

INDIVIDUAL PROPERTY/DISTRICT  
MARYLAND HISTORICAL TRUST  
INTERNAL NR-ELIGIBILITY REVIEW FORM

Property/District Name: RR Bridge over Wills Creek Survey Number: AL-V-B-152

Project: Proposed demolition Agency: \_\_\_\_\_

Site visit by MHT Staff:  no  yes Name \_\_\_\_\_ Date \_\_\_\_\_

Eligibility recommended  Eligibility **not** recommended

Criteria:  A  B  C  D Considerations:  A  B  C  D  E  F  G  None

Justification for decision: (Use continuation sheet if necessary and attach map)

Located in Locust Grove, the railroad bridge over Wills Creek is a brick structure constructed in the 1840s. It is one of the earliest remaining railroad bridges in Maryland. The structure is a four span brick arch bridge supported by granite piers. Marble and granite capstones line the deck. Built by the Cumberland Pennsylvania Railroad, the bridge was a component of the network of rail lines that brought Western Maryland coal to the eastern markets. The bridge is significant for its association with the development of rail transportation in the county as well as the state (Criterion A). Abandoned in the 1980s, the bridge exhibits signs of serious deterioration but its integrity of location, design, materials, feeling and association remain intact. The railroad bridge over Wills Creek at Locust Grove illustrates distinctive characteristics of type, period and method of construction.

N.B. To ease flooding, CSX is pursuing the demolition of the bridge. It appears that MHT does not have a regulatory handle on the demolition. We recommended that the bridge be recorded prior to demolition.

Documentation on the property/district is presented in: Maryland Inventory

Prepared by: G.J. Melodini, CSX Transportation/Inventory Form=David Dorsey

Lauren Bowlin October 24, 1997  
Reviewer, Office of Preservation Services Date

NR program concurrence:  yes  no  not applicable

Patricia E. Keady 10/27/97  
Reviewer, NR program Date

*[Handwritten signature]*



AL-V-B-152  
Eckhart Branch Railroad Bridge

On the 7th of April in 1845 the Maryland Mining Company broke ground for the construction of a railway from Eckhart Mines to the Cumberland and Pennsylvania Railroad's Mount Savage line on the northern side of Wills Creek at the Narrows. The most interesting feature of the Eckhart Branch railroad is the masonry bridge across Wills Creek where it joined the Cumberland and Pennsylvania line. The bridge consists of four elliptical brick arches with gauged brick borders. Walls of the bridge are laid in english bond with pilasters at the piers. It rests on three stone piers and stone abutments. Although it is the oldest standing railroad bridge in Allegany County and in poor condition, the brick bridge is still in use serving local industries.

## MARYLAND HISTORICAL TRUST

## INVENTORY FORM FOR STATE HISTORIC SITES SURVEY

**1 NAME**

HISTORIC Eckhart Branch Railroad Bridge

AND/OR COMMON

**2 LOCATION**

STREET &amp; NUMBER Wills Creek ( at the Narrows) Sixth

CITY, TOWN LaVale CONGRESSIONAL DISTRICT

VICINITY OF

STATE Maryland COUNTY Allegany

**3 CLASSIFICATION**

CATEGORY	OWNERSHIP	X STATUS	PRESENT USE	
<input type="checkbox"/> DISTRICT	<input type="checkbox"/> PUBLIC	<input type="checkbox"/> OCCUPIED	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> MUSEUM
<input type="checkbox"/> BUILDING(S)	<input checked="" type="checkbox"/> PRIVATE	<input type="checkbox"/> UNOCCUPIED	<input type="checkbox"/> COMMERCIAL	<input type="checkbox"/> PARK
<input checked="" type="checkbox"/> STRUCTURE	<input type="checkbox"/> BOTH	<input type="checkbox"/> WORK IN PROGRESS	<input type="checkbox"/> EDUCATIONAL	<input type="checkbox"/> PRIVATE RESIDENCE
<input type="checkbox"/> SITE	<b>PUBLIC ACQUISITION</b>	<b>ACCESSIBLE</b>	<input type="checkbox"/> ENTERTAINMENT	<input type="checkbox"/> RELIGIOUS
<input type="checkbox"/> OBJECT	<input type="checkbox"/> IN PROCESS	<input type="checkbox"/> YES: RESTRICTED	<input type="checkbox"/> GOVERNMENT	<input type="checkbox"/> SCIENTIFIC
	<input type="checkbox"/> BEING CONSIDERED	<input checked="" type="checkbox"/> YES: UNRESTRICTED	<input type="checkbox"/> INDUSTRIAL	<input checked="" type="checkbox"/> TRANSPORTATION
		<input type="checkbox"/> NO	<input type="checkbox"/> MILITARY	<input type="checkbox"/> OTHER

**4 OWNER OF PROPERTY**

NAME The Chessie System Telephone #: 724-3000

STREET &amp; NUMBER 722 Virginia Avenue

CITY, TOWN Cumberland STATE, zip code Maryland 21502

VICINITY OF

**5 LOCATION OF LEGAL DESCRIPTION**

COURTHOUSE, REGISTER OF DEEDS, ETC. Allegany County Courthouse

Liber #:

