

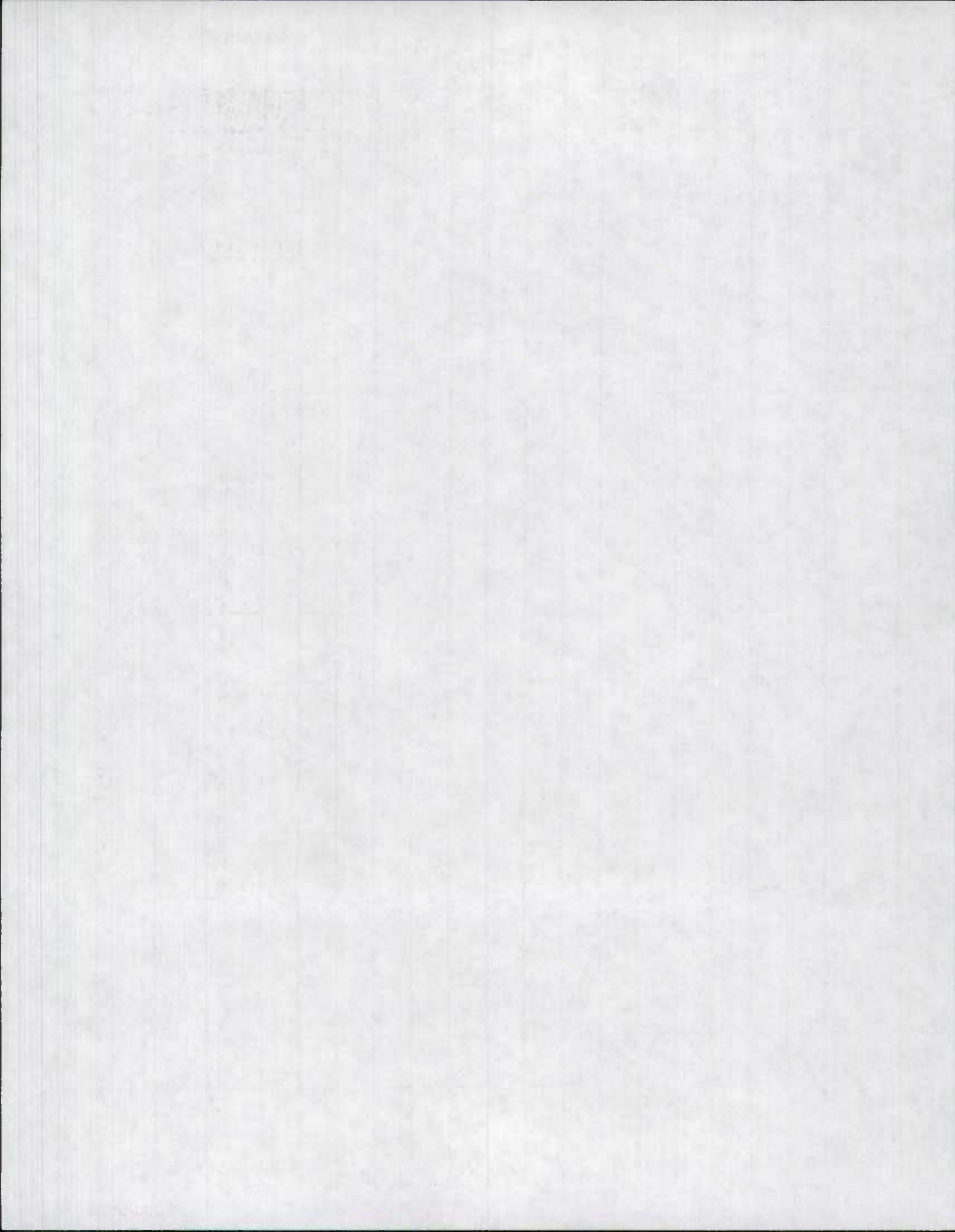
STATE HIGHWAY ADMINISTRATION OF MARYLAND

County Prince George's

REQUEST FOR FIELD INVENTORY

Sheet 1 of 1

(1) Imp. Number	(2) Road Number	(3) Road Name	(4) General Limits of Inventory Required				(5) Map Designation			(6) Previous Inventory Attached?
			From		To		Map Name or Number	Coordinates		
			Rd. Number	Road Name or Termini Description	Rd. Number	Road Name or Termini Description		X	Y	
2001 ST EX #4	MD197	Collington Rd	CO 4465	Basswood Dr	MD198	Fort Meade Rd	E12C F11D	847	453	yes
Special Instructions: <u>Reinventory &amp; tie in under constr during 2001 stre &amp; imps. P-000-000-004</u>										
2001 ST EX #5	SR84	Greenhowe Dr	SR103	Rosborough La	SR224	No Name	F-11B	818	419	yes
Special Instructions: <u>Reinventory, gps &amp; tie under constr during stre &amp; imps (2001)</u>										
2002 ST EX #1	MD4	Pennsylvania Ave		9.22	-	Donnel Dr @ 10.40	G-11B	838	365	yes
Special Instructions: <u>still under constr. during 2001 imps. Please reinventory</u>										
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2002

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Dr. Gridlock




## With Delays on Parkway, a Bad Time to Compliment Construction

Thursday, June 6, 2002; Page AA06

Dr. Gridlock can be reached at (703) 279-3200 or by e-mail at [drgridlock@washpost.com](mailto:drgridlock@washpost.com).

Dear Dr. Gridlock:

My neighbor, Richard Relac, has responded [on May 9] to my [April 4] letter about the continuing construction on the Baltimore-Washington Parkway. He claims that traffic has kept "moving with minimal disruption."

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I wonder when he travels the parkway. I have endured twice-daily traffic jams that usually extend for miles and then end after passing the construction zone at Route 197.

Even worse are the off-hour and weekend surprises, when a lane is closed and everyone sits for 45 minutes or an hour (while every other route in town is clear).

I am an architect and have overseen construction projects in Maryland, Virginia and the District for 20 years.

Anyone involved in the construction industry knows that a successful project is judged on three criteria: quality of the final product, keeping the cost under budget and completing the project in a timely manner.

I certainly favor complimenting a project that is well done, but it must be successful in all three criteria.

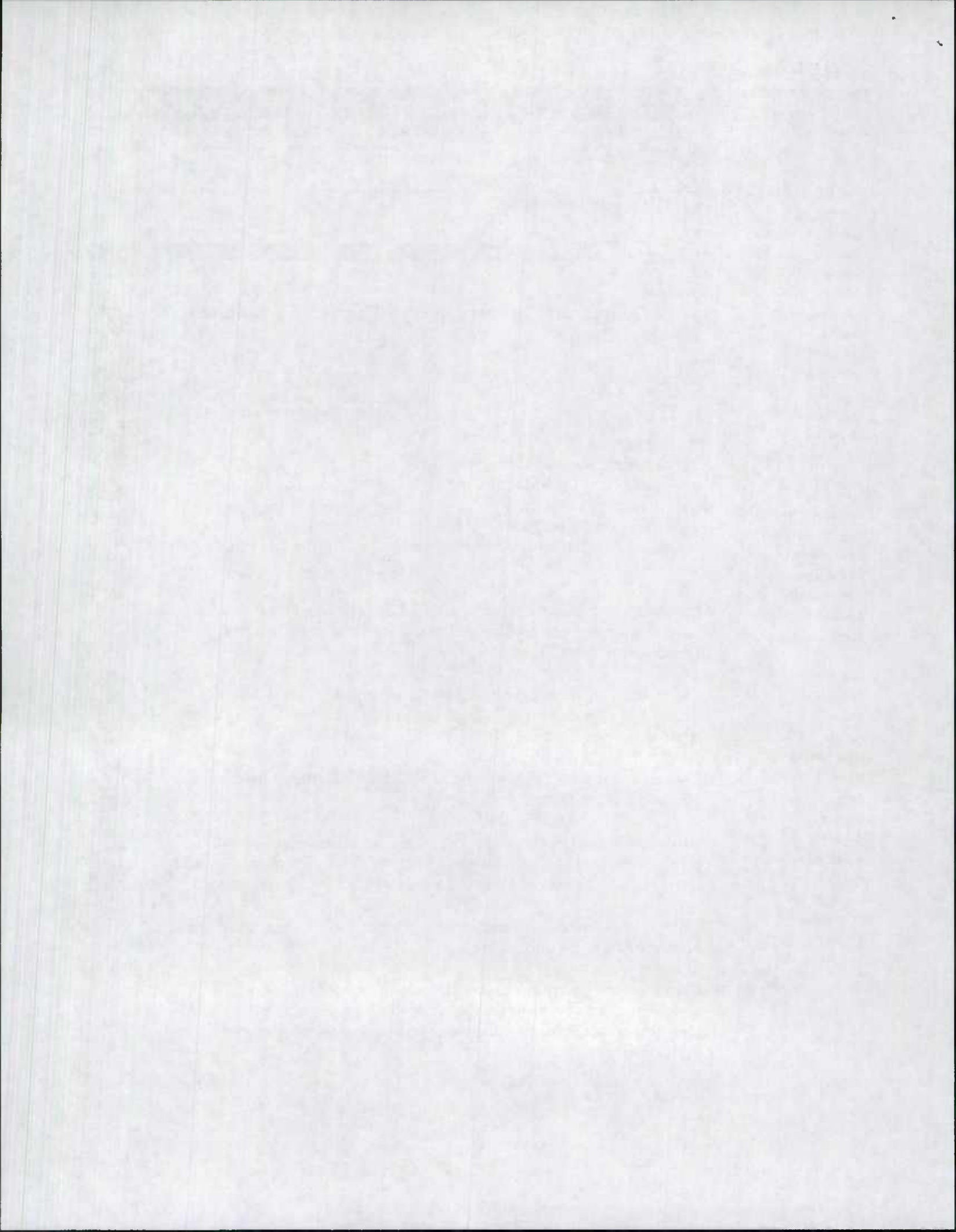
Complimenting a project that stretches out for decades is counterproductive. It will only encourage future planners to disregard the pain and suffering of the drivers who endure their leisurely performance.

**Kevin J. McPartland**

*Jessup*







Considering we're in the 15th year of reconstruction of the parkway segment from Kenilworth Avenue 20 miles out to Route 175 in Howard County, your frustration is understandable, Mr. McPartland.

The good news, if there is any left here, is that the entire project is scheduled to be finished by the end of September, with the completion of the last big parkway project, the Route 197 (Laurel Bowie Road) interchange, said Dave Hammers, a spokesman for the National Park Service.

So in a few months, we'll be done with it.

Well, not quite. Hammers said that because of new design guidelines that have come into existence since construction started, the feds will likely be extending the on-ramp from eastbound Route 197 to the southbound parkway.

"That will not be done any time soon and will be designed to cause the minimal amount of disruption," Hammers said. "Essentially, the new project would extend the southbound ramp from eastbound Route 197 to allow additional space for ramp traffic to merge into the mainline traffic. As currently constructed, this merging takes place on the crest of a hill, which is not considered acceptable under the new guidelines."

Dr. Gridlock will take it. We're almost there (I think). What they have finished looks great. (Remember the washboard effect of the buckled pavement joints before this project began around 1987? It's a smooth ride now.)

Your comments?

#### **A Tight Spot**

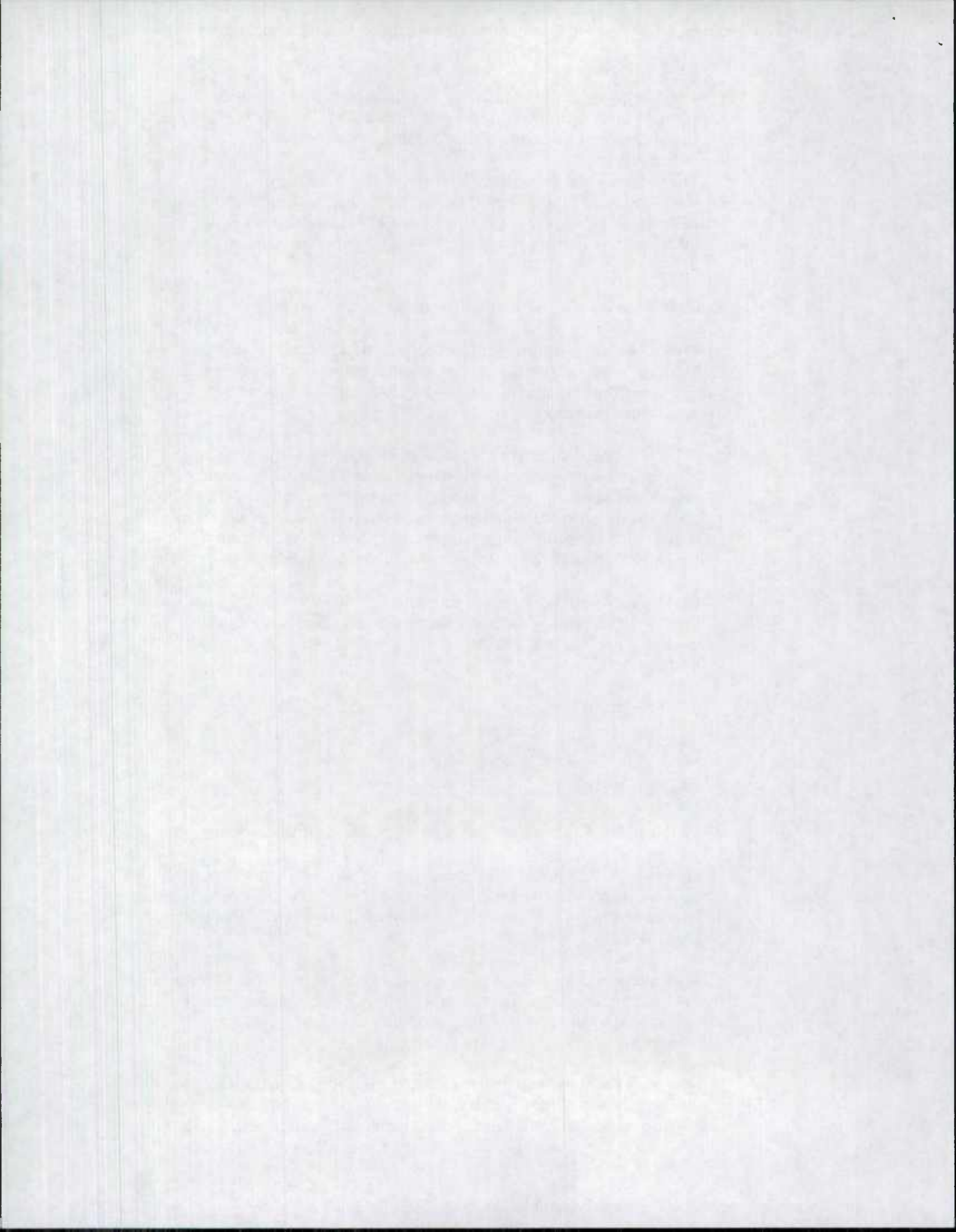
**Dear Dr. Gridlock:**

Maryland has some strange quirks on its roads, and I want to know why.

For instance, take Route 5 (Branch Avenue): Traveling south from the Naylor Road Metro station, Route 5 is three lanes. After the stoplight for Iverson Street, the road narrows to two lanes. Less than a half-mile later, the road is three lanes again.

Then after about six miles, the road narrows back to two lanes after the Route 223 interchange. About another half-mile later, heading toward Southern Maryland, the road becomes three lanes again just to go through the traffic light at Surratts Road.

Why can't the Maryland State Highway Administration make Route 5 three continuous lanes from Naylor Road until Surratts? The Route 5 bridge over St. Barnabas Road is designed for three lanes; there seem to





be no obstacles to widening.

These unnecessary merges cause backups, road rage and accidents.

**Matt Pedersen**

*Upper Marlboro*

The obstacle is money. The state wants to widen Route 5 to three lanes in each direction between the Capital Beltway and the Route 5/Route 301 split at Brandywine but doesn't have the funds to do so, at least for the next six years, said Dave Buck, highway administration spokesman.

The reason you have the two-lane/three-lane schizophrenia is that the state has widened Route 5 at certain spots while doing other work.

The other work includes the replacement of signalized intersections with interchanges (overpass/underpass) at Manchester Drive, Allentown Road, Coventry Way and Woodyard Road/Piscataway Road, all in Prince George's County. These are no small projects -- about \$25 million apiece.

In August, the state will begin improvements at the Surratts Road intersection, including new turn lanes. This will take about a year, Buck said.

Down the line -- but not yet funded -- are full Route 5 interchanges at Surratts Road, Burch Hill Road/Earnshaw Drive and Accokeek Road, Buck said. The goal is to eliminate all Route 5 traffic lights in the Beltway-to-Route 301 corridor and widen to three lanes in each direction.

**Working Out Works for Commuters**

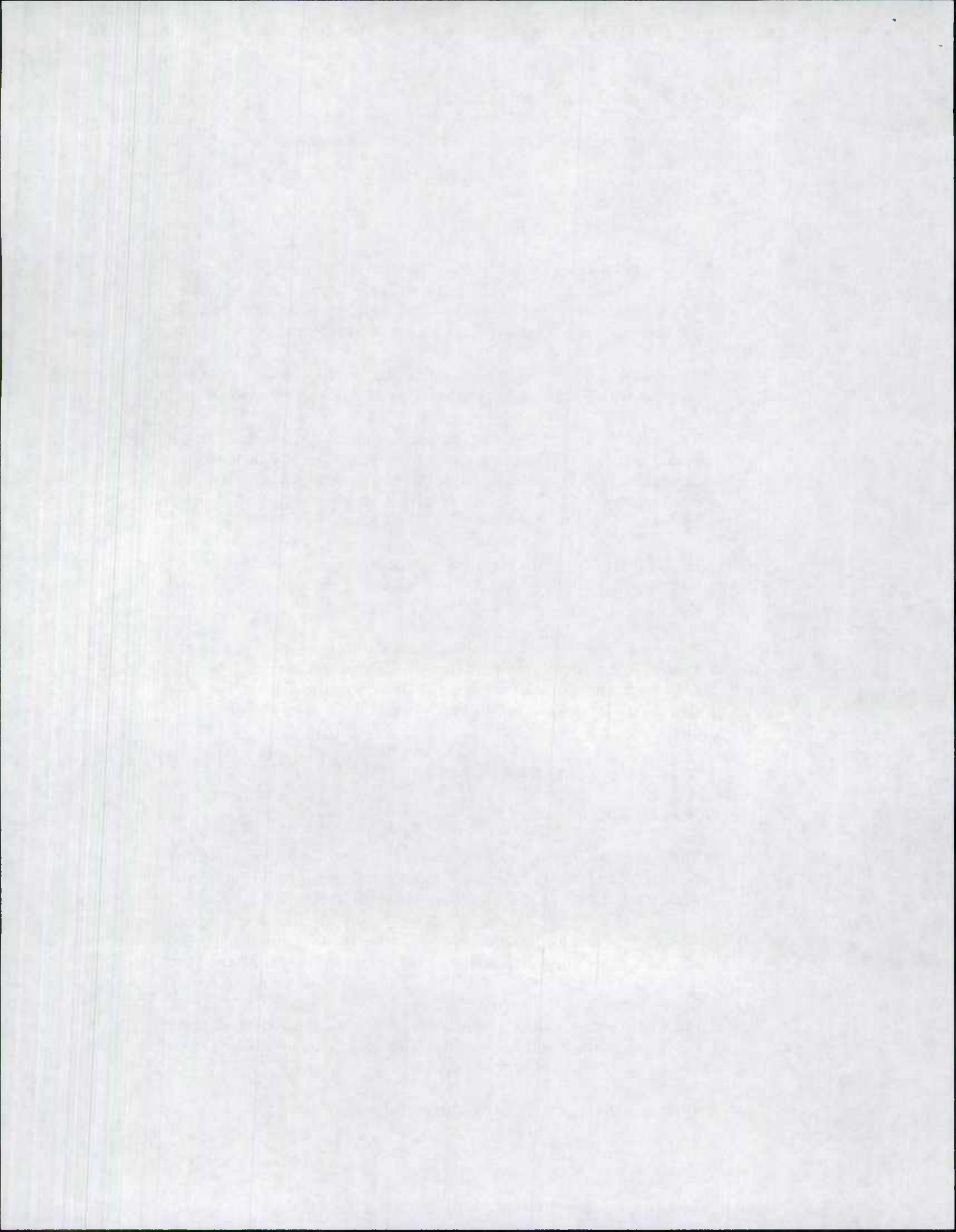
**Dear Dr. Gridlock:**

I have been commuting from Columbia to Crystal City for a number of years now, and to avoid the hassle and wasted time of rush hour traffic, I usually make the trip to work between 5 and 5:30 a.m.

As compensation for this insanity, I go straight to the gym by my office and work out, shower and drink a cup of coffee until about 8, when I'm ready for the workday.

