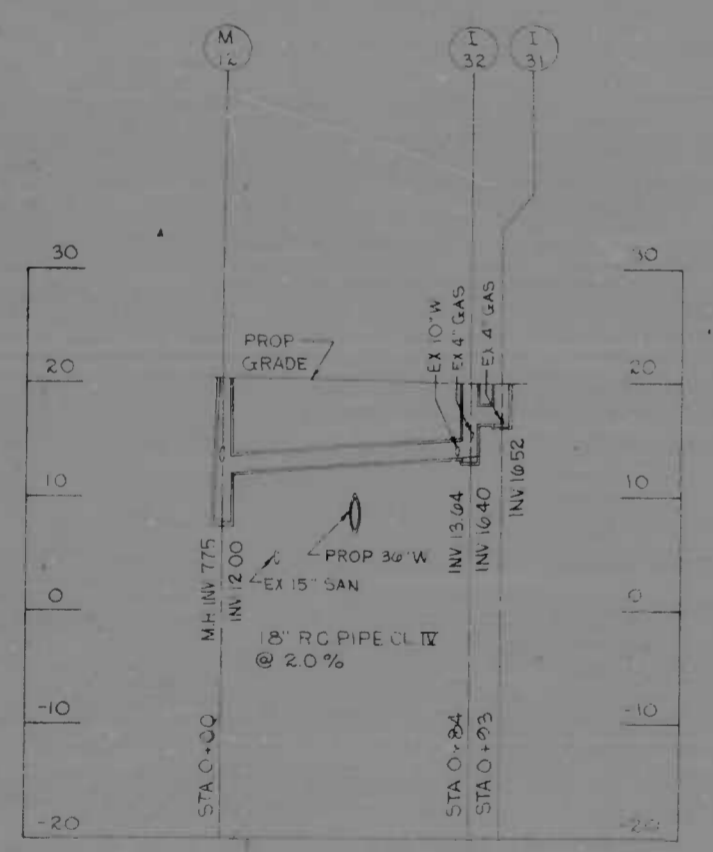
I-26 & I-27



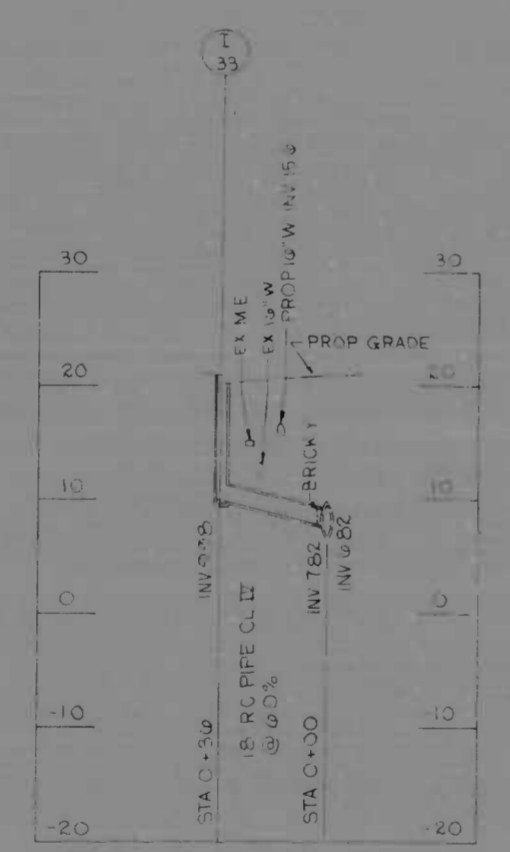
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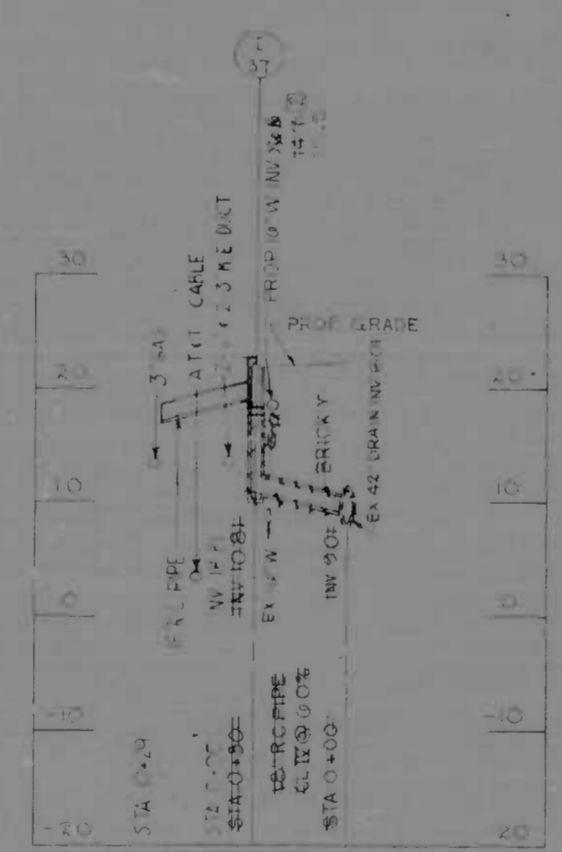
I-29 & I-30



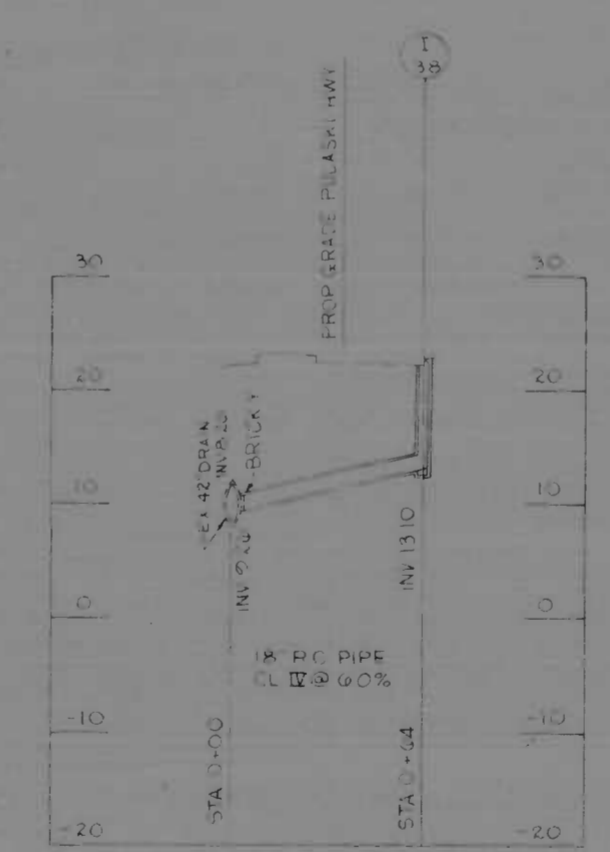
I-31 & I-32



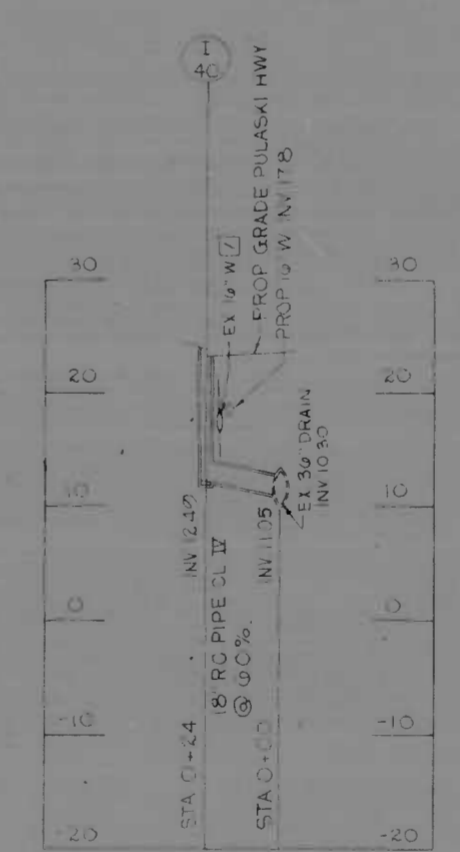
I-33



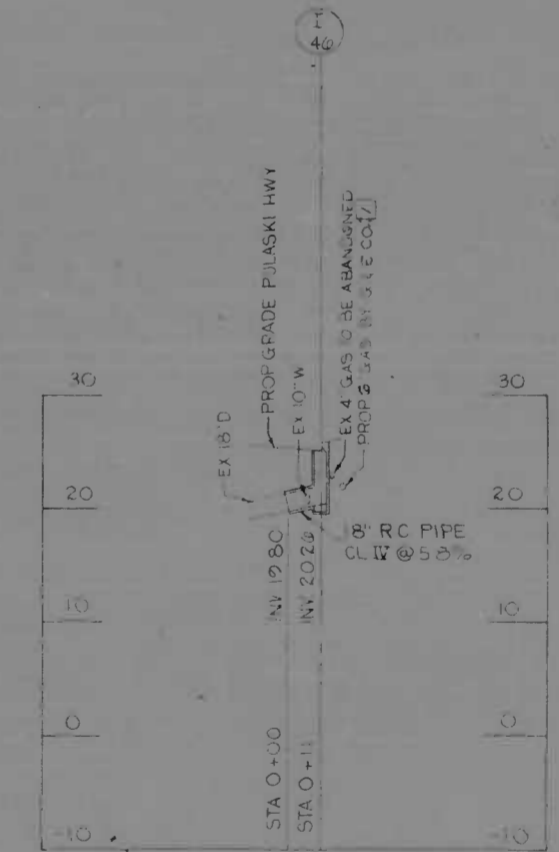
I-37



I-38



I-40



I-46

REVISIONS Addendum No. 2 - Feb. 19, 1971 Revision No. 34, 1972 University of the Water Man.	CONSULTANT KIMBERL BENDER, STONE & ASSOC., INC. AND WATZ, CHILDS & AGROC, INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF THE B & O RAILROAD TO NORTH OF PULASKI HIGHWAY			
SCALE: HORIZ. 1" = 40' VERT. 1" = 10' DATE:					

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	1-95-4140138	33	82

LIGHT STANDARDS TO BE RESET

NUMBER	LOCATION	SIZE
1	103-23	24"
2	103-25	24"
3	103-26	24"
4	103-28	24"
5	106-30	24"
6	106-35	24"
7	107-60	24"
8	108-25	24"
9	109-30	24"

TRAFFIC LIGHT STANDARDS TO BE RESET

LOCATION	DISTANCE	SIZE	TYPE
103-25	9	24"	FOR 24 POLE
103-26	5	24"	FOR 20 POLE

HAND BONES

LOCATION	DISTANCE	SIZE
103-25	54	L
103-26	55	L
103-28	60	R
104-25	61	L
104-71	60	R
105-22	65	L
106-30	65	L
107-60	65	L
107-73	65	L
108-25	58	L

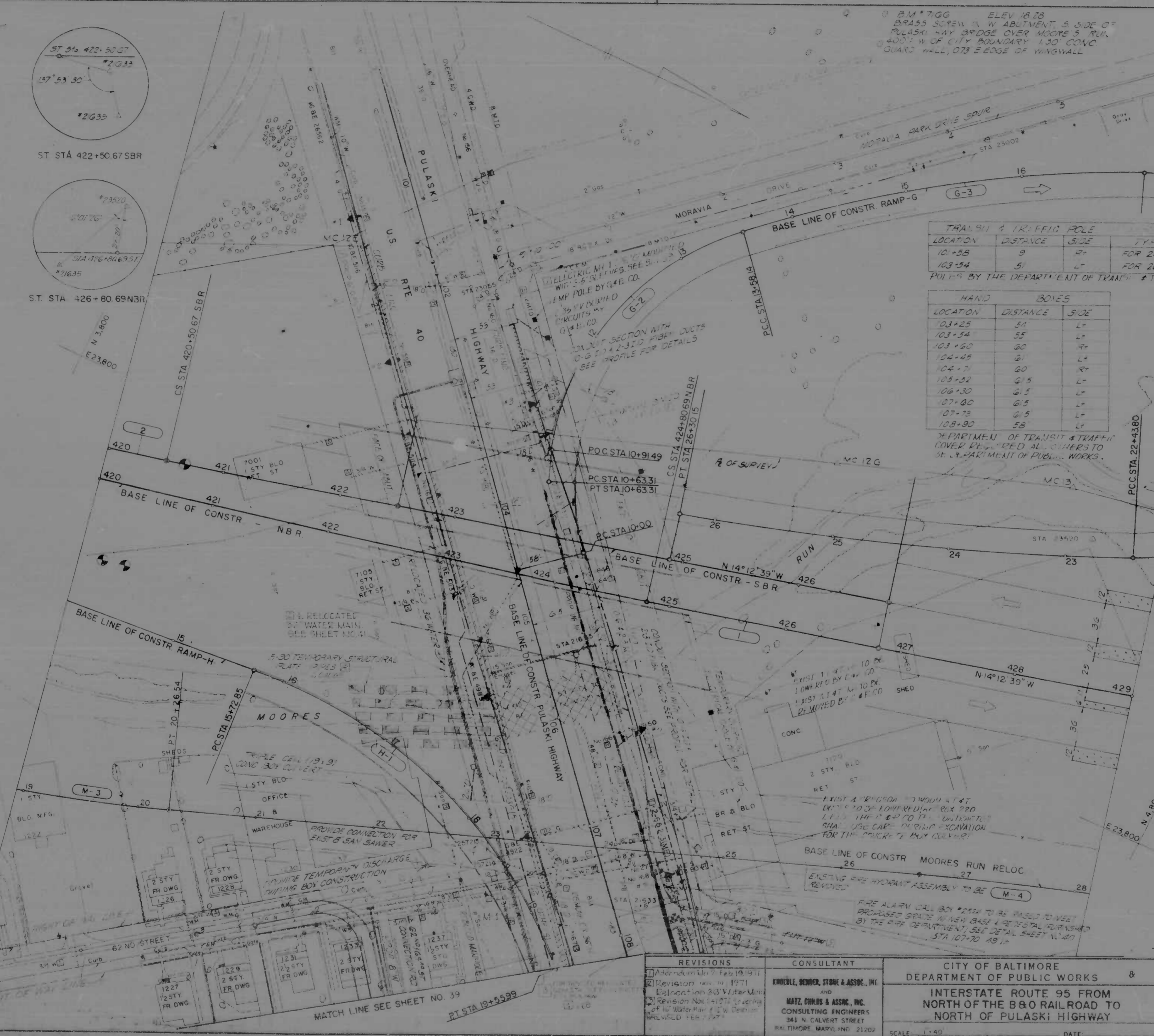
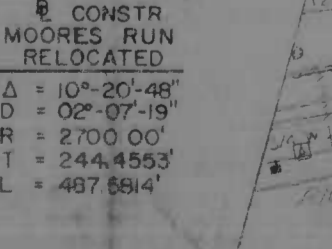
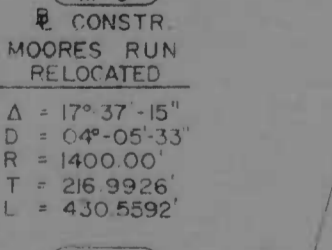
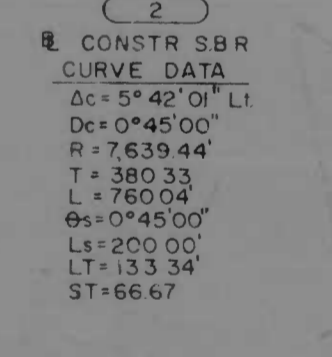
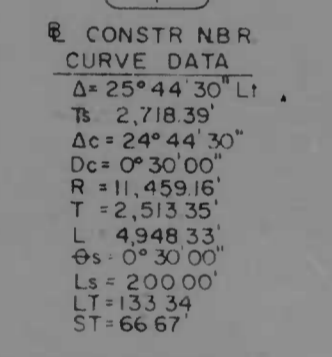
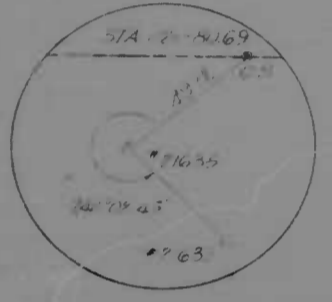
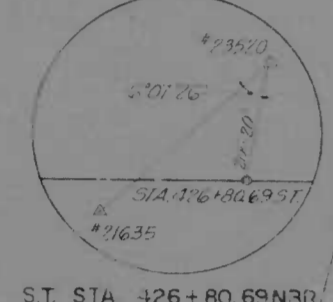
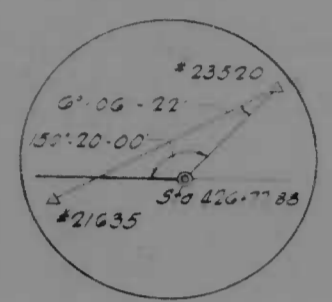
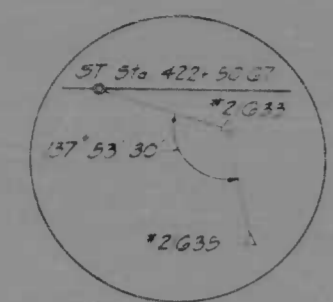
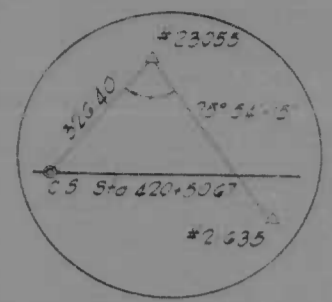
1. DISTANCE FROM FACE OF CURB TO BE 25' ALL OTHERS TO BE 50'
 2. ALL LIGHT STANDARDS NOT RESET SHALL BE SALVAGED AND RETURNED TO THE CITY (SEE SPECIAL PROVISIONS)
 3. BASE SHALL BE CONSTRUCTED ON TOP SLAB OF BOX CULVERT (SEE DETAIL SHEET 40)
- BEFORE THE REMOVAL OR ABANDONING OF ANY EXISTING UTILITIES THE CONTRACTOR SHALL PAY SPECIAL ATTENTION TO THE SUGGESTED CONSTRUCTION SEQUENCE AND THE SPECIAL PROVISIONS.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL PROVISIONS HEREIN BY STATUTES WITHIN THE LIMITS OF CONTRACT.

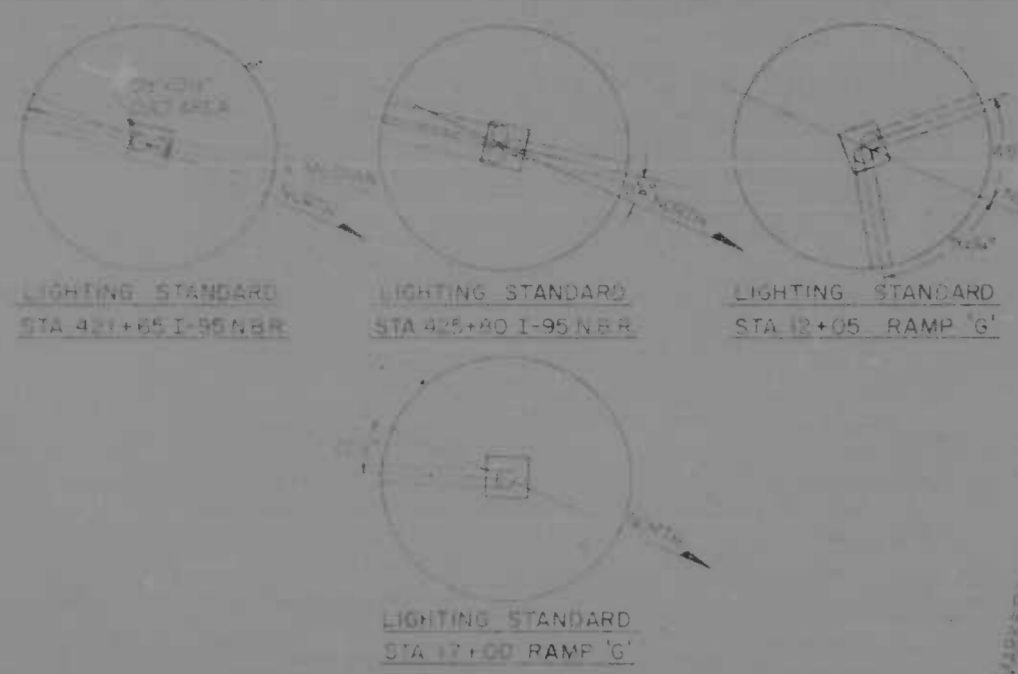
REFERENCE DRAWINGS

NO.	SHEET NO.
1	FOR PULASKI HIGHWAY ROAD CONSTRUCTION PLAN
2	FOR PULASKI HIGHWAY ROAD PROFILE
3	FOR TEMPORARY DETOUR ROAD PLANS
4	FOR TEMPORARY DETOUR ROAD PROFILE
5	FOR RELOCATED WATER MAIN PLAN
6	FOR RELOCATED WATER MAIN PROFILE
7	FOR TYPICAL SECTIONS
8	FOR 5' SANDY DRAIN & RELOCATED SANITARY SEWER PROFILES
9	FOR MOORES RUN RELOCATED PROFILE
10	FOR TRIPLE CELL CONCRETE BOX CULVERT
11	FOR STRUCTURE EMB OVER PULASKI HIGHWAY
12	FOR WALL SHOTBY DUGCAN
13	FOR WALL SHOTBY DUGCAN