Folio #:

STREET &amp; NUMBER 30 Washington Street

CITY, TOWN Cumberland STATE Maryland 21502

**6 REPRESENTATION IN EXISTING SURVEYS**

TITLE

DATE

FEDERAL STATE COUNTY LOCAL

DEPOSITORY FOR SURVEY RECORDS

CITY, TOWN

STATE

A-1-V-3-152

**7 DESCRIPTION**

<b>CONDITION</b>		<b>CHECK ONE</b>	<b>CHECK ONE</b>
<input type="checkbox"/> EXCELLENT	<input type="checkbox"/> DETERIORATED	<input checked="" type="checkbox"/> UNALTERED	<input checked="" type="checkbox"/> ORIGINAL SITE
<input type="checkbox"/> GOOD	<input type="checkbox"/> RUINS	<input type="checkbox"/> ALTERED	<input type="checkbox"/> MOVED    DATE _____
<input type="checkbox"/> FAIR	<input type="checkbox"/> UNEXPOSED		
<input checked="" type="checkbox"/> POOR			

DESCRIBE THE PRESENT AND ORIGINAL (IF KNOWN) PHYSICAL APPEARANCE

The bridge consists of four elliptical brick arches with gauged brick borders. Wall are laid in english bond and wall features include brick pilasters located above the piers. A stone railing is raised approximately one-half foot above the deck of the bridge. It rests on rectangular stone piers with rounded ends and cement fenders which absorb the force of water, striking debris, and ice. The stone abutment on the north is in relatively good condition. The southern abutment and a small part of the northernmost arch have been partially obscured by construction of Route 36. The bridge and a small part of the railroad are still in use serving local industries.

CONTINUE ON SEPARATE SHEET IF NECESSARY

**8 SIGNIFICANCE**

PERIOD	AREAS OF SIGNIFICANCE -- CHECK AND JUSTIFY BELOW			
<input type="checkbox"/> PREHISTORIC	<input type="checkbox"/> ARCHEOLOGY-PREHISTORIC	<input type="checkbox"/> COMMUNITY PLANNING	<input type="checkbox"/> LANDSCAPE ARCHITECTURE	<input type="checkbox"/> RELIGION
<input type="checkbox"/> 1400-1499	<input type="checkbox"/> ARCHEOLOGY-HISTORIC	<input type="checkbox"/> CONSERVATION	<input type="checkbox"/> LAW	<input type="checkbox"/> SCIENCE
<input type="checkbox"/> 1500-1599	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> ECONOMICS	<input type="checkbox"/> LITERATURE	<input type="checkbox"/> SCULPTURE
<input type="checkbox"/> 1600-1699	<input checked="" type="checkbox"/> ARCHITECTURE	<input type="checkbox"/> EDUCATION	<input type="checkbox"/> MILITARY	<input type="checkbox"/> SOCIAL/HUMANITARIAN
<input type="checkbox"/> 1700-1799	<input type="checkbox"/> ART	<input checked="" type="checkbox"/> ENGINEERING	<input type="checkbox"/> MUSIC	<input checked="" type="checkbox"/> THEATER
<input checked="" type="checkbox"/> 1800-1899	<input type="checkbox"/> COMMERCE	<input type="checkbox"/> EXPLDRATION/SETTLEMENT	<input type="checkbox"/> PHILOSOPHY	<input type="checkbox"/> TRANSPORTATION
<input type="checkbox"/> 1900-	<input type="checkbox"/> COMMUNICATIONS	<input type="checkbox"/> INDUSTRY	<input type="checkbox"/> POLITICS/GOVERNMENT	<input type="checkbox"/> OTHER (SPECIFY)
		<input type="checkbox"/> INVENTION		

SPECIFIC DATES      1845                                      BUILDER/ARCHITECT      The Maryland Mining Company

STATEMENT OF SIGNIFICANCE

On the 7th of April in 1845 the Maryland Mining Company broke ground for the construction of a railway from Eckhart Mines to the Cumberland and Pennsylvania Railroad's Mount Savage line on the northern side of Wills Creek at the Narrows. (1) "It required a full year of construction for the railroad to literally tear its way throught the 'steep and rocky and rugged sides of the mountains'."(2) Provisions of the charters of most Maryland mining companies enabled them to build such service railroads from the mining region to larger transportation centers. (3) Later in the 1840's the Maryland Mining Company built another branch of the railroad through the Narrows into the town of Cumberland. (4) Scharf noted that in 1950 the company was making regular shipments of coal from Eckhart Mines to Cumberland, probably to the C & O Canal which was opened to traffic in that year. (5)

The most interesting feature of the Eckhart Branch railroad is the masonry bridge across Wills Creek where it joined the Cumberland and Pennsylvania line. The bridge consists of four elliptical brick arches with gauged brick borders. Walls of the bridge are laid in english bond with pilasters at the piers. It rests on three stone piers and stone abutments.