I promised myself that as long as I was willing to get up at o'dark-thirty to beat the traffic to work, I would at least do something beneficial to my health and wellness as compensation.

It has kept me loving my commute for many years now.





**Dennis Kreps**

*Columbia*

Anyone who has found a way to love their commute is on to something. Well done. Commuters all over the area are telling me they are leaving an hour or more early for work (thus missing peak rush hour) to work out and, in the process, are getting prime parking spaces. And they are losing weight.

**Motorcyclists Are No Saints**

**Dear Dr. Gridlock:**

I am responding to the motorcycle fanatic who had nothing good to say about moms in minivans. Well, let me tell you, the experiences I've had with motorcyclists have been less than amicable.

Who do these idiots think they are? They race up and down the streets, in and out of traffic without regard to anything around them! Just because they *can* weave in and out of traffic doesn't mean that they *should*.

I have a child who is frequently in the car with me. I would never take a chance with him in the car.

Sure, I'll bet there are some moms in their minivans who are guilty of multitasking while driving, but I see men and women without children reading the paper, eating, putting on makeup, talking on cell phones.

There are idiots, and there are drivers, then there are driving idiots, no matter what vehicle they are driving. All drivers should be considerate of others.

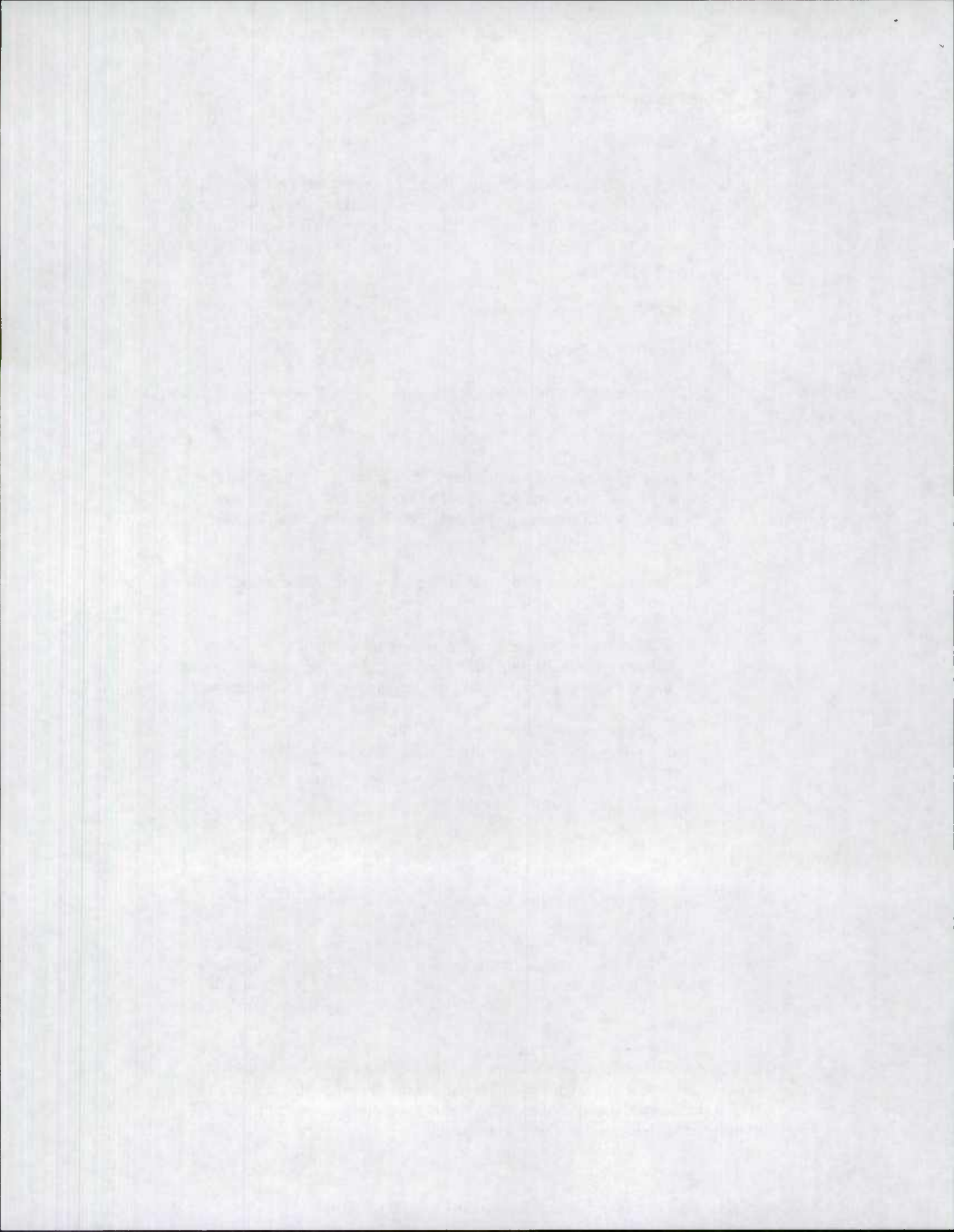
**Lynne Griffin**

*Potomac*

**Dear Dr. Gridlock:**

I *fully* agree with Barbara Jean McAtlin about the lousy driving habits of mothers with children in minivans. Time and time again, I have one of those tailgaters behind me, with the mom on the cell phone and/or bending over, addressing (or *dressing!*) kids. They are truly menaces on the road.

I say it again: Young women driving minivans do *not* need minivans. In times past, there were women who managed transporting their kids in ordinary sedans or, at most, low-slung station wagons and it *worked*, folks, it *worked* and will work *now*.



All it takes is *sane* people driving cars and paying attention to the task at hand!

**Suzanne M. Rucker**

*Alexandria*

**Pull Over to Curb**

**Dear Dr. Gridlock:**

I am a native Washingtonian, and it bothers me to see drivers, especially taxis, stop in traffic to pick up or drop off passengers when they could just as easily pull over to the curb.

This even happens in the morning rush hour, blocking an entire lane, which clearly inconveniences dozens of drivers, slows traffic as a whole and is illegal.

Why do people do this? I'm baffled. We're not talking about parallel parking, just pulling over to the curb. Please ask drivers to make an effort to be considerate of others, at a minimum where it is easy to do so.

**Elizabeth Berry**

*Washington*

Some drivers were born with a hole in the brain. People will stop in place because, they rationalize, "it'll just be a minute." In reality, they don't care about holding up traffic, be it two or 200 other human beings.

Taxi drivers do it because they are concerned if they pull to the curb it'll take time to get someone to let them back into the traffic flow (probably true). Better (for the taxi drivers) to stop the traffic flow.

I'd like to hear from traffic stoppers as to why they do it.

**Londoners Do It**

**Dear Dr. Gridlock:**

Regarding the complaints in your column of people blocking passage on Metro escalators by standing on the left, it is beyond my comprehension that Metro officials refuse to post "Stand Right, Walk Left" signs. In London, there are signs every 10 to 15 feet on the escalators in both directions, saying, in three languages: "Stand on the Right." During my more than six years there, I never saw anybody standing on the left.

Washington's Metro system should seriously consider posting such signs. That will not only help thousands of commuters but will also be a





good education for the children and, with time, make it a habit.

### Subhash Vohra

#### *Falls Church*

Metro spokeswoman Lisa Farbstein, quoted in this column as saying it was not necessary to post such signs, elaborated at a lunch with me:

"We don't want them to move on the escalators at all. They can bump into people, or get clothing parts caught in the machinery or they can trip on the moving stairs. Also, people walking or sprinting mean more wear and tear on the escalators."

All that makes sense. I just wonder how European systems manage.

*Dr. Gridlock appears Sunday in the Metro section and Thursday in Anne Arundel Extra. You can write to Dr. Gridlock, 1150 15th St. NW, Washington, D.C. 20071. He prefers to receive e-mail, at [drgridlock@washpost.com](mailto:drgridlock@washpost.com), or faxes, at 703-352-3908. Please include your full name, town, county and day and evening phone numbers. Dr. Gridlock cannot take phone calls.*

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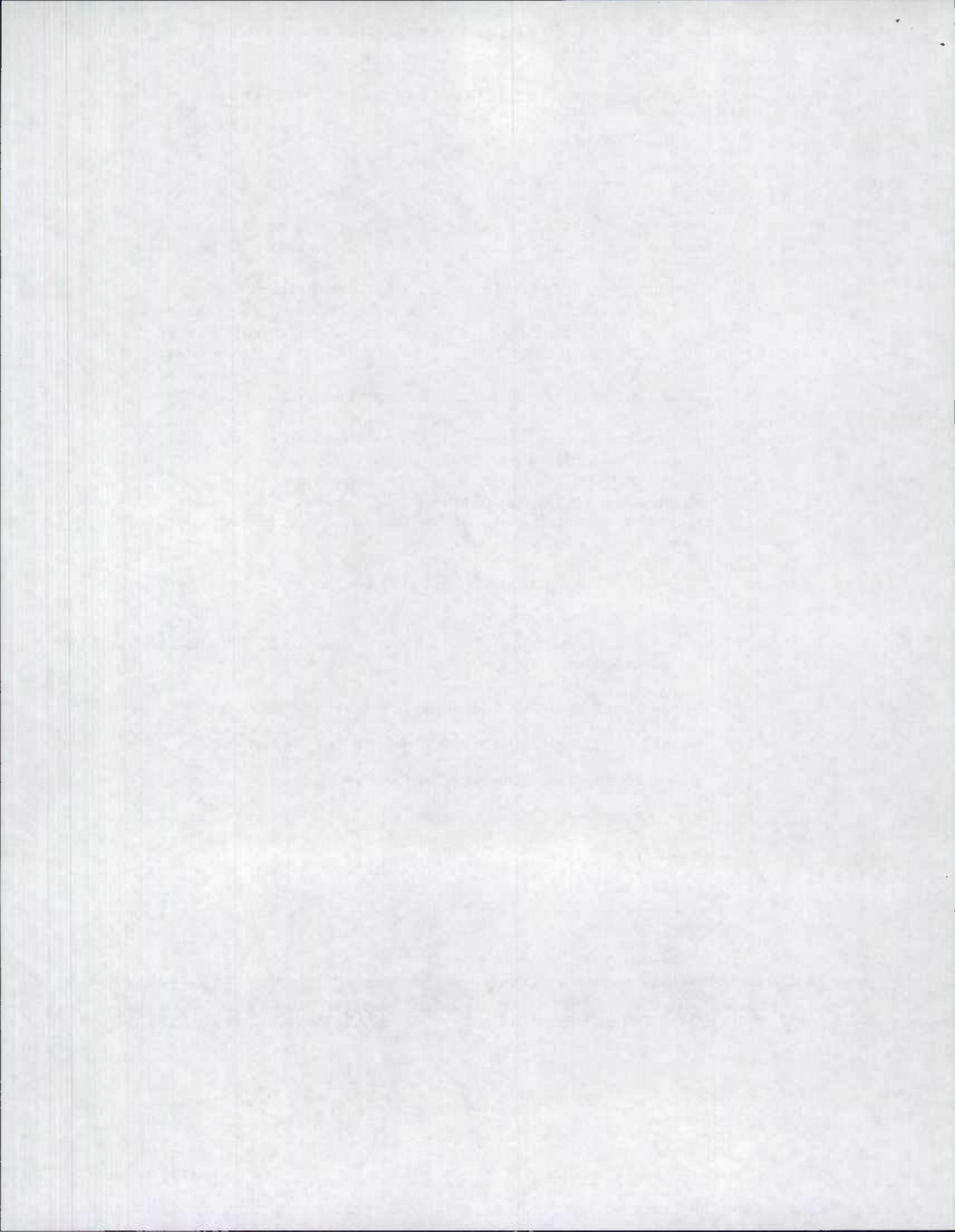
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**From:** KEVIN POWERS  
**To:** AMY DEMAINE; JOHN KLAUSING; WERNER SCHLOUGH  
**Date:** 6/11/02 11:17AM  
**Subject:** MD 295 & MD 197, Prince George's County

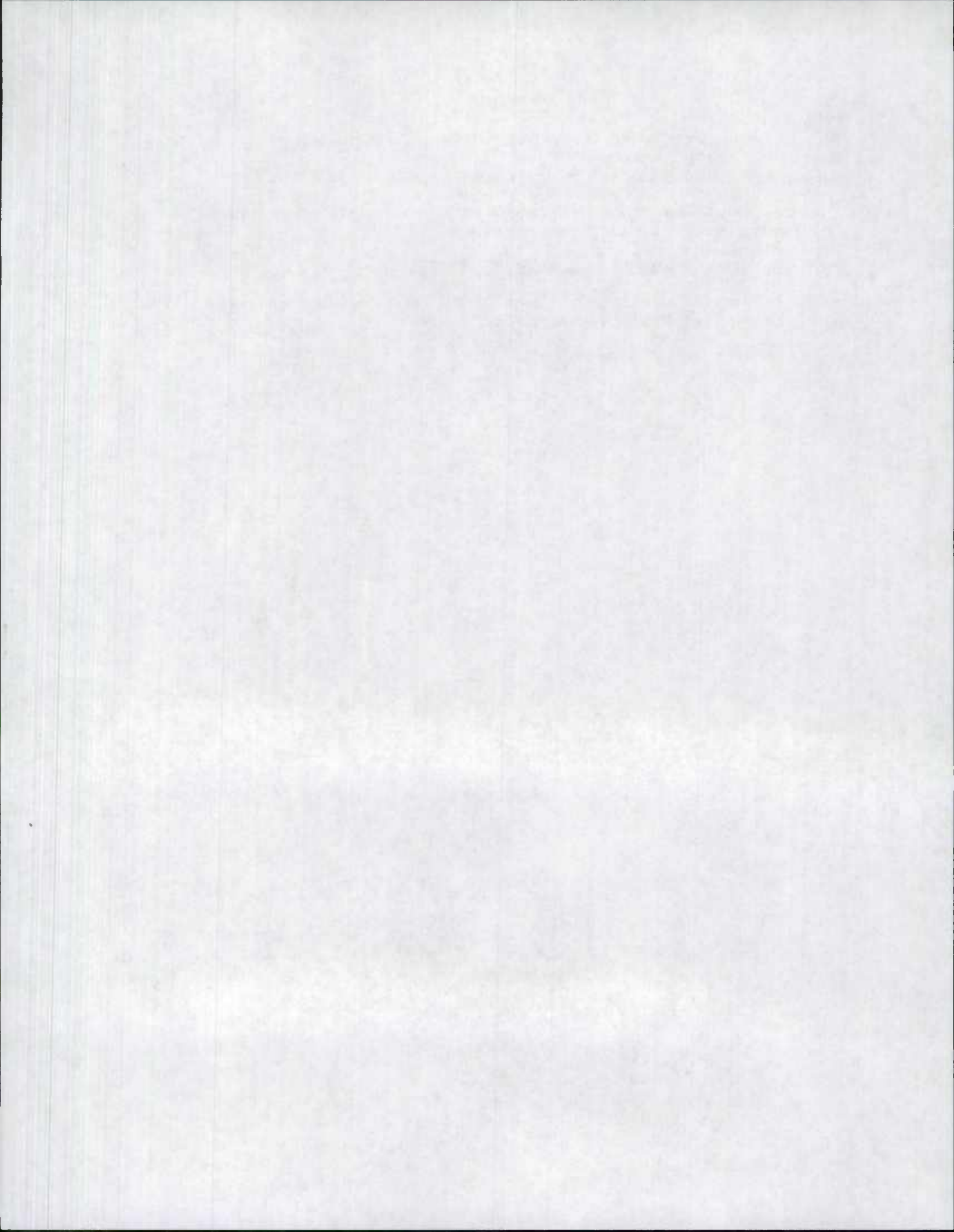
According to the following article, this interchange is due to be completed around September of this year. Please arrange for both of these roads to be re-inventoried.

<http://www.washingtonpost.com/wp-dyn/articles/A63961-2002Jun5.html>

When was the last time MD 295 in Prince George's County was re-inventoried? It might be time to do it again, especially if there are any HPMS samples located on that road.

Any questions or comments, please see me.

Thanks.



2001

CONTRACTS EXECUTED BY THE STATE HIGHWAY ADMINISTRATION  
FROM 07/05/2000 TO 07/05/2000  
PUBLISHED BY THE OFFICE OF CONSTRUCTION

06-Jul-00

Contractor	Address	Contract #	FAP #	PDMS #	Description	Date	Amount
DENVILLE LINE PAINTING	ROCKAWAY, NJ	492D31402			LINESTRIPING ROADWAY PAVEMENTS AT VARIOUS LOCATIONS IN DISTRICT #3	07/05/20	\$1,401,000.00
FORT MYER CONSTRUCTION	WASHINGTON, DC	PG2745184*	AC-CM-G-251-1(43)E	162175 12.72 CO5241	INTERSECTION RECONSTRUCTION ON US 1 FROM OAK STREET TO MD RTE. 198 ← 13.49	07/05/20	\$4,304,701.00
ASPLUNDH TREE EXPERT CO	ODENTON, MD	4261631402			APPLICATION OF HERBICIDE SOLUTIONS AT VARIOUS LOCATIONS IN PRINCE GEORGE'S COUNTY	07/05/20	\$562,343.00-
RAYE VEST CORPORATION	WHITE PLAINS, MD	CH6255176	AC-NH-8500(5)E	082068 12.85 CO62	CONSTRUCT ACCESS ROAD (MATTAWOMAN - MO5 BEANTOWN ROAD) NORTH OF COUNCIL OAK DRIVE TO SOUTH OF SUBSTATION ROAD ← CO 936	07/05/20	\$633,068.84
SLURRY PAVERS, INC	GLEN ALLEN, VA	4072T61402			JOINT SHAPING & SEALING OF JOINTS & CRACKS AT VARIOUS LOCATIONS IN WASHINGTON COUNTY	07/05/20	\$84,000.00-
<b>TOTAL--&gt;</b>							<b>\$6,985,112.84</b>

\* PG 2745184

INTERSECTIONS:

12.72 OAK ST - CO5241

TOWN #89

12.85 CHERRY LA - CO 7 / MU 83

LAUREL

13.14 DOMEZ AVE - MU 145

13.34 MARSHALL AVE - CO 2310

13.40 BOWIE RD - MU 55

13.44 GORMAN AVE - OP 1163

13.49 MD 198

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HIGHWAY INFORMATION SERVICES DIVISION



Attach to Improvement  
list when sending  
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Just FYI - the Capital Beltway interchange  
at MD 210/I-295

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## Route 210 traffic could worsen in next phase of bridge project

by Guy Leonard  
 Staff Writer

Jan. 25, 2002

South county residents and commuters using Route 210 could be in for a more clogged traffic flow this Friday and for at least the next 15 months.

The State Highway Administration will close off one lane of the Route 210 off ramp, which leads to Route 295.

The closure will begin the process of building a new overpass for a wider Capital Beltway in the future.

"It's going to have to be shrunk down to one lane," said bridge project spokesman John Undeland. "It will take nearly the entire ramp and traffic will shift to the left."

Undeland said the ramp is often clogged during morning rush hour but the new traffic conditions will have the traffic spill back down onto Route 210.

"There aren't a lot of options when you're doing this kind of work," Undeland said.

Undeland said commuters might want to look at other options, such as mass transit and telecommuting to avoid the traffic problems.

"We recognize the inconvenience this places on Maryland drivers and we will do what we can to minimize its impact," said Bob Douglass, bridge project manager for the State Highway Administration. "If there was a practical way to avoid this closure we would do so. But the reality is that we have to shift all of the traffic onto one of the twin overpasses in order to build the new one."

While the roadwork proceeds apace, the actual bridge project is still delayed -- while state officials study how to get the project back on track.