UTILITY RELOCATION

<p>REVISIONS</p> <p>1. Alteration 10/2/1971</p> <p>2. Revision 10/10/1971</p> <p>3. Relocation 3/3/1971</p> <p>4. Revision Nov. 1971</p> <p>5. Revision 10/10/1971</p>	<p>CONSULTANT</p> <p>KIMBLE, BENDER, STONE & ASSOC., INC.</p> <p>AND</p> <p>HATZ, OPKUS & ASSOC., INC.</p> <p>CONSULTING ENGINEERS</p> <p>241 N. CALVERT STREET</p> <p>BALTIMORE, MARYLAND 21202</p>	<p>CITY OF BALTIMORE</p> <p>DEPARTMENT OF PUBLIC WORKS &</p> <p>INTERSTATE ROUTE 95 FROM</p> <p>NORTH OF THE B&O RAILROAD TO</p> <p>NORTH OF PULASKI HIGHWAY</p>	<p>STATE ROADS COMMISSION OF MARYLAND</p> <p>INTERSTATE DIVISION FOR BALTIMORE CITY</p> <p>DRAWN BY P.W.Z.</p> <p>TRACED BY P.W.Z.</p> <p>F.A.P. NO. 1-25-4140138</p> <p>S.R.C. NO. BC 246-37-815</p> <p>BALTO. CITY NO. 1969</p>
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DETAILS - FOUNDATION CONDUIT ENTRANCES
NOT TO SCALE



NOTE: CONTRACTOR TO VERIFY BY TEST CENTER DIRECT SERIAL 4" DIA. STEEL CONDUIT AND 10 CONDUIT MARKERS

REVISIONS ADDITION TO EXISTING CONTRACT AUGUST 3, 1973	CONSULTANT KNUDSEN, BENDER, STONE & ASSOC., INC. AND WATZ, CURTIS & ASSOC., INC. CONSULTING ENGINEERS 343 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS		STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
		INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY		DRAWN BY: J.V.S. CHECKED BY: J.V.S. DATE: 8-1-73	DESIGNED BY: A.L. CHECKED BY: R.H. DATE: 8-1-73
SCALE: 1" = 40'		DATE: AUG 3, 1973		SHEET NO. 18A - 82	

B.M. # 7717 ELEV 15.13
BRASS SCREW IN N.W. END OF SEWER
INLET AT N.E. CORNER PULASKI HWY
AND 63RD ST

B.M. # 7722 ELEV 33.20
BRASS DISK IN CONC MONUMENT FLUSH
WITH SURFACE OF PULASKI HWY. MEDIAN,
30' N.E. OF 64TH ST

STA 108+79 - 109+58 IN SIDE
CONC ENCASE 85 OF EXISTING B
SANITARY SEWER FOR DETAILS
SPECIAL PROVISIONS

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4H0138	39	82



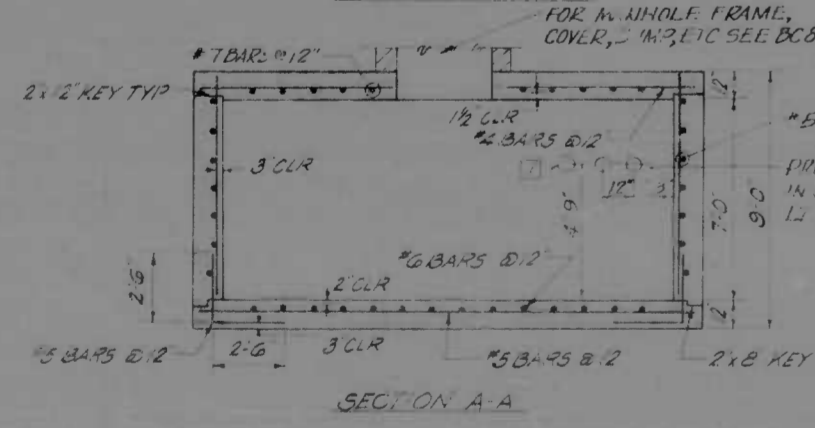
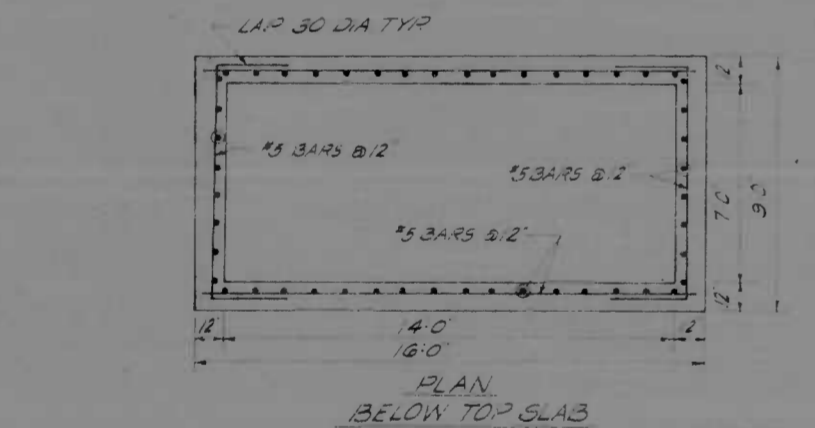
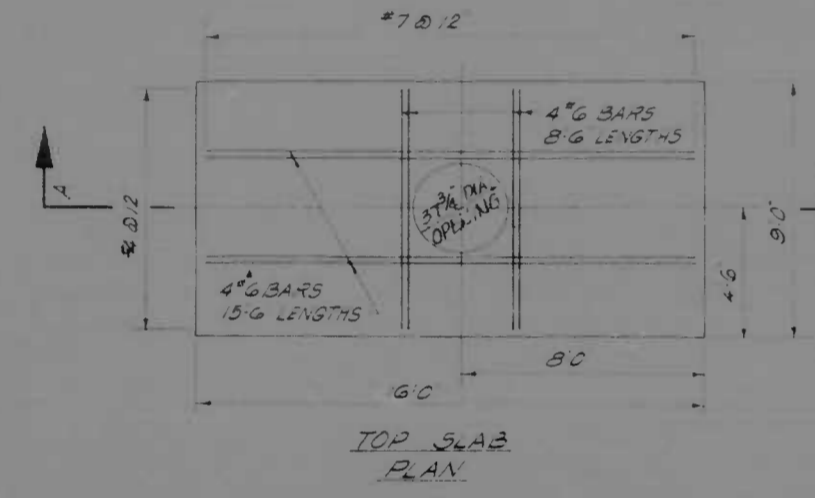
CONDUIT MANHOLES

LOCATION	DISTANCE	SIDE	TYPE	REMARKS
102+44	53	L*	71.4x71	STD TYPE G MOD
108+37	53	L*	71.4x71	STD TYPE G MOD
111+07	68	L*	81.4x71	STD TYPE G

HOUSE CONNECTION DATA

HOUSE	CAI	OFFSET	CON. MAT.	REMARKS
1001	103+49	206' RT	206' RT	*
1001	105+10	208' RT	208' RT	*
1105	104+54	186' RT	186' RT	*
1105	103+28	186' RT	186' RT	*
1109	1+05	19' RT	19' RT	*
1120	42+68	8' RT	8' RT	*
1205	108+58	119' RT	119' RT	*
1211	110+20	117' RT	117' RT	*
1210	4+48	115' RT	115' RT	4.5'
1222	4+10	13' RT	13' RT	1.6'
1236	3+13	19' RT	19' RT	2.0'
1228	2+60	19' RT	19' RT	4.0'
1230	2+22	20' RT	20' RT	4.0'
1232	1+60	20' RT	20' RT	4.0'
1233	1+0	11' RT	11' RT	2.0'
1231	0+91	11' RT	11' RT	6.0'
1232	1+10	26' RT	26' RT	*
1235	0+76	13.5' RT	13.5' RT	4.0'

* UNRECORDED HOUSE CONNECTION DATA UNRECORDED AT BALTIMORE CITY WASTE WATER DIVISION.



TYPE 'G' MANHOLE MODIFIED
SCALE 1/4" = 1'-0"

- REFERENCE DRAWINGS**
- 1 FOR PULASKI HIGHWAY ROAD CONSTRUCTION PLAN SEE SHEET NO 21 109
 - 2 FOR PULASKI HIGHWAY ROAD PROFILE SEE SHEET NO 20
 - 3 FOR TEMPORARY DETOUR ROAD PLANS SEE SHEET NO 46 109
 - 4 FOR TEMPORARY DETOUR ROAD PROFILE SEE SHEET NO 48
 - 5 FOR RELOCATED WATER MAIN PLAN SEE SHEET NO A 109
 - 6 FOR RELOCATED WATER MAIN PROFILE SEE SHEET NO 45
 - 7 FOR TYPICAL SECTION SEE SHEET NO 25
 - 8 FOR STORM DRAIN RELOCATED SANITARY SEWER PROFILE SEE SHEET NO 34 87
 - 9 FOR MODIFIED RELOCATED PROFILE SEE SHEET NO 32
 - 10 FOR TRIPLE CELL CONCRETE BOX CULVERT SEE SHEET NO 34 87
 - 11 FOR STRUCTURE I-95 OVER PULASKI HIGHWAY SEE SHEET NO 29 87
 - 12 SEE SHEET NO 17 109

REVISIONS	CONSULTANT	CITY OF BALTIMORE	STATE ROADS COMMISSION OF MARYLAND
<ul style="list-style-type: none"> 1 2 3 4 	KNOERLE, DENGLER, STONE & ASSOC., INC. MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N CALVERT STREET BALTIMORE, MARYLAND 21202	DEPARTMENT OF PUBLIC WORKS INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAIL ROAD TO NORTH OF PULASKI HIGHWAY	INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: F.W.Z. TRACED BY: F.W.Z. F.A.P. NO. I-95-4H0138 S.R.C. NO. BC 246.37-815 BALTO CITY NO. 1969
DATE: _____		SHEET NO. 39 OF 82	

REVISIONS			
NO	DESCRIPTION	DATE	BY
1	Sewers Lowered 0.87'	Jun 4, 72	RABK

SHEET NO	TOTAL SHEETS
39A	82



F.A.P. NO. 1-95-4(40)38
 S.H.A. NO. B.C.-246-37-815
 BALTO. CITY # 1969

CITY OF BALTIMORE
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING

8" SANITARY SEWER
 TO BE BUILT IN U.S. ROUTE 40 FROM 62ND STREET
 TO MORAVIA PARK DRIVE

REVISIONS
 1. ADDITION TO EXISTING CONTRACT 6-15-72
 2. REVISION N 7/24/72
 3. REVISION 8/2/72

PRINCIPAL ENGINEER: [Signature]
 HEAD BUREAU OF ENGINEERING: [Signature]
 CHIEF DIVISION OF WASTE WATER: [Signature]
 DIRECTOR OF PUBLIC WORKS: [Signature]

SCALE: AS SHOWN
 DATE: [Blank]
 SHEET 39A OF 82

12158
 29-32 UB



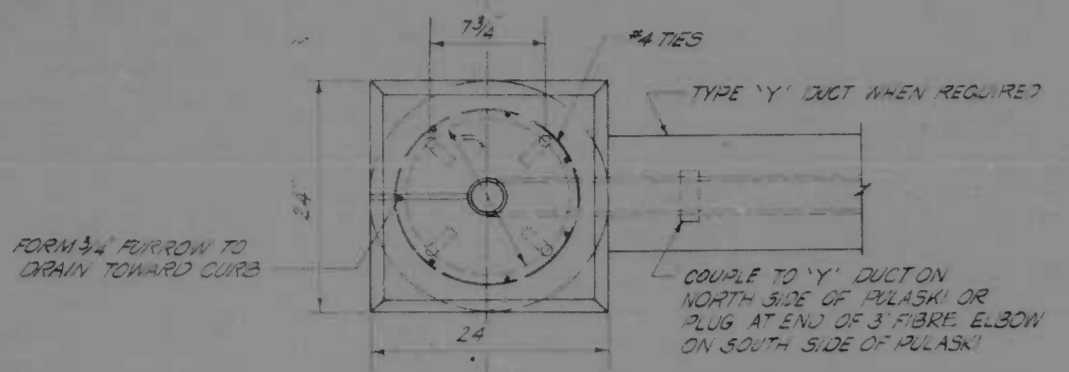
LIGHTING STANDARD
STA 20+00 RAMP 'H'

DETAILS - FOUNDATION
CONDUIT ENTRANCES
NOT TO SCALE

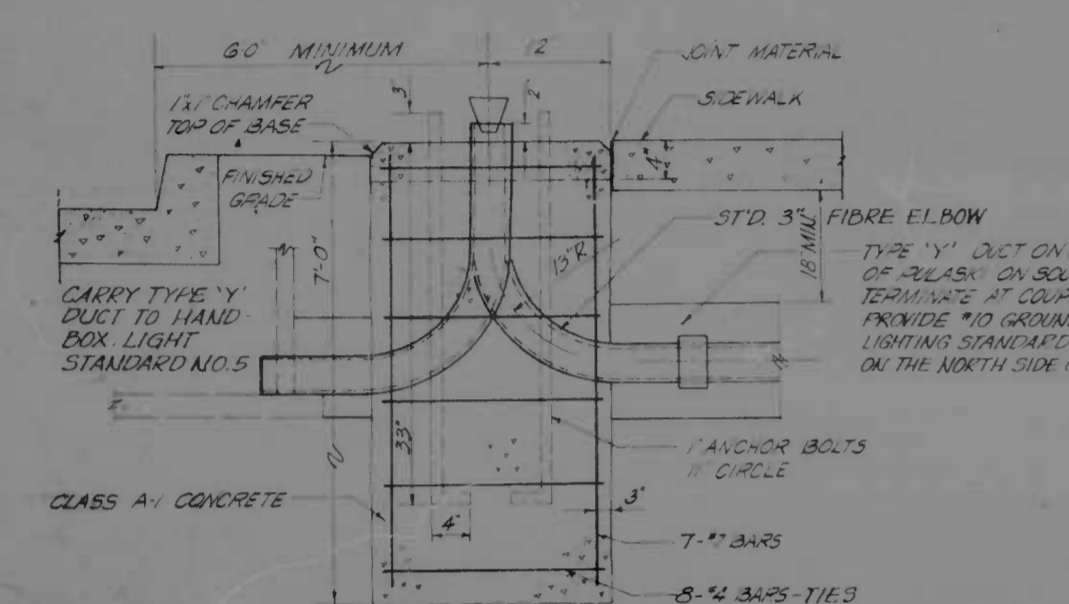


NOTE: CONTRACTOR TO ONLY INSTALL
SOLID CORED GALVANIZED
STEEL CONDUIT AND 2 CONDUIT
MARKERS

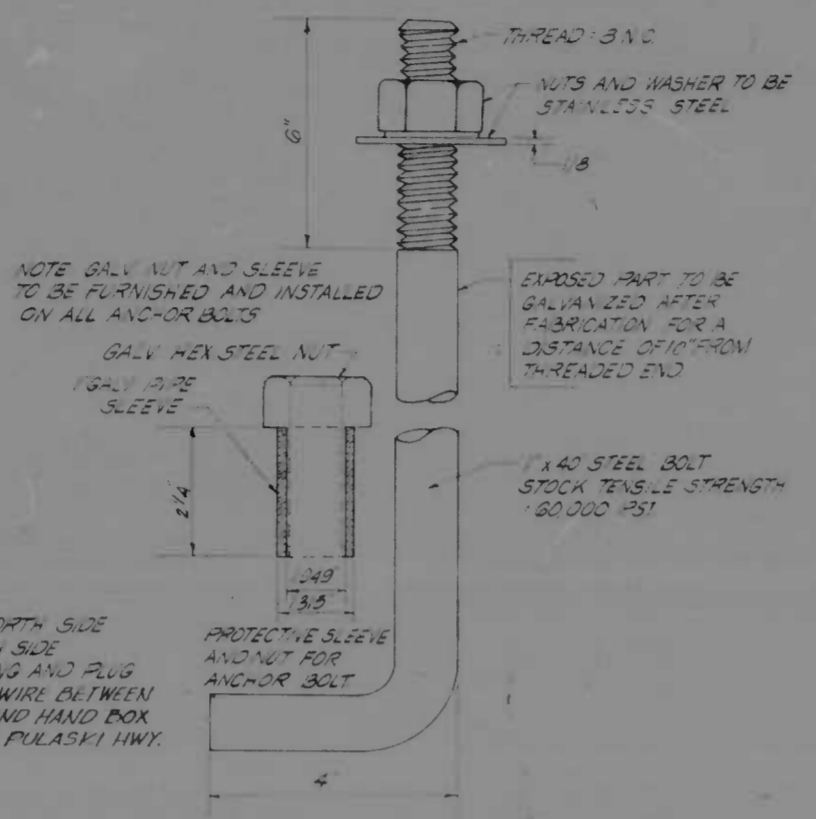
REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. ADDITION TO EXISTING CONTRACT AUGUST 3, 1973	KWITELL, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 145 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY J.V.J. CHECKED BY J.V.J. F.P. NO. I-95-440-38 A.R.C. NO. BC 246-37-815 BALTO. CITY NO. 1969
		SCALE 1"=40'	DATE AUG 3, 1973
			DESIGN BY A.L. CHECK BY J.V.J. SHEET NO. 398 OF 82



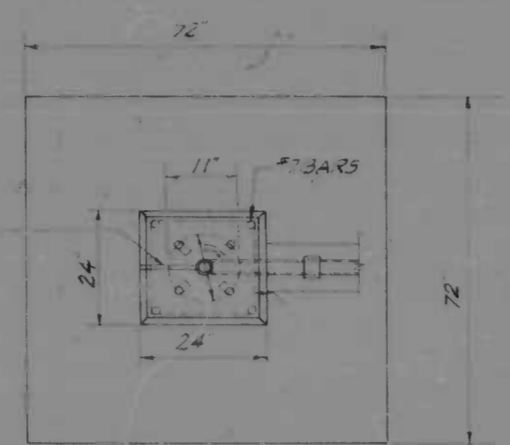
PLAN



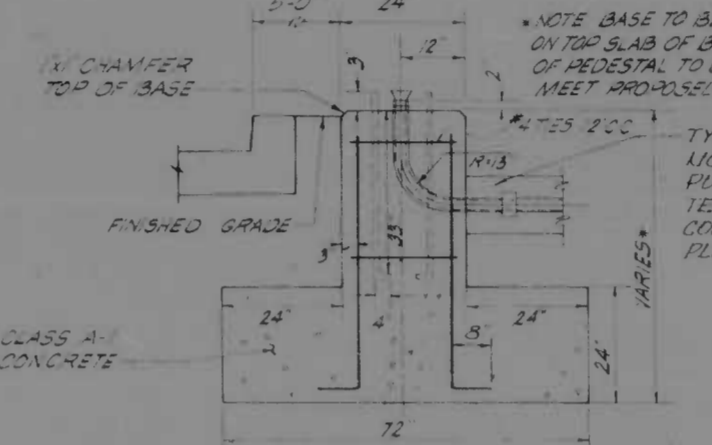
ELEVATION ROADWAY LIGHT PEDESTAL BASE
NO SCALE



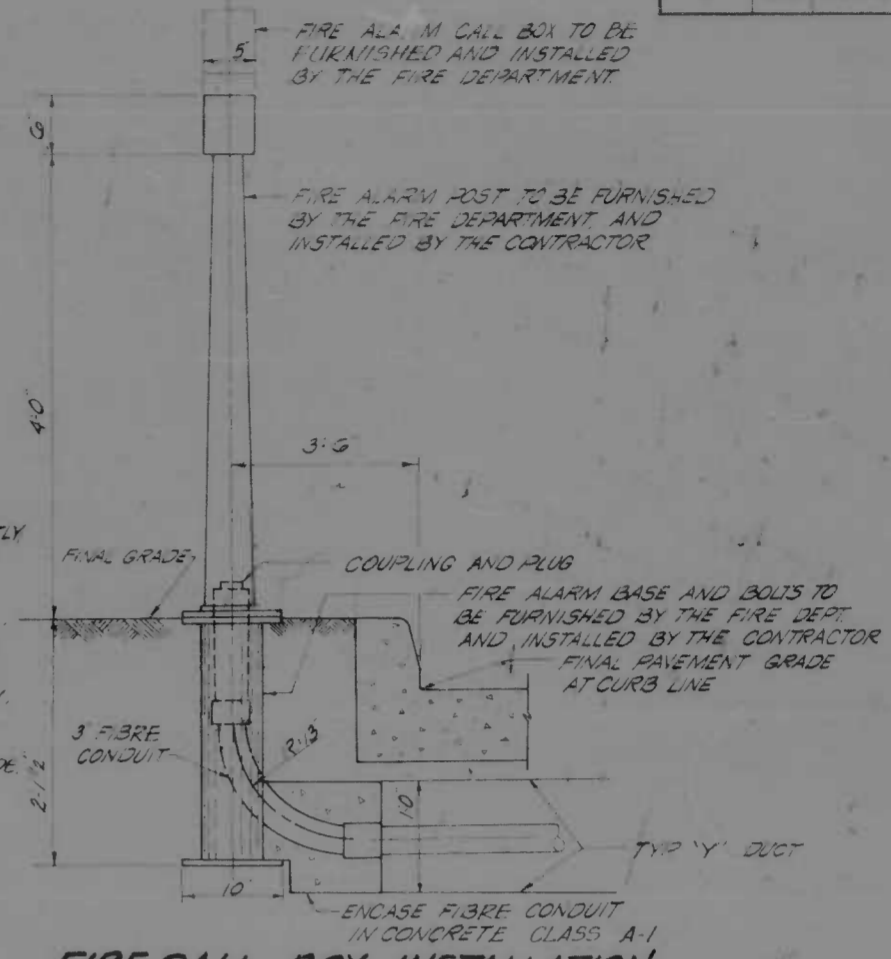
ANCHOR BOLT
NO SCALE



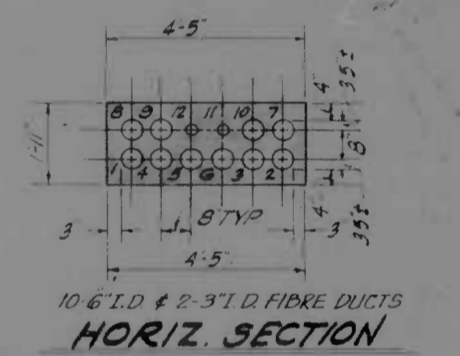
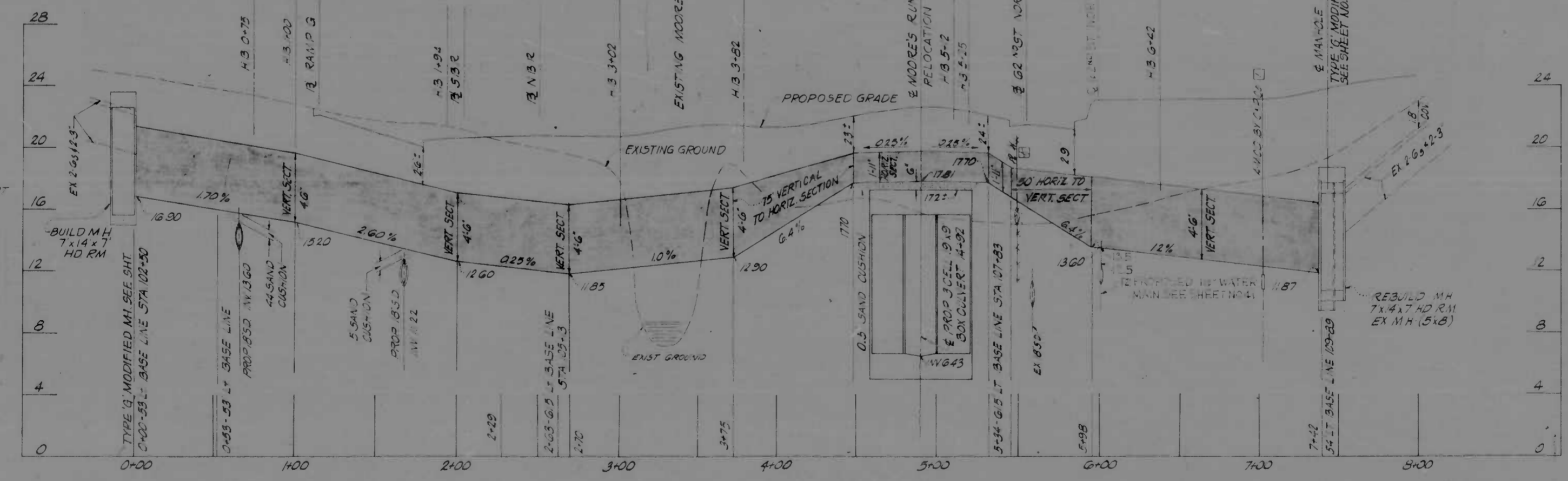
PLAN