In 1852 both branches of the Maryland Mining Company's railroad were sold to the Cumberland Coal and Iron Company when the company went out of business. (6) It became part of the Cumberland and Pennsylvania Railroad Company's system during the 1850's or 1860's when that company "absorbed" most of the local service railroads. (7) Later in the nineteenth century the bridge became the focal point of competition between the C and P and the George's Creek and Cumberland Railroad. The latter company eventually won the right to use the C and P's bridge in court. (8)

The C an P Railroad was itself absorbed by the Western Maryland Railway in 1944, 99 years after the construction of the bridge. (9) Although it is the oldest standing railroad bridge in Allegany County and in poor condition, the brick bridge is still in use serving local industries.

(cont.)

CONTINUE ON SEPARATE SHEET IF NECESSARY

**9 MAJOR BIBLIOGRAPHICAL REFERENCES**

Stegmaier, Harry Jr. et al, Allegany County, A History Parsons W Va. McClain Pr ng  
Co.

Scharf, John T. History of Western Maryland, Philadelphia: L. H. Everts Co. 1882

Lowdermilk, Will H. History of Cumberland Baltimore: Regional Publishing Co. 1971

Harvey, Katherine A. Best Dressed Miners: Life and Labor in the Maryland Coal Region

1835, 1910 Ithaca: Cornell University Press, 1969

CONTINUE ON SEPARATE SHEET IF NECESSARY

**10 GEOGRAPHICAL DATA**

ACREAGE OF NOMINATED PROPERTY \_\_\_\_\_

VERBAL BOUNDARY DESCRIPTION

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE COUNTY

STATE COUNTY

**11 FORM PREPARED BY**

NAME / TITLE

David A. Dorsey, Historic Sites Coordinator

ORGANIZATION

Allegany County Historic Sites

DATE

March 3, 1980

STREET & NUMBER

3 Pershing Street

TELEPHONE

777-5944

CITY OR TOWN

Cumberland

STATE

Maryland 21502

The Maryland Historic Sites Inventory was officially created by an Act of the Maryland Legislature, to be found in the Annotated Code of Maryland, Article 41, Section 181 KA, 1974 Supplement.

The Survey and Inventory are being prepared for information and record purposes only and do not constitute any infringement of individual property rights.

RETURN TO: Maryland Historical Trust  
The Shaw House, 21 State Circle  
Annapolis, Maryland 21401  
(301) 267-1438

Footnotes

- (1) John T. Scharf, History of Western Maryland (Philadelphia: L.H. Everts Co. ). p. 1446
- (2) Harry I. Stegmaier, Jr. et al., Allegheny County, A History (Parsons, W. Va.: McClain Printing Co.) p. 141-142
- (3) Katherine A. Harvey, Best Dressed Miners: Life and Labor in the Maryland Coal Region, 1835-1910 (Ithaca: Cornell University Press) p. 12
- (4) Will H. Lowdermilk, History of Cumberland (Baltimore: Regional Publishing Co.) p.361
- (5) Scharf. p. 1435 and Lowdermilk, P. 368
- (6) Allegheny County Land Records, Liber 8, Folio 717
- (7) Harvey, Katherine p. 12
- (8) Stegmaier, p. 205
- (9) Harvey, Katherine pp. 369-370

INVENTORY NO: AL-V-B-152

DATE: 2/37

REMODELING: Drastic, moderate, minor

68404 2/27/8

OF PROPERTY: ECKHART BRANCH R P BRIDGE

LOCATION OF PROPERTY: WILLS CREEK AT NARROWS

DIRECTION DWELLING FACES: \_\_\_\_\_

NAME OF OWNER: CHESSIE SYSTEM

ADDRESS: English bond w/ some lateral soldier course  
alternating leaders - stretchers

STORIES: 1( ) 2( ) 3( ) 4( ) 5( ) 6( ) spans BAYS: 1( ) 2( ) 3( ) 4( ) 5( ) 6( ) 7( ) ( )

WINGS, ADDITIONS: pilasters

DWELLING SHAPE: (square, cross, rectangular)

WALL CONSTRUCTION:

- ( ) FRAME: Bevel, clapboard, weatherboard, wood shingles, board & batten (type?)
- () BRICK: Bond type - common, English, Flemish. Sketch variants.
- ( ) STONE: Bond type - rubble, ashlar (random or regular), quoins-plain, rusticated
- ( ) LOG
- ( ) OTHER: Coverings

WALL FEATURES: Belt Course, Pilasters, Others

FOUNDATIONS: High, Low, Brick, Stone rock faced ashlar stone bases w/ rounded ends

WATER TABLE: None, Plain, Beveled, Moulded Brick

WINDOWS, TRIM, SHUTTERS: 1/1( ) 2/2( ) 6/6( ) 9/6( ) 9/9( ) other( )  
pegged( ) nailed( ) wide( ) narrow( ) mitred( ) pediment( )

ENTRANCE, DOORS: LOCATION: \_\_\_\_\_  
HARDWARE: original( ) replaced( )  
FAN LIGHT, TRANSOM, SIDE LIGHTS, PLAIN  
Paneled, Vertical, Horizontal Boards

CORNICE, BARGE, EAVES: (crown, fascia, soffit, bed)  
original( ) replaced( ) rafter ends, modillions, dentils, frieze, architrave