John -  
pass this around, then store it  
with your 2002 state improvement  
info.  
Thanks, Kevin



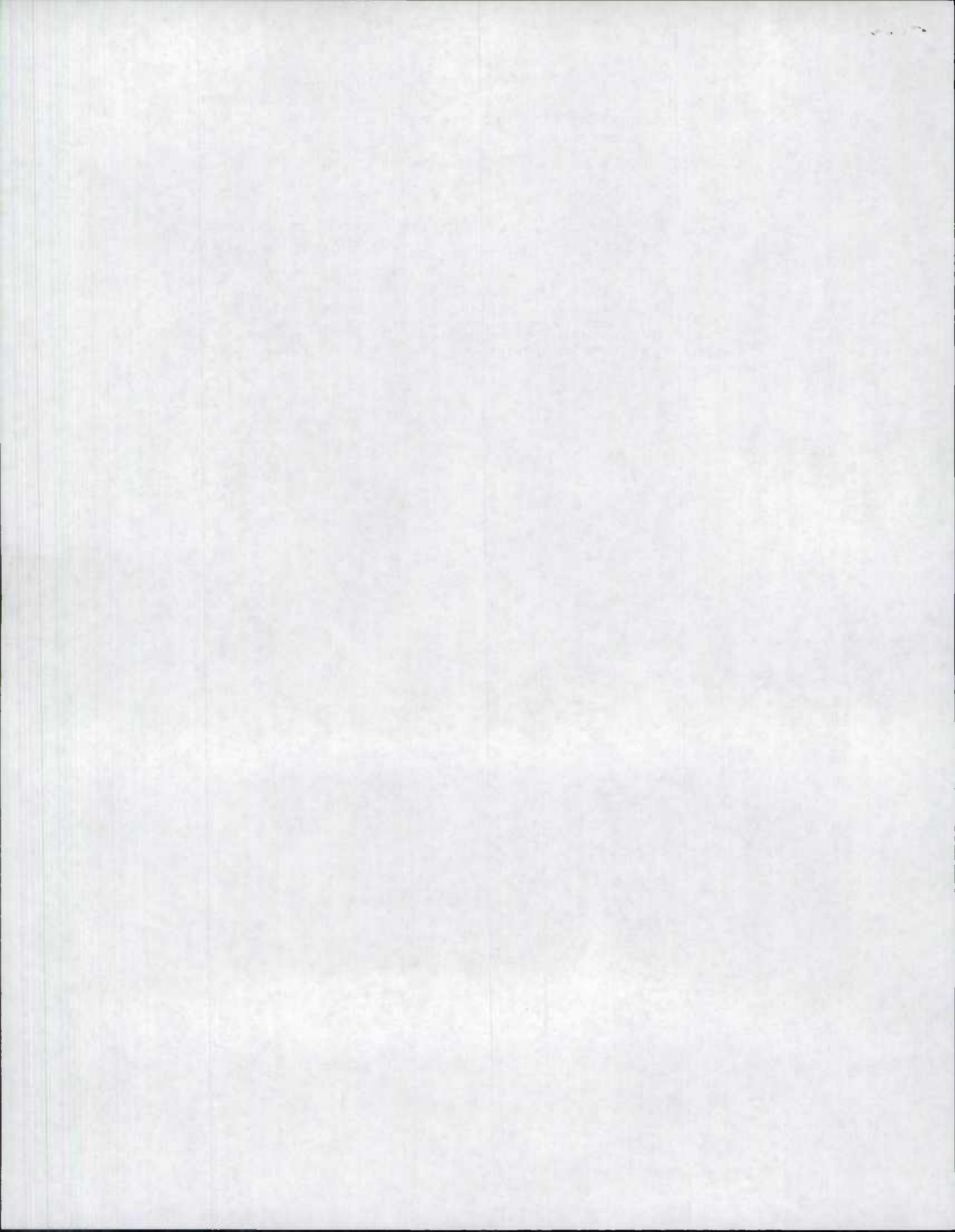
Only one bid has been made to build the infrastructure, from Kiewit, Tidewater and Clark. The company was the one, which laid the foundations for the bridge.

Its bid came in at \$860 million dollars, almost twice the original estimate to build the bridge's infrastructure.

The state's study showing why the bid was so large, and what it plans to do about it, should be available by the end of February.

*E-mail Guy Leonard at [gleonard@gazette.net](mailto:gleonard@gazette.net).*

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PG 200B21

POMS<sup>A</sup> 162159



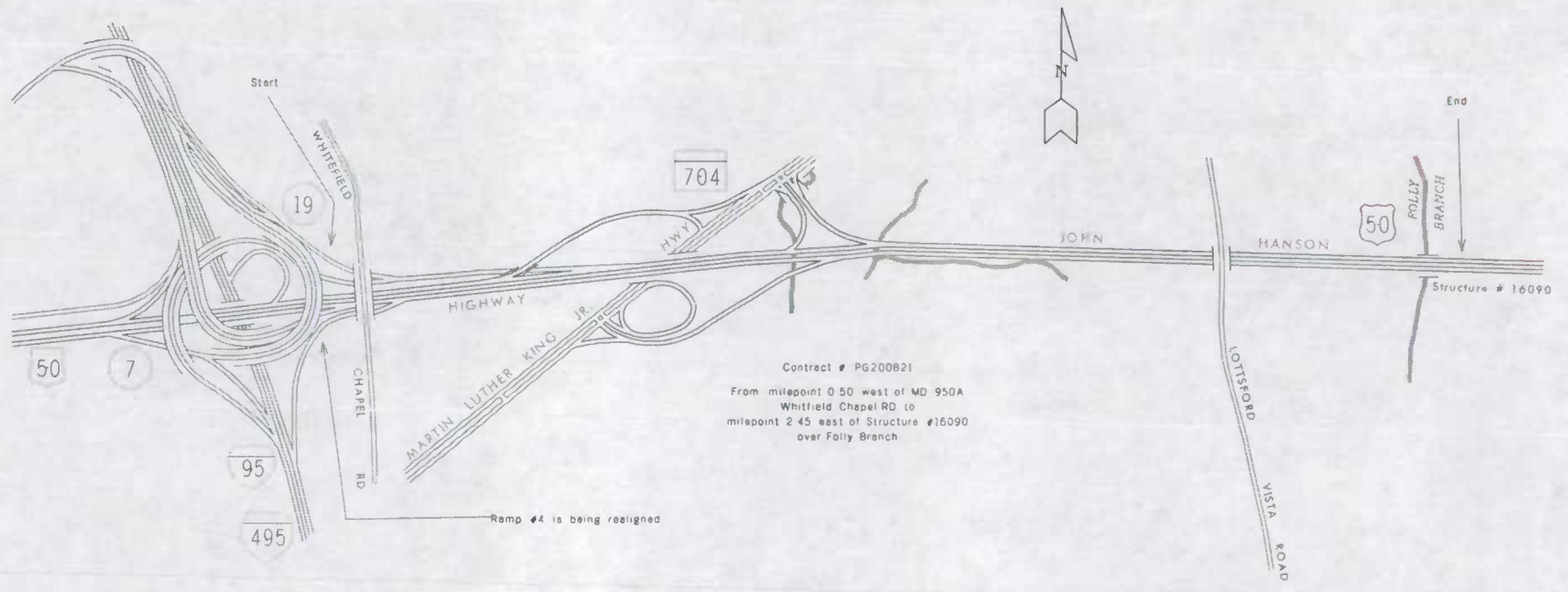
HOV LANE S

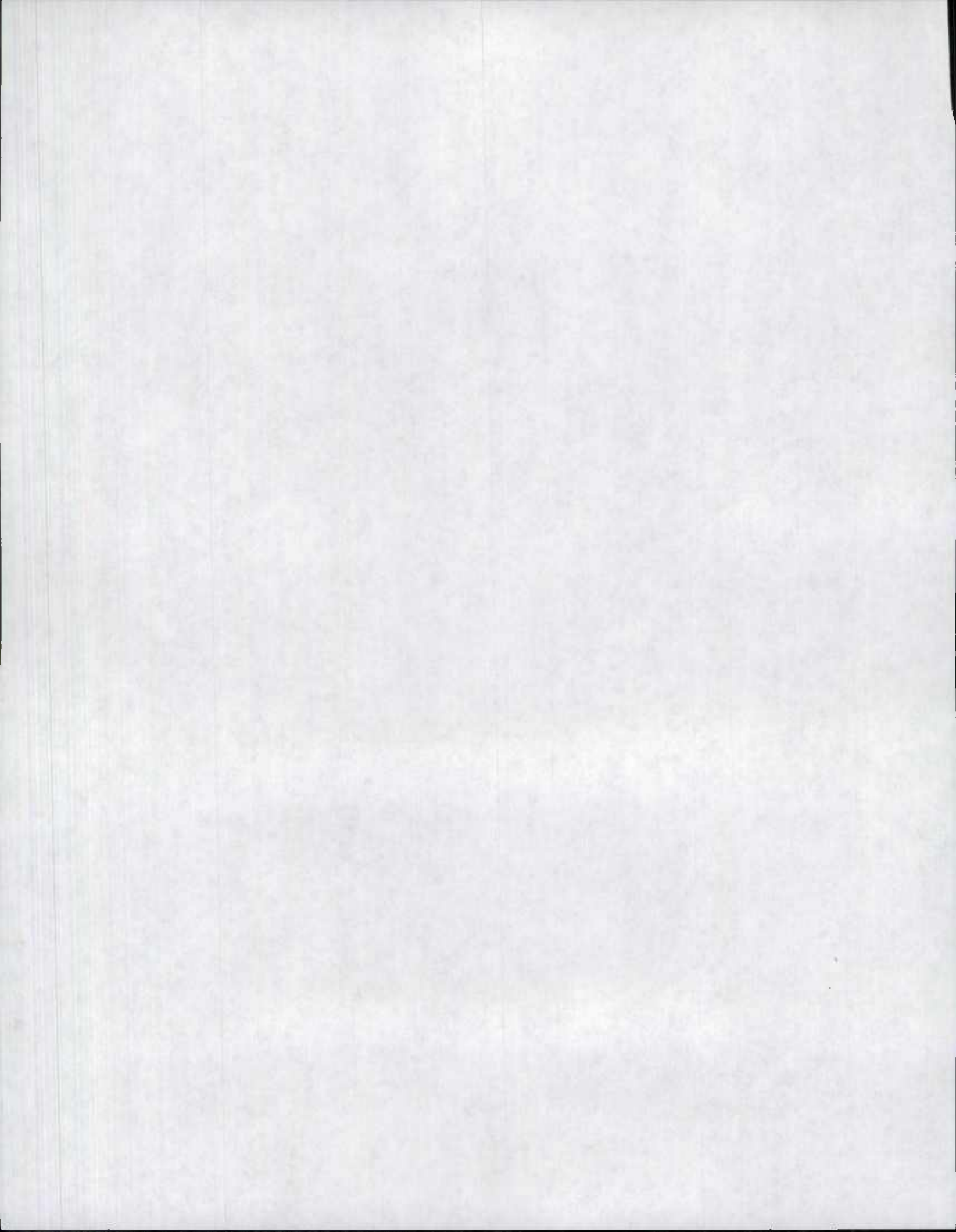
From: (W of) MD 3 & IS 595  
MP (8.08)

To: (E of) MD 410 & US 50  
MP (4.10)













**Maryland Department of Transportation**  
**State Highway Administration**

March 24, 2000

Parris N. Glendening  
 Governor  
 John D. Porcari  
 Secretary  
 Parker F. Williams  
 Administrator

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2000 MAR 30 A 9:54

MEMORANDUM

To: Mr. Robert D. Douglass  
 Deputy Chief Engineer  
 Highway Development

From: Mr. Kirk G. McClelland  
 Chief, Highway Design Division

By: Matthew Clark  
 Project Engineer

Subject: FMIS No.: PG200B21  
 PDMS No.: 162159  
 US 50 HOV Lanes  
 From East of MD 3 to West of MD 410

Re: Final Review Report

*Kirk G. McClelland*  
 OFFICE OF  
 DEVELOPMENT

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MAR 31 2000

HIGHWAY INFORMATION  
 SERVICES DIVISION

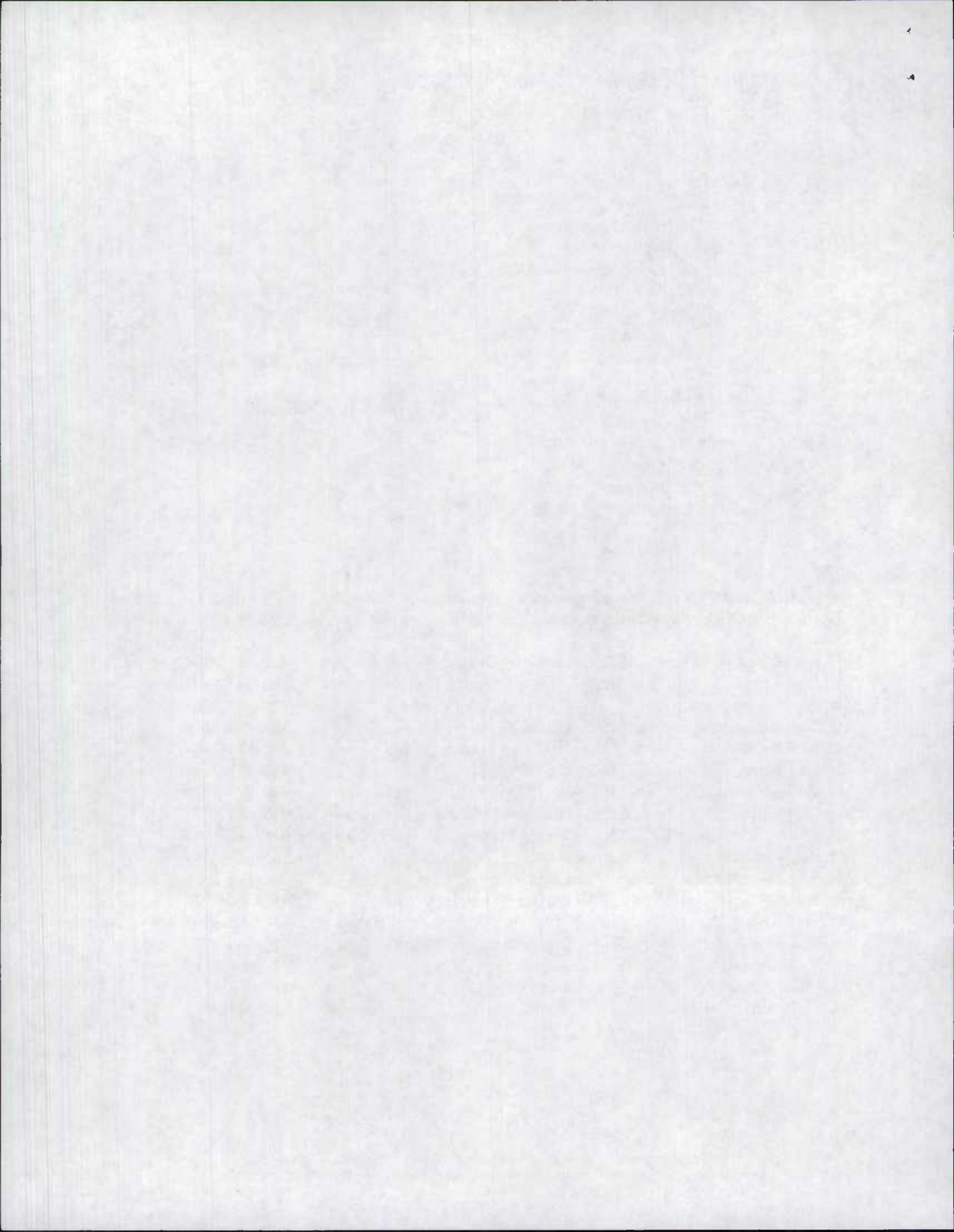
On February 22, 2000, a Final Office Review was conducted in the 211 Building at the Maryland State Highway Administration. The following people were in attendance:

Mr. Matt Clark	SHA - Highway Design Division	410-545-8765
Mr. David Phillips	SHA - Highway Design Division	410-545-8823
Mr. Brian Young	SHA - Highway Design Division	410-545-8863
Mr. Tom Sistik	SHA - Laurel Shop	301-776-7619
Mr. Neil Haines	SHA - District 3 Construction	301-513-7384
Mr. Ed Oberc	SHA - Geotechnical	410-321-3186
Mr. William Kuhl	SHA - Landscape Architecture	410-545-8618
Mr. Chris Brooks	SHA - Highway Hydraulics Division	410-545-8413
Ms. Cheryl Schreiber	SHA - Traffic Engineering Design Division	410-787-4044
Mr. Ray Mercado	SHA - District 3 Traffic	301-513-7359
Mr. Roger Campbell	SHA - Laurel Shop	301-776-7619
Mr. Darrell Sacks	SHA - Project Planning Division	410-545-8527
Mr. Fred Eisen	SHA - Office of Environmental Design	410-545-8598
Ms. Kelly Sullender	SHA - Bridge Design Division	410-545-8074
Mr. Rick Schmuff	STV, Incorporated	410-944-9112
Mr. Matt Storck	STV, Incorporated	410-944-9112
Mr. Steve Parker	STV, Incorporated	410-944-9112

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## PROJECT DESCRIPTION

### GENERAL

This project, located in Prince George's County, proposes to create HOV lanes on US 50 east of I-95. Also, there are a number of other major work items that have been incorporated in this project. The other major elements of this project are:

1. Grind and Resurface 1.58 miles of US 50 from west of I-95 to west of MD 410.
2. Reconstruct and Realign the ramp from Northbound I-95 to Eastbound US 50.
3. Replace 3500 linear feet of concrete median barrier east of Whitfield Chapel Road.
4. Install new median drainage structures throughout the closed section between Whitfield Chapel Road and Lottsford-Vista Road.
5. Overlay and restripe approximately 10,000 linear feet of US 50 from east of I-95 to east of Lottsford-Vista Road.
6. Construct new 4 foot HMA median shoulders from east of US 301 to east of Lottsford-Vista Road.
7. Full and partial depth patching at various locations along US 50.
8. Construct a 4000-foot extension of the existing acceleration lane from MD 197 to westbound US 50.
9. Remove existing HMA shoulder between MD 193 and MD 197, originally constructed for Maintenance of Traffic purposes during the reconstruction of US 50.
10. Install HOV signing and pavement markings from east of US 301 to east of I-95.

### EXISTING CONDITIONS

The existing median of US 50 is an open section from east of MD 3 to the vicinity of Lottsford-Vista Road. This existing median has 12-foot shoulders that were constructed to function as travel lanes. There is 26 feet between the inside edges of these paved shoulders.

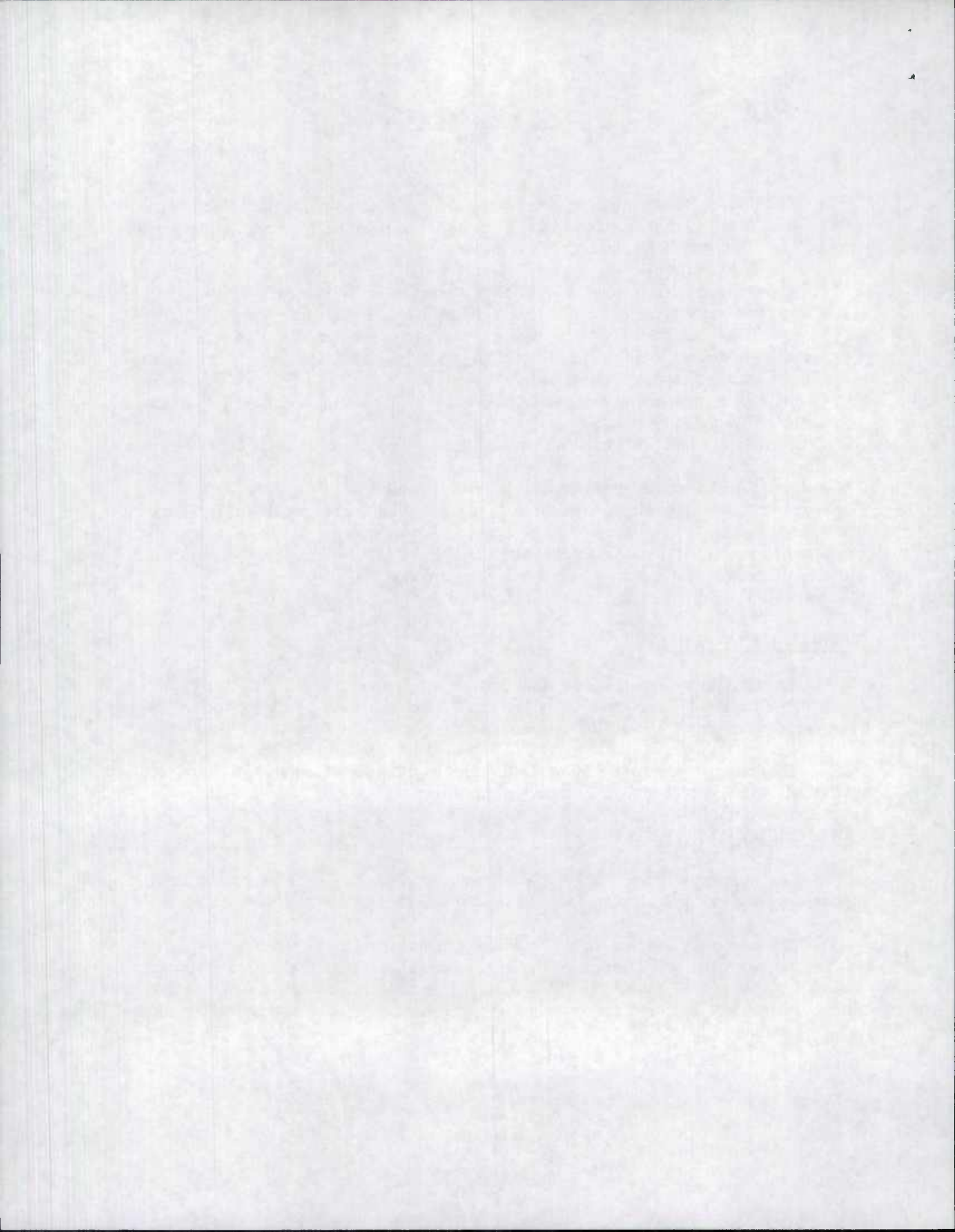
The existing median of US 50 west of Lottsford-Vista Road transitions to a closed section with 12-foot shoulders abutting a concrete median barrier.