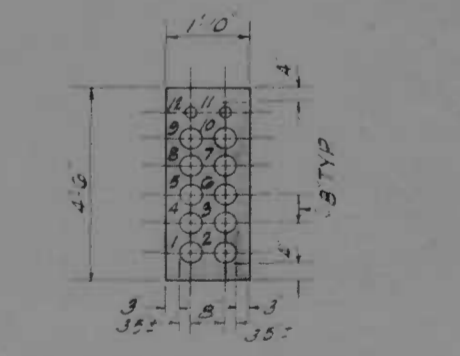
ELEVATION ROADWAY LIGHT PEDESTAL BASE FOR USE OVER BOX CULVERT
NO SCALE



FIRE CALL BOX INSTALLATION
NO SCALE



DUCT SIZES
DUCT NO. 1 THRU NO. 10 - 6" I.D. FIBRE
DUCT NO. 11 & NO. 12 - 3" I.D. FIBRE

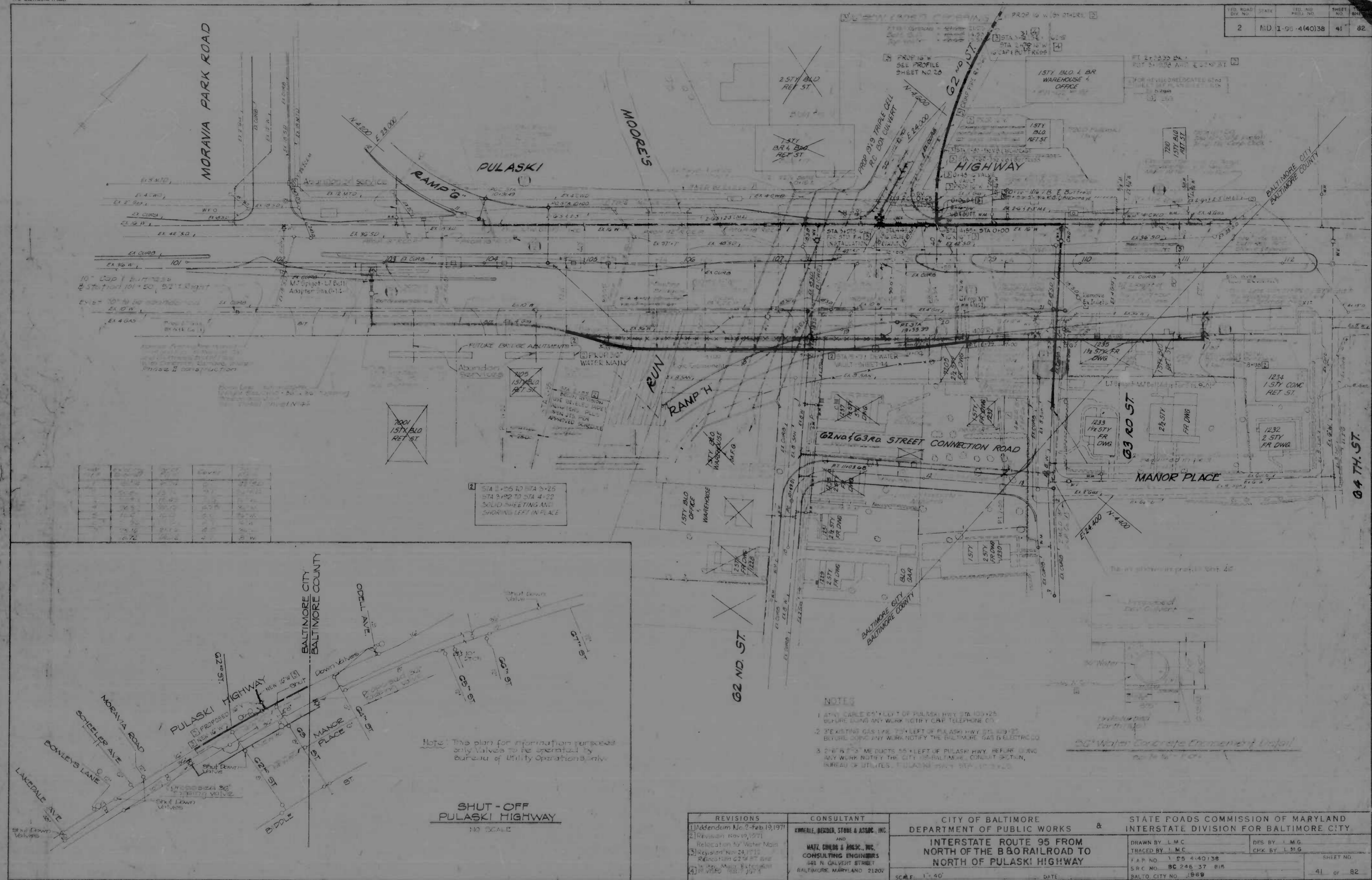


TYPE 'MI' DUCT
SCALE 3/8" = 1' 0"

TYPE 'MY' DUCT
SCALE 3/4" = 1' 0"

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. Addendum No. 2 Feb. 18, 1971	KIMBERLE, DEANER, STONE & ASSOC., INC.	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	SCALE: As Shown
2. Revision Nov. 24, 1972	HATZ, BIRNBAUM & ASSOC., INC. CONSULTING ENGINEERS		DATE:
3. Change of WATER-ADD IN WATER AT STA. 1+00	343 N. CALVERT STREET BALTIMORE, MARYLAND 21202		

DRAWN BY: AL	DES. BY: AL
TRACED BY: PWZ	CHK. BY: JLC
F.A.P. NO. I-95-4(40)38	SHEET NO. 40 OF 82
S.R.C. NO. EC246-37-815	
BALTO. CITY NO. 1969	



STATION	DESCRIPTION	DATE
10+00	START OF RAMP G	1971
10+50	END OF RAMP G	1971
11+00	START OF RAMP H	1971
11+50	END OF RAMP H	1971
12+00	START OF 62ND ST	1971
12+50	END OF 62ND ST	1971
13+00	START OF 63RD ST	1971
13+50	END OF 63RD ST	1971
14+00	START OF 64TH ST	1971
14+50	END OF 64TH ST	1971

STA 1+05 TO STA 3+25
 STA 3+25 TO STA 4+22
 SOLD SHEETING AND
 SHORING LEFT IN PLACE

- NOTES**
1. ALL CABLES EAST OF PULASKI HWY STA 10+25 BEFORE WORK BEGINS. TELEPHONE CO.
 2. EXISTING GAS LINES LEFT OF PULASKI HWY STA 10+25 BEFORE WORK BEGINS. NOTIFY THE BALTIMORE GAS ELECTRIC CO.
 3. ALL 6" & 8" ME DUCTS EAST OF PULASKI HWY. REPAIR WORK ANY WORK NOTIFY THE CITY OF BALTIMORE, CONSULT SECTION, BUREAU OF UTILITIES, PULASKI HWY STA 10+25.

Note: This plan for information purposes only. Valves to be operated by Bureau of Utility Operations only.

**SHUT - OFF
 PULASKI HIGHWAY**
 1/2" SCALE

REVISIONS	CONSULTANT
1. Addendum No. 2 - Feb 19, 1971	KIMBLE, BENDER, STONE & ASSOC., INC. AND HATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 441 N. CALVERT STREET BALTIMORE, MARYLAND 21202
2. Revision Nov 10, 1971 Relocation of Water Main	
3. Revision Nov 24, 1971 Revised Plan 62nd St. and 63rd St. Interchange	
4. Revision Dec 1, 1971	

CITY OF BALTIMORE
 DEPARTMENT OF PUBLIC WORKS &
 INTERSTATE ROUTE 95 FROM
 NORTH OF THE B&O RAILROAD TO
 NORTH OF PULASKI HIGHWAY

STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
DRAWN BY L.M.C.	DES BY L.M.C.
TRACED BY L.M.C.	CHK BY L.M.C.
FILE NO. I-95-4(40)38	SHEET NO.
SRC NO. BC 248 37 818	41 of 82
BALTO. CITY NO. 1969	

SCALE: 1" = 40'

DATE:



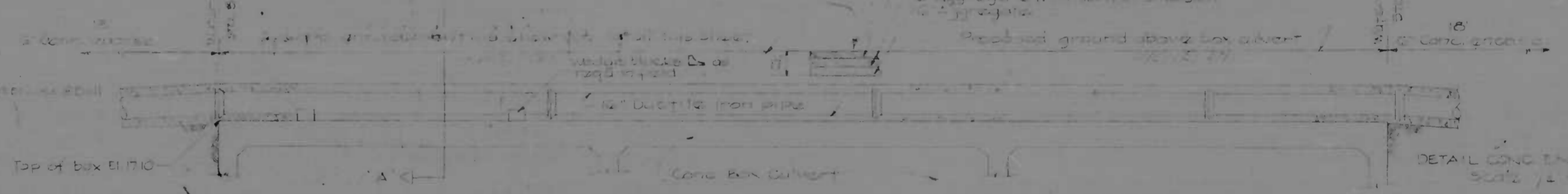
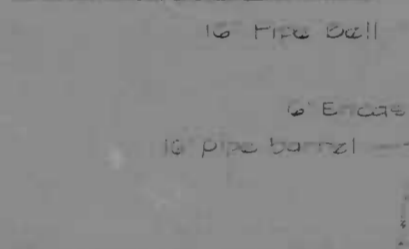
2" W MAIN PROFILE
 Horiz. 1" = 40'
 Vert. 1" = 4'

2" W MAIN PROFILE BUTTRESS DETAIL
 NOT TO SCALE

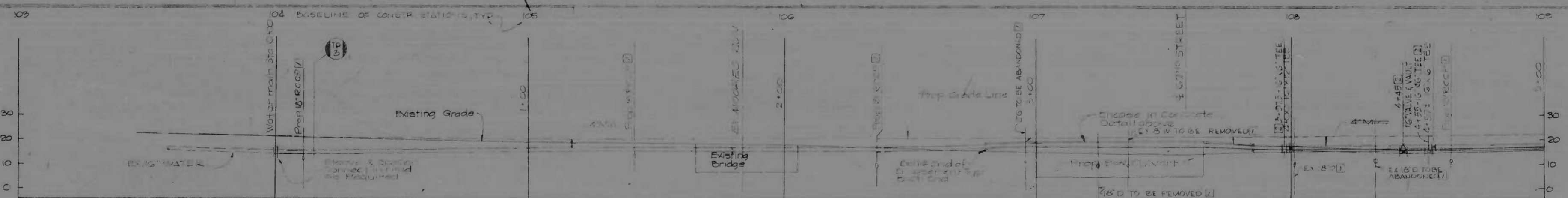
NO.	DESCRIPTION	QTY	UNIT	REMARKS
1	CONCRETE	120	CY	
2	STEEL	120	LB	
3	BRICK	120	SQ	
4	GRAVEL	120	CY	
5	SAND	120	CY	
6	CEMENT	120	BA	
7	LABOR	120	HRS	
8	EQUIPMENT	120	HRS	

Note:
 Contractor shall construct V.D.L. embankment
 to top of sub grade before constructing base
 course. Excavate 2 ft. tall 12" water main with
 full trench to station.

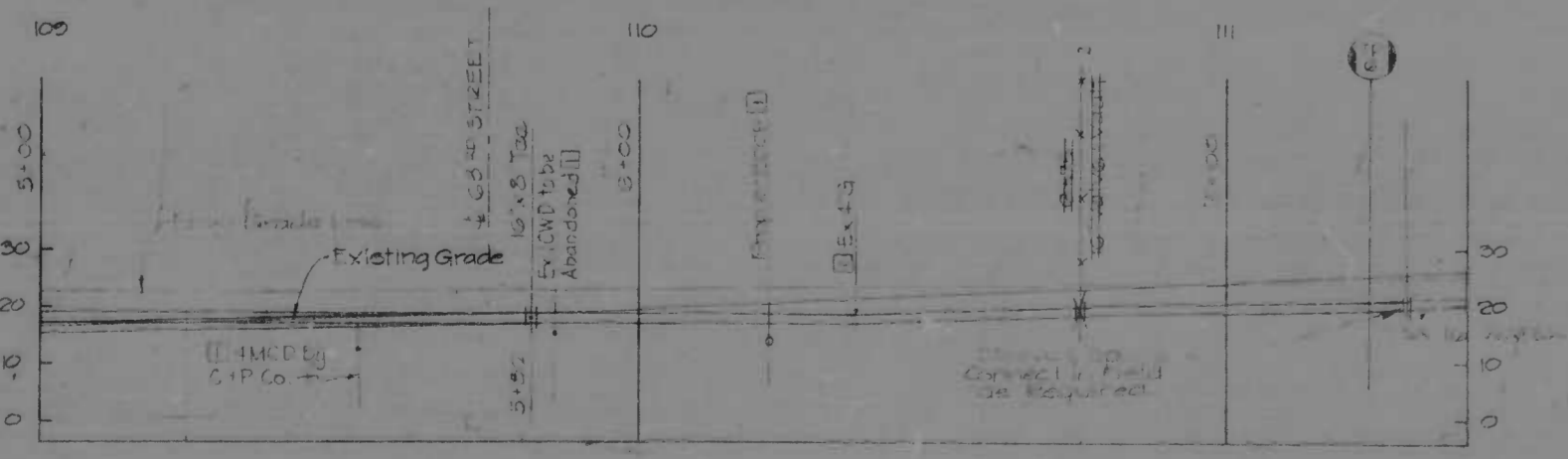
SECT. ON A-A'
 ENCASUREMENT DETAIL



DETAIL CONC. ENCASUREMENT
 SCALE 1/2" = 1'-0"

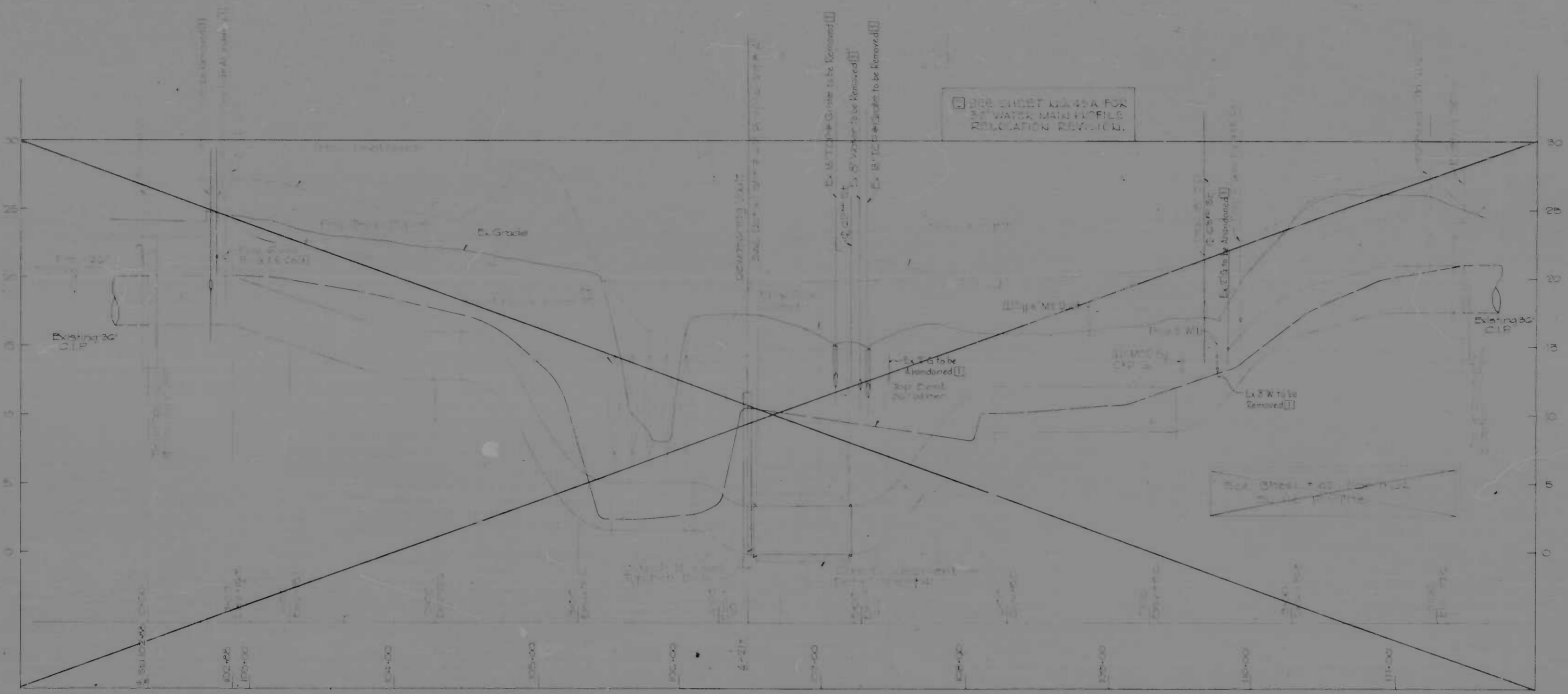


2" W WATER PROFILE
 Horiz. 1" = 20'
 Vert. 1" = 2'

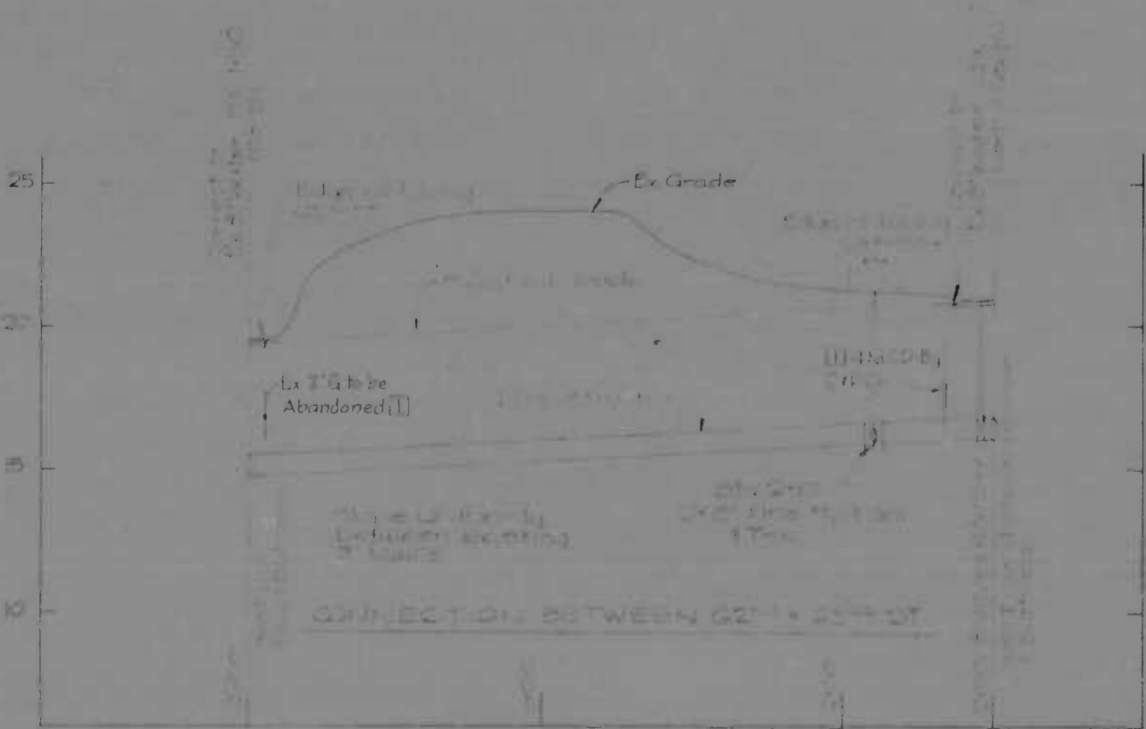


REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1] Addendum No. 2 - Feb 1971 2] Revision No. 24 - Lowering of Water Main	CHOBELL, THORNDY, STONE & ASSOC., INC. AND MATZ, CURRIS & ASSOC., INC. CONSULTING ENGINEERS 441 N. CALVERT STREET BALTIMORE, MARYLAND 21201	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: LMC TRACED BY: LMC F.A.P. NO. 1-95-440138 S.P.C. NO. BC 246-17-815 BALTO. CITY NO. 1969
		SCALE: As Shown	DES. BY: LMG CHK. BY: LMG SHEET NO. 42 of 82