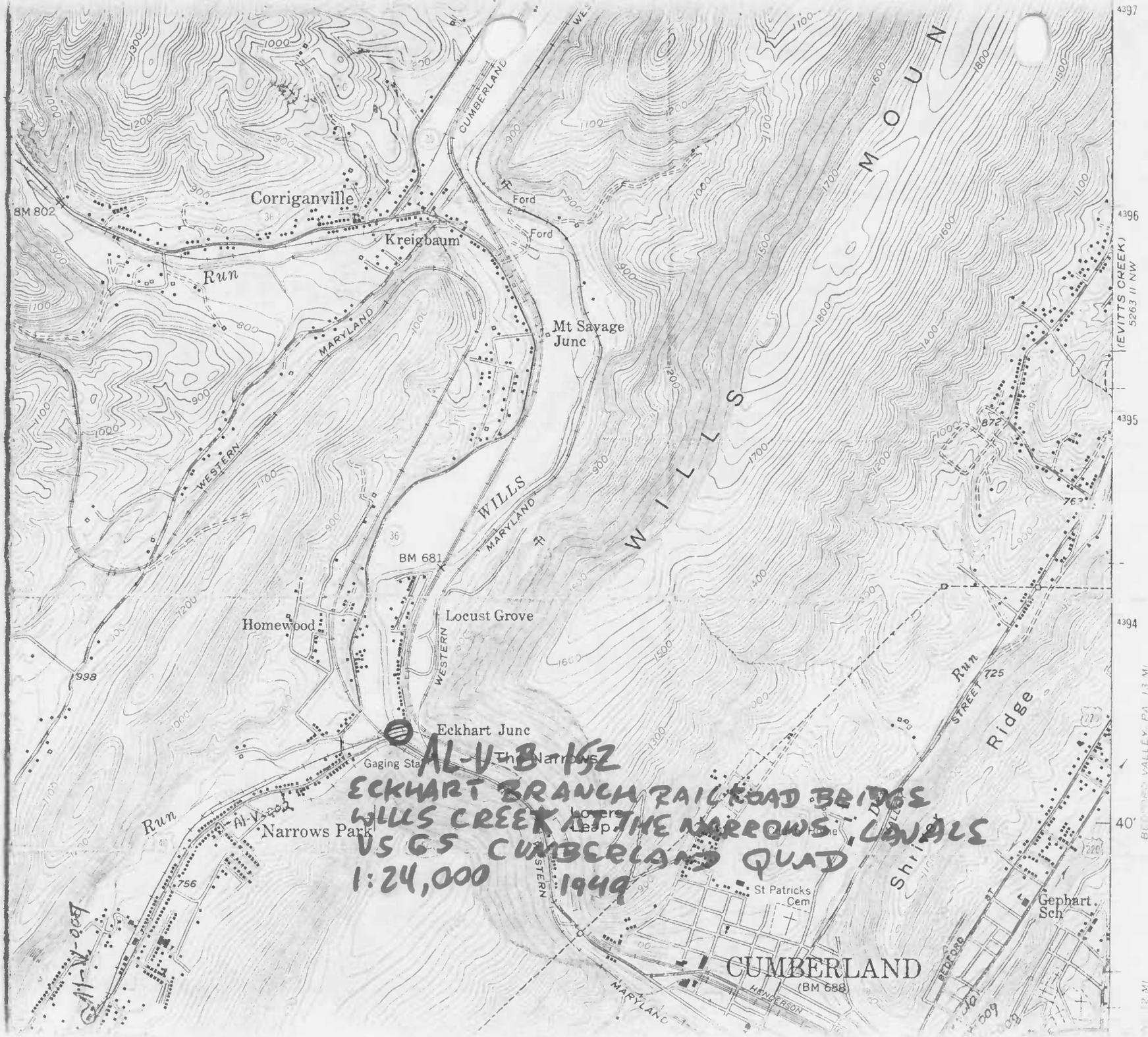
ROOF: Gable front, Gable flank, Hip, Shed, Mansard, Gambrel, Flat, Dormers # \_\_\_\_\_  
MATERIAL: wood shingles, slate, tin, asphalt; original( ) replaced( )

PORCHES: SHAPE OF ROOF - shed( ) hip( ) gable( )

CHIMNEYS: Number: \_\_\_\_\_ Brick( ) Stone( ) Corbeled( ) original( ) replaced( )  
Location: Flush end( ) Inside end( ) Outside end( ) Central( ) Interior( )

DOORS: Door & Windows: Keystone, Flat, Segmental, Semicircular ganged  
one on other

COMMENTS: \_\_\_\_\_  
USE: \_\_\_\_\_  
SIGNIFICANCE: \_\_\_\_\_  
DATE CONSTRUCTED: \_\_\_\_\_  
POOR



BM 802

Corriganville

Kreigbaum

Mt Savage Junc

Homewood

Locust Grove

Eckhart Junc

Narrows Park

CUMBERLAND

(BM 688)