### PROPOSED CONDITIONS

The proposed westbound HOV lane will begin approximately 1600 feet east of the MD 3 interchange and end approximately 1500 feet west of MD 704.

The proposed eastbound HOV lane will begin approximately 1500 feet west of MD 704 and end approximately 4800 feet east of the MD 3 interchange. Both HOV lanes will occupy the existing 12-foot paved shoulder in both the open and closed median sections. Drainage improvements will be constructed within the closed section to accommodate traffic on the existing median shoulder.





In the open median sections east of Lottsford Vista Road where the existing 12-foot-wide shoulder is being converted to the HOV lane, a proposed 4-foot-wide HMA shoulder will be constructed. In sections where the existing traffic barrier w-beam is 4 feet or less from the edge of the existing shoulder, the proposed HMA shoulder will be reduced in width to maintain a 6-inch clearance between the edge of proposed shoulder and the face of the barrier panel. Where the existing w-beam traffic barrier is less than 2.5 feet from the existing shoulder, a new shoulder will not be constructed.

The project also proposes a 2-inch grinding and a 2-inch HMA resurfacing on US 50 approximately 3500 feet west of MD 410 to approximately 1650 feet west of I-95.

The ramp (Ramp H) from northbound I-95 to eastbound US 50 will be reconstructed and realigned. The existing ramp currently experiences maintenance problems and does not meet AASHTO criteria for compound curves.

The existing WB US 50 acceleration lane from MD 197 currently ends at approximately Station 113. This acceleration lane will be extended to Station 154. Construction of this acceleration lane will require removal of the existing end taper from Station 108 to Station 113 as well as the existing 10-foot Plain PCC shoulder from Station 108 to Station 154. The proposed 12-foot wide acceleration lane and 6-foot shoulder will be 10-inch jointed reinforced concrete pavement.

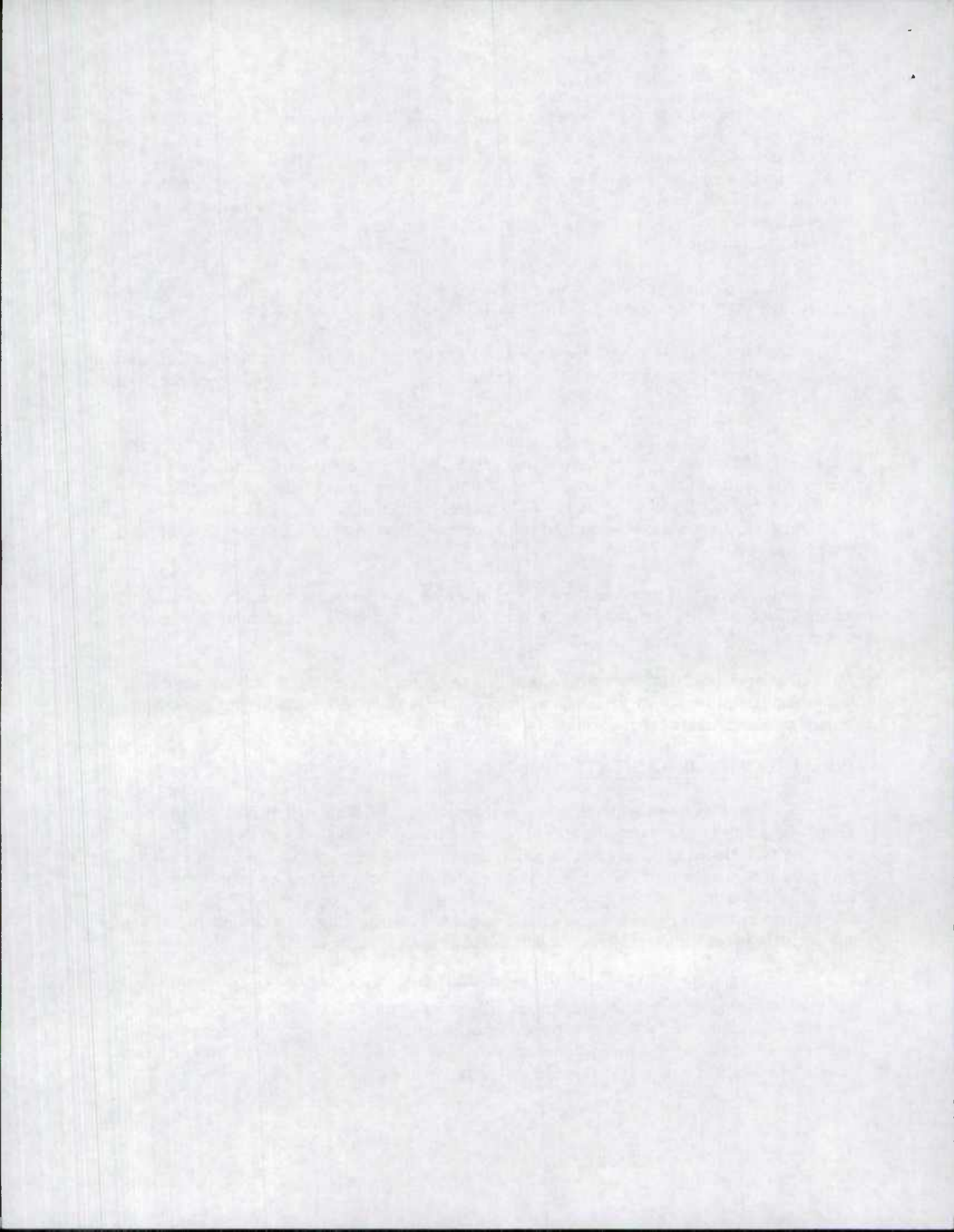
This project also proposes the removal of a 6'-8' wide HMA shoulder, approximately 6 inches thick, which lies outside of the 10-foot PPCC shoulder and the placement of topsoil, seed and mulch.

The proposed HOV lanes will be in effect during morning peak hours only for westbound and evening peak hours only for eastbound traffic. The proposed HOV lanes are not to become general use lanes during off peak hours.

### **CONSTRUCTION DOCUMENTS**

The documents will be prepared as a "Proposal Book Job," and will include full-size plan sheets for drainage improvements and pavement markings throughout the closed section of US 50. Also included are roadway and maintenance of traffic plans for the reconstruction and realignment of the Ramp H. The balance of the drawings, including required typical sections, standards and details will be provided on either 8-1/2" x 11" or 11" x 17" sheets. All construction notes, instructions and special details are included within this Invitation for Bids and are listed in the Table of Contents under Standards and Detail Sheets.

Prior to beginning construction, the Contractor will be provided with "As-built" plans for those contracts under which this section of US 50 was built. The Contractor will also be provided with a copy of "As-built" plans for recently installed fiber-optic cable throughout the corridor. The baseline of construction shown on these plans has been used to reference and define the location of proposed work under this project in lieu of a current field survey.





No warranty is implied or expressed regarding the accuracy or completeness of the "As-built" plan data provided within this Invitation or on the prints of the earlier plans. The Contractor is responsible for examining the actual site of this project prior to submitting a bid and for seeking answers to questions at the Pre-bidding Session. During construction the SHA Engineer will have final authority to interpret these documents and to provide detailed direction.

### PROGRAM IDENTIFICATION

US 50 is classified as an Urban Major Arterial in the State Functional Classification System. This project is included in the Primary Construction Program of the State Highway Administration's 2000-2005 Consolidated Transportation Program, line 6, page H-167. This project is scheduled to be advertised on July 18 2000. It is funded for engineering and construction. The final construction cost estimate for this project including overhead is \$12,872,659.

### DESIGN AND GEOMETRICS

The design geometrics will meet the following design speeds:

US 50: 65 mph

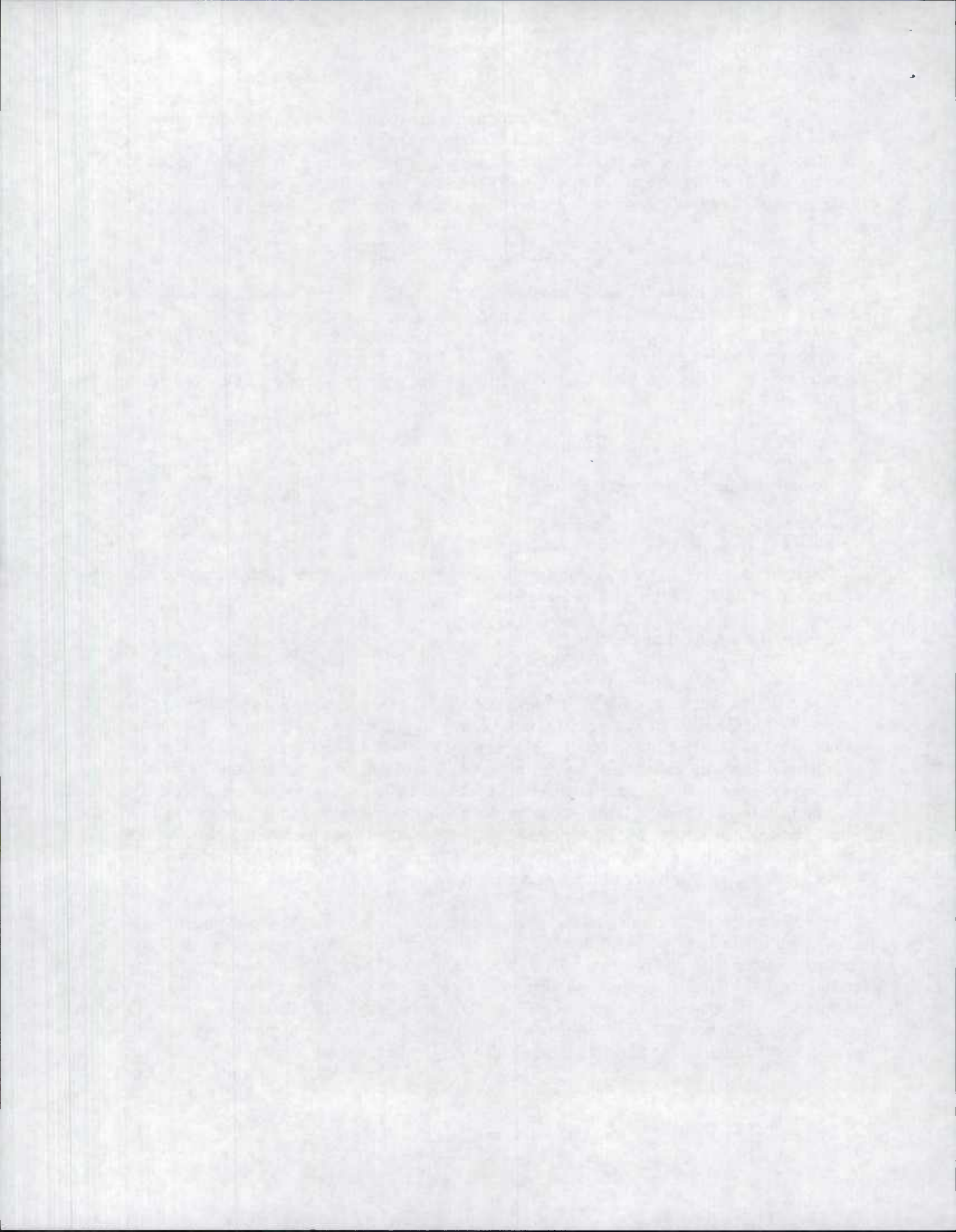
US 50 is currently posted for a 65 mph speed limit east of Lottsford-Vista Road and posted for a 55 mph speed limit west of Lottsford-Vista Road.

Current traffic data for this project is:

US 50 ADT: 2000/2020 96,875/133,900

A design exception to the 1990 AASHTO *Policy for Geometric Design for Highway and Streets* for Shoulder Width was requested for this project. This design exception is for the inside shoulder width adjacent to the US 50 HOV Lane. The 1990 AASHTO *Policy for Geometric Design for Highway and Streets* states on page 338 that "heavily traveled and high-speed highways and those carrying large numbers of trucks should have usable shoulders at least 10 ft and preferably 12 ft wide". It also states on that page that "where roadside barriers, walls, or other vertical elements are used, the graded shoulder should be wide enough that these vertical elements can be offset a minimum of 2 ft from the outer edge of the usable shoulder". US 50 is classified as a heavily traveled and high-speed highway.

We are requesting a design exception to construct the proposed inside shoulder to a 4-foot width in the open section of this US 50 HOV project. Given this impact in terms of cost, construction duration, and potential right-of-way required and the fact that this lane will be used during peak hours only, we are requesting a design exception to construct the proposed inside shoulder to a 4-foot width. In the section from MD 3 to Lottsford Vista Road, provision of 10-foot shoulders would add considerable expense to the project for the median work and also require construction of additional storm water management facilities





We are requesting a design exception to reduce shoulder widths throughout the closed section and across the MD 193 and Folly Branch structures due to the fact this lane will be used during peak hours only.

We are requesting a design exception to construct the proposed outside shoulder to a 6-foot width. The construction of a 4000-foot extension to the MD 197 acceleration lane will require widening beyond the existing edge of pavement. The provision of a new 10-foot shoulder throughout this length would both increase the side slope and require guardrail, or adversely impact the existing ditch.

#### EARTHWORK

Earthwork computations indicate approximately 17,730 CY of excavation will be required. That excavation includes unsuitable roadway material. Approximately 4,230 CY of additional borrow will be required.

#### RIGHT-OF-WAY

The proposed improvements will be constructed within the existing right-of-way and no additional right-of-way is required for this project.

#### UTILITIES

Final plans were forwarded to the District Utility Engineer and subsequently to the utility companies involved in the project. The utility companies report no utility conflicts with the project. The following utility companies have facilities located within the limit of work:

Washington Suburban Sanitary Commission  
Washington Gas & Light Company  
Baltimore Gas & Electric Company  
Potomac Electric Power Company  
Level 3 Communications

#### TRAFFIC CONTROL PLANS

A representative of the District 3 Traffic Office was sent the Final Review Plans. The Final Traffic Control Plans were reviewed at the Final Review meeting with representatives of the District 3 Construction Office. Excluding any mitigating circumstances, the Traffic Control Plans, supplemented by standard plates, will be included in the contract documents. As of this time, MOT approval has not been received from District 3 Traffic.

#### ENVIRONMENTAL EVALUATION

This project is classified as a Categorical Exclusion (CE).





### PERMITS AND LICENSES

A Joint Federal/State Permit is not required for this project, as all proposed work is above the 100-year storm flood elevation and does not include wetlands.

A Preliminary Stormwater Management Report was submitted to MDE in April 1996. Review comments (pertaining to both stormwater management and erosion/sediment control issues) received in March 1997 and November 1999 have been addressed. The response to the November 1999 comments was forwarded to MDE in early February 2000. We are awaiting MDE's approval of this latest response, so that permits for both stormwater management and erosion/sediment control can be issued.

### OTHER

The following items were also discussed at the meeting:

- There was a question about what would be done with the 22 (approximate) terminal joints that need to be fixed. Subsequent to the meeting it was decided that this issue would be handled by patching and there will be a specific patching detail included in the contract for patching of the terminal joints.
- The Administrator will approve the HOV operation and appropriate revisions will be made by the design team.
- There was concern about the time needed to perform patching and the time allowed because of lane closure restriction. The patching work will be limited to weekends.
- The median between STA. 314+50 and 325+10 will have low maintenance landscaping. This will consist of a 6" layer of topsoil and a 1' layer of furnished subsoil over borrow material. A 2" layer of mulch will be added to the top. In narrower median sections, decorative stone will be used. STV will incorporate the landscaping work into the contract.

### APPROVAL

In accordance with the Certification Operating Procedure, we are hereby requesting your concurrence in this Final Report. Your concurrence will constitute geometric design approval for the improvements shown on the Final plans with adjustments described in this report.

This report was prepared by Matthew Clark. Should you have any questions, please contact him at (410) 545-8765.

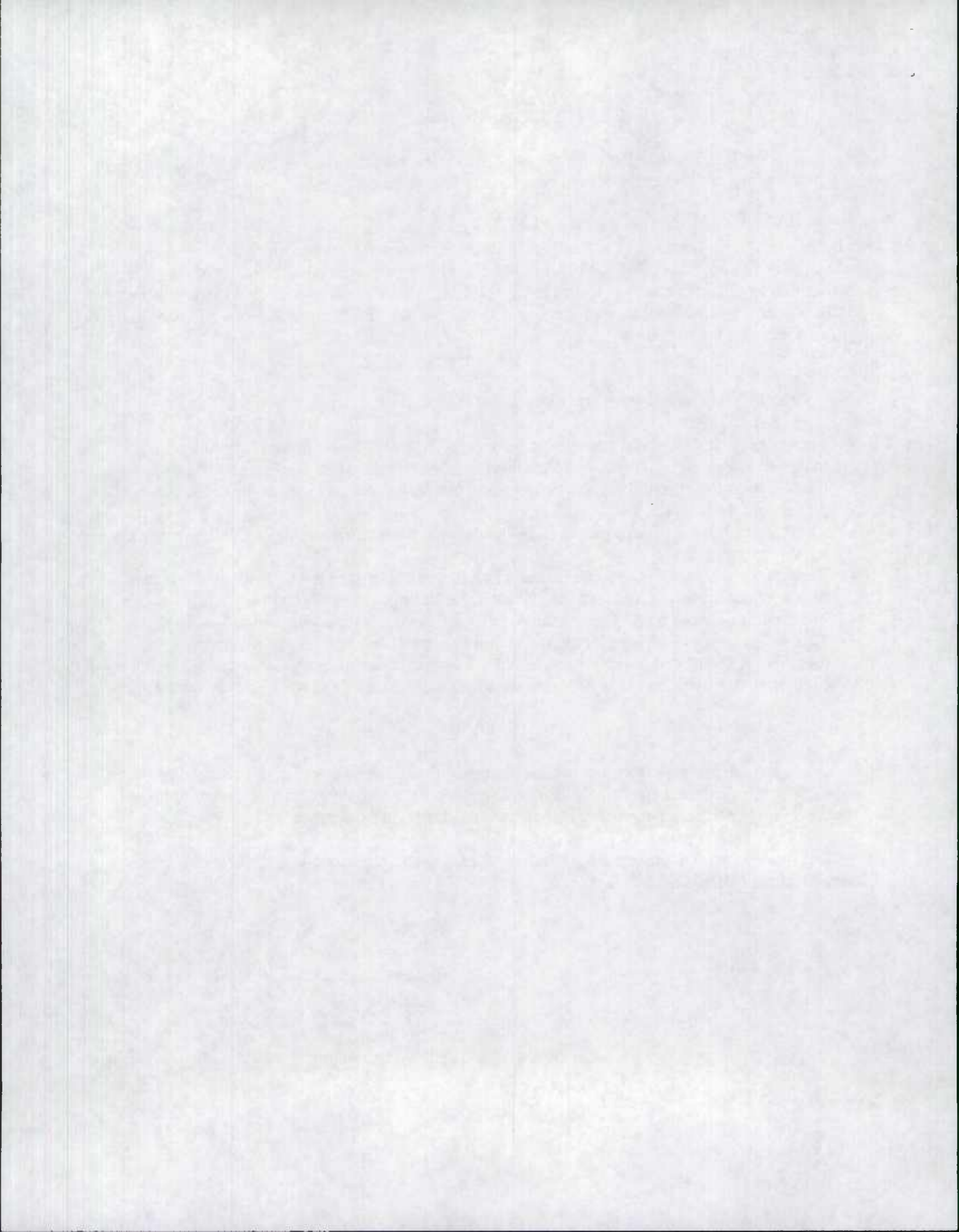
Approved

Date

3/24/00

  
Deputy Chief Engineer  
Highway Development

cc: All Attendees



Project Description: US 50 HOV – From East of MD 3 to West of MD 410

SHA Contract No. PG2005170

FAP No. CM-595-1(3)N

- |                                     |            |                                     |            |
|-------------------------------------|------------|-------------------------------------|------------|
| <input checked="" type="checkbox"/> | Expressway | <input type="checkbox"/>            | Rural Road |
| <input type="checkbox"/>            | Arterial   | <input checked="" type="checkbox"/> | Urban Road |
| <input type="checkbox"/>            | Collector  |                                     |            |