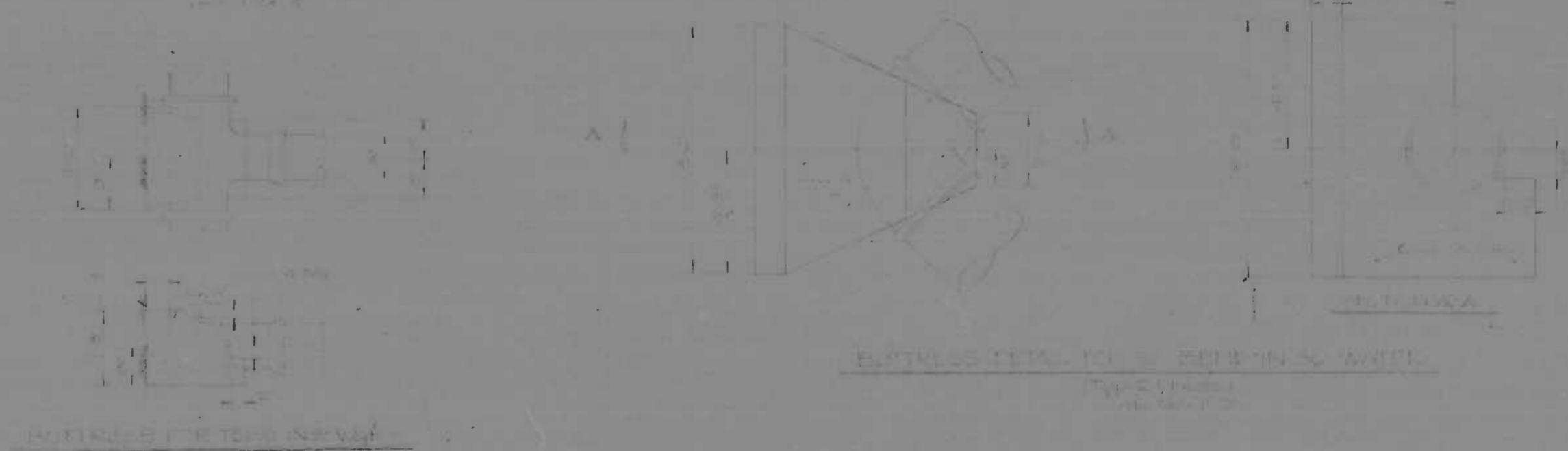
FED. ROAD DIST. NO.	STATE	F.P. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4140/38	43	82



36" WATER PROFILE



8" WATER PROFILE

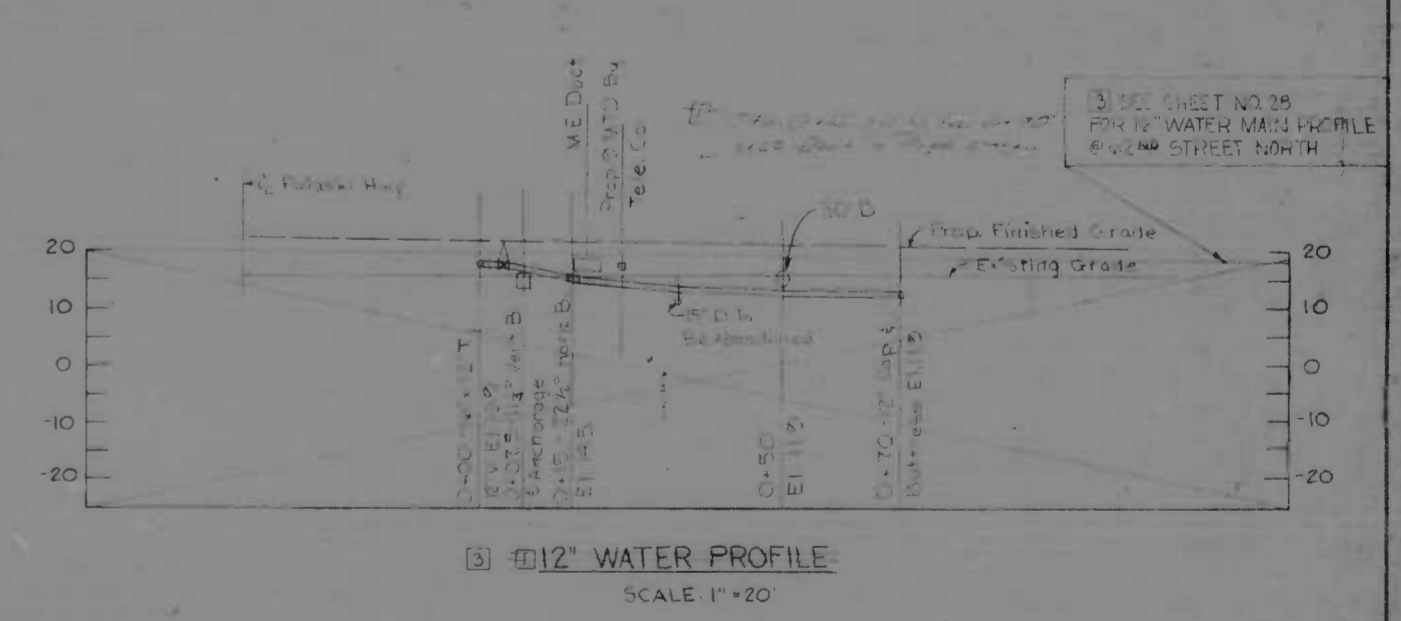
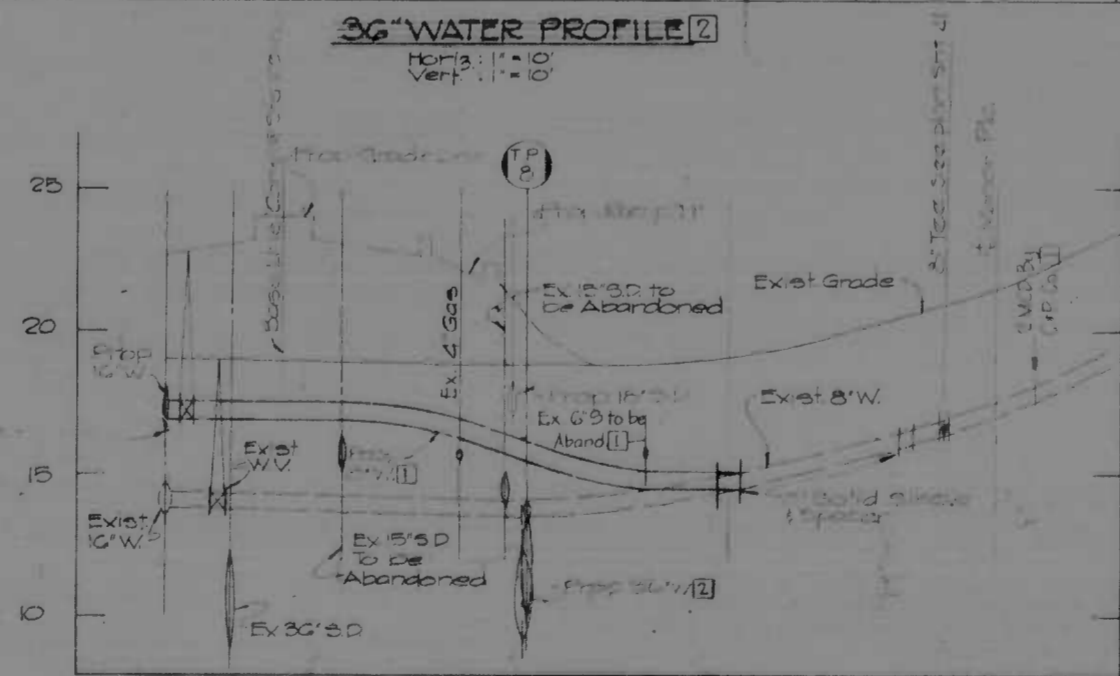
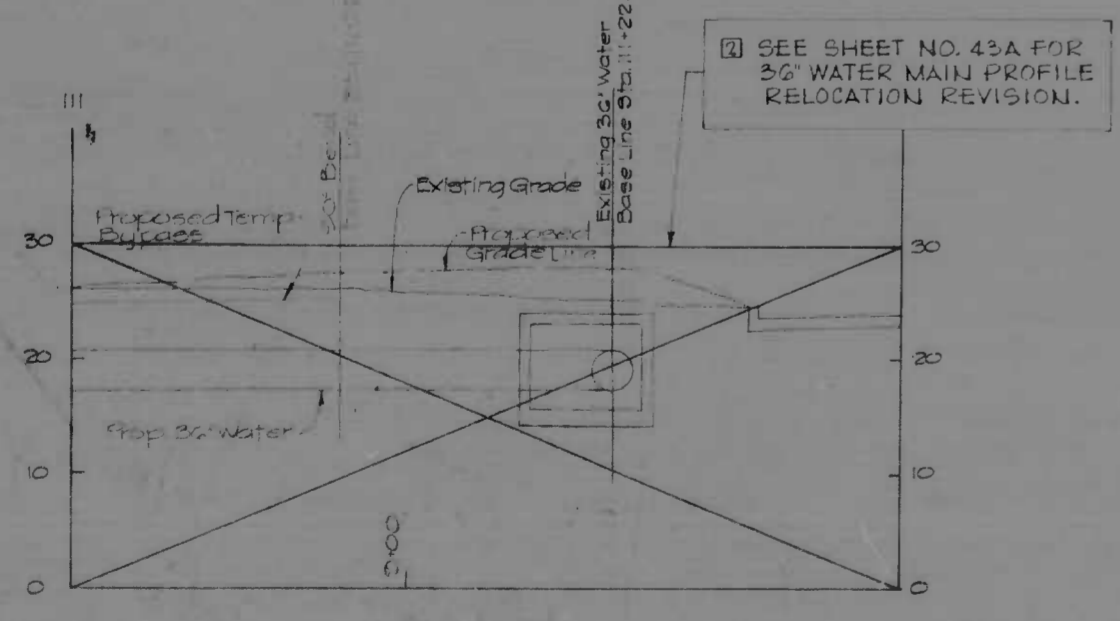
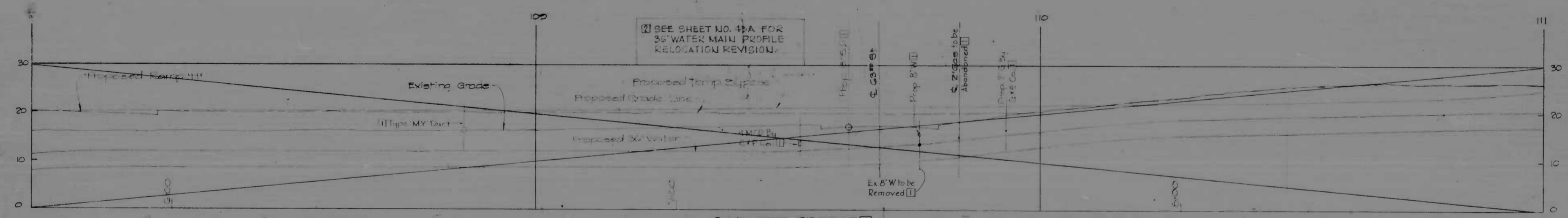
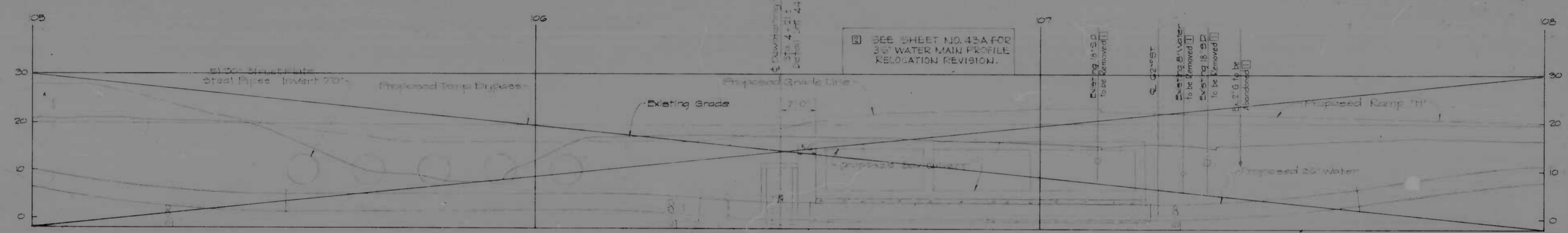
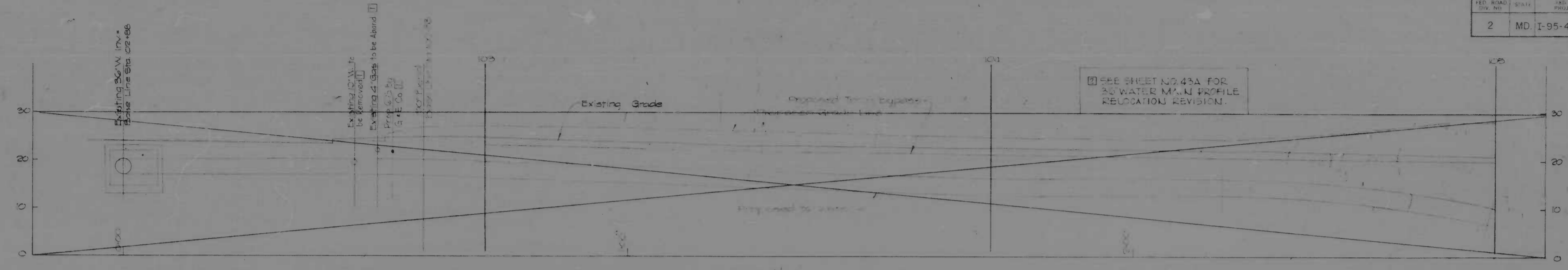


REVISIONS 1) Addendum No. 10 - Feb. 1971 2) Revision No. 10 - 12/71 Reference to 36" Water Main	CONSULTANT KROEHL, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 141 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	
		DRAWN BY: L.M.C. TRACED BY: L.M.C. F.A.P. NO. I-95-4140-38 S.R.C. NO. BC 248-37-415 BALD. CITY NO. 1969	DES. BY: L.M.C. CHK. BY: L.M.C. SHEET NO. 43 of 62



36" WATER MAIN PROFILE
RELOCATION REVISION

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. REVISION 11-23-71 TRENCH DETAIL	WALTER BERRY, STUBBINS & ASSOC. INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND	INTERSTATE ROUTE 95 FROM NORTH OF THE B & O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: AL CHECKED BY: PZ DATE: 1-95-4140138 FILE NO. BC 246-57-815 BALTIMORE, MD 1969



36\"/>

REVISIONS	CONSULTANT
1) Addendum No 2, Feb 19, 1971	
2) Revision Nov 10, 1971 Relocation 36\"/>	
3) Revision Nov 24, 1972 Detail 12\"/>	

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS &
INTERSTATE ROUTE 95 FROM
NORTH OF THE B & O RAILROAD TO
NORTH OF PULASKI HIGHWAY

STATE ROADS COMMISSION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

DRAWN BY L.M.G.	DES BY L.M.G.
TRACED BY L.M.G.	CHK BY L.M.G.
F.A.P. NO. I-95-4(40)38	SHEET NO.
S.R.C. NO. BC 246-37-815	45 of 62
BALTO. CITY NO. 1969	

SCALE: As Shown

DATE:

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(4)38	46	92

LEGEND

- EXISTING ISLAND UTILIZED
- NEW PAVING TO BE UTILIZED FOR TEMPORARY AND FINAL ROAD
- NEW PAVING FOR TEMPORARY DETOUR ROAD
- EXISTING PAVEMENT TO BE UTILIZED
- PROP. 6 1/2" G3 ST. CONCRETE

NOTES-TEMPORARY DETOUR ROAD

1. EXISTING MANHOLE FRAMES COVERS AND GRATES SHALL BE REMOVED AND SALVAGED EXISTING STRUCTURES SHALL BE BACKFILLED WITH SELECT MATERIAL AND ALL EXISTING PIPES PLUGGED OR REMOVED AS REQUIRED.
2. REMOVE TOP PORTION OF EXISTING INLET EXTEND TO NEW INLET CURB HEADPIECE ON NEW INLET SALVAGE EXISTING FRAME COVER AND GRATE.
3. EXISTING INLETS 46 1/2" OF STA 106+92 AND STA 107+85 AND DOWNSTREAM DRAINS ARE TO REMAIN IN OPERATION UNTIL THE CLOSING OF THE WEST BOUND LANES OF THE TEMP. DETOUR. AFTER WHICH THE CONTRACTOR SHALL ABANDON, SALVAGE AND PLUG STORM DRAINS AS DIRECTED IN NOTE 4.
4. REMOVE AND STORE CONCRETE COVER FROM THE EXISTING INLET 36" OF STA 107+87 EASTBOUND TEMPORARY ROAD AND 47" WID WITH A TEMPORARY (SALVAGED) GRADE THE INLET SHALL BE RESTORED TO ORIGINAL CONDITION FOR FINAL CONSTRUCTION.
5. TEMPORARY GUARD RAIL POSTS ANCHORED TO THE TOP SLAB OF THE REINFORCED CONCRETE BOX CULVERT ARE DETAILED ON SHEET NO. 47 OF THE CULVERT DETAILS.
6. REFERENCE DRAWINGS:
 PULASKI HIGHWAY ROAD CONSTR. PLANS SEE SHEET NO. 42-26
 TEMP. DETOUR ROAD PROFILE AND DETAILS SEE SHEET NO. 48
 PULASKI HIGHWAY UTILITY PLANS SEE SHEET NO. 38-40
 SEQUENCE OF CONSTRUCTION FOR ROAD CONSTRUCTION SEE SHEET NO. 47
7. THE CONTRACTOR SHALL AFTER THIS PHASE OF OPERATION IS COMPLETED RESTORE AND/OR FINISH THE AREA IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
8. 7 1/2" BALTIMORE GAS & ELECTRIC COMPANY FORCES WILL. (RECT. REMOVE) 400 WATT MERCURY VAPOR LAMPS MOUNTED WITH 20 FOOT MAST ARM ON WOOD POLES FOR TEMPORARY LIGHTING OF THE DETOUR ROAD.

NOTES - DETOUR FOR BRIDGE CONSTR.

- FOR REFERENCE DRAWINGS SEE NOTE 6 TEMPORARY DETOUR ROAD THIS SHEET
1. THE EXISTING CURB SHALL BE REMOVED AND LATER REPLACED FOR THIS PHASE OF CONSTRUCTION.
 2. CURB BETWEEN STA 102+74.00 L+ AND STA 102+74.00 R+ IS TO BE INSTALLED AFTER THIS PHASE OF OPERATIONS IS COMPLETE.
 3. THE CONTRACTOR SHALL AFTER THIS PHASE OF OPERATIONS IS COMPLETE RESTORE AND/OR FINISH THE AREA IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.