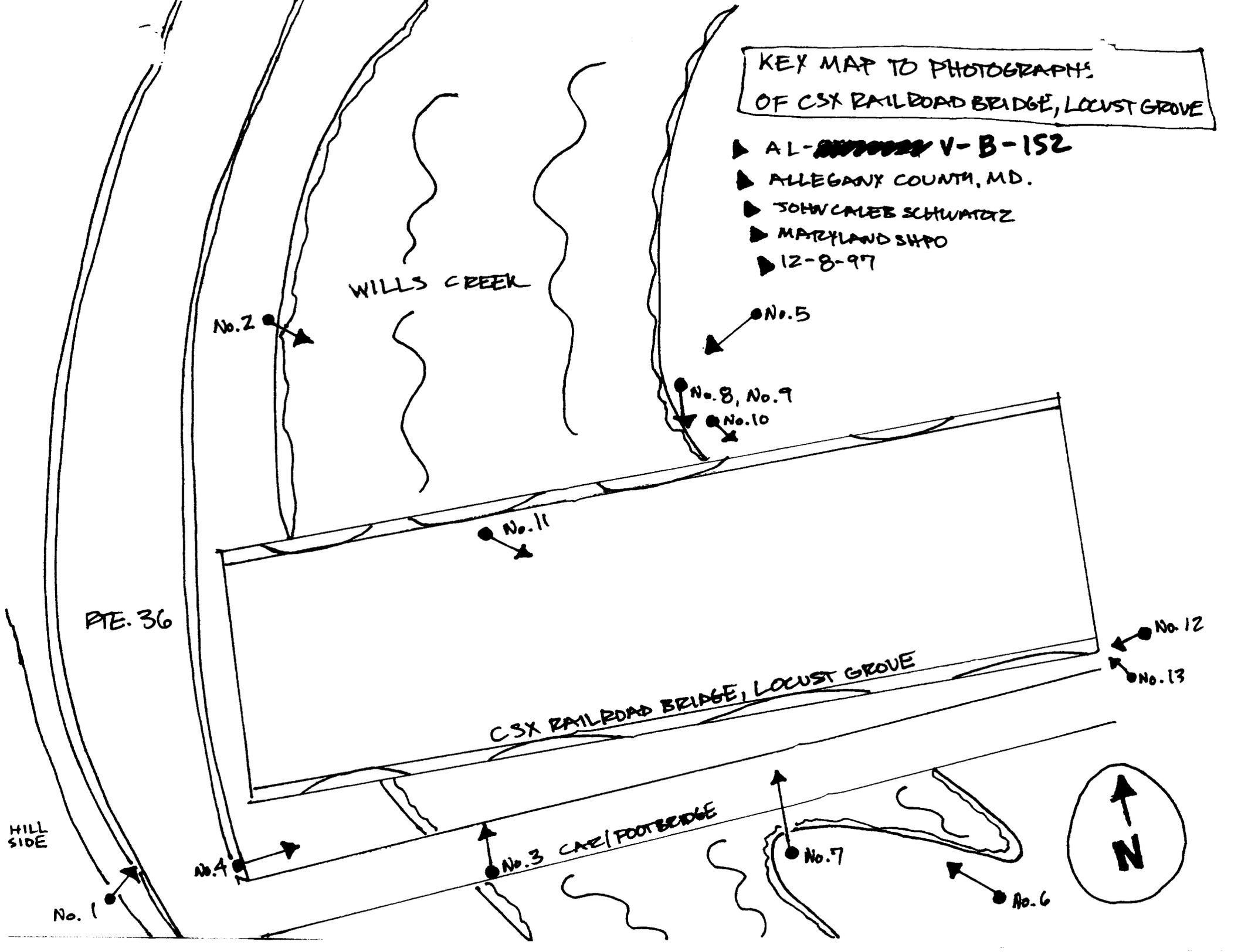
**AL-V-B-152**  
**ECKHART BRANCH RAILROAD BRIDGE**  
**WILLS CREEK AT THE NARROWS, CONALS**  
**VS GS CUMBERLAND QUAD**  
**1:24,000**  
**1949**