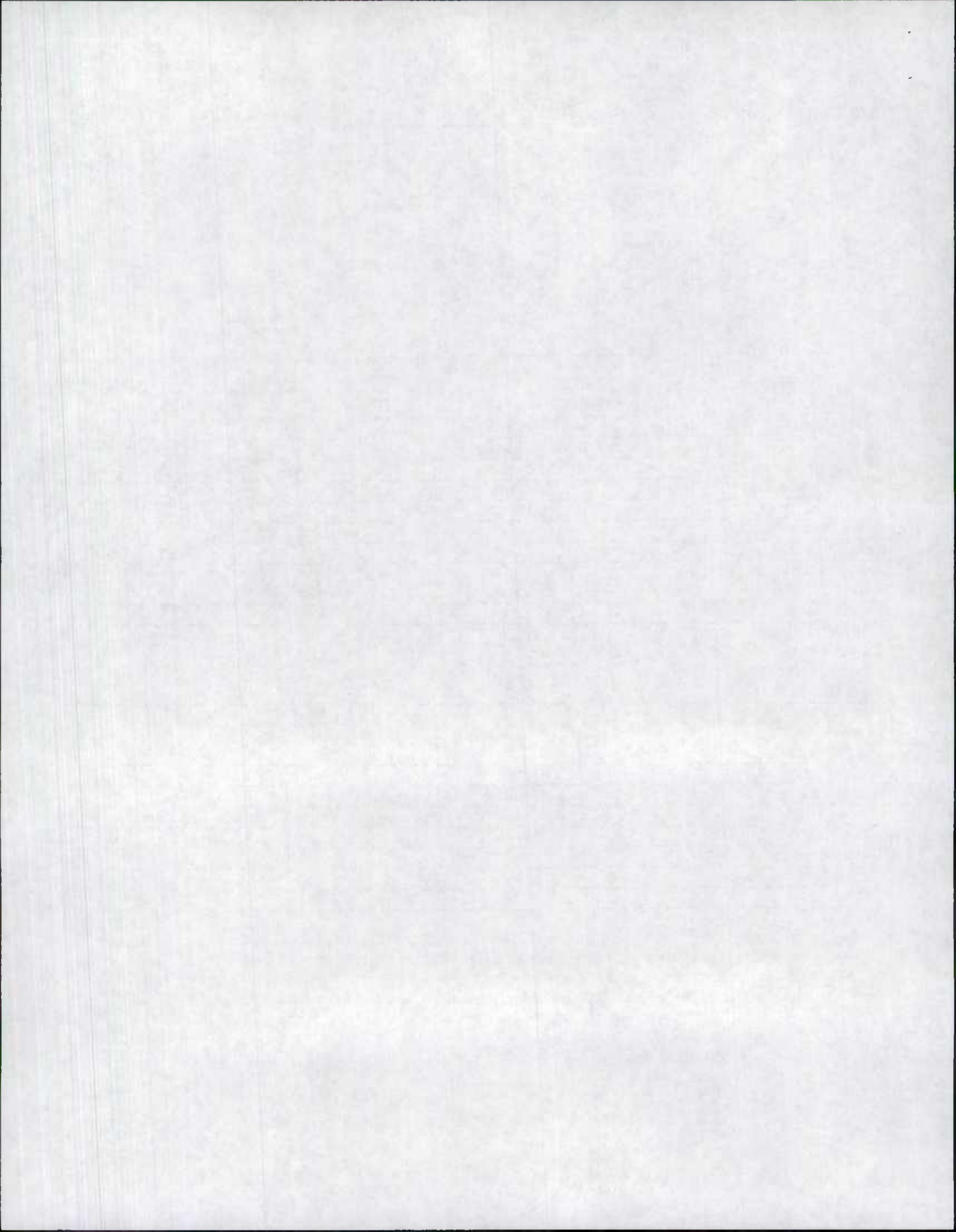
1. Design Data

DESIGN ELEMENTS	EXISTING CONDITION	PROPOSED DESIGN CRITERIA	**MEETS SHA/AASHTO DESIGN STANDARDS
ADT	96,875	133,900	YES
% Trucks (DHV)	5%	5%	YES
Design Speed	65 mph	65 mph	YES
Posted Speed Limits	55/65 mph	55/65 mph	YES
Number of Lanes	Varies	Varies	YES
Through-Lane Width	12 ft.	11 to 12 ft.	NO
Outside-Lane Width	12 ft.	12 ft.	YES
Shoulder Width Right	Varies	Varies	NO
Left	Varies	Varies	
Cross Slope	Varies	Varies	YES
Horizontal Alignments; Curvature*	NO	YES	YES
Superelevation*			
Sight Distance*			
Vertical Alignments Grades*	YES	YES	YES
Sight Distance*			
Bridge Clear Width	8' to 12'	3' to 8'	NO
Bridge Railings*	YES	YES	YES
Median Width	Varies	Varies	YES
Clear Zone Width	12' to 30'	4' to 30'	YES
Ditch Slopes (front/back)	6:1 to 2:1	6:1 to 2:1	YES
Culvert End Treatments*	YES	YES	YES
Guardrail*	YES	YES	YES

\* Indicate Yes or No, whether Existing Condition meet applicable standard. Indicate Yes or No, whether Proposed Design will improve existing condition. If both answers are no, the PI Report shall contain an explanation of constraints.

\*\* If criteria does not meet applicable standards, a design exception will be requested.





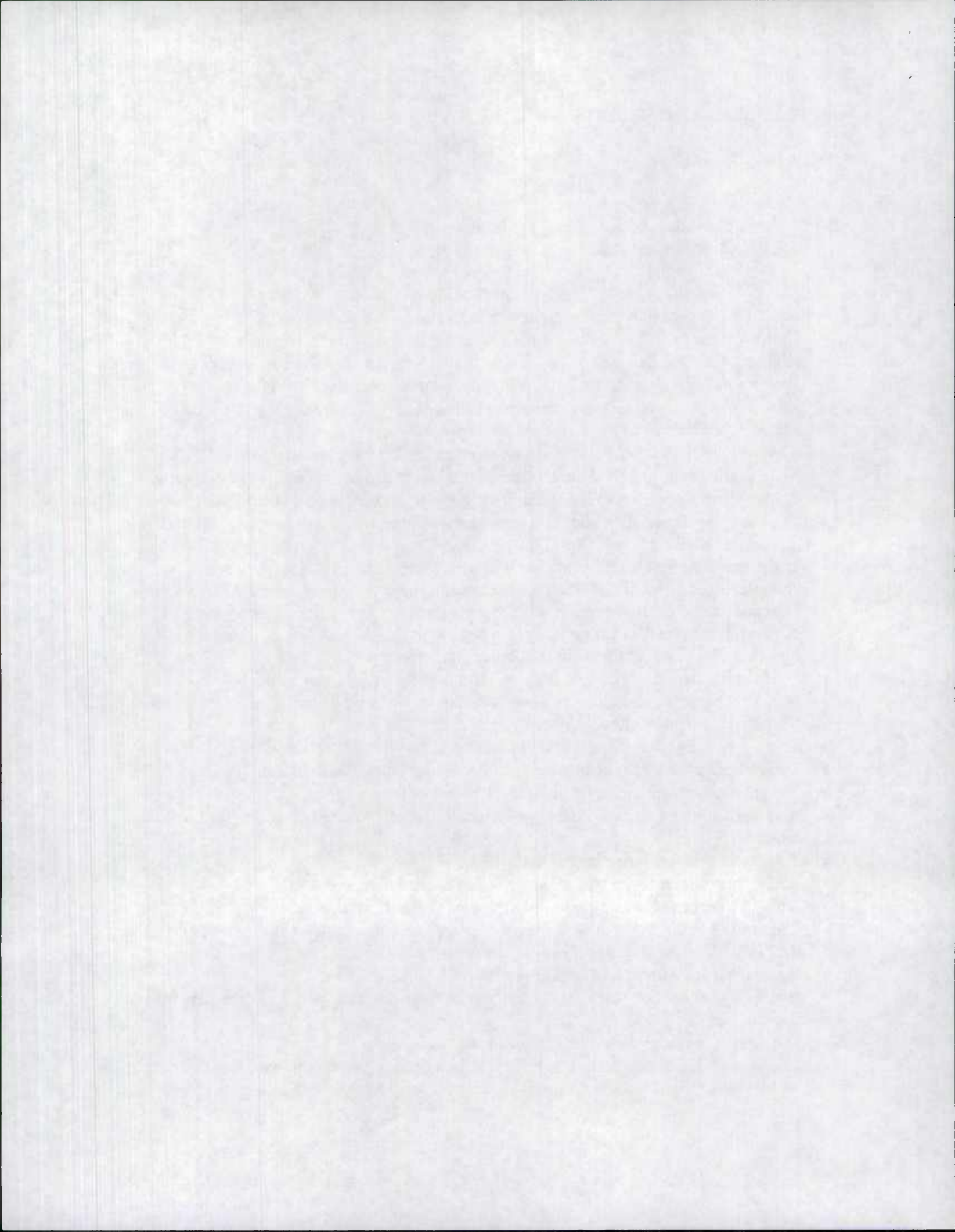


## MISCELLANEOUS COMMENTS

### GENERAL COMMENTS

- Under the revised schedule:

PS&E Submittal	May 2000
Advertisement Date	July 18, 2000
Bid Opening	August 24, 2000
Notice to Proceed	October 30, 2000
- An estimate needs to be sent to Neil Haines and Chris Brooks.
- Highway Design will verify that there is underdrain in the project estimate.
- Highway Design will add 4,000 LF of underdrain to the contract estimate for the acceleration lane extension at MD 197. Existing underdrain is to be abandoned.
- STV will look at as-builts to determine whether the I-95 ramp shoulder is traffic bearing and will forward this to Pavement for their approval.
- STV will check to see if Pavement has reviewed the Patching Detail on sheet DR-8.
- MDE is still reviewing the Storm Water Management and Sediment Control response.
- A Joint Permit Application is not required. Page 40 of the IFB will be revised.
- The book should include notice that noise wall work may be ongoing along US 50 during this project construction.
- There was concern about leaving the underdrain in place. The concern was that it might collapse especially with all the construction machinery running over it. The Office of Materials and Research feels it will be ok and that there is enough underdrain in the contract in case there is a need to replace some of it.
- More detail is required for ground mounted sign supports.
- Steel details should be added to the pavement details.
- Add a note that the contractor is to be provided with a copy of the as-builts of the recent fiber-optic line installation.
- There is nothing concerning RPM's in the specs on construction.
- There needs to be signature box for Tom Hicks on signing and marking.
- Sawcut locations should be on plans and typical sections.
- Add a note to the typical for existing shoulder removal to stabilize with topsoil, seed and mulch.
- Add a note that the boring data is in the IFB.
- Add a note to refer to the IFB for sequence of construction.
- It may be necessary to add contours to the E&SC plan for Ramp H.
- There was a question about 57 stone being placed near pipes and the stone settling. Chris Brooks will follow up to see if hydraulics has any ideas about this.
- Hydraulics will verify that aluminized pipe is acceptable.
- Hydraulics mentioned putting an E&S note on the plans that references the sequence of construction.
- The asphalt top layer should be surface instead of base.
- STV will investigate whether it is better to place drainage system down the middle of the median rather than on the NB side and tearing up shoulder.

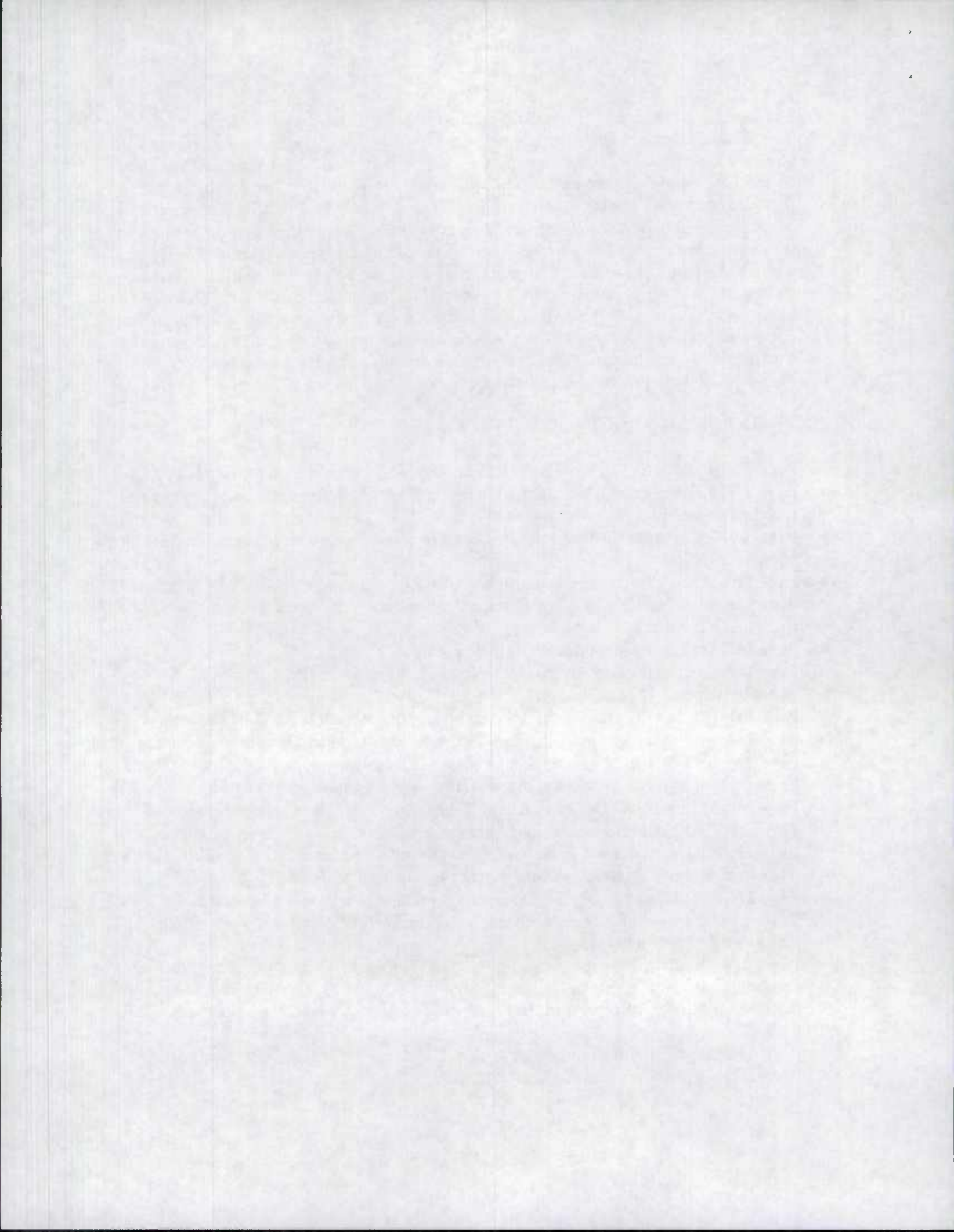




- The 6 inch wide pavement markings shown in the details are temporary tape for maintenance of traffic, since there is no 5 inch wide tape. All permanent markings will be either 5 inch or 10 inch as recommended. Regarding the HOV Diamond symbol, there is a debate as to whether these should be used due to the part-time HOV/part-time shoulder operation. The Office of Traffic & Safety will provide a final recommendation.
- The storm drain design is currently being modified to include the replacement of much of the existing median barrier as well as a shift in the longitudinal pipe. This shift will place it much closer to the barrier and thus out from under the vehicle wheel track. Based on discussions with Highway Hydraulics regarding the installation of Drainable Barriers, it was determined early in the project design that they would not be utilized. They were eliminated primarily due to their poor hydraulic capacity through the trunk line within the barrier. This trunk line is hydraulically limited by pipe size and by the roadway slope. This is particularly problematic in sump areas.

### INVITATION FOR BIDS BOOK

- **Page 72**-The dates for "Reach The Beach" and Redskin games should be included
- **Page 87**-The estimate will be modified to be consistent with the methods of payment as specified in the SPI.
- **Page 107**- Questions included for underdrain connection. Is an item needed for underdrain outlet?
- **Page 129** - There was concern that grinding CRCP would expose or adversely impact the reinforcement of the facility. The Pavement Division will be asked to determine if this will cause a problem.
- **Page 147**-Asphalt Cement price needs to be updated.
- **Page 149**-Construction would like clarification on the note for HMA section of roadway where the depth is 10" to 12".
- **Page 161**-Traffic will check to see if the Epoxy pavement markings will be ok to use.
- **Page 264**-The top layer of asphalt is listed as base. It should include a surface course.
- **Page 266**-Show Existing Longitudinal Underdrain and New Longitudinal Underdrain. Show graded aggregate base under the new HMA as new graded aggregate base.
- **Page 268**-Ask Pavement about this detail. There also needs to be a reference legend.
- **Page 273**- There was concern about having a 6' shoulder match up against a 10' shoulder. This matter arose in an earlier review and was examined. The decision was made to keep the 6' shoulder. A warning sign should be considered.
- **Page 274**-Construction would like to see a page number reference for General Note #3
- **Page 275**-Construction under Sequence of Construction Note #9 work should be performed before Note #7.
- **Page 276**-Construction had a concern on MOT General Note #9 stating that the contractor has to get approval for staging areas. So do we have to note he will have to pay for construction entrances, silt fences, or other devices to maintain his site, or should we pay?
- **Page 276**-Construction recommended noting SHA drop-off policy must be maintained. Payment is incidental in Class I Excavation area.

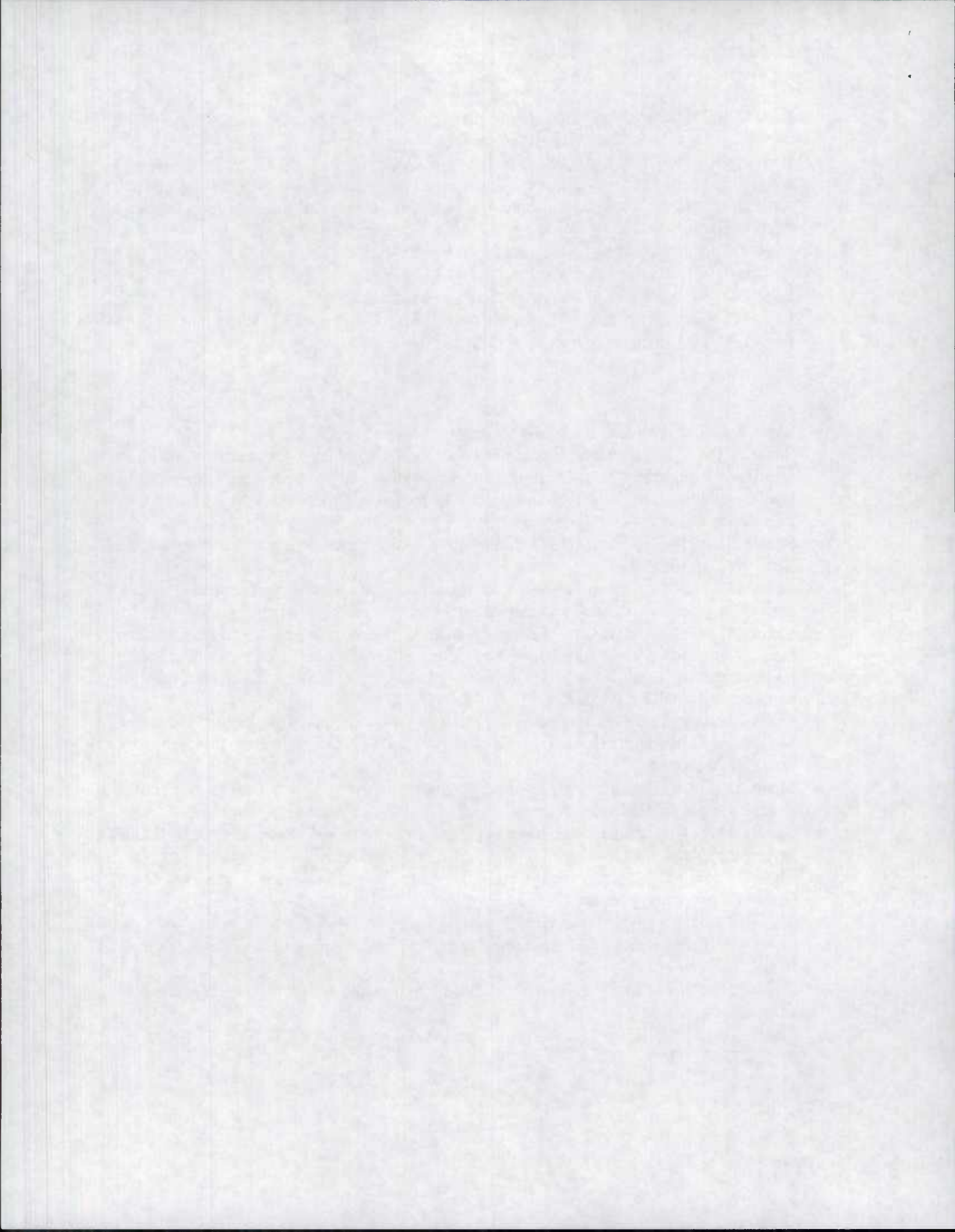




- **Page 277**-Construction noted under Sequence of Construction that note #7 should be moved before note #5. Patching should be done before overlay.
- **Page 280**-Construction had concern on the construction of the 4' Shoulder. The first was note #2 protect existing fabric and if the fabric is bonded to the pavement the existing underdrain may be damaged. Second was note #6, seeding and mulch topsoil within 2 days, but it may conflict with E & S requirements to stabilize within 24 hours.
- **Page 280**-Construction had concern with full and partial depth patching needing to take place before overlaying.
- **Page 292**- Structural verification is needed for sign structures.
- **Page 321**-Specifies Modified Mix 6. Where is it specified what the modification is?
- **Page 327**-HMA detail should show the HMA edges at an angle.