LEGEND

- NEW PAVING FOR TEMPORARY DETOUR ROAD
- EXISTING PAVEMENT TO BE UTILIZED
- EXISTING ISLAND UTILIZED

M-2

CONSTR SBR CURVE DATA

$\Delta = 5^\circ 42' 01''$ LI

$D = 0^\circ 45' 00''$

$R = 7639.44'$

$T = 380.33'$

$L = 760.04'$

$O = 0^\circ 45' 00''$

$L = 200.00'$

$LT = 133.34'$

$ST = 66.67'$

M-3

CONSTR MOORES RUN RELOCATED

$\Delta = 17^\circ 37' 15''$

$D = 0^\circ 05' 33''$

$R = 1400.00'$

$T = 216.9926'$

$L = 430.5592'$

M-4

CONSTR MOORES RUN RELOCATED

$\Delta = 10^\circ 20' 48''$

$D = 0^\circ 07' 19''$

$R = 2700.00'$

$T = 244.4534'$

$L = 487.5814'$

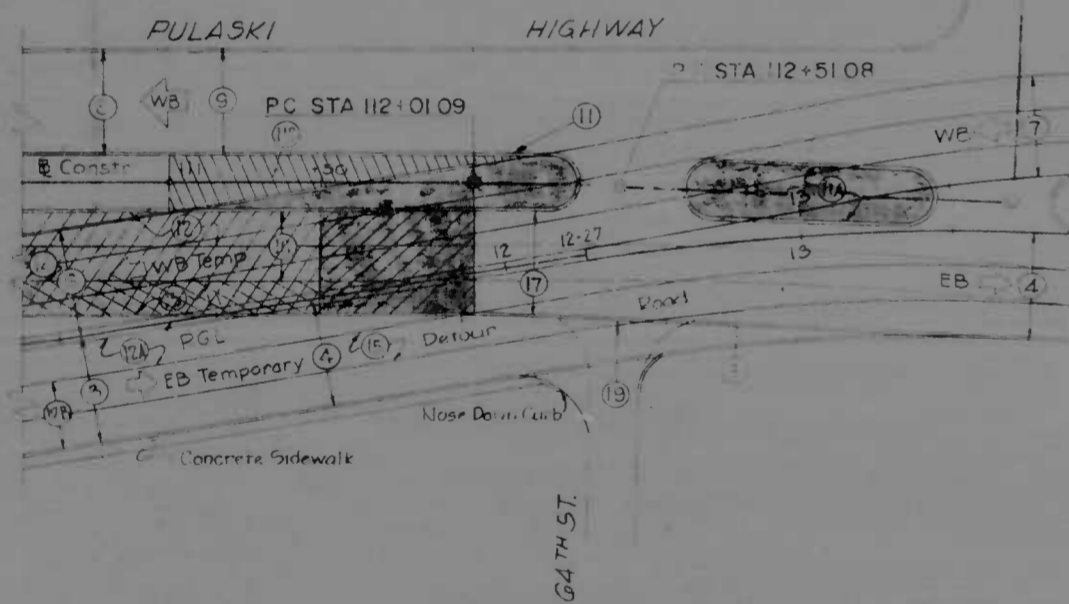


DETOUR FOR BRIDGE CONSTRUCTION PHASE II

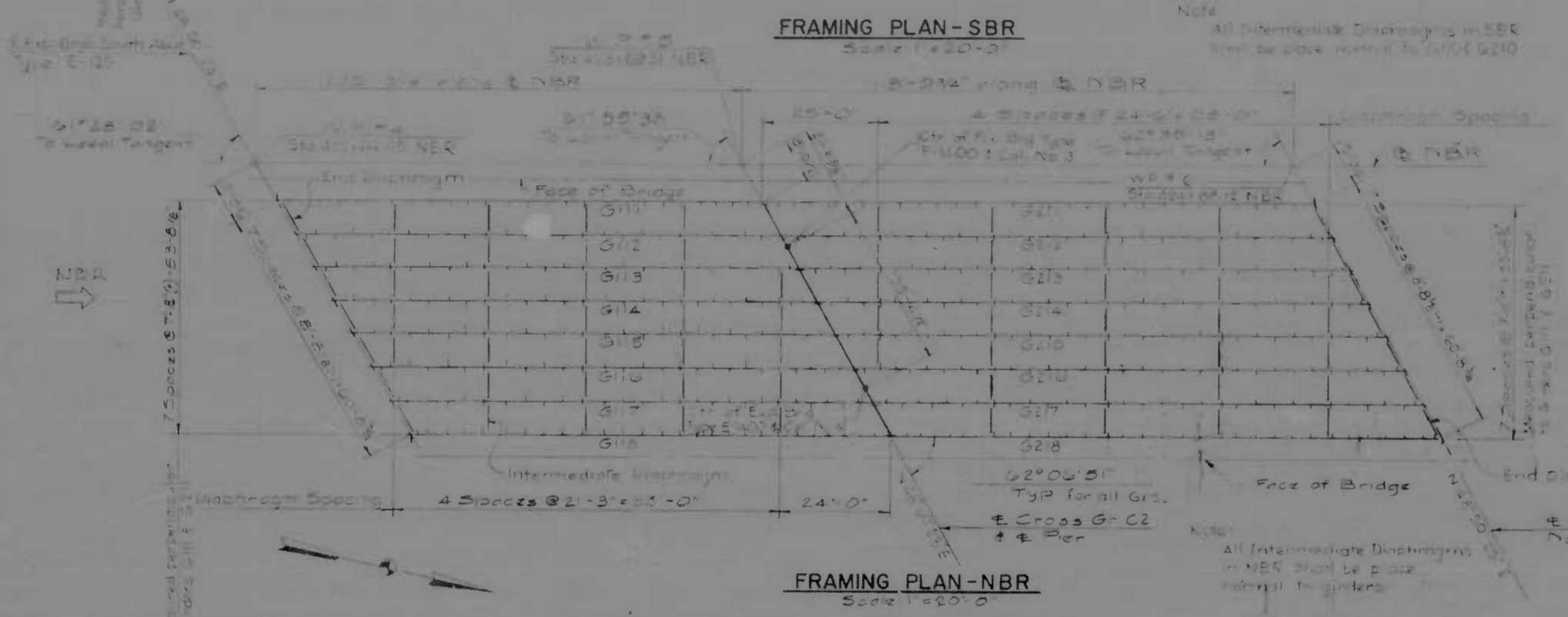
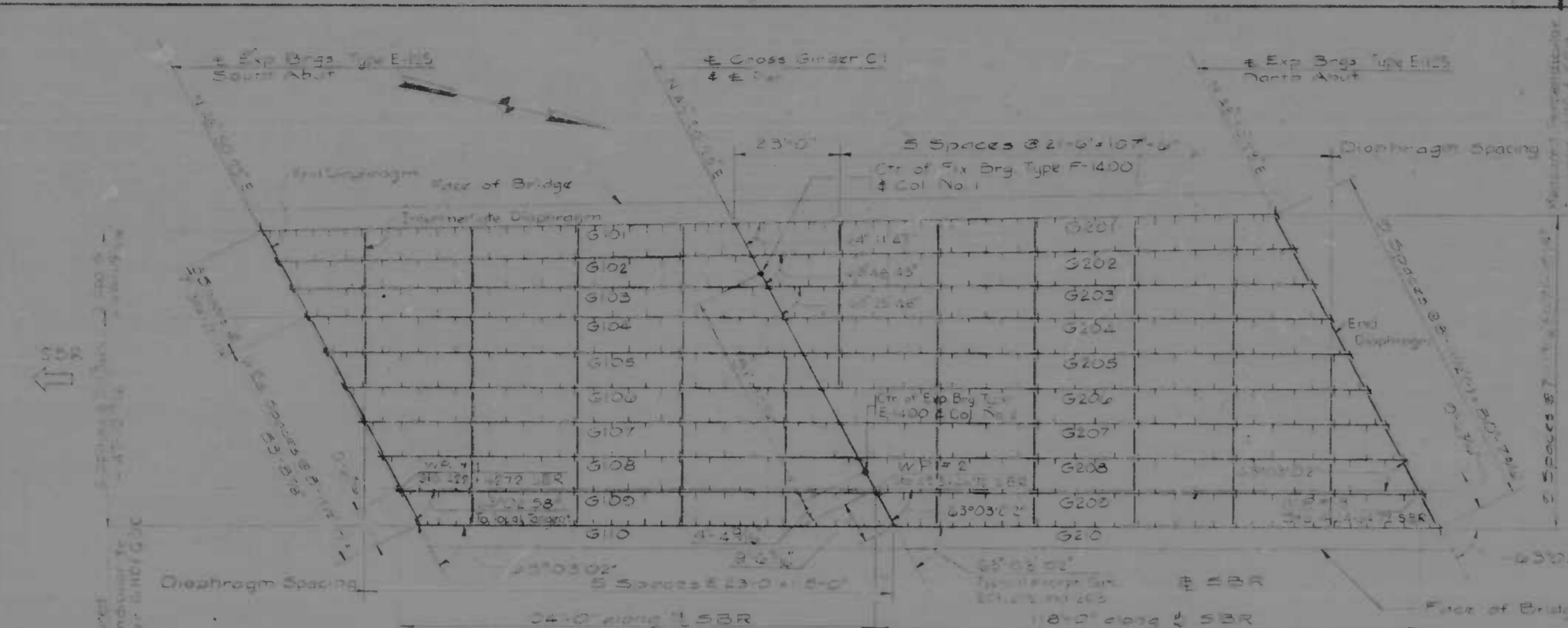
REVISIONS Addendum No. 1 Feb 1971 Revision No. 10 10 71 Deleted the 24" W.P.E.L. poles installed by the Department of Public Works (TRAFFIC) SEE DETAIL SHEET NO. 42	CONSULTANT KNOXLE, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21204	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
		INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: P.R.Z. CHECKED BY: P.R.Z. F.A.P. NO. I-95-4(4)38 S.R.C. NO. BC 245-37-614 BALTO. CITY NO. 1969

**SUPPLEMENTARY
SEQUENCE OF CONSTRUCTION
FOR ROAD CONSTRUCTION
PHASE I THRU PHASE V**

- PHASE I**
- Remove the right curb of Pulaski Highway, Eastbound Between Sta 100+00 and Sta 103+50 and construct the Taper and 8' Widening Lane of Pulaski Highway Eastbound to Sta 103+50. Exclude curb between Sta 100+00 and Sta 103+50.
 - Remove the right curb of Pulaski Highway, EB, Sta 111+00 to Sta 13+92 EB. Detour.
 - Construct the eastbound lanes of the Temporary Detour Road.
 - Divert all eastbound traffic to the Temporary Detour Road.
 - Keep westbound traffic on Pulaski Highway.
 - Remove existing right curb of Pulaski Highway, Eastbound between Sta 103+50 and Sta 105+00.
- PHASE II**
- Construct the Westbound Lanes of the Temporary Detour Road.
 - Divert the East and Westbound traffic to the Temporary Detour Road.
 - Construct the Westbound Lanes of Pulaski Highway, left curb between Sta 102+25.2 and Camp 11, Sta 11+10(L), Median Curb to Sta 112+01.09 and Median to Sta 111+00.
 - Open the Westbound Lanes of Pulaski Highway to traffic.
 - Construct the Median 4' on the Eastbound of Pulaski Highway Sta 103+50 to Sta 112+01.09 and the Median Curb to Sta 112+01.09 and the Merging Lane from Moravia Park. Exclude Island in Sta 103+25.2.
 - Restore the Median Curb and Island in Sta 112+01.09.
 - Median Curb and Island East of 64th Street and construct Median Sta 111+00 to 112+01.09.
 - Open the Median Lane of Pulaski Highway Eastbound to traffic and close the Left Lane of the Eastbound Temporary Detour Road (Center and Right lanes remain in use).
- PHASE III**
- Construct the Right Lane (Left) Eastbound of Pulaski Highway, between Sta 102+25.2 and Sta 111+00. Curb Lane and Taper between Sta 103+50 and Sta 107+70, and Right Curb between Sta 103+50 and Sta 109+85.44.
 - Close the Center Lane of the eastbound traffic lanes of the Temporary Detour Road.
 - Close the Center Lane of the westbound traffic lanes of the Temporary Detour Road.
 - See Roadway Plans for entrance of Ramp H between Sta 101+85.44 and Sta 12+01.09.
- PHASE IV**
- Open Eastbound lanes of Pulaski Highway to traffic.
- PHASE V**
- Restore the Intersection of Pulaski Highway and 64th Street.
 - See Notes included in 1, 8, and 9.



REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
1. Addition to 2 Feb 1969	KNORRLE, BENDER, STONE & ASSOC., INC.		
2. Revision No. 19.1			
3. Relocation 36" Water Main	WATZ, CHILDS & ASSOC., INC.		
4. Revision No. 4	CONSULTING ENGINEERS		
5. Revision No. 5	341 N. CALVERT STREET		
6. Revision No. 6	BALTIMORE, MARYLAND 21202		
		INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: PAZ DES BY: C TRACED BY: PAZ CHK BY: C
		SCALE: 1"=40'	F.A.P. NO. I-95-440138 S.R.C. NO. BC 246-37-815 BALTO. CITY NO. 1969
		DATE	SHEET NO. 47 of 52



SOUTHBOUND ROADWAY											
GIRDER	L	FLANGE PLATE			GIRDER	L	FLANGE PLATE				
		A	B	C*			D*	E	F	G	H
G101	102'-11"	TOP 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G201	107'-9 3/4"	TOP 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0
G102	102'-3 1/2"	TOP 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G202	117'-2 1/8"	TOP 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0
G103	103'-7 1/2"	TOP 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G203	117'-7 1/4"	TOP 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0
G104	104'-0"	TOP 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G204	118'-0"	TOP 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0
G110		BOTTOM 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G210		BOTTOM 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0

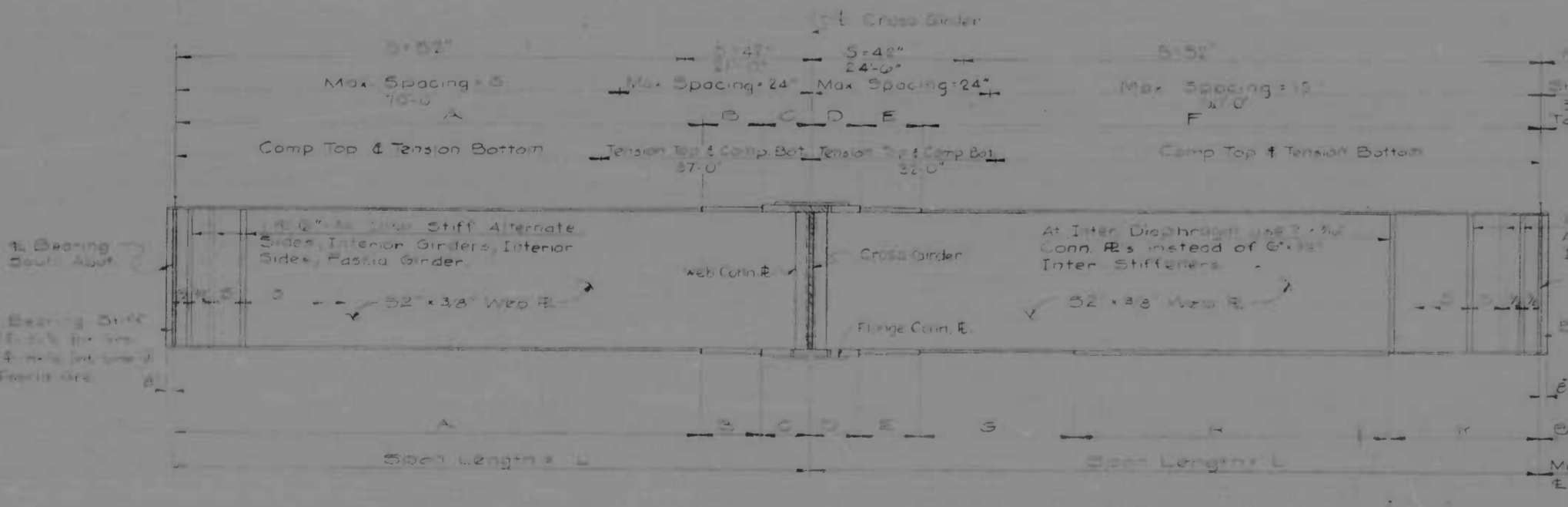
* C.A. Dimensions to R. of Cross Girder. The plates are to be added to meet flange connections as shown in Cross Girder Details. Sheet No. 10.

NORTHBOUND ROADWAY											
GIRDER	L	FLANGE PLATE			GIRDER	L	FLANGE PLATE				
		A	B	C*			D*	E	F	G	H
G111	104'-10 1/2"	TOP 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G211	119'-0"	TOP 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0
G112		BOTTOM 24x14x1/2	16x1 1/2x10-0	20x2 1/2x10-0	G212		BOTTOM 20x2 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0	16x1 1/2x10-0

NOTES:
 All girders and designed bracing, and members, shall be as temporary intermediate supports while concrete is being placed.
 All structural steel shall conform to A.S.T.M. Designation A 588.
 All anchor bolts shall be hot dipped galvanized.
 All intermediate stiffeners and connection plates shall be placed right against the tension flange and fillet welded to compression flange.
 All bearing stiffeners shall be fillet to bear against tension flange and fillet welded to compression flange.
 All stiffeners and the end of girders shall be 1/4" thick.
 All dimensions given are non-zonal dimensions.
 All bearing stiffeners shall be placed along E girder.

REFERENCES:

General Plan & Elevation	Sheet No. 57
Cross Girder Details	Sheet No. 10
Diaphragm Details	Sheet No. 11
Member Diagrams & Tables	Sheet No. 12
Super Structure Elevations	Sheet No. 13
Truck Plan	Sheet No. 14
Bearing Details	Sheet No. 15
Flange Splice Details	Sheet No. 16
Weld Details	Sheet No. 17

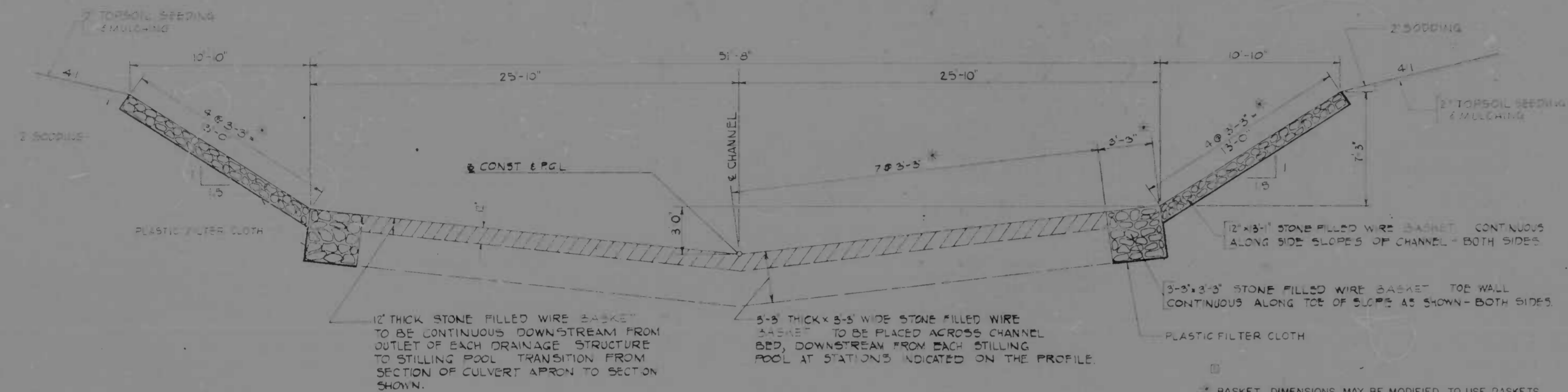


NOTE: For dimensions see table on this sheet.

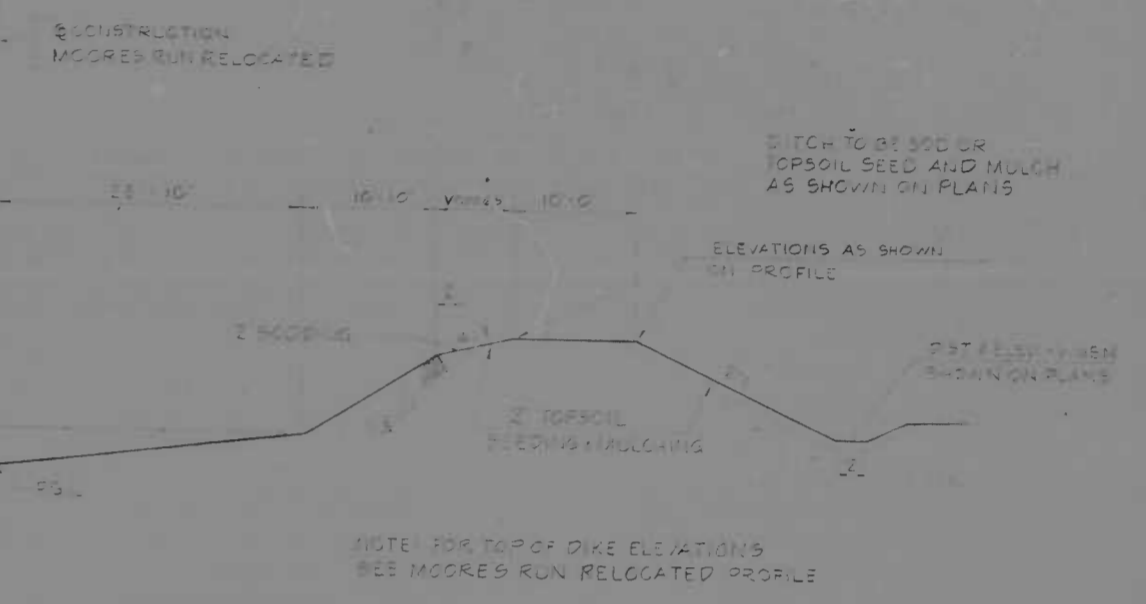
TYPICAL GIRDER ELEVATION
 Not to Scale

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	THORPE, SANDER, STONE & ADAMS, INC. HAYL, GUYER & COOKE, INC. CONSULTING ENGINEERS 841 S. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 OVER PULASKI HIGHWAY U.S. RTE. 40 FRAMING PLAN	DRAWN BY P.D. TRACED BY P.D. DES. BY M.S.C. CHK. BY F.F.M.
		SCALE: As Shown	DATE
			SHEET NO. 57 OF 82

REV. NO.	DATE	BY	CHKD.	APP. NO.
2	MD. 1-35-48	DLI	RWC	6



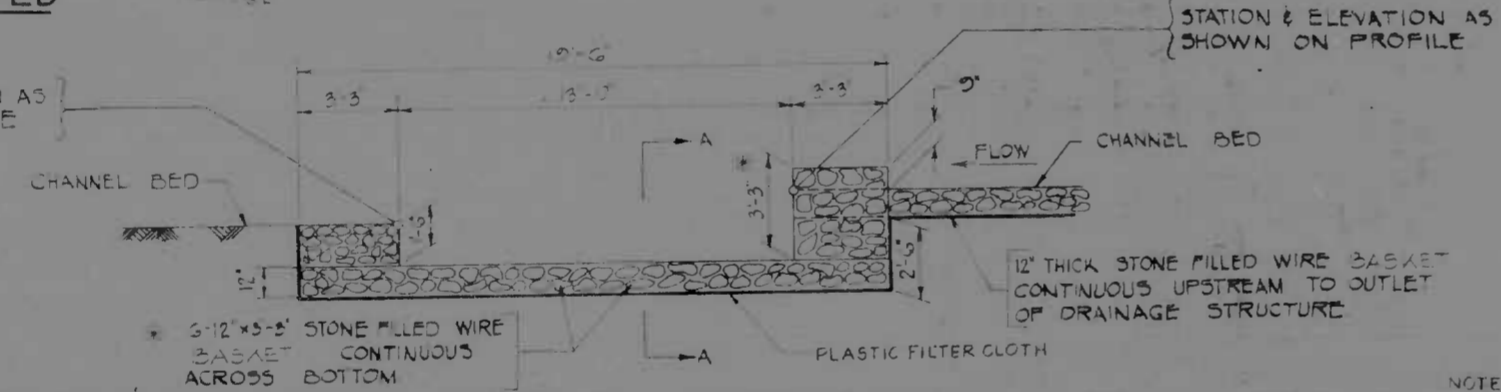
TYPICAL CHANNEL SECTION
MOORES RUN RELOCATED
 SCALE 1" = 4'-0"