*Bob-1-11-49*

4397  
4396  
4395  
4394  
40'  
BEDFORD VALLEY, PA 13 MI  
BEDSPRING, PA 24 MI  
12 MI

KEY MAP TO PHOTOGRAPHS  
OF CSX RAILROAD BRIDGE, LOCUST GROVE

- ▶ AL-~~XXXXXXXX~~ V-B-152
- ▶ ALLEGANY COUNTY, MD.
- ▶ SOHWCALEB SCHWARTZ
- ▶ MARYLAND SHPO
- ▶ 12-8-97





AL-U-B-152  
ECKHART BRANCH RR BRIDGE  
WILKS CREEK AT THE NARROWS  
CAVAGE

DAVID A. DORSEY

2/16/80

Northwest



AL. U-B-152

ECKHART BRANCH RR BRIDGE  
WILLS CREEK AT THE NARROWS LAUCE

DAVID A. DORSEY

2/16/80

EAST



AL. U. B. 152

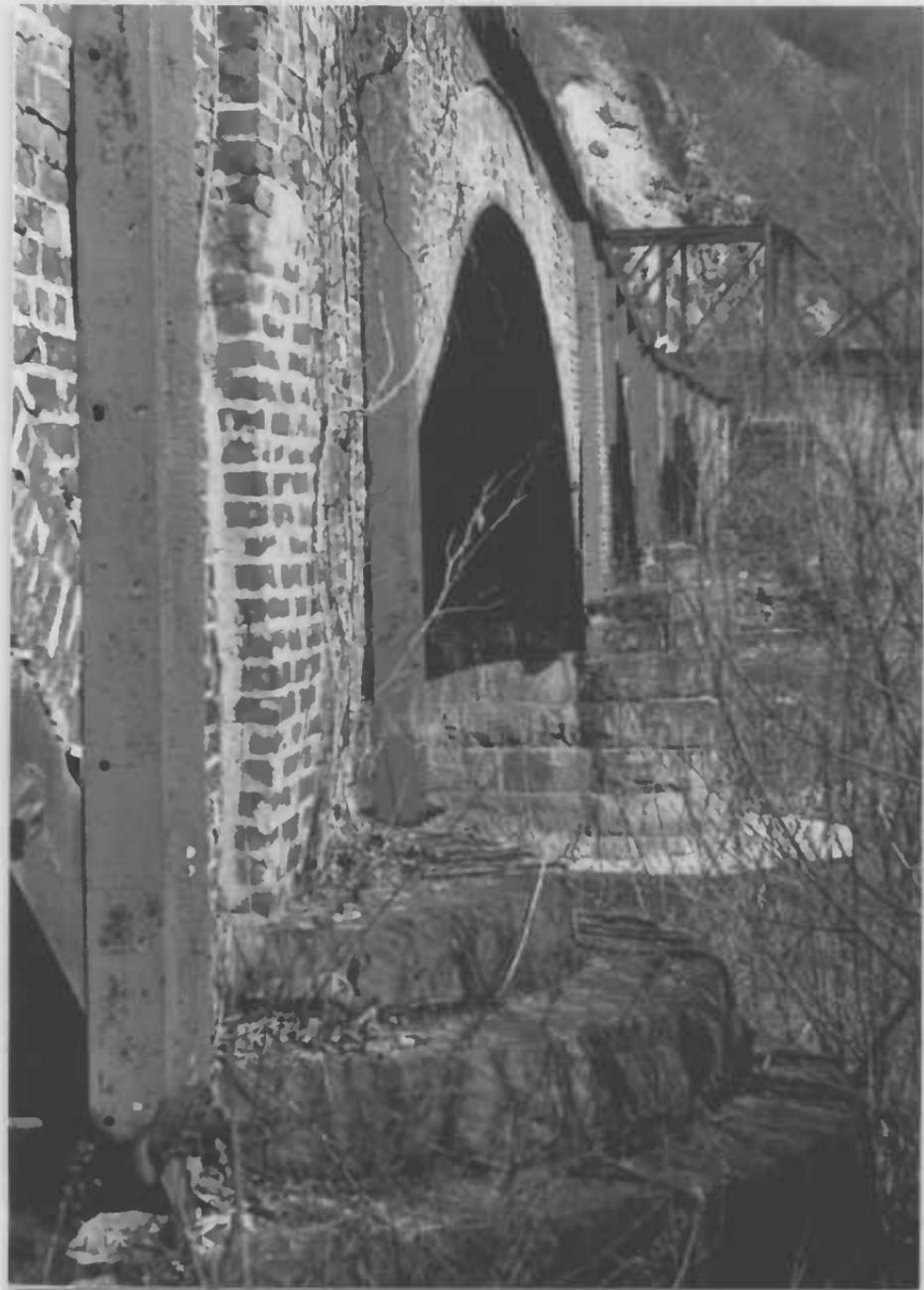
ECKHART BRANCH RR BRIDGE

WILLS CREEK AT THE NARROWS CAVAZE

DAVID A. DOTSEY

11/20/79

Warth



AL-U-B-152

ECKHART BRANCH RR BRIDGE  
WILLS CREEK AT THE NARROWS LAVALS

DAVID A. DORSEY

2/16/80

Northeast



~~AL-057A~~ AL-V-B-152

CSX RAILROAD BRIDGE (AKA) ECKHART BRIDGE

ALLAGANY COUNTY, MD.

JOHN CALEB SCHWARTZ,

12-8-97

MARYLAND SHPO

VIEW NORTH FROM SW HILLSIDE OF WILLS CREEK, GENERAL OVERVIEW  
OF BRIDGE

1 OF 13



~~AL 857A21~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD BRIDGE)

MILLESPIENY COUNTY, MD.

SONICALEB SCHWARZ

12-8-97

MARYLAND SHPO

VIEW SW FROM BANK OF WILLS CREEK OF NE ELEVATION OF BRIDGE

2 OF 13



~~ALBERTA~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOUST GROVE (AKA ECKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, M.D.

