

FINDING OF NO SIGNIFICANT IMPACT

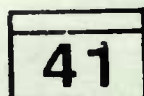
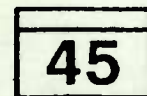
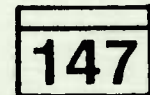
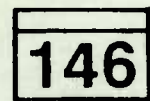
Contract No. B 635-101-472

Interstate Route 695

Baltimore Beltway

MD 140 To MD 702

Baltimore County, Maryland



prepared by
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

and
MARYLAND DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY ADMINISTRATION

FEDERAL HIGHWAY ADMINISTRATION
FINDING OF NO SIGNIFICANT IMPACT
FOR

INTERSTATE ROUTE 695 (BALTIMORE BELTWAY)
FROM MD 140 TO MD 702, BALTIMORE COUNTY, MARYLAND

The Federal Highway Administration has determined that the Selected Action, Alternate 2 and the improvement to 11 interchanges within the limits of the proposal will have no significant impacts on the human environment. Alternate 2 will provide one additional lane in each direction for a total of four-lanes from MD 140 to MD 702 as follows: From MD 140 to I-83 (JFX) widening will occur in the median; From I-83(JFX) to MD 147 widening will occur on the outside of existing lanes; From MD 147 to MD 702 widening will occur primarily in the median. (Pages III-10 to III-17A of the attached documentation further describes the selected action.) This Finding of No Significant Impact (FONSI) is based on the Environmental Assessment and the attached documentation which summarizes the assessment and documents the selection of the selected action. In addition, the selected alternative conforms with the Clean Air Act Amendments of 1990, in accordance with the US DOT/EPA June 7, 1991 guidance.

This FONSI has been independently evaluated by the FHWA and determined to adequately and accurately discuss the need, environmental issues, and impacts of the proposed project and appropriate mitigation measures. It provides sufficient evidence and analysis for determining that an Environmental Impact Statement is not required. The FHWA takes full responsibility for the accuracy, scope, and content of the Environmental Assessment and attached documentation.

11/15/91
Date

Humans Rodriguez
For Division Administrator

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FROM MD 140 TO MD 702
CONTRACT NO. B 635-101-472

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I-695 (BALTIMORE BELTWAY)
FROM MD 140 TO MD 702
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I. RECORD OF DECISION

MEMORANDUM OF ACTION OF ADMINISTRATOR HAL KASSOFF
WEDNESDAY, AUGUST 21, 1991

A Final Environmental Document (Finding of No Significant Impact) has been prepared for the project listed below. Location/Design approval will be requested from the Federal Highway Administration as shown.

Contract No. B-635-151-472-N
I-695 (Baltimore Beltway)
MD 140 to MD 702
PDMS# 031113

Recommendation : Location/Design approval
for Alt. 2 and specific
interchange improvements.

The decision to proceed in this manner was made by the Administrator at a team meeting held on August 21, 1991.

/vdl

cc: E. L. Homer
R. Olsen
D. Harrison
R. Douglass
N. Pedersen
L. Ege
E. Freedman
S. Drumm
C. Simpson
R. Sanders
SRC-Baltimore County File



Maryland Department of Transportation
State Highway Administration

10
O. James Lighthizer
Secretary
Hal Kassoff
Administrator

MEMORANDUM

TO: Mr. William I. Slacum, Secretary
State Roads Commission

FROM: Neil J. Pedersen, Director
Office of Planning and Preliminary Engineering *Neil J. Pedersen*

DATE:

SUBJECT: Contract No. B 635-101-472
I-695 (Baltimore Beltway)
MD 140 to MD 702
PDMS No. 031113

RE: Selection of Alternate

The Project Planning Division is preparing a Finding Of No Significant Impact (FONSI) for the subject project. This document is now ready for submission to the Federal Highway Administration.

The decision to proceed with the FONSI recommending Alternate 2, as well as specific interchange improvements, was made by Administrator Kassoff when he approved the staff recommendation on August 21, 1991.