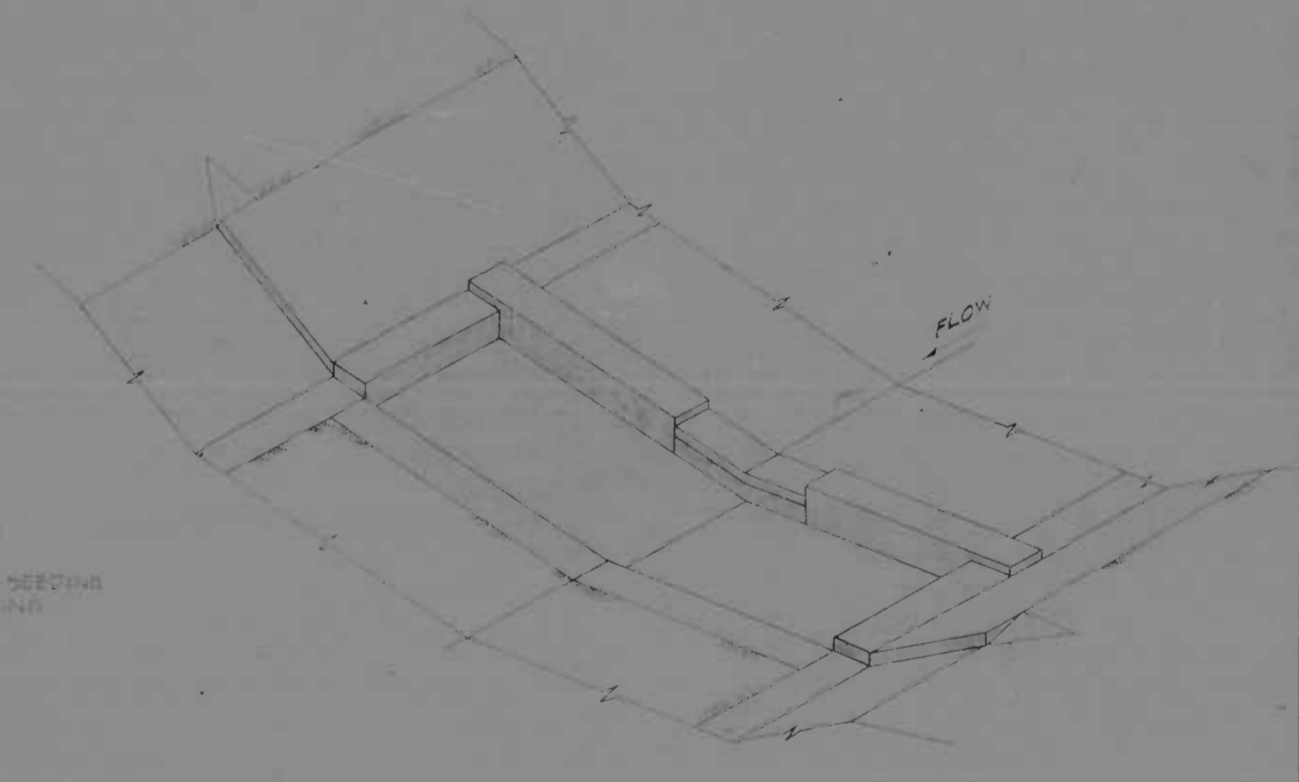
MOORE'S RUN RELOCATED W/DIKE
 SCALE 1" = 10'-0"



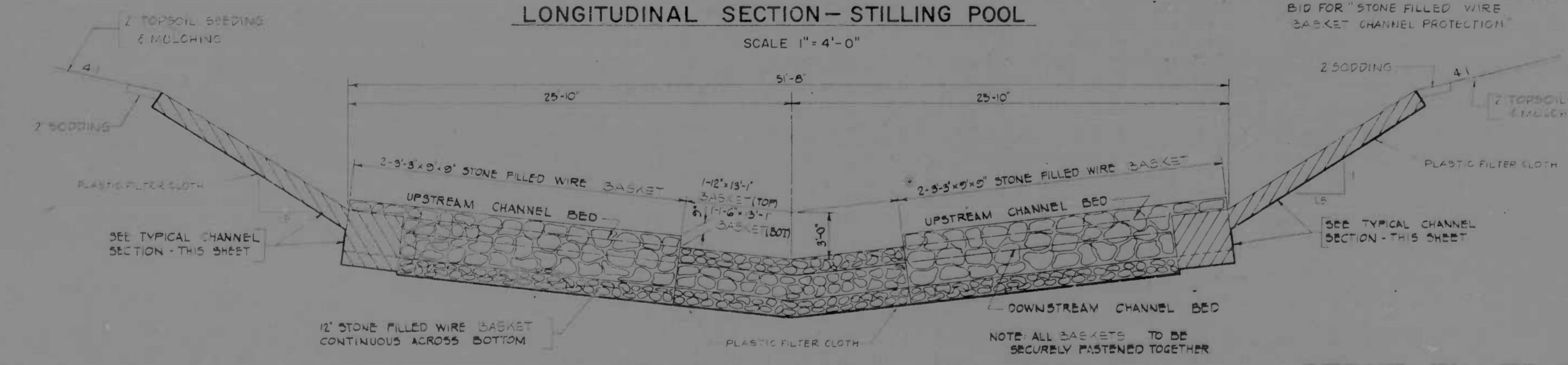
MOORE'S RUN RELOCATED
CHANNEL DETAIL (Left Side)
 SCALE 1" = 10'-0"



LONGITUDINAL SECTION - STILLING POOL
 SCALE 1" = 4'-0"



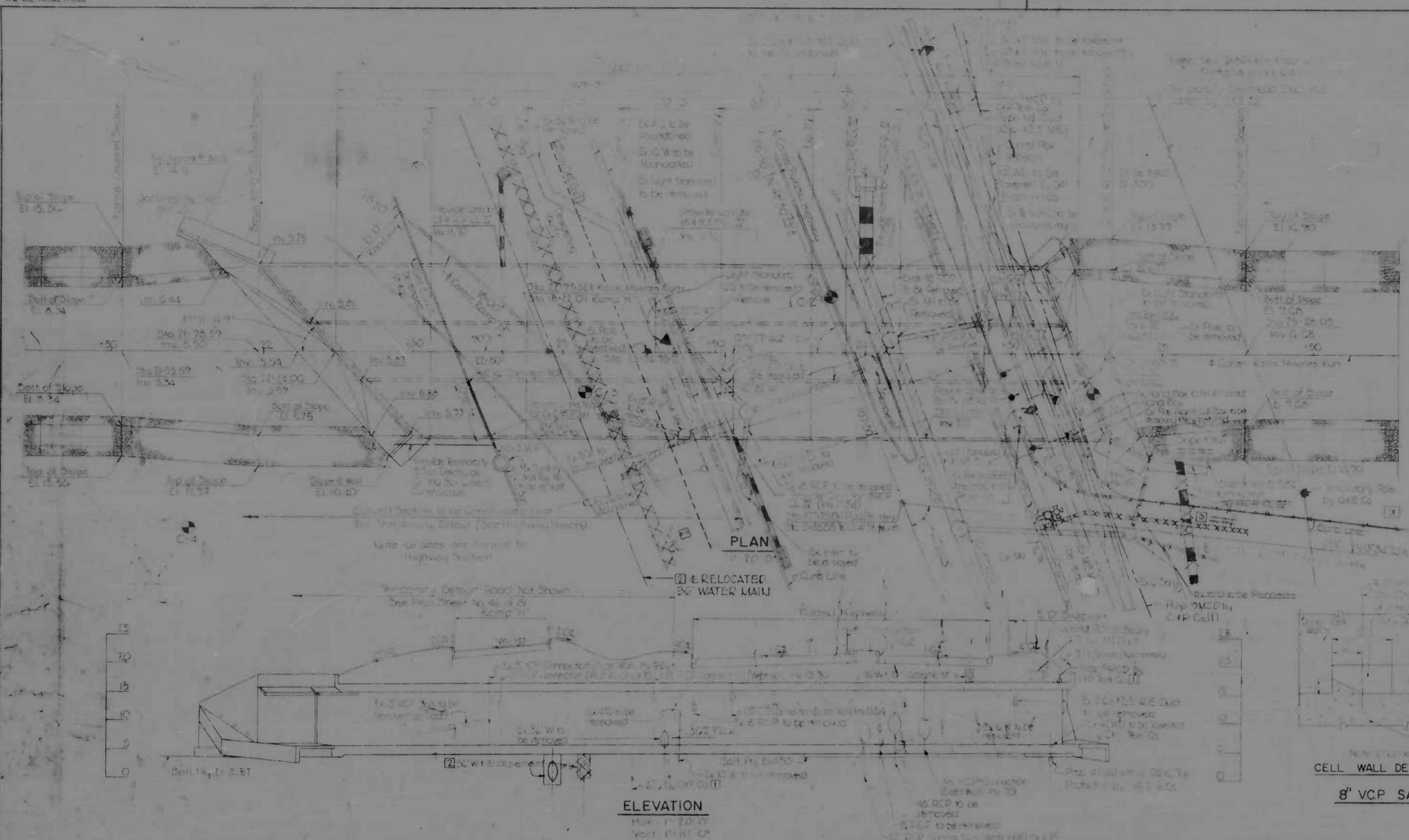
ISOMETRIC VIEW - STILLING POOL
 NO SCALE



SECTION A-A - STILLING POOL
 SCALE 1" = 4'-0"

STONE FILLED WIRE BASKET DETAILS

REVISIONS 1 ADDENDUM NO 2 2/19/71	CONSULTANT KNOWLER, BENDER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 301 N. CALVERT STREET BALTIMORE, MARYLAND 21208	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY DRAWN BY: DLI TRACED BY: DLI F.A.P. NO. 1-95-340138 S.R.C. NO. 87246-37-815 BALTO. CITY NO. 1862
		SCALE: AS SHOWN	DATE: _____



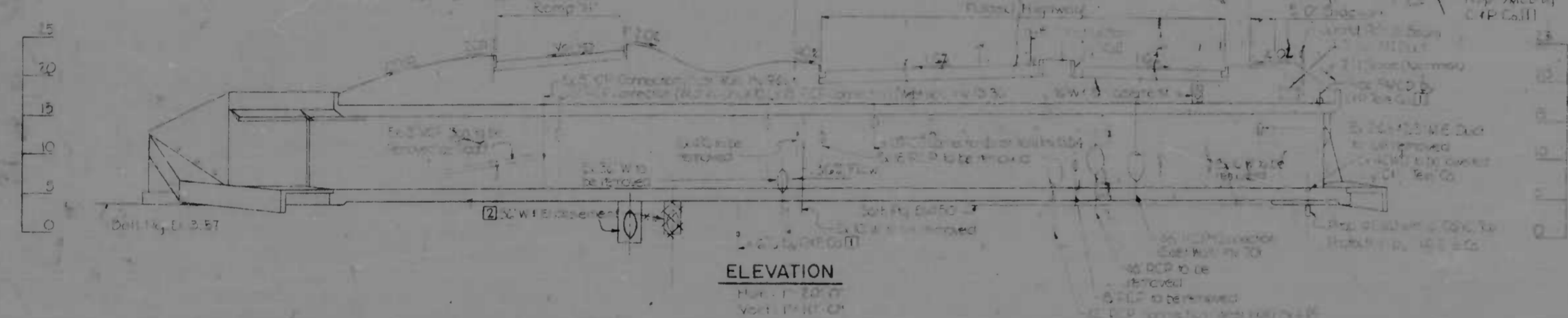
CULVERT END ELEVATION

REFERENCE:

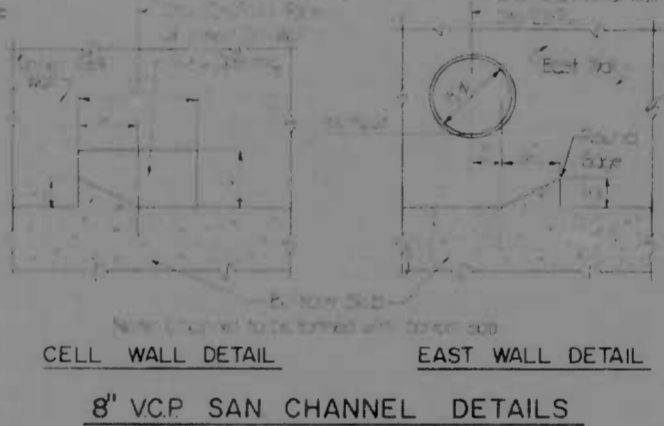
Standard Section - Pulaski Highway, E2nd St.	1
Standard Section - Ramp	2
Standard Plans - Deck Road Drains	3
Plan & Section - San Relocated	4
Utility Relocation	5
Water Main Relocations	6
New Location of Traffic	7
General Details	8
Proposed Details	9
Temporary Details	10
Foundation Details	11
Detailing Wall Details	12
Original Section - 4th St. - N. 95	13

GENERAL NOTES:

- SPECIFICATIONS: Maryland S.F.C. Specifications and Bridge Specifications dated Mar. 1968 and General Provisions for Materials and Construction A.A.S.H.O. Standard Specifications for Highway Bridges dated 1962 and 1970 and Specifications for Pavement.
- LOADING: Live Load, H-50, 44 ft. x 24,000 lb. or as otherwise specified.
- CONCRETE: Class A-1 Concrete having a minimum compressive strength of 3000 psi @ 28 Days.
- REINFORCING STEEL: ASTM Specification A-65 Grade 40. All splices shall be lapped a minimum of 24 bar diameters. Minimum Cover shall be 2" unless otherwise noted.
- CHAMFER: All chamfered corners shall be chamfered 3/4" x 1/4" unless otherwise noted.
- DAMP-PROOFING: Damp-proofing shall be applied to those portions of the outside surface of subwalls, top of the walls, base of risers and wingwalls that are in contact with earth and full face of contraction joints.
- MEMBRANE WATERPROOFING: All contraction joints above the normal water surface with earth on one side and air on the other shall be waterproofed by the application to the earth side of the joint of 2 mil membrane 18 inches wide centered on the joint. 14" membrane waterproofing on contraction and/or expansion joints.
- SOIL FILLING: A three foot strip shall be laid adjacent to and in back of headwalls and wingwalls.
- EQUIPMENT: No equipment shall be allowed to pass over the structure until backfill has obtained a minimum weight of 50% above top of top soil.
- BEARINGS: PRESTRESS: For design purposes maximum bearing capacity does not exceed 10 tons per sq. ft.
- EXCAVATION: See General Provisions.
- PAVING: 5" indicates flows per foot on 2" slope by a 140 lb weight falling 30 inches.



ELEVATION

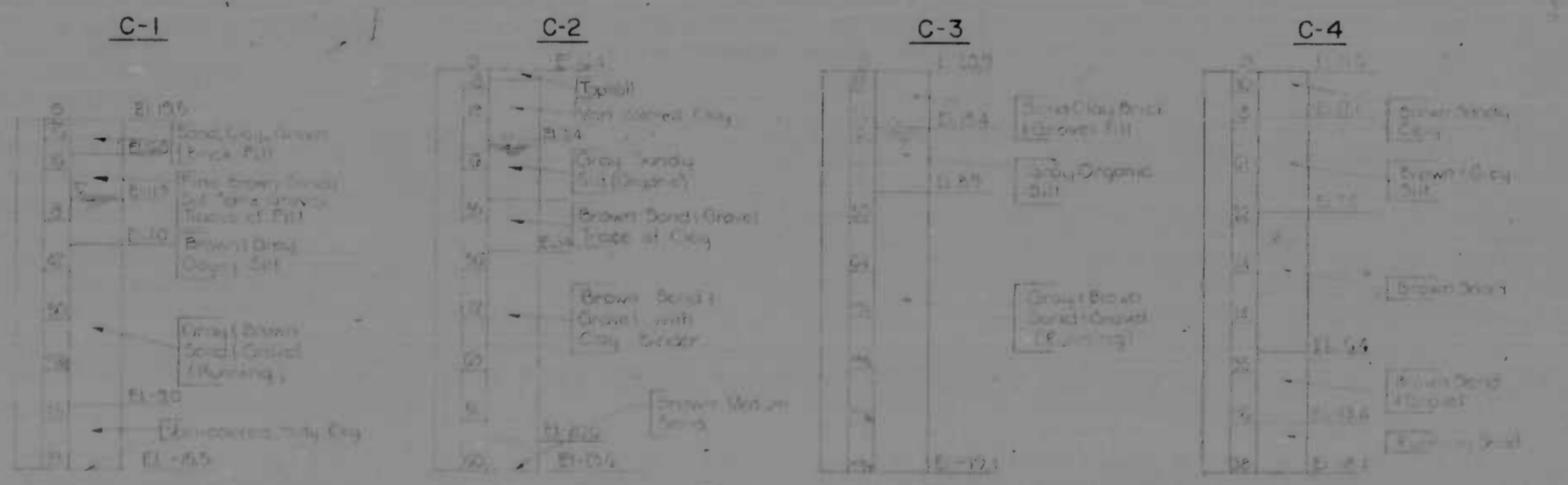


CELL WALL DETAIL EAST WALL DETAIL

8' VCP SAN CHANNEL DETAILS

SUMMARY OF QUANTITIES

CATEGORY	ITEMS	MEASUREMENT	QUANTITY
GRADING	Class 5" Excavation	C.Y.	See Notes 5th
	Selected Backfill 10" Aggregate	C.Y.	50
DRAINAGE	Relocated Backfill using CR-C	C.Y.	150
	300 Touchdown Drilling	L.F.	10
STRUCTURES	Class 73" Excavation for Structure	C.Y.	7050
	Class 74" Excavation for Structure	C.Y.	4000
	Triple 17.0 ft. x 2.0 ft. Reinforced Concrete Box Culvert with Approach (See 107-1033 Pulaski Hwy)	L.S.	1-5
	Conventional Class C Concrete Span Transportation	C.Y.	70
	Conventional Concrete for Box Culvert	C.Y.	10
LANDSCAPING	Soil Grading	S.Y.	50



BORING LOGS

WATER TABLE	WATER TABLE	WATER TABLE	WATER TABLE
Depth Below Surface	Time	Date	Depth Below Surface
1.5	11:00 AM	12/2/67	1.5
1.5	11:00 AM	12/2/67	1.5
1.5	11:00 AM	12/2/67	1.5
1.5	11:00 AM	12/2/67	1.5