JOHN CALEB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW NNW FROM NEW CRZ/FOOTBRIDGE OF SOUTH ELEVATION OF BRIDGE

3 of 13



~~AL-857A2~~ AL-V-13-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

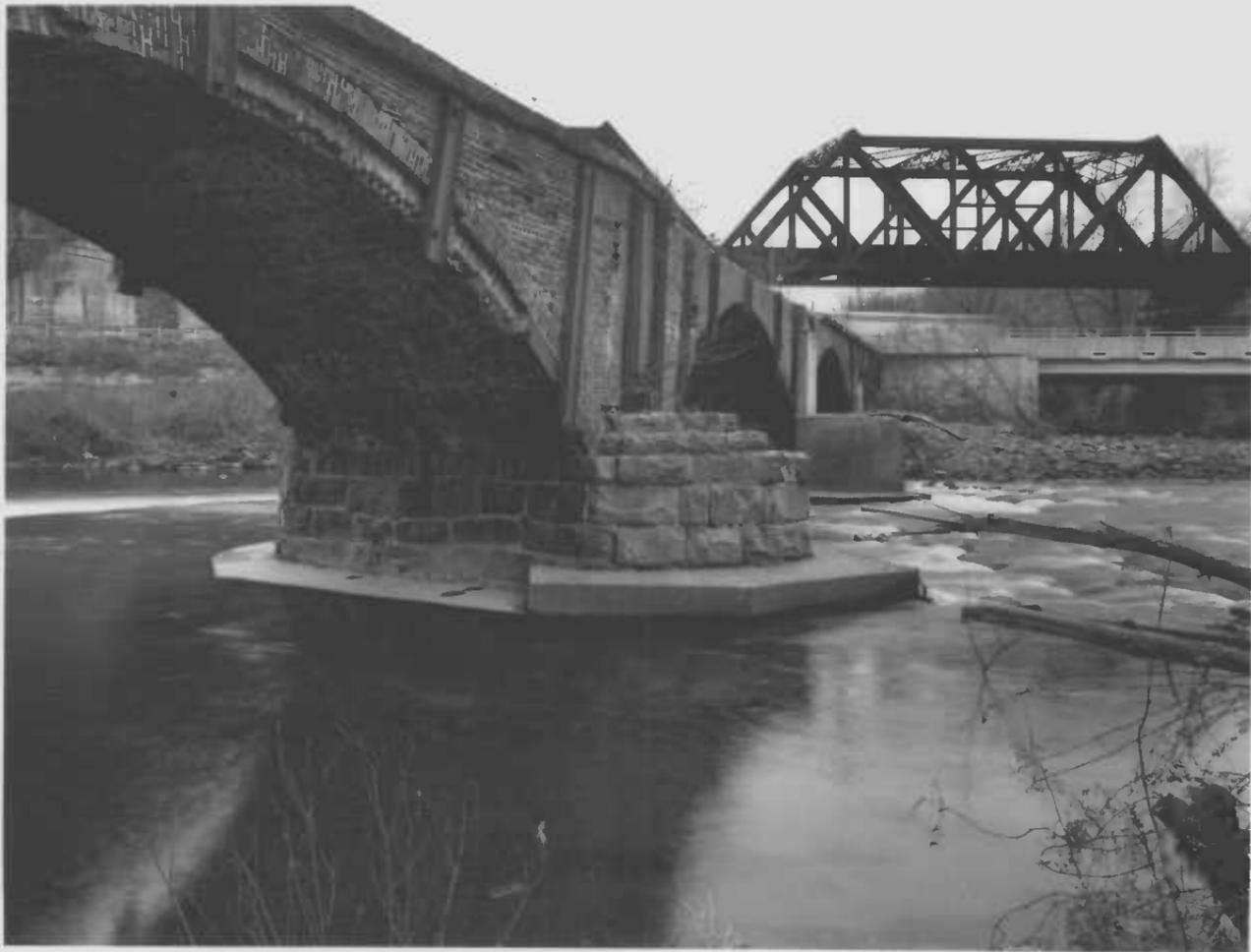
JOHN CALEB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW FROM RTE. 36N, SOUTH ELEVATION OF BRIDGE SHOWING STONE PIERS AND  
BRICK LOSS

4 OF 13



~~AL-857A21~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA EGHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

JOHN CALEB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW FROM NE BANK OF WILLS CREEK, 3/4 NORTHEAST ELEVATION.  
STEEL GIRDERS AND CABLE REINFORCEMENT VISIBLE UPPER LEFT OF PHOTO.

5 OF 13



~~AL-857A21~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)

ALLGANY COUNTY, MD.

JOHN CALIB SCHWARTZ

12-8-97

MARYLAND SHPD

VIEW FROM SE BANK OF WILLS CREEK, 3/4 ELEVATION NE.

6 OF 13



~~AL 857A21~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ELKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

JOHN CALEB SCHWARTZ

12-8-97

MARYLAND SHPS

VIEW FROM SOUTH, DETAIL OF ARCH CONSTRUCTION, SOUTH ELEVATION.

7 OF 13



~~AL 857AZ1~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ELKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

JOHN CALIB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW FROM NE BANK OF WILLS CREEK. DETAIL OF STONE PIER, NORTH  
ELEVATION.

8 OF 13



~~AL-8572~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)

ALLAGANY COUNTY, MD.

JOHN CARL SCHWARTZ

12-8-97

MATCHLAND SHPO

VIEW FROM NE BANK OF WILLS CREEK. DETAIL OF BRICK BOND,  
NORTH ELEVATION.

9 OF 13



~~AL-257A2~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

JOHN CALEB SCHWARTZ  
12-8-97

MARYLAND SHPO

VIEW FROM NE BANK OF WILLS CREEK, DETAIL OF EAST STONE PIER.