## PLANS

- **Sheet 2**-Seed and Mulch on typical section.
- **Sheet 4-(DR-1)** Construction recommends noting the patching / replacement quantities for all work on these sheets. There is a concern with the storm drain interfering with the bridge pier at station 314+00 RT and if it can be moved to the center of the median it may be cheaper.
- **Sheets 6 and 7-(DR-3 and DR-4)** Construction recommends noting and calling out the barrier replacement.
- **Sheet 11 – (DR-8)** Remove the word “continuous” from the pipe typical section. Also, revise this section so that arrows designating the sawcut are show correctly.
- **Sheet 11 – (DR-8)** “Saw Cut Existing Pavements” leader pointers should be shifted to the saw cut area. A notation should be added to the Typical Section, with a leader pointing to the longitudinal tie devices, advising the contractor to refer to the appropriated detail for the device.
- **Sheet 12**-Construction questioned how the overlay would be placed without it separating to aggregate. Assuming that two levels of paving will be used, show how this will work on a typical section.
- **Sheet 12-(DR-9)** Construction recommends adding a pavement detail of the wedge and change the current HMA mixes on the shoulder.
- **Sheets 13&14**-Hydraulics will have someone from the SHA Shop Drawings Group review the inlet details.
- **Sheet 13-(S-1)** Construction has a concern of drainage after installing the new inlets before final overlay is placed.
- **Sheet 15** - Need a pavement legend.
- **Sheet 15**- Construction suggests taking cores of existing pavement prior to shifting traffic.
- **Sheet 15**- It appears to be better to build Phase I to baseline and all the shoulder could be constructed in Phase II.
- **Sheet 21-(SN-5)** Construction recommends changing the note “FEATHER OVERLAY” to “TRANSITION OVERLAY”.



STATE HIGHWAY ADMINISTRATION OF MARYLAND

County Prince Georges

REQUEST FOR FIELD INVENTORY

EXTRAS

Sheet \_\_\_ of \_\_\_

(1) Imp. Number	(2) Road Number	(3) Road Name	(4) General Limits of Inventory Required				(5) Map Designation			(6) Previous Inventory Attached?
			From		To		Map Name or Number	Coordinates		
			Rd. Number	Road Name or Termini Description	Rd. Number	Road Name or Termini Description		X	Y	
		MD450 A								
Special Instructions: Verify state maintenance before 2001 improvement cycle.										
		MD 704 A								
Special Instructions: Verify state maintenance (see above) Project Engineer: Murty Cohn 8901 Contract # PG 900 5171 / PG 900A24										
		MD 382		@ 0.87						
Special Instructions: Access Permit: PG 5897-95-completed, road not complete check: Poplar Ridge Rd - state extra										
		MD 450		@ M.P.: 7.71						
Special Instructions: LAKEVIEW LA, is this road private? if public GPS, Inventory & Item Access Permit PG: 7169-99 - 2002										
Special Instructions:										
Special Instructions:										
Special Instructions:										
Special Instructions:										
Special Instructions:										



Called Marty Cohn 5901  
(Project Manager)

4-9-01

Called: Raleigh Medley  
301 513 7304  
District 3 Asst DE Maint  
4-9-01



CONTRACT # PG 900 5171  
PG 900A24

Are MD 450 A & MD 704A still  
state maintained? Check the  
plans for maintenance, or transfer  
agreements for these sections.

450

BALTIMORE  
ANNAPOLIS

BUENA VISTA

SELTZER RD

LOTTSFORD RD  
VISTA RD

MD 450 A

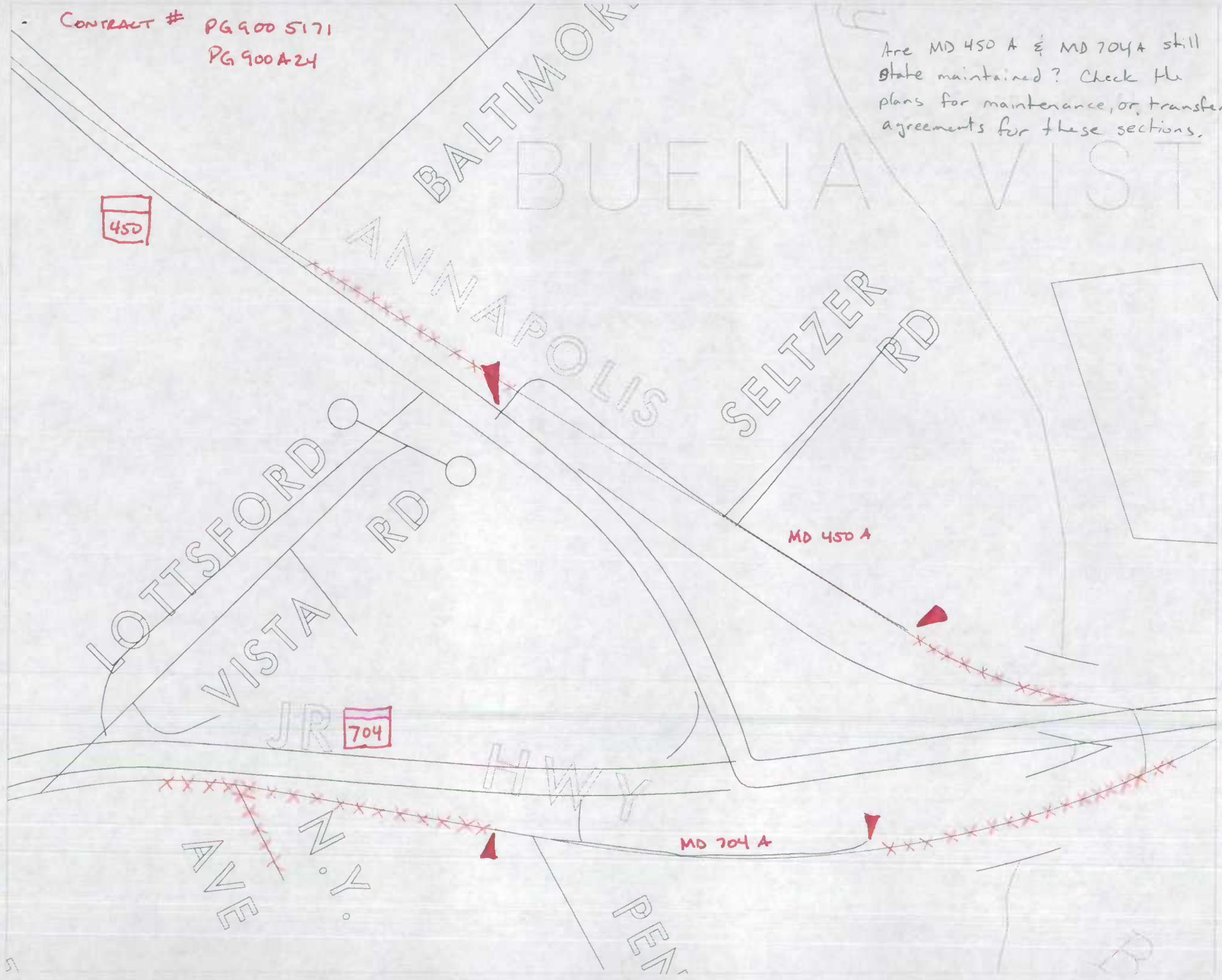
JR 704

HWY

MD 704 A

AVE  
N.Y.

PEE



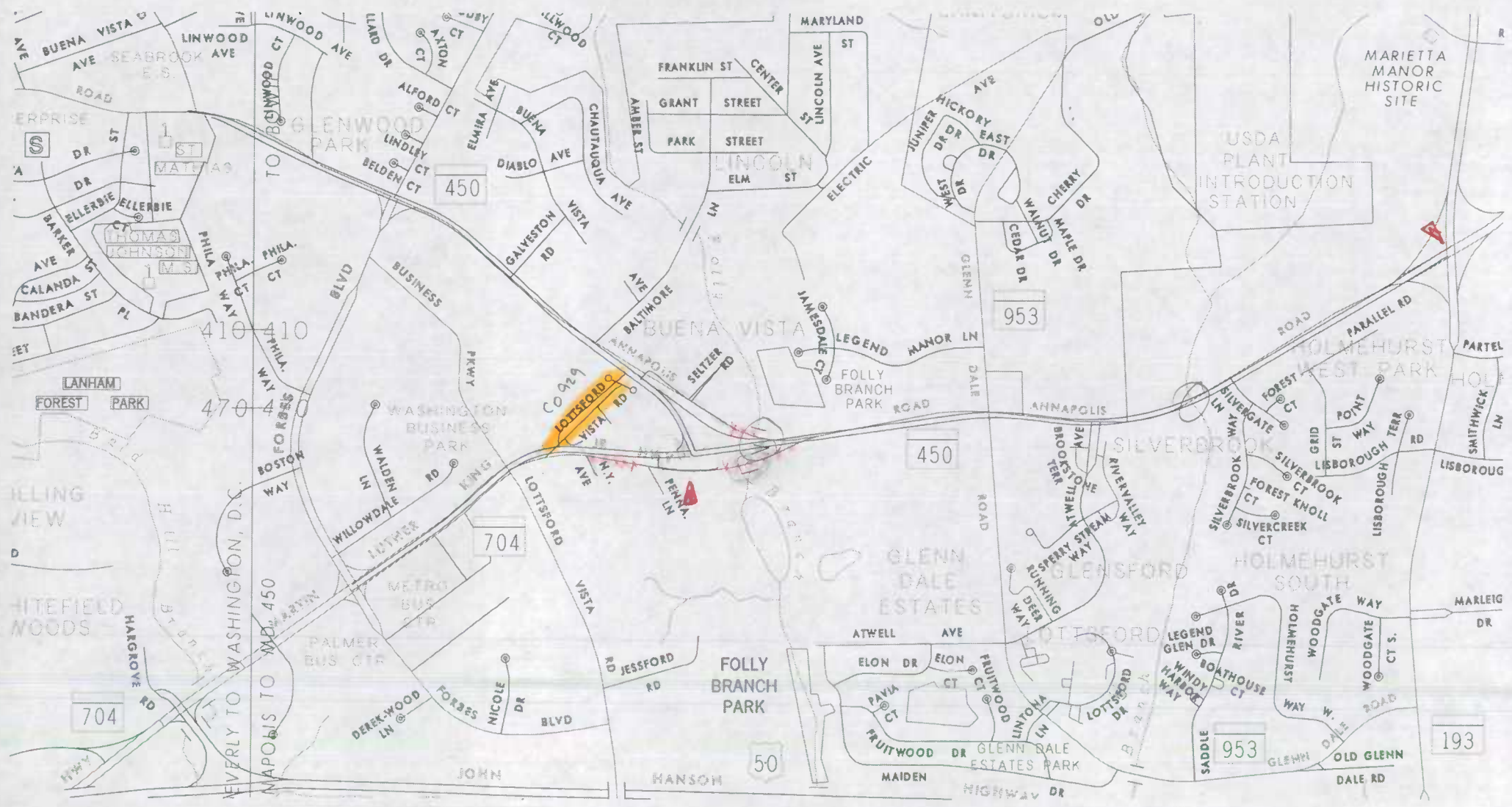
1887

1887

1887

1887

1887



MARIETTA MANOR HISTORIC SITE

USDA PLANT INTRODUCTION STATION

HOLMEHURST WEST PARK

GLENN DALE ESTATES

HOLMEHURST SOUTH

FOLLY BRANCH PARK

GLENN DALE ESTATES PARK

BUENA VISTA AVE  
SEABROOK AVE  
LINWOOD AVE

ERPRISE DR  
BARKER DR  
ELLERBIE CT  
CALANDA ST  
BANDERA ST

LANHAM FOREST PARK

WILKING VIEW  
WHITEFIELD WOODS

704

GLENGWOOD PARK

BUSINESS BLVD

WASHINGTON BUSINESS PARK

PALMER BUS CTR

JOHN HANSON

450

704

FRANKLIN ST  
GRANT ST  
PARK ST

LINCOLN ELM ST

BUENA VISTA AVE

PERNA LN

HANSON

MARYLAND ST

CENTER ST

ST LINCOLN AVE

JAMESDALE CT

450

ATWELL AVE

MAIDEN HIGHWAY DR

953

450

ELON DR

50

HICKORY AVE

JUNIPER DR

WEST DR

W. WALKER DR

CHERRY DR

MANOR LN

DALE RD

ANNAPOLIS RD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

ROAD

953

450

LOTTSFORD

953

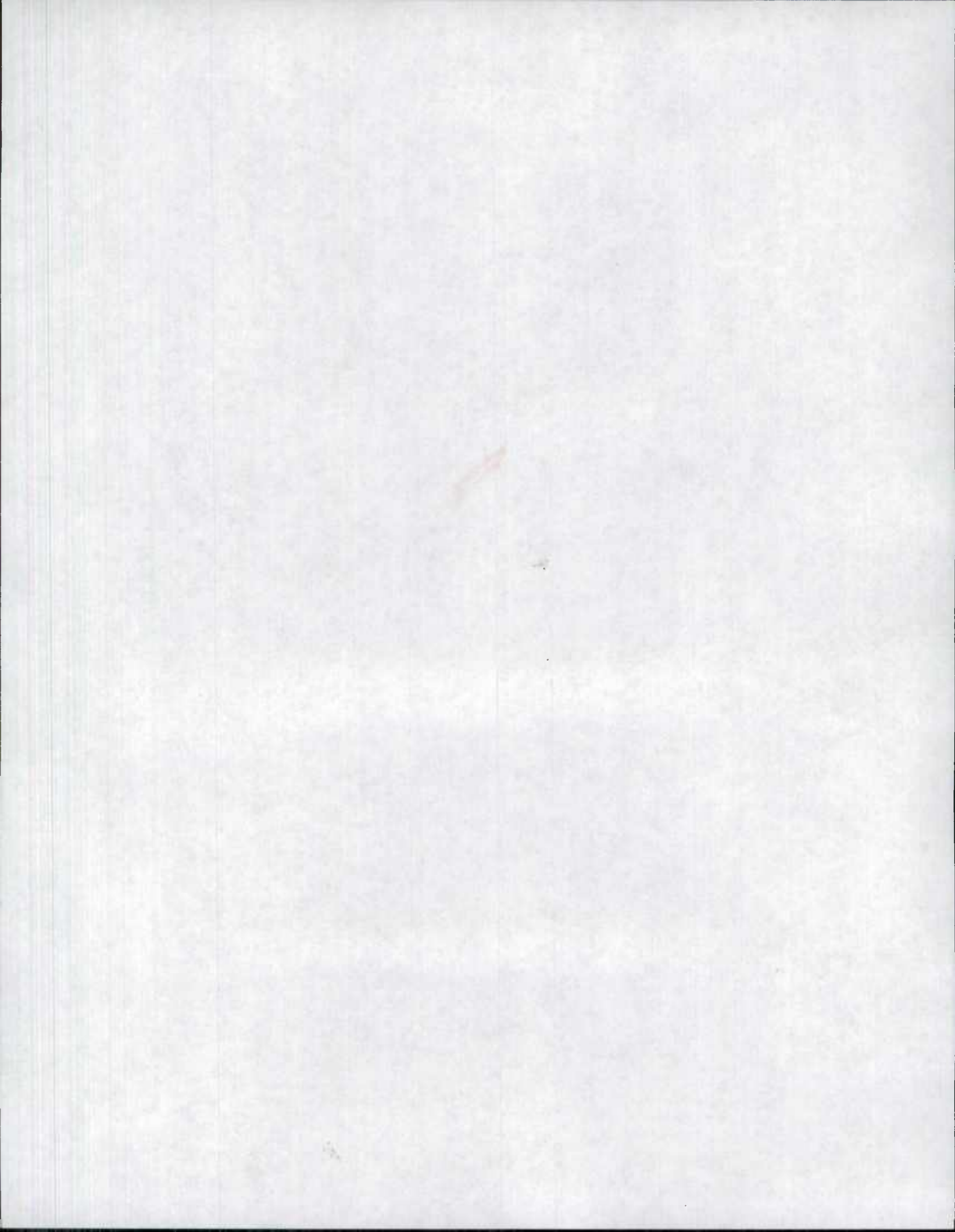
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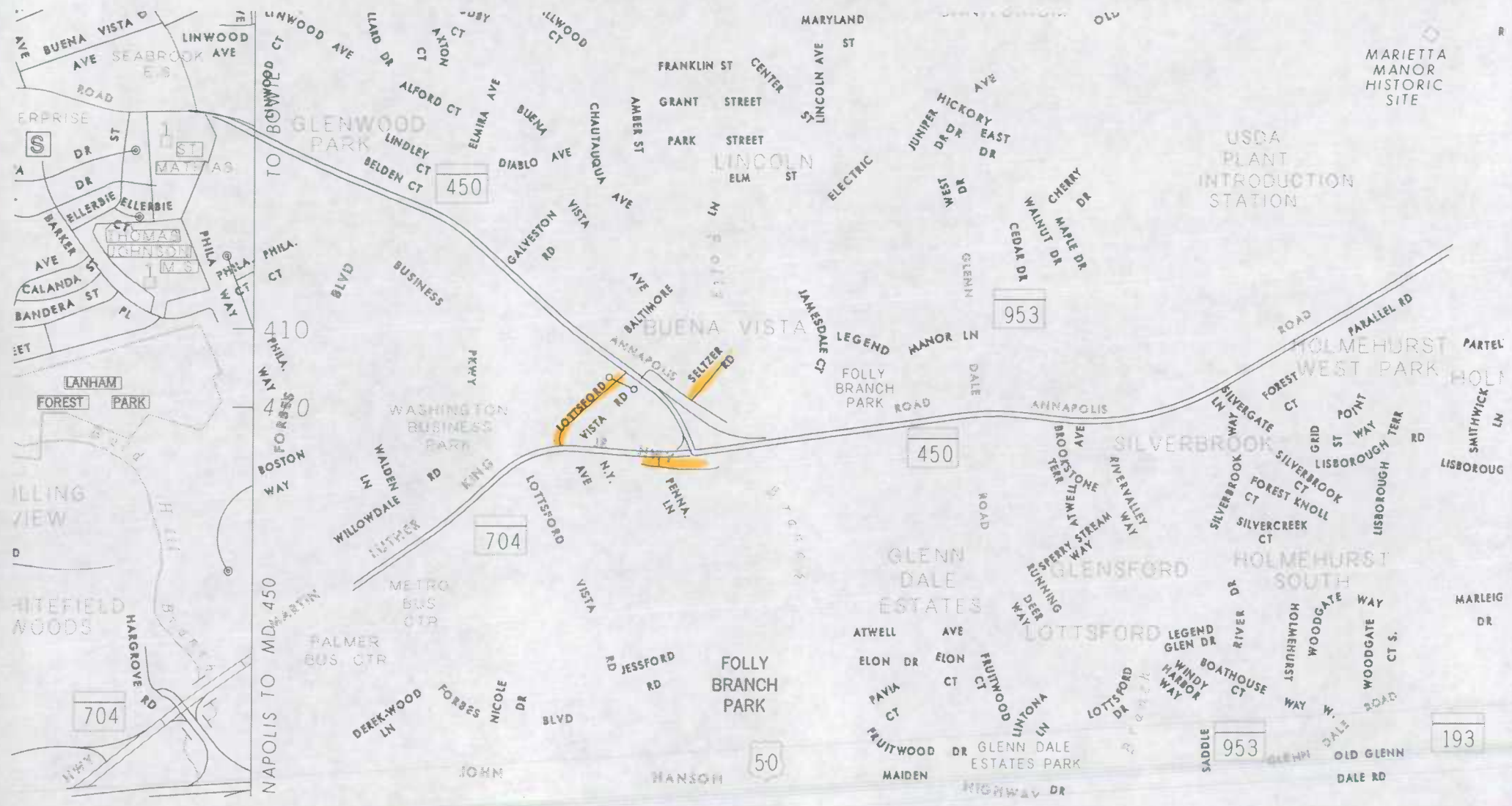
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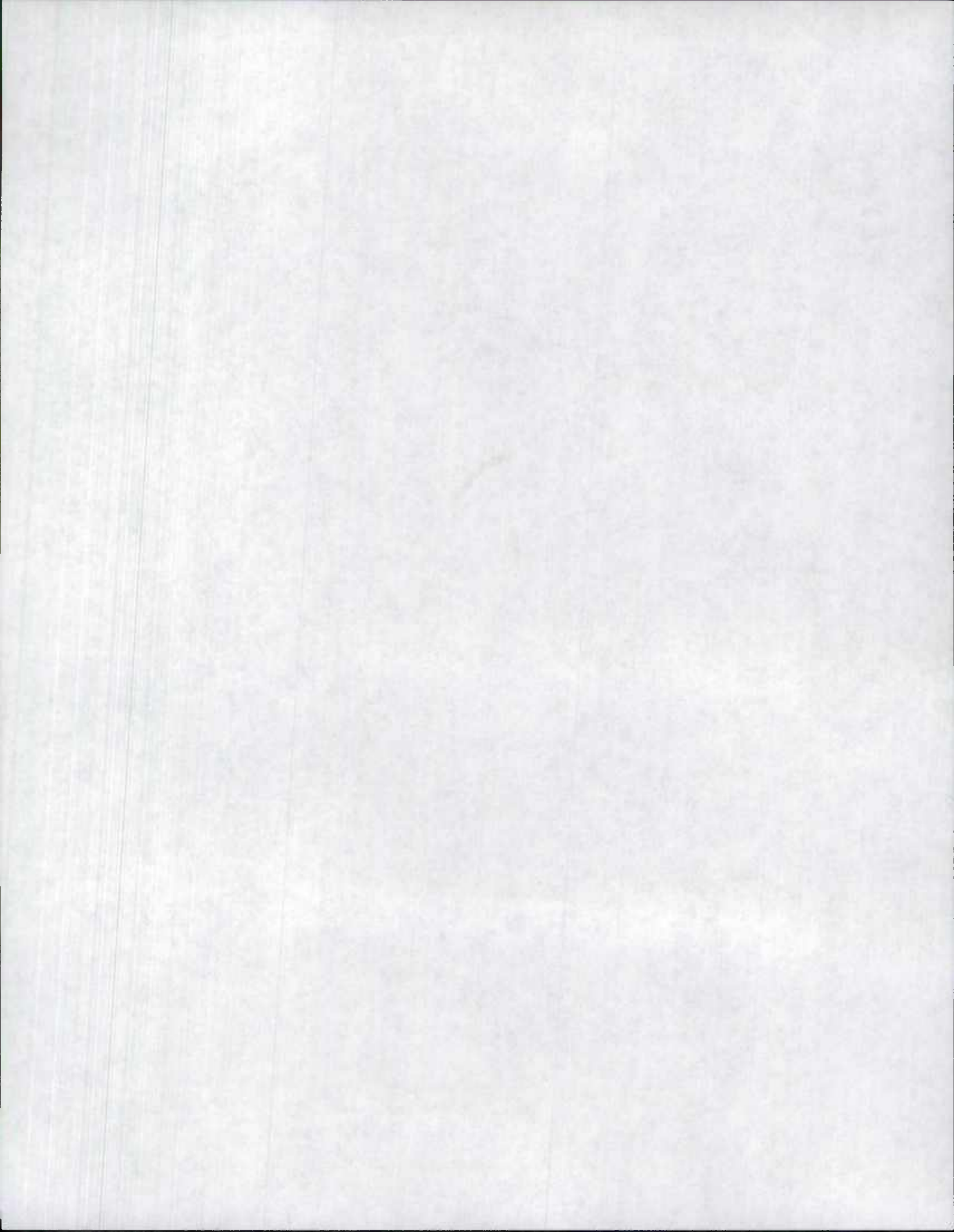
953