HYDROLOGIC AND HYDRAULIC DATA

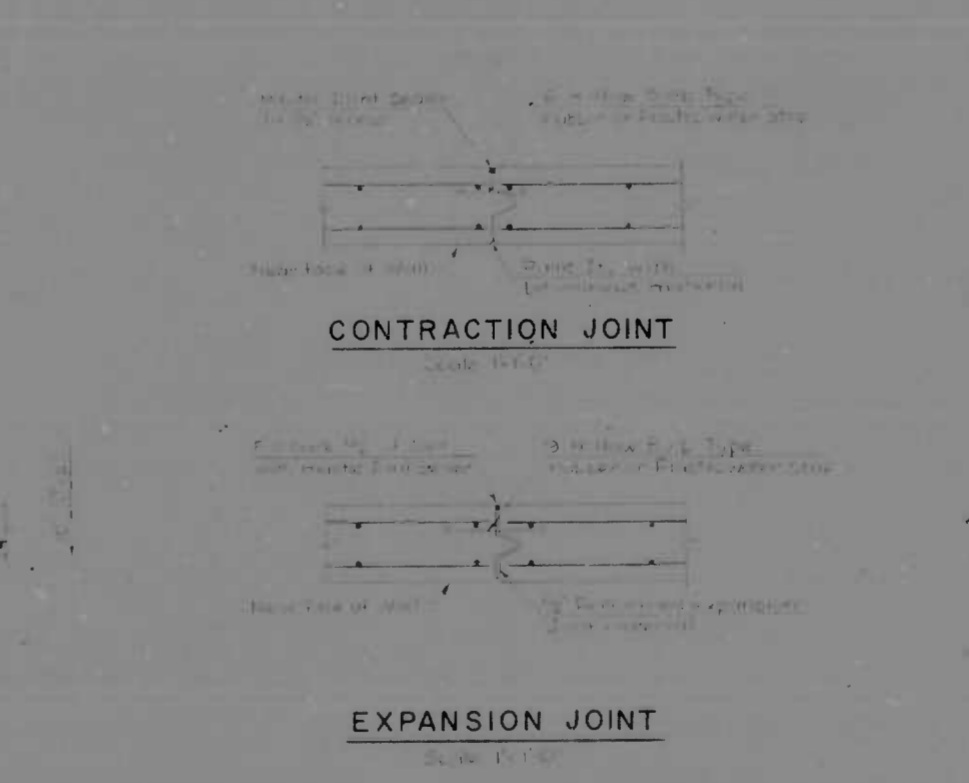
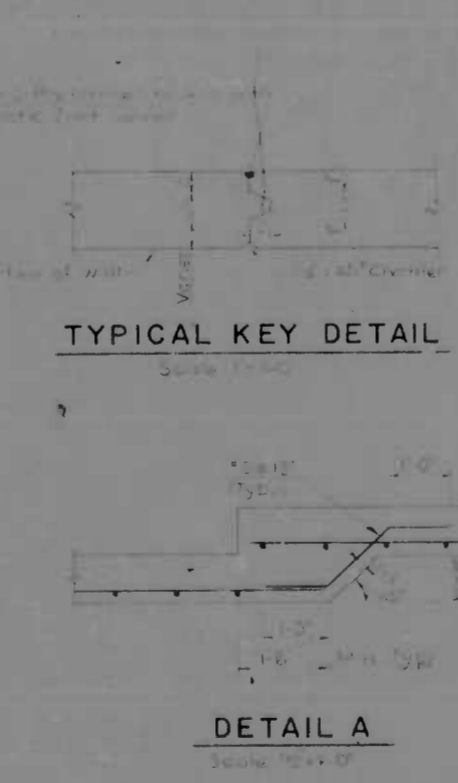
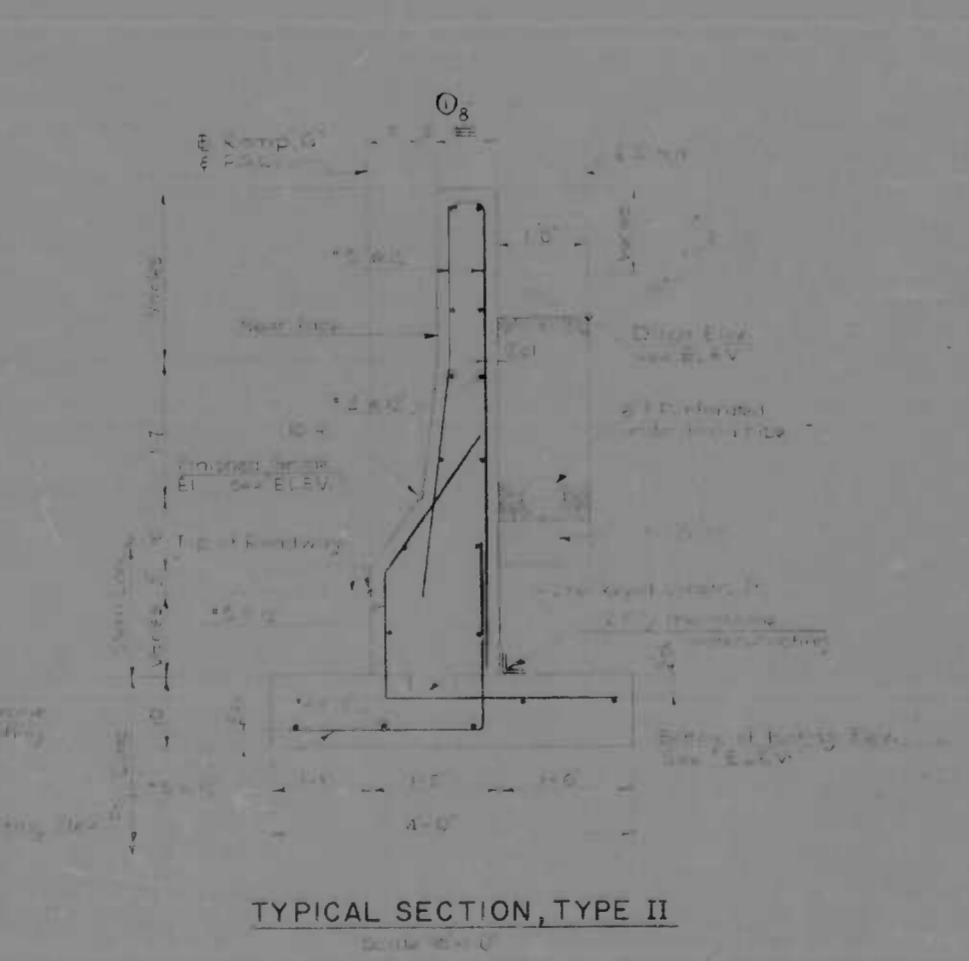
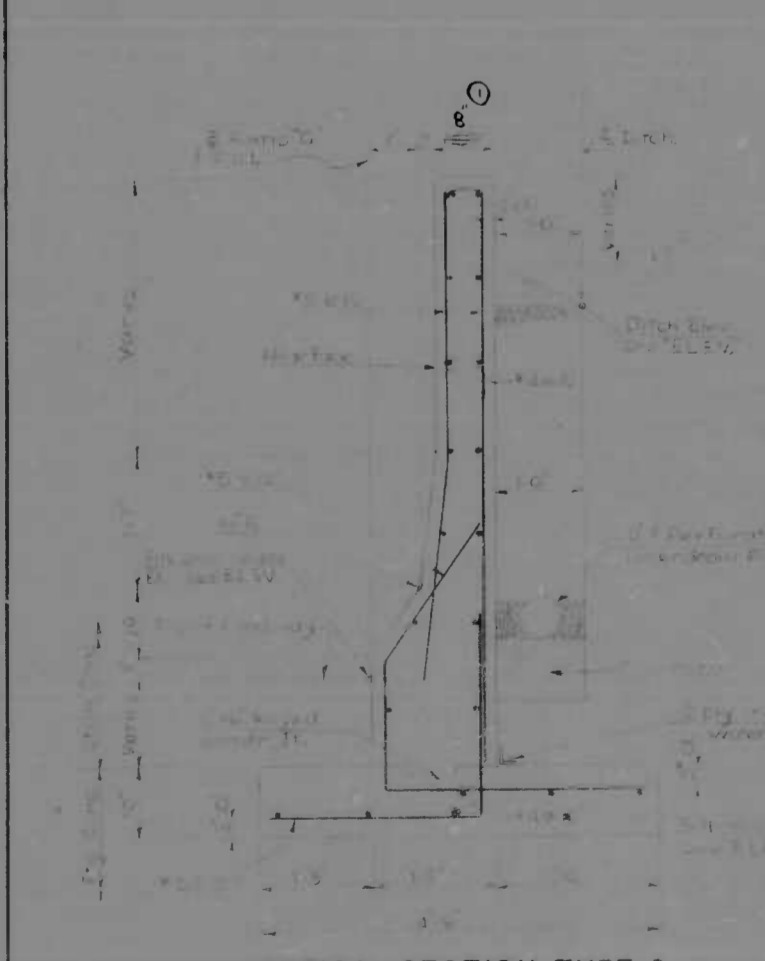
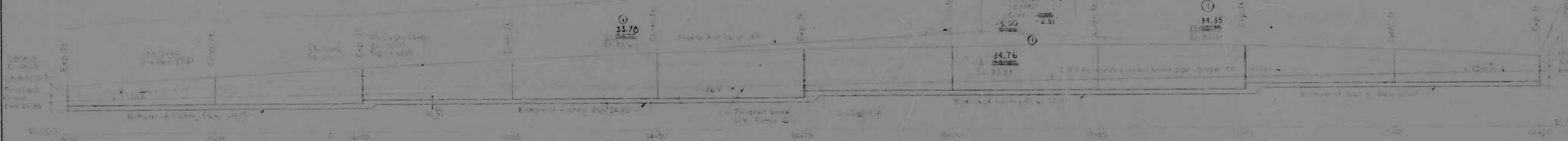
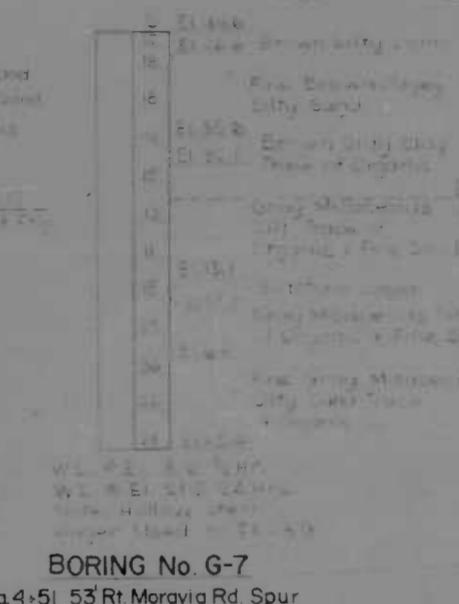
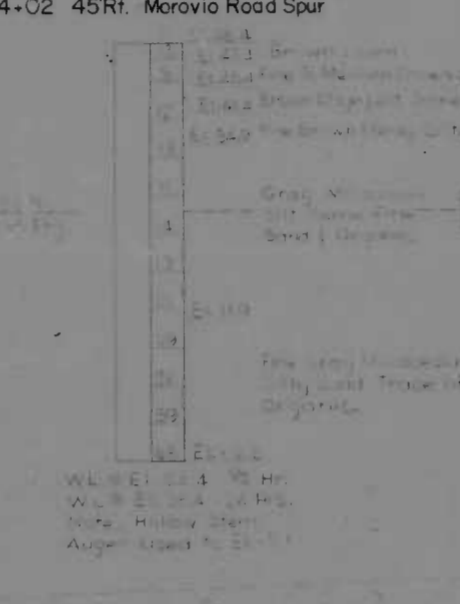
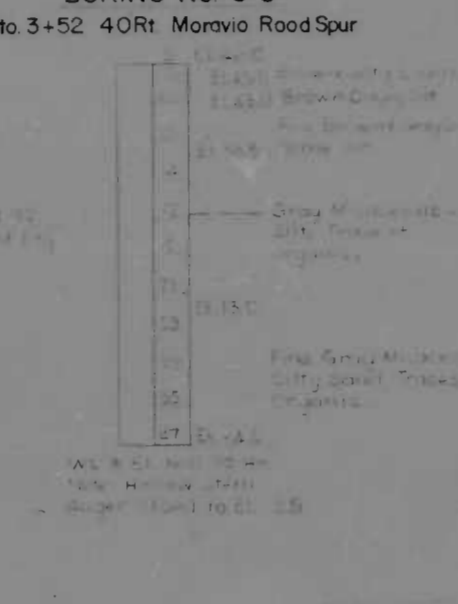
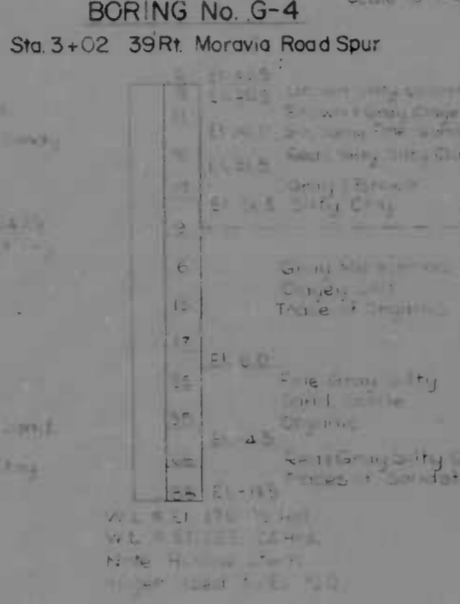
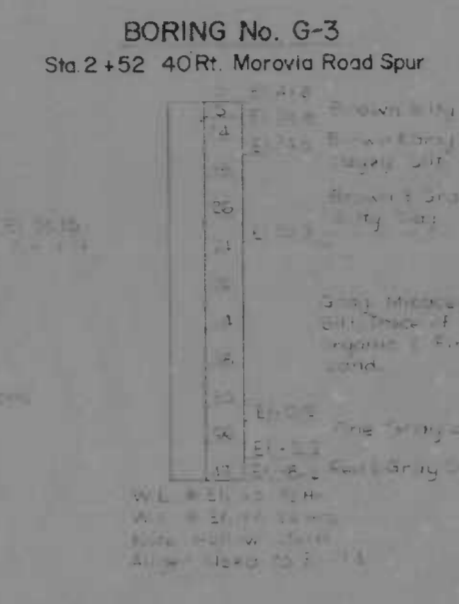
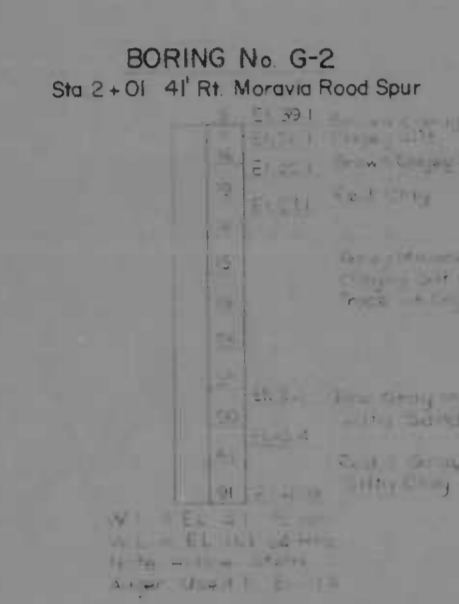
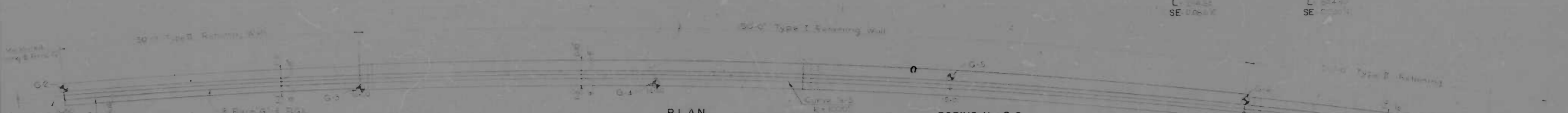
GAGING STATION
 DRAINAGE AREA = 4.66 SQ. MI. = 2989 ACRES
 STORMWATER DISCHARGE @ 25 Year Storm = 5370 c.f.s.
 TIDAL FLOW = 1.70 Per Foot of Rise
 TOTAL MAXIMUM DISCHARGE @ 1.5 = 11.70
 MAXIMUM FLOW DEPTH AT H.W. = 5.13
 VELOCITY AT OUTLET = 1.37 FT. PER SEC.

EXISTING STRUCTURE
 TYPE BRIDGE
 WATERWAY 266 SQ. FT.
 UNDERCLEARANCE 7.0'
 DATE BUILT 1952
 OWNERSHIP STATE ROADS COMMISSION
 DISPOSITION TO BE DEMOLISHED
 REMARKS NONE

<p>REVISIONS</p> <ul style="list-style-type: none"> 1) Addendum 1 to 2 Feb 1971 2) Revision May 10, 1971 3) Relocation 32" Water Main 4) Revision July 24, 1972 5) Water Main Ass. FR 10/2/68 	<p>CONSULTANT</p> <p>KENDRICK DENNER, STONE & ROSS, INC.</p> <p>440</p> <p>RAY, CHOS & ASSOC., INC.</p> <p>CONSULTING ENGINEERS</p> <p>341 N. CALVERT STREET</p> <p>BALTIMORE, MARYLAND 21202</p>	<p>CITY OF BALTIMORE</p> <p>DEPARTMENT OF PUBLIC WORKS</p> <p>INTERSTATE ROUTE 95</p> <p>PULASKI HIGHWAY OVER MOORES RUN</p> <p>GENERAL PLAN AND ELEVATION</p> <p>SCALE: AS SHOWN</p> <p>DATE:</p>	<p>STATE ROADS COMMISSION OF MARYLAND</p> <p>INTERSTATE DIVISION FOR BALTIMORE CITY</p> <p>DRAWN BY: MSF & JRW</p> <p>DES. BY: J.J.M.</p> <p>TRACED BY: MSF & JRW</p> <p>CHK. BY: P.F.M.</p> <p>F.A.P. NO. 1-95-4(40)38</p> <p>S.R.C. NO. BC 246-37-B15</p> <p>BALTO. CITY NO. 1969</p> <p>SHEET NO. 67 of 82</p>
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REV. NO.	DATE	BY	CHKD.	TOTAL SHEETS
2	MD I-95-44038	70	82	

Curve G-2	Curve C-1
Δ = 77° 05' 30"	Δ = 94° 35' 30"
Dc = 25' 46' 30"	Dc = 15' 45' 40"
R = 1000'	R = 1000'
T = 108.04'	T = 174.93'
L = 108.04'	L = 174.93'
SE = 108.04'	SE = 174.93'



SUMMARY OF QUANTITIES

ITEM	UNIT	AMOUNT
Concrete	cu yd	100
Reinforcement	lb	10000
Formwork	sq ft	1000
Gravel	cu yd	50
Backfill	cu yd	1000

- NOTES:
1. Foundation and retaining wall to be constructed in accordance with the specifications of the Interstate Highway Act of 1956, as amended.
 2. All borings were made in June, 1969.
 3. All soil samples were analyzed in accordance with the specifications of the Interstate Highway Act of 1956, as amended.
 4. See Notes 1 through 3 for details of soil sampling and analysis.

REVISIONS ① Rev 10/1/71 RWC	CONSULTANT KNOERLE, BENDER, STONE & ASSOC., INC. AND NATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE ROUTE 95 RAMP "G" RETAINING WALL	
		DRAWN BY: R.V.P. TRACED BY: R.V.P. F.A.P. NO: I-95-4(40)38 S.H.C. NO: BC 246-27-815 BALTO. CITY NO. 1969	DES. BY: I.H. CHK. BY:
SCALE: As Shown		DATE:	
			SHEET NO. 70 of 82

INDEX OF SHEETS

2	MD I-95-44(0)38	1	32
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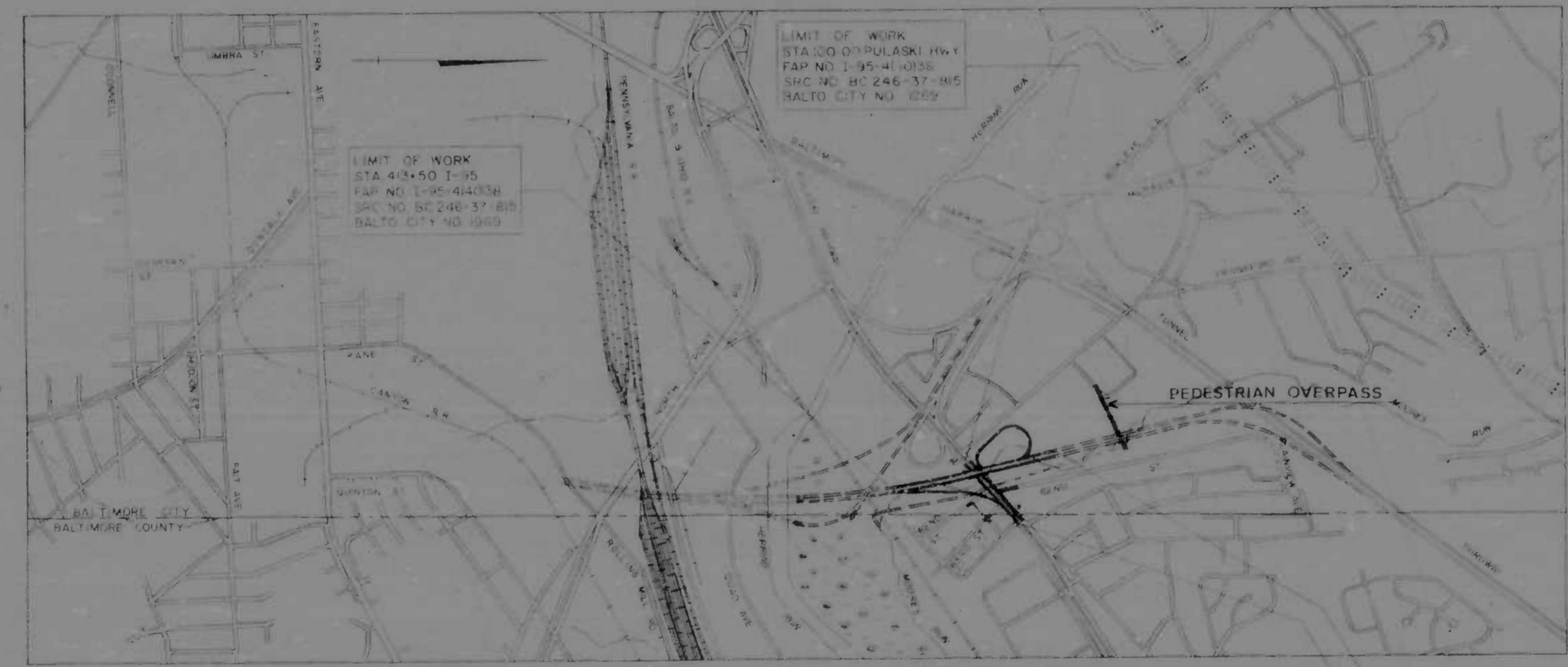
- 1 TITLE SHEET
- 2-5 ROADWAY TYPICAL SECTIONS
- 6 MOORES RUN RELOCATED TYPICAL SECTIONS
- 7 GENERAL NOTES
- 8 GEOMETRY
- 9 REFERENCES
- 10 SUPERELEVATION TABLES
- 1-18 DETAILS
- 19 PLAN SHEET STA 413+50 TO STA 414+00 I-95
- 20 PLAN SHEET STA 414+00 TO STA 420+00 I-95
- 21 PLAN SHEET STA 420+00 TO STA 429+00 I-95
- 22 PLAN SHEET STA 108+20 TO STA 113+84 PULASKI HIGHWAY
- 23 PLAN SHEET STA 429+00 TO STA 434+00 I-95
- 24-25 GRADING AND JOINT LAYOUT
- 26 PULASKI HIGHWAY 20' SCALE PLAN
- 27 PROFILE I-95
- 28 PROFILE PULASKI HIGHWAY
- 29 PROFILE RAMP "G"
- 30 PROFILE RAMP "H"
- 31 PROFILE MOORES RUN RELOCATED STA 12+50 TO STA 15+00
- 32 PROFILE MOORES RUN RELOCATED STA 25+00 TO STA 32+00
- 62ND STREET CONNECTOR ROAD
- 62ND STREET NORTH
- 37 STORM DRAIN SCHEDULE
- 38 STORM DRAIN PROFILES
- 39 UTILITY RELOCATION
- 41-43 12" AND 8" WATER MAIN RELOCATION
- 42-48 12" AND 8" WATER MAIN RELOCATION
- 49 12" AND 8" WATER MAIN RELOCATION
- 55 OVER PULASKI HIGHWAY
- 45 GENERAL PLAN AND ELEVATION
- 50 SECTIONS AND DETAILS
- 51 SOUTH ABUTMENT-SBR
- 52 SOUTH ABUTMENT-NBR
- 53 NORTH ABUTMENT-SBR
- 54 NORTH ABUTMENT-NBR
- 55 APPROACH SPAN DETAILS
- 56 NBR AND SBR PIER DETAILS
- 57 FRAMING PLAN
- 58 CROSS GIRDER DETAILS
- 59 DECK SECTION AND DETAILS
- 60 CAMBER DIAGRAMS AND TABLES
- 61 NB AND SB SUPERSTRUCTURE ELEVATIONS
- 62 BEARING DETAILS
- 63 EXPANSION JOINT AND MISCELLANEOUS DETAILS
- 64 ERECTION PLAN
- 65 UNDERBRIDGE LIGHTING AND CONDUIT DETAILS
- 66 BORING DATA
- TRIPLE CELL 19'-0" X 9'-0" BOX CULVERT PULASKI HWY STA 107+20.3
- 67 GENERAL PLAN AND ELEVATION
- 68 CULVERT DETAILS
- 69 WINGWALL DETAILS
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- 71 METHODS OF SEDIMENTATION AND EROSION CONTROL
- 72-82 SUMMARY OF QUANTITIES
- * NOTE SHEET 40A ADDED
- * NOTE SHEET 40A ADDED
- * NOTE SHEET 40A ADDED
- * NOTE SHEET 40A ADDED
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- * NOTE SHEET 40A ADDED