10 OF 13



~~AL-285-132~~ AL-V-B-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)  
WILLAGONY COUNTY, MD.

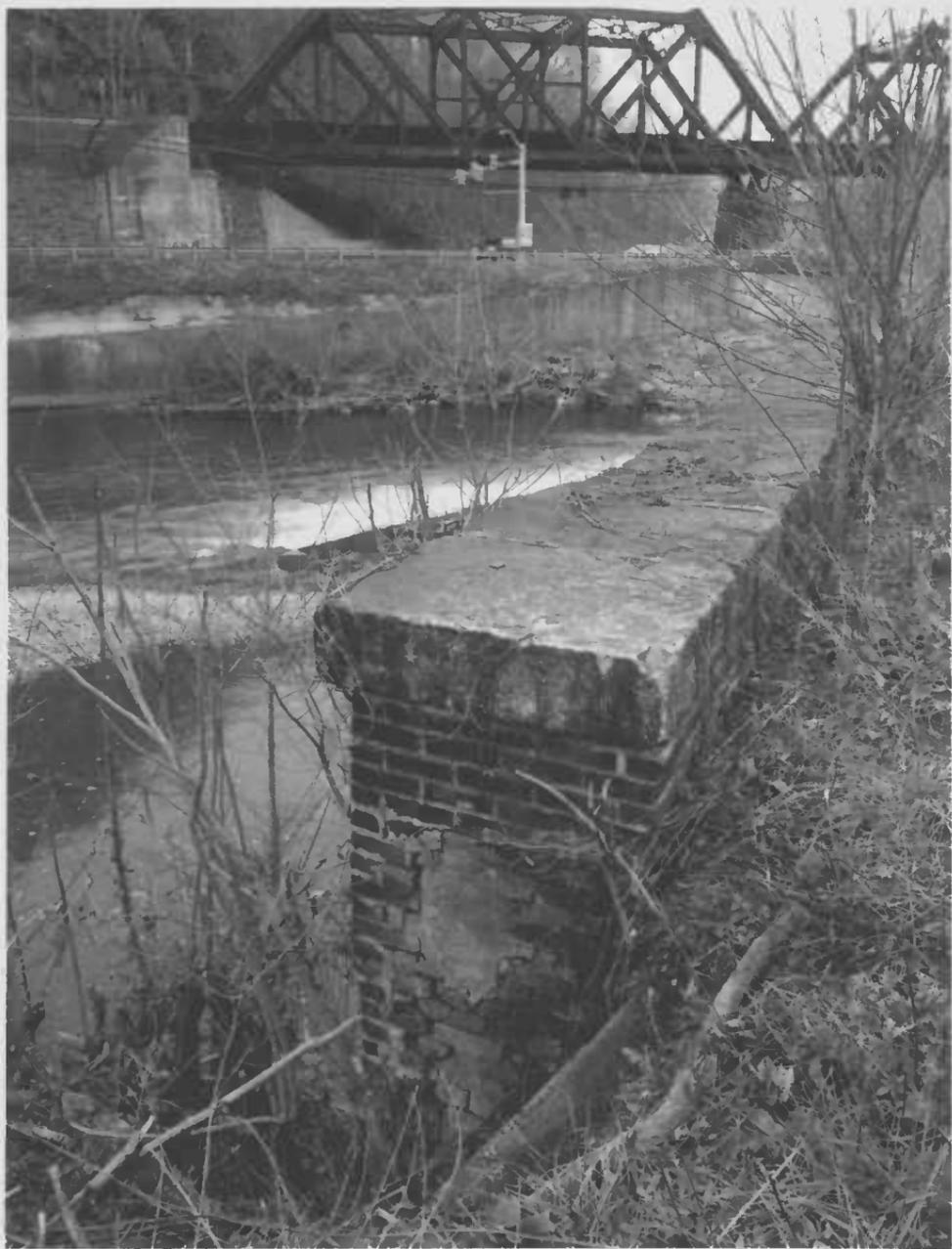
JOHN CALEB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW FROM SURFACE OF BRIDGE, LOOKING SE. DETAIL OF BRIDGE  
WALL-EDGE STONES (DISLOCATED) AND LATER REINFORCING STEEL  
CABLE.

11 OF 13



~~AL-8572~~ AL-V-B-152

CSX RAIL ROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)  
ALLAGANY COUNTY, MD.

SOLW CARLB SCHWARTZ

12-8-97

MARYLAND SHRO

DETAIL VIEW FROM EAST TO WEST OF STONE TOP TO BRICK EDGE  
OF BRIDGE ABUTTING EAST BANK OF WILLS CREEK.

12 OF 13



~~AL-857-2~~ AL-V-13-152

CSX RAILROAD BRIDGE, LOCUST GROVE (AKA ECKHART BRANCH RAILROAD)  
ALLEGANY COUNTY, MD.

JOHN CALIB SCHWARTZ

12-8-97

MARYLAND SHPO

VIEW FROM SE  $\frac{3}{4}$  PERSPECTIVE DETAIL OF END OF BRIDGE AND  
WOOD BEAM ABUTTING EAST BANK OF WILLS CREEK.

13 OF 13