193



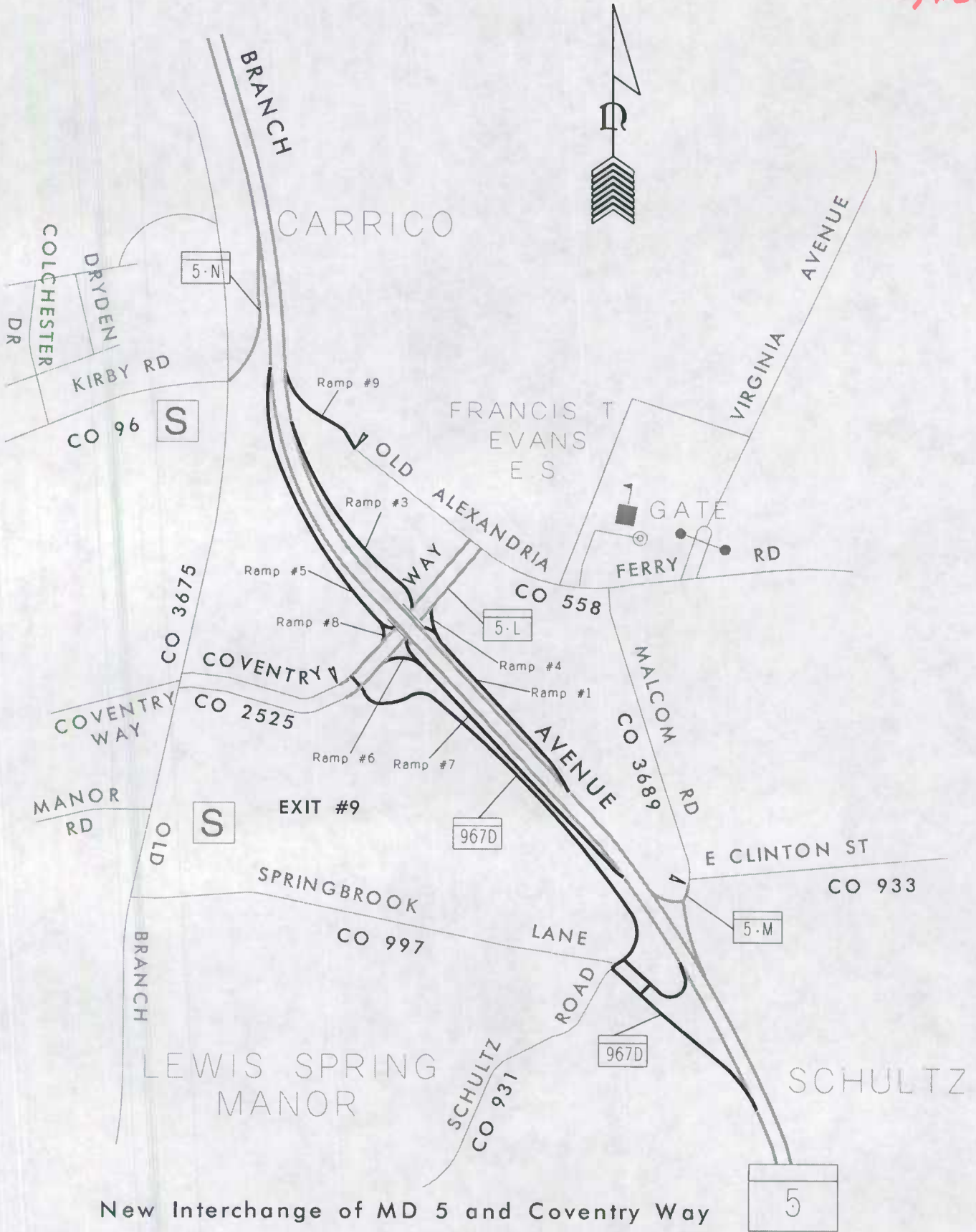




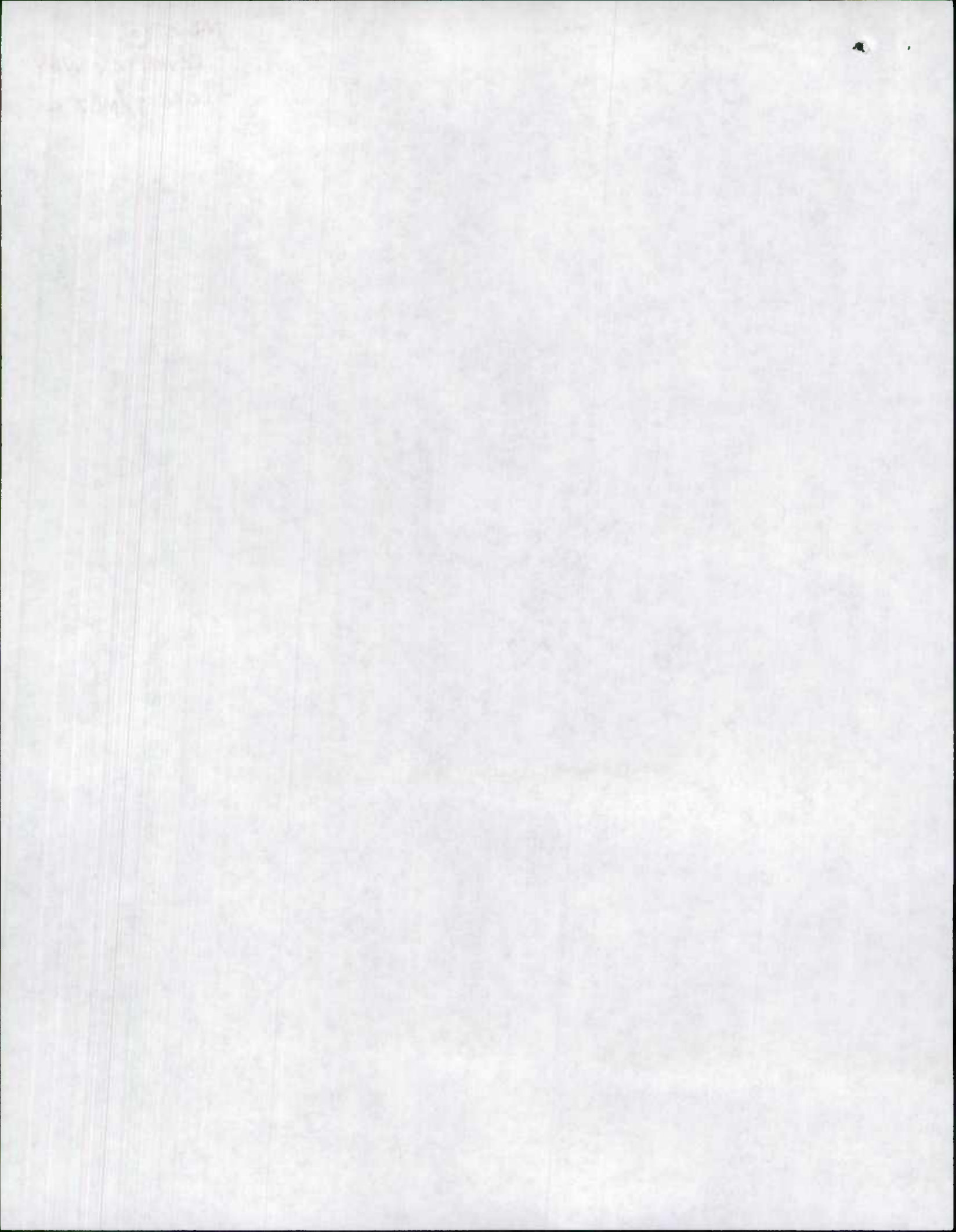




MD 5 @  
COVENTRY WAY  
CO 2525 / MD 5 L



New Interchange of MD 5 and Coventry Way





ROSECRANS AVE  
COLCHESTER  
DRYDEN  
KIRBY RD

CARRICO

VIRGINIA AVENUE

FRANCIS T EVANS E S

BRANCH WAY

GATE

COVENTRY

OLD DR HEFLIN

ALEXANDRIA

AVENUE

MALCOM RD

FERRY AVE

ARRON LN

5

EAST CLINTON ST

MANOR RD

S

SPRINGBROOK

CO 997

LANE

JOHN SAM VISMANCO LN

BLVD

OLD

CIRCLE MANOR DR

LEWIS SPRING MANOR

CO 931

179970

FRIENDSHIP RD

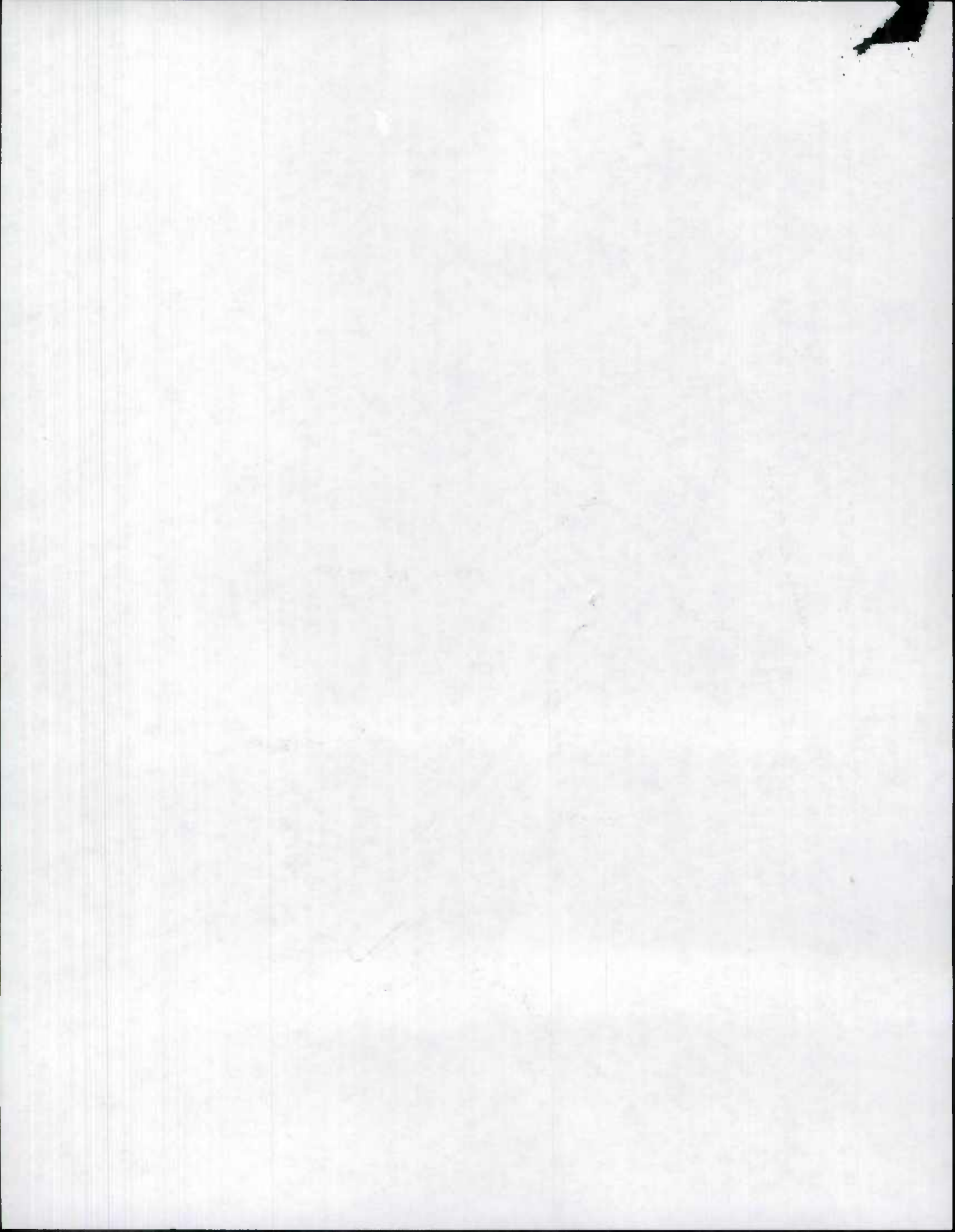
SCHULTZ WAY

ST

ROAD SCHULTZ RD

TT





# HISD: DATABASE MANAGEMENT SECTION

## CODE SHEET

COUNTY: Prince George's

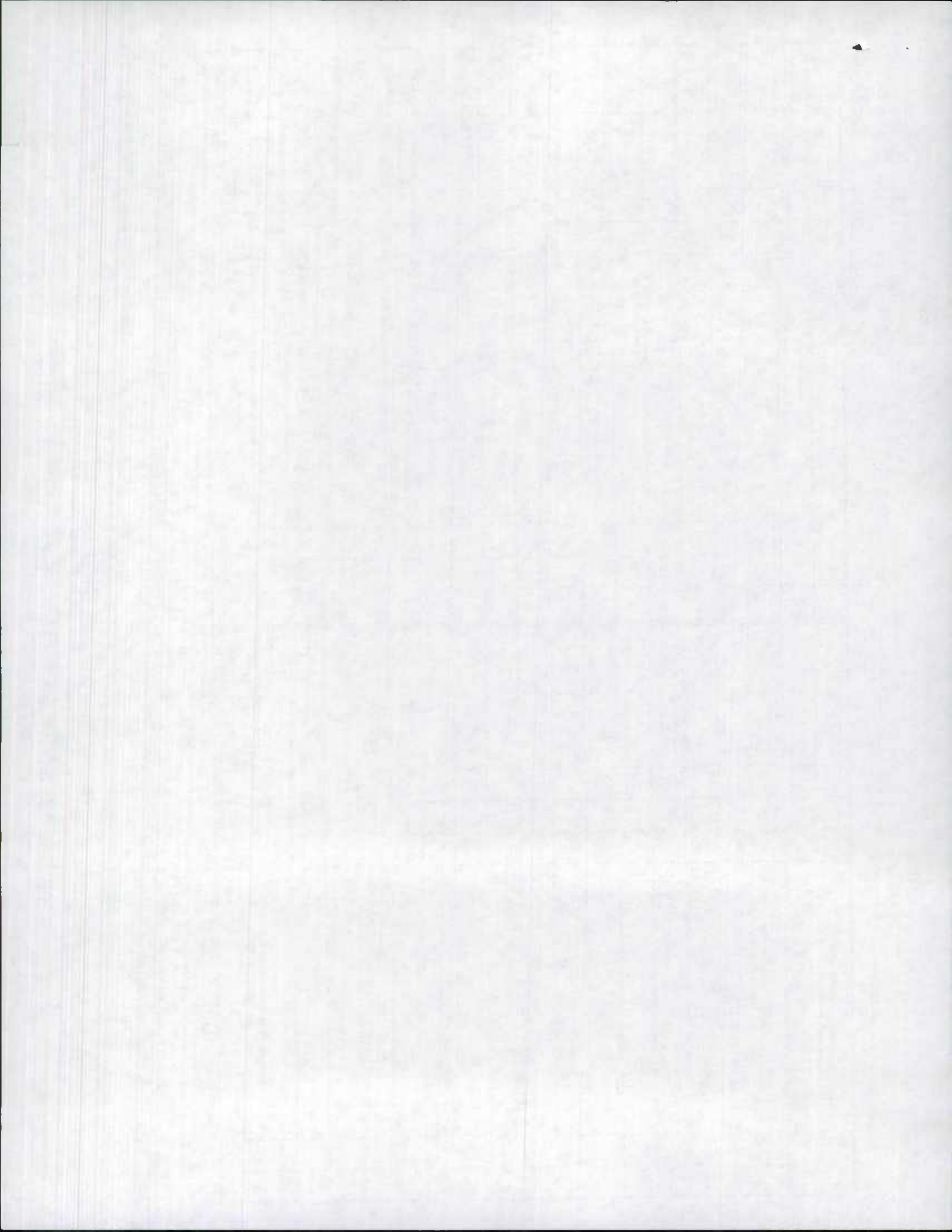
STATE   
 COUNTY   
 MUNICIPAL

CHECK:  19 99 IMPROVEMENTS  
 19 \_\_\_\_\_ RE-INVENTORY  
 \_\_\_\_\_ MISC. FILE UPDATES

SHEET 1 OF     

IMP NUM	ROUTE NUMBER	CODE			REASON	CODED			IMP NUM	ROUTE NUMBER	CODE			REASON	CODED		
		I	M	H		I	M	H			I	M	H		I	M	H
99-1 ST	MD 202	✓	✓	✓	Remove accel La	✓	✓	✓	99-4 ST EXIT 9	RAMP 1	✓	✓	✓	New		✓	✓
99-2 ST	MD 450				Under construction					RAMP 3	✓	✓	✓	"		✓	✓
99-3 ST	MD 410	✓	✓	✓	T/W (Turnala, spurs)	✓	✓	✓		RAMP 4	✓	✓	✓	"		✓	✓
"	MD 500	✓	✓		Add spur @ 217	✓	✓			RAMP 5	✓	✓	✓	"		✓	✓
	MD 410A	✓	✓	✓	Delete spur @ .23 adjust LTL mileage	✓	✓	✓		RAMP 6	✓	✓	✓	"		✓	✓
99-4 ST	MD 5	✓	✓	✓			✓	✓		RAMP 7	✓	✓	✓	"		✓	✓
"	MD 967D	✓	✓	✓	T/W/L CHANGES, New ties		✓	✓		RAMP 8	✓	✓	✓	"		✓	✓
										RAMP 9				Intersection reconstruction @ COSSY & MD 5			
	MD 5 L	✓	✓	✓	COVENTILY WAY: rebuilt by state, Now MD 5 L to MP .123												
	CO 2525	✓	✓	✓	Now State maintained fr MP 0 → 0.23					MD 5 M/ CO 3689	✓	✓	✓	Intersection reconstr. road rebuilt fr MP 0.34 → 0.43 Now MD 5 M			
	CO 2525	✓	✓	✓	T/W/L Changes: road rebuilt, new interchange @ MD 5, New Ramps & ties		✓	✓									
	CO 558	✓	✓	✓	Intersection @ MD 5 reconstructed Road shortened		✓	✓		MD 5 N	✓	✓	✓	was spur to Kirby rd; road is to large to be a spur. New road		✓	✓
	CO 931	✓	✓	✓	Shorten length, adjust ties			✓									
	CO 997	✓	✓		ADJUST TIES			✓									