CITY OF BALTIMORE
DEPARTMENT OF PUBLIC WORKS
AND
STATE ROADS COMMISSION OF MARYLAND
INTERSTATE DIVISION FOR BALTIMORE CITY

FEDERAL AID PROJECT NO. I-95-4(40)38
STATE ROADS COMMISSION PROJECT NO. BC 246-37-815
CITY OF BALTIMORE BUREAU OF ENGINEERING,
HIGHWAY ENGINEERING DIVISION CONTRACT NO. 1969

INTERSTATE ROUTE 95

I-95 FROM NORTH OF THE B&O RAILROAD TO
NORTH OF PULASKI HIGHWAY



LOCATION PLAN
SCALE 1"=1000'
DESIGN SPEED - 60 MPH
LENGTH OF PROJECT - 0.3336 MI

FIELD BOOKS	
BOOK NO.	DESCRIPTION
1-95-001	LEVELS
1-95-002	TRAVERSE
1-95-004	TOPO & UTILITIES
1-95-005	STREAM SPURS
1-95-009	SPURS AND CROSS SECTIONS
1-95-010	SPURS AND CROSS SECTIONS
1-95-011	ALIGNMENT
1-95-012	ALIGNMENT
1-95-015	CROSS SECTIONS

TRAFFIC DATA		
INTERSTATE I-95	N PULASKI HWY	S PULASKI HWY
1970 ADT	26,800	42,400
1990 ADT	52,125	84,500
1990 D/V	4,250	6,780
1990 T	60 %	60 %
1990 % TRUCKS ADT	13	13
1990 % TRUCKS D/V	7	7
PULASKI HWY	EAST I-95	WEST I-95
1970 ADT	25,600	7,500
1990 ADT	56,375	25,000
1990 D/V	4,510	2,000
1990 T	60 %	60 %
1990 % TRUCKS ADT	7	20
1990 % TRUCKS D/V	7	7
MAX CURVE - 66 + 13° 18' 41"		
MAX GRADE - 5.56 %		

RIGHT OF WAY LINES SHOWN ON THESE PLANS DO NOT INCLUDE EASEMENT. THEY ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THESE LINES DO NOT REPRESENT THE OFFICIAL PROPERTY ACQUISITION LINES. FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION, SEE THE APPROPRIATE RIGHT OF WAY PLAN OR PLATS.

<p>CHECKED BY: BUREAU OF ENGINEERING WATER DIVISION WASTE WATER DIVISION HIGHWAY ENGINEERING DIVISION SURVEY AND RECORDS DIVISION</p> <p>BUREAU OF UTILITY OPERATIONS LIGHTING SECTION - HIGHWAY MAINTENANCE DIV. CONDUIT SECTION - HIGHWAY MAINTENANCE DIV.</p> <p>DEPARTMENT OF TRANSIT AND TRAFFIC</p>	<p>INITIALS</p> <p>DATE</p>	<p>CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS</p> <p>APPROVAL RECOMMENDED</p> <p>HEAD BUREAU OF ENGINEERING</p> <p>APPROVED</p> <p>DIRECTOR OF PUBLIC WORKS</p>	<p>PREPARED BY: KNOXEL BENDER, STONE & ASSOCIATES, INC. AND MATY, CHELUS & ASSOCIATES, INC. CIVIL ENGINEERS BALTIMORE, MARYLAND</p> <p style="text-align: center;"><i>John L. Calton</i></p> <p>NOVEMBER 19 1969</p>	<p>STATE ROADS COMMISSION OF MARYLAND</p> <p>REVIEWED AND APPROVAL RECOMMENDED</p> <p>DEPUTY BUREAU OF ENGINEERING</p> <p>APPROVAL RECOMMENDED</p> <p>CHIEF INTERSTATE DIVISION FOR BALTIMORE CITY</p>	<p>APPROVAL RECOMMENDED</p> <p>DEPUTY CHIEF ENGINEER - DEVELOPMENT</p> <p>APPROVED</p> <p>CHIEF ENGINEER</p>	<p>U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION</p> <p>APPROVED</p> <p>ENGINEER</p> <p>DATE</p>
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FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD	I-95-4(40)38	8	82

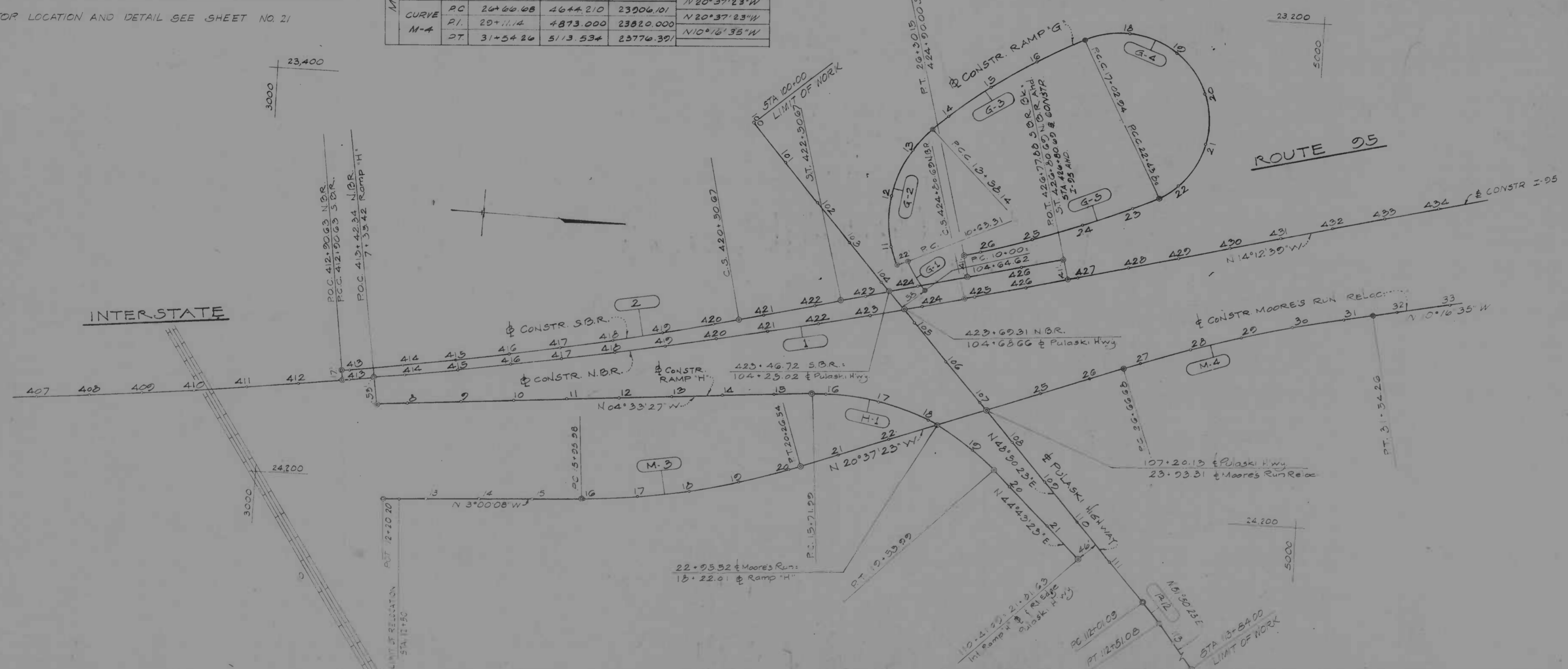
LOCATION	POINT	STATION	COORDINATES		BEARING
			NORTH	EAST	
N.B.R.	SC	375+32.35	-577.449	23902.018	
	PI	400+45.70	1889.460	24352.912	N11°01'51"E
	CS	424+80.65	4331.185	23787.193	N13°42'39"W
CURVE #1	POC	413+42.34	3213.697	24001.685	
	PI	422+90.63	3162.485	24008.776	
	PT	423+47.36	4335.93	3771.303	
S.B.R.	SC	426+80.69	4325.207	23733.661	N14°12'39"W
	PI	426+90.63	3160.189	23991.932	
	CS	416+70.56	3537.037	23940.874	N13°27'39"W
CURVE #2	POC	420+50.87	3906.921	23852.041	N13°27'39"W
	PI	421+17.34	3971.787	23835.322	N14°12'39"W
	PT	422+50.67	4101.013	23803.789	N14°12'39"W
RAMP "H"	POC	426+77.88	4513.147	23698.914	N14°12'39"W
	PI	413+42.34	3213.697	24001.685	
	PT	7+35.42	3221.050	24054.167	N61°39'53"E
CURVE #1	POC	15+71.39	4055.009	23987.699	N04°33'27"W
	PI	17+76.78	4289.147	23971.424	N04°33'27"W
	PT	19+53.99	4404.650	24113.925	N44°43'23"E
PULASKI HIGHWAY CURVE #1	POC	21+81.63	4564.973	24274.310	N44°43'23"E
	PI	116+41.09	4899.608	24244.034	N41°09'37"W
	PT	10+49.01	4184.468	24159.357	N37°23'13"W
PULASKI HIGHWAY CURVE #2	POC	10+84.01	4222.278	24138.708	N53°20'41"E
	PI	11+03.68	4243.099	24166.839	
	PT	112+01.09	4704.909	24364.491	N48°50'23"E
PULASKI HIGHWAY CURVE #3	POC	112+26.09	4721.363	24383.313	N51°50'23"E
	PI	112+31.08	4736.810	24402.970	
	PT				

* FOR LOCATION AND DETAIL SEE SHEET NO. 21

CURVE	POINT	STATION	COORDINATES		BEARING
			NORTH	EAST	
RAMP "G"	POC	104+24.62	4220.190	23810.023	N41°09'36"W
	PI	10+00	4263.856	23771.850	S48°50'23"W
	PT	10+31.85	4242.876	23747.850	S65°22'11"W
CURVE G-1	POC	10+63.91	4229.608	23718.866	S24°35'46"E
	PI	10+63.91	4229.608	23718.866	S24°35'46"E
	PT	10+63.91	4229.608	23718.866	S24°35'46"E
CURVE G-2	POC	10+63.91	4229.608	23718.866	S24°35'46"E
	PI	12+32.35	4139.243	23574.519	S65°24'11"W
	PT	13+58.14	4289.807	23455.827	N44°30'12"W
CURVE G-3	POC	13+58.14	4289.807	23455.827	N44°30'12"W
	PI	15+32.27	4383.997	23333.771	N46°30'12"W
	PT	17+02.94	4542.135	23260.877	N24°44'51"W
CURVE G-4	POC	17+02.94	4542.135	23260.877	N24°44'51"W
	PI	22+43.80	4635.684	23573.494	S24°44'51"E
	PT	22+43.80	4635.684	23573.494	S24°44'51"E
CURVE G-5	POC	22+43.80	4635.684	23573.494	S24°44'51"E
	PI	24+37.45	4509.633	23654.023	S15°21'24"E
	PT	26+30.15	4322.951	23705.291	N75°47'24"E
MOORE'S RUN	POC	424+90.00	4833.016	23745.037	
	PI	12+20.20	3250.044	24239.046	N3°00'08"W
	PT	15+95.08	3625.305	24210.365	N3°00'08"W
CURVE M-3	POC	18+12.97	3842.000	24208.000	N20°37'23"W
	PI	20+26.54	4045.087	24311.571	N20°37'23"W
	PT	24+66.68	4644.210	23906.101	N20°37'23"W
CURVE M-4	POC	29+11.14	4873.000	23820.000	N10°16'35"W
	PI	31+34.26	5113.534	23776.391	
	PT				

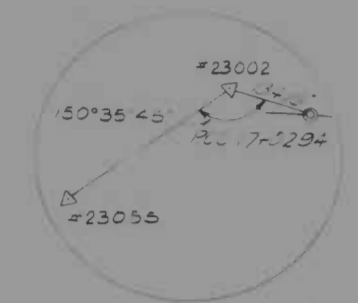
LOCATION	CURVE	Δ	D	L	T	R	θ	L _s	L _t	ST	SE
M-1	1	24°44'30"	00°30'00"	404.835	25.335	114.5916	00°30'00"	200.00	133.33	66.67	0016'
	2	08°42'01"	00°45'00"	760.04	380.33	763.814	00°45'00"	200.00	133.33	66.67	0021'

LOCATION	CURVE	Δ	D	L	T	R	θ	L _s	L _t	ST	SE
RAMP "G"	G-1	16°33'47"	26°09'43"	215.00'	31.28	62.31					
	G-2	7°05'38"	23°46'27"	241.00'	169.04	294.83				0.0864	
	G-3	19°45'20"	05°43'46"	1000.00'	174.13	344.80				0.0304	
	G-4	18°10'30"	33°18'41"	172.00'	117.60	540.86				0.1004	
	G-5	00°13'24"	02°28'14"	2400.00'	193.59	386.35				0.0284	
RAMP "H"	H-1	49°16'50"	12°50'00"	446.46'	204.79	384.00				0.1004	
MOORE'S RUN	M-3	17°37'15"	04°05'53"	1400.00'	216.99	430.86					
	M-4	10°20'48"	02°07'19"	2700.00'	214.26	487.58					
PULASKI HIGHWAY	S-1	80°52'54"	168°4'20"	34.466	35.00	54.07					
	P-12	08°00'00"	06°00'00"	954.71	23.00	49.99					



REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS	STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY
	EMERALD, BENDER, THOMAS & ASSOC., INC. AND MATT, GIBBS & BIRN, INC. CONSULTING ENGINEERS 341 N. CALVERT STREET, BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF THE B&O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY: JLC CHECKED BY: JWC F.P. NO. I-95-4(40)38 S.R.C. NO. EG 245-37-B15 BALTO. CITY NO. 1989
		SCALE: 1"=100'	DATE: _____

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
2	MD.	I-95-4(40)38	9	82



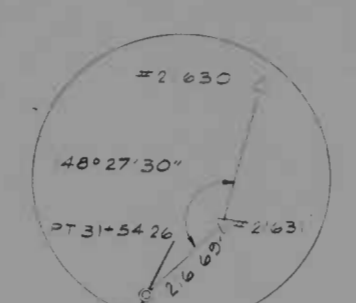
RAMP "G"
P.C. STA 17+02.94



RAMP "H"
P.T. STA 19+55.99



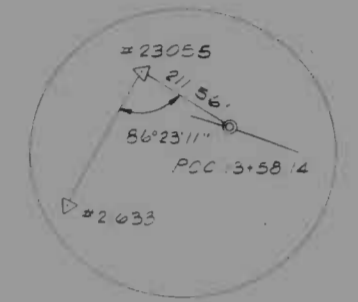
62nd & 63rd ST. CONN.
P.O.T. STA 13+29.99



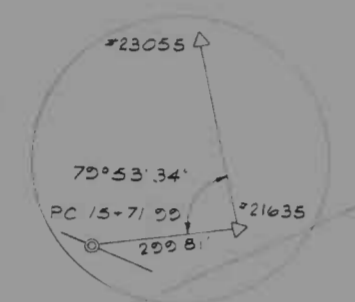
RELOC. MOORE'S RUN
P.T. STA 31+54.26



PULASKI HWY
P.O.T. STA 113+84.00



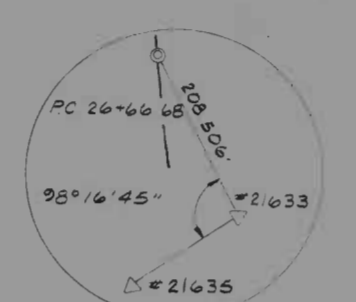
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RAMP "H"
P.C. STA 15+71.99



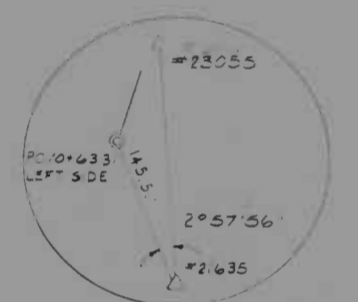
62nd & 63rd ST. CONN.
P.T. STA 11+03.68



RELOC. MOORE'S RUN
P.C. STA 26+66.68



PULASKI HWY
P.T. STA 112+51.08



RAMP "G"
P.C. STA 10+63.31



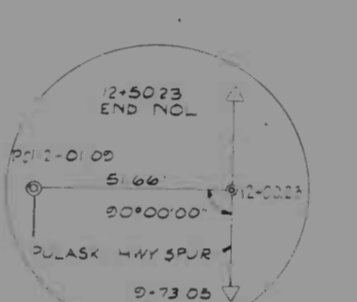
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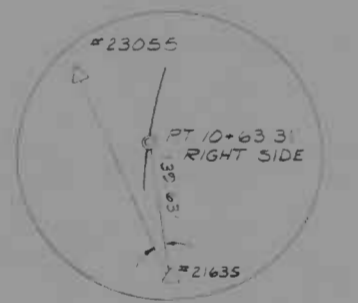
62nd & 63rd ST. CONN.
P.C. STA 10+49.01



RELOC. MOORE'S RUN
P.T. STA 20+26.54



PULASKI HWY
P.C. STA 112+01.09



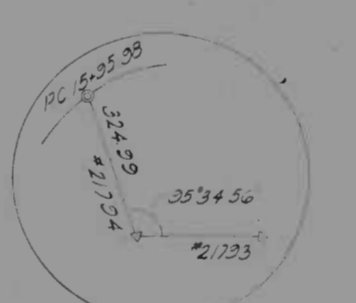
RAMP "G"
P.T. STA 10+63.31



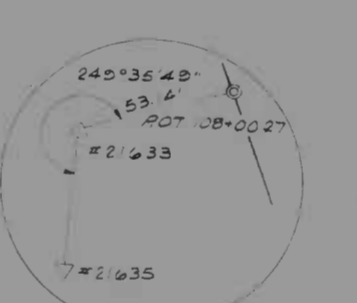
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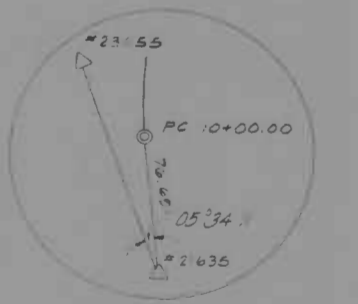
62nd & 63rd ST. CONN.
P.O.T. STA 10+00



RELOC. MOORE'S RUN
P.C. STA 15+95.98



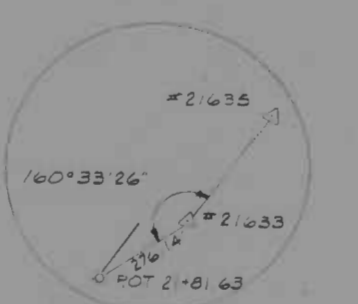
PULASKI HWY
P.O.T. STA 108+00.27



RAMP "G"
P.C. STA 10+00.00



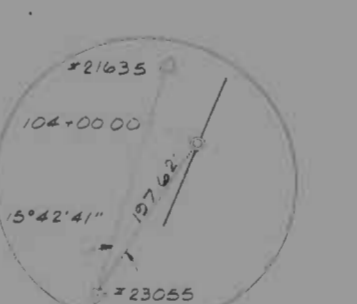
RAMP "G"
P.C. STA 22+43.80



RAMP "H"
P.O.T. STA 21+81.63



RELOC. MOORE'S RUN
P.O.T. STA 12+20.20



PULASKI HWY
STA 104+00.00

REVISIONS	CONSULTANT	CITY OF BALTIMORE DEPARTMENT OF PUBLIC WORKS & STATE ROADS COMMISSION OF MARYLAND INTERSTATE DIVISION FOR BALTIMORE CITY	
	ENGINEER, DESIGNER, STONE & ASSOC., INC. AND MATZ, CHILDS & ASSOC., INC. CONSULTING ENGINEERS 341 N. CALVERT STREET BALTIMORE, MARYLAND 21202	INTERSTATE ROUTE 95 FROM NORTH OF THE B & O RAILROAD TO NORTH OF PULASKI HIGHWAY	DRAWN BY J.R. TRACED BY P.D. F.A.P. NO. I-95-4(40)38 S.R.C. NO. BS 246-37-815 BALTO. CITY NO. 1969
			DES. BY J.L.C. CHK. BY R.W.C. SHEET NO. 9 of 82
		SCALE: None	DATE