CODE BY: INVENTORY, TIE-INS, MAP UPDATE  
MASTER FILE UPDATE  
HPMS UPDATE





Maryland State Highway Administration  
Highway Information Services Division

PG 3915370

4

Data Collection Team  
Road Inventory Log

Date 2/15/00  
Page 1 of 1

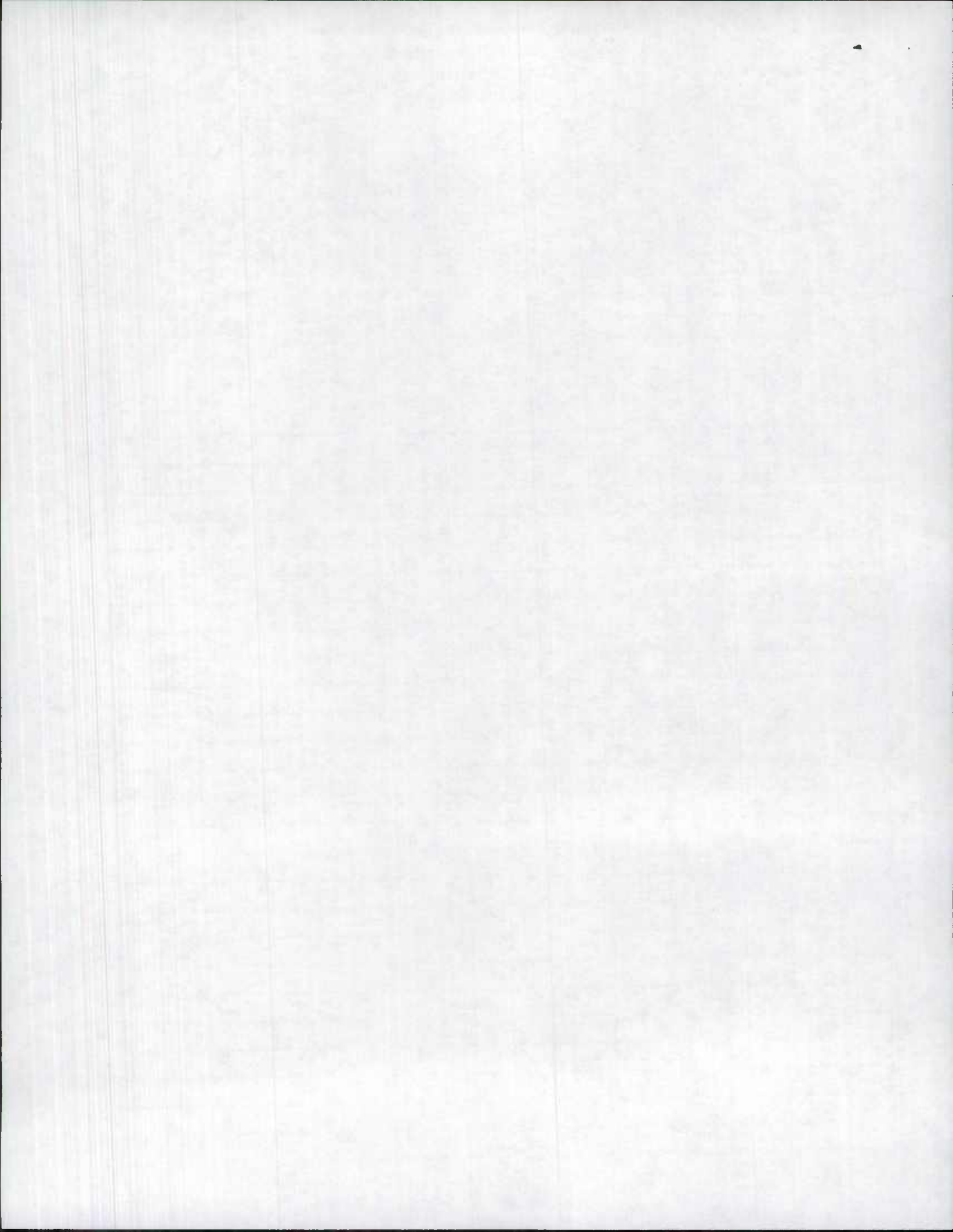
Crew Chief GD  
Assistant FF  
County PG  
Remarks: 99 IMPMTS

Odometer      Time  
Start of the Day 63615      6  
Begin Days Work 63721      955  
End Days Work 63739      240  
End of the Day 63833      455  
Total Mi. Travelled 223

RELOCATION OF MD 5  
+ INTERSECTIONS / TIES

Total No. of Improvements 6  
Total Mi. of Road Inventoried 3.42  
Total GPS 3  
Total A/T 0  
Total Extra 0  
Total Tie-in 1

No.	Improvement	Length	GPS	A/T	Extra	Tie-in	Remarks
1	99-4 ST	1.65					MD 5 (MP 8.35 - 10.07) T/W SNI
2	"	.74	✓				CO 931 "SCHULTZ RD" SHORTENED - SNI
3	"	.00				✓	CO 997 TIE-IN
4	"	.49	✓				SNI (BRANCH AVE SERVICE RD)
5	"	.19	✓				MD 967 D T/W/L CHS
6	"	.35					SNI T/W/L CHS
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Maryland State Highway Administration  
 Highway Information Services Division  
 Data Collection Team  
 Road Inventory Log

4  
33

Date 2/16/03  
 Page 1 of 1

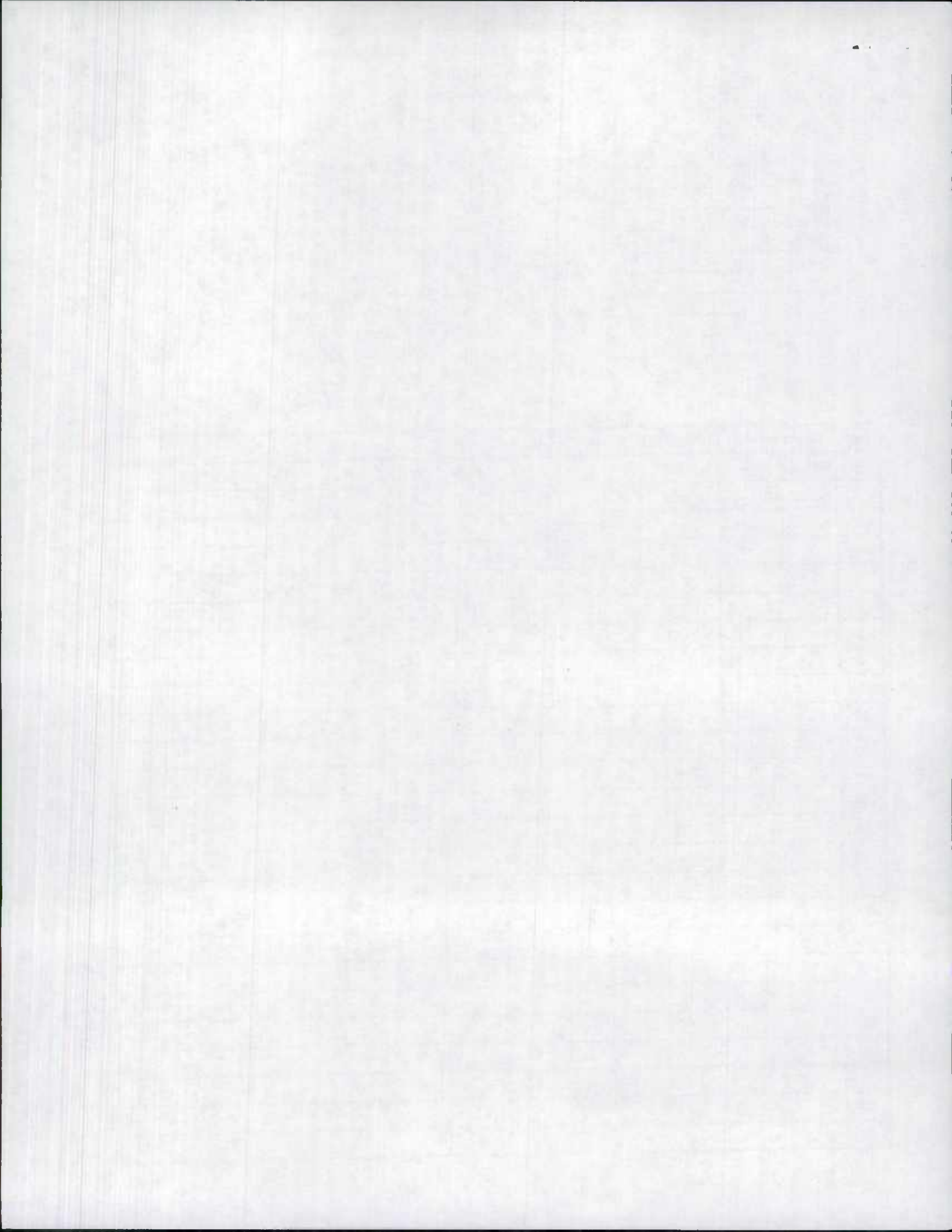
Crew Chief GD  
 Assistant EF  
 County PG  
 Remarks: 99 IMP.  
MD 5 RELOCATION  
INTERSECTIONS + TIES

Odometer  
 Start of the Day 63838 Time 5:45  
 Begin Days Work 63950 9:15  
 End Days Work 63975 2:30  
 End of the Day 64073 5:00  
 Total Mi. Travelled 235

Total No. of Improvements 15  
 Total Mi. of Road Inventoried 4.00  
 Total GPS 14  
 Total A/T 0  
 Total Extra 0  
 Total Tie-in 0

No.	Improvement	Length	GPS	A/T	Extra	Tie-in	Remarks
1	99-4 ST	1.75	✓				T/W/2 CHGS CD 558 SI
2	99-4 23 ST	.52	✓				T/W/2 CHGS CD 2525 SNI
3	99-4 ST	.12					T/W CHGS CD 3675 SI
4	"	.14	✓				KIRBY RD SPUR
5	"	.07	✓				SCHULTZ RD SPUR
6	"	.08	✓				MALCOM RD SPUR
7	"	.04	✓				" " "
8	"	.09	✓				OLD ALEXANDRIA FERRY RD SPUR
9	MDS/COPIES RAMP	1	✓				SNI
10		3	✓				"
11		4	✓				"
12		5	✓				"
13		6	✓				"
14		7	✓				"
15		8	✓				"
16							
17							
18							
19							
20							
21							
22							
23							
24							
25							





Maryland State Highway Administration  
Highway Information Services Division

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Data Collection Team  
Road Inventory Log

Date 2/17/00  
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Crew Chief GD  
Assistant EF  
County PG

Remarks: AM - C SERVICE  
HANDOVER - HORN REPAIR

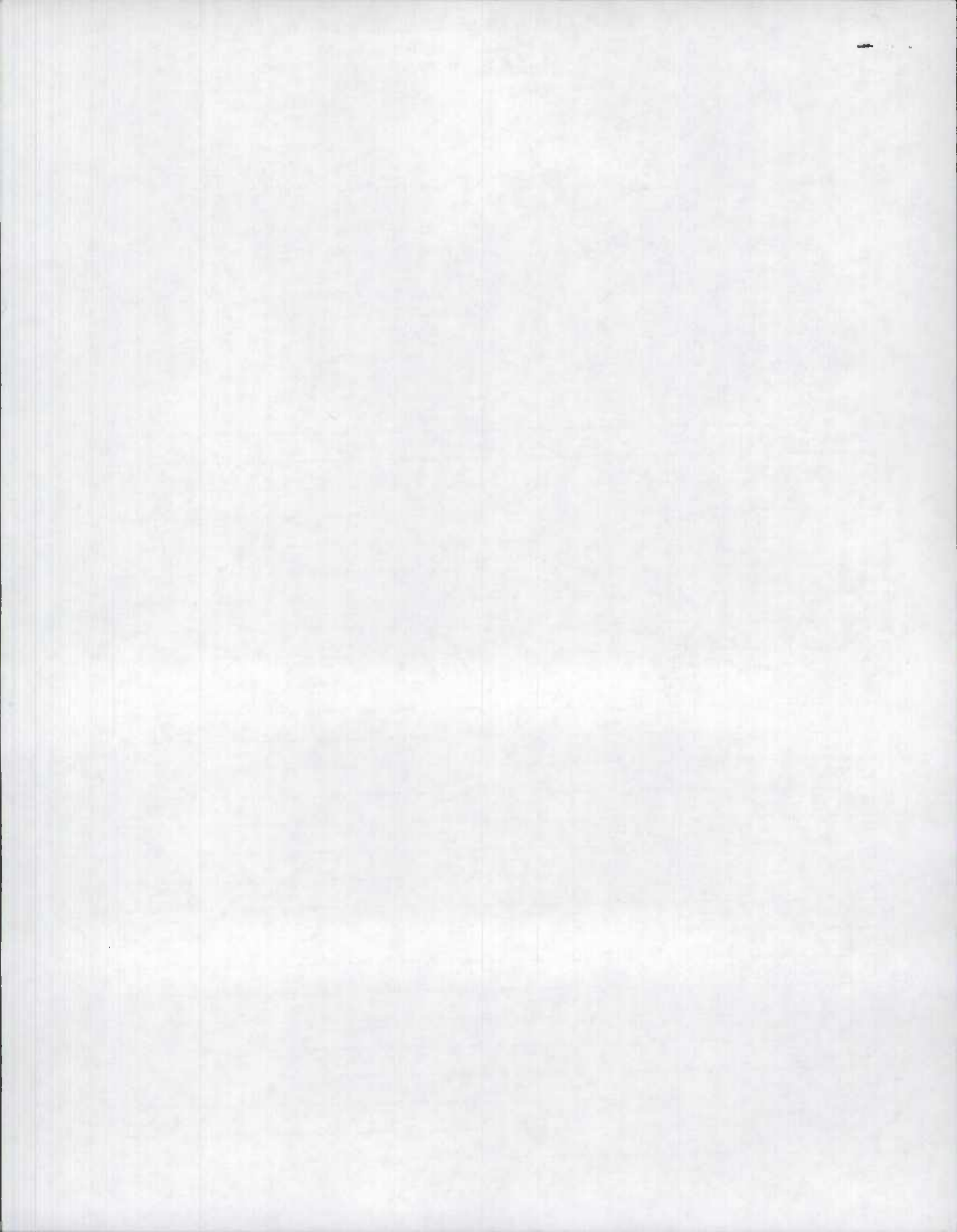
PM GPS MD 5  
RELOCATION

NOTE: ONLY HAD TIME TO  
GPS ROADS PREVIOUSLY  
INVENTORIED FOR MD 5  
RELOCATION

Odometer      Time  
Start of the Day 64073      5:45  
Begin Days Work 64195      1:40  
End Days Work 64222      3:05  
End of the Day 64316      5:45  
Total Mi. Travelled 243

Total No. of Improvements NA  
Total Mi. of Road Inventoried N/A  
Total GPS NA  
Total A/T NA  
Total Extra NA  
Total Tie-in NA

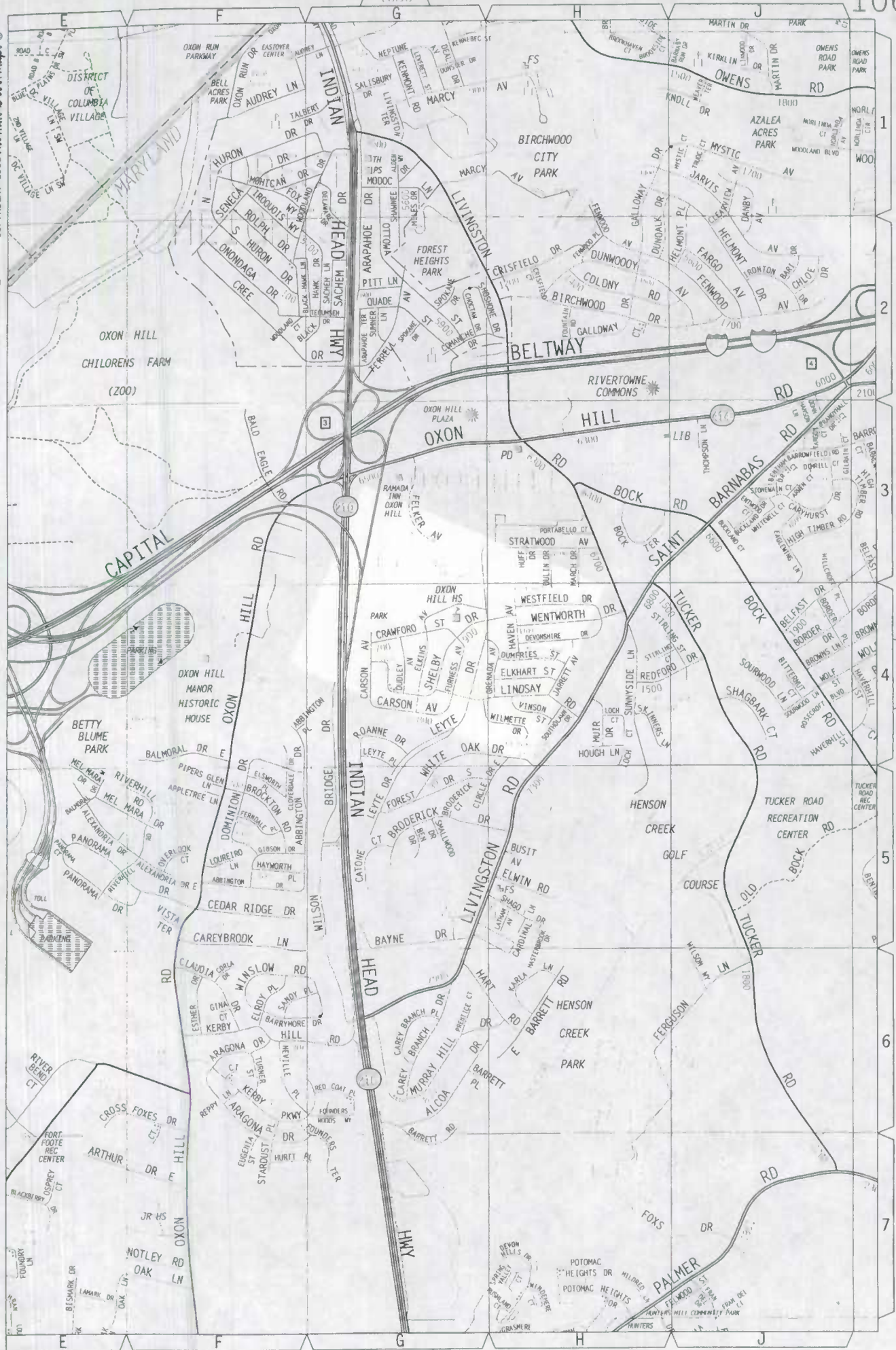
No.	Improvement	Length	GPS	A/T	Extra	Tie-in	Remarks
1	99-4 ST	.					SEE LOC FOR 2-15-16-33
2		.					
3		.					
4		.					
5		.					
6		.					
7		.					
8		.					
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11		.					
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16		.					
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22		.					
23		.					
24		.					
25		.					





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METRO DC



SEE 1067 MAP

MAP

This is part of the  
Woodrow Wilson  
bridge project