The Project Planning Team Recommendation of Alternate 2 and specific interchange improvements are documented in the concurrence memorandum, which was signed by the Administrator, and attached for your information.

This information is being sent to you as part of the procedure by which you submit the action to Mr. Kassoff, receive his approval, formally record and file this action.

My telephone number is 333-1110

Mr. William I. Slacum
Page Two

I concur with the above information.

for Elizabeth L. Homer

Hal Kassoff, Administrator

11/13/91

Date

Attachment

cc: Ms. Liz Homer
Mr. C. Robert Olsen
Mr. Robert Douglass
Mr. Earle Freedman
Mr. Stephen Drumm
Mr. James Gatley
Mr. Dick Harrison
Mr. Louis H. Ege, Jr.



Maryland Department of Transportation
State Highway Administration

O. James Lighthizer
Secretary
Hal Kassoff
Administrator

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NOV 16 1991

NOV 28 1991

MEMORANDUM

TO: Mr. Hal Kassoff
Administrator

FROM: Neil J. Pedersen, Director
Office of Planning and Preliminary Engineering *Neil J. Pedersen*

DATE: August 21, 1991

SUBJECT: Contract No. B 635-101-472
I-695 (Baltimore Beltway)
MD 140 to MD 702
PDMS No. 031113

Attached are summaries of the Location/Design recommendation meetings held on November 21 and December 20, 1990 and June 4 and July 30, 1991. The summaries indicate a consensus for selection of the mainline and interchange alternatives listed below. Also attached is a summary giving the details of the selected alternatives. The selected alternatives were:

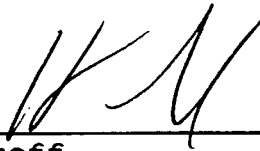
- Alternate 2 Mainline Widening
- Stevenson Road Intersection Free Right Turn Option
- Greenspring Avenue/MD 133 (Old Court Road) Intersection Widening Option
- I-83 (JFX)/MD 25 (Falls Road) Interchange Option C
- I-83 (HX)/MD 139 (Charles Street) Interchange Option A
- I-83 (HX)/MD 139 (Charles Street) Interchange Option D
- MD 45 (York Road) Interchange Option B
- MD 146 (Dulaney Valley Road) Interchange Ramp Relocation Option
- MD 41 (Perring Parkway) Interchange Options A and B
- MD 147 (Harford Road) Option B
- US 1 (Belair Road) Interchange Auxiliary Lane Option
- MD 7/US 40/MD 702 Option 3

Preparation of a Finding of No Significant Impact (FONSI) is underway. Location/Design approvals from the Federal Highway Administration (FHWA) will be received in November of this year.

My telephone number is 333-1110

I concur with the recommendation to proceed with the above listed alternates.

8/21/91
Date


Hal Kassoff
Administrator

NJP/ih

Attachments

- cc: Mr. Charles B. Adams
- Mr. Stephen F. Drumm
- Mr. Louis H. Ege, Jr.
- Mr. James K. Gatley
- Ms. Cynthia D. Simpson



**Maryland Department of Transportation
State Highway Administration**

O. James Lighthizer ¹⁴
Secretary
Hal Kassoff
Administrator

MEMORANDUM

TO: THE FILE

FROM: ROBERT K. SANDERS

DATE: DECEMBER 4, 1990

SUBJECT: CONTRACT NO. 635-101-472
I-695 (BALTIMORE BELTWAY)
MD 140 TO MD 702
PDMS NO. 031113

RE: LOCATION/DESIGN APPROVAL, 1ST MEETING

1. The project planning team met on November 21, 1990 to present recommendations to the SHA Administrator for Location/Design Approval for I-695 from MD 140 to MD 702. Three meetings have been scheduled (December 20, 1990 and January 8, 1991) to obtain Location/Design approval. The first meeting was to review mainline alternates.

2. Project Background

This project is included in the Interstate Development and Evaluation Program of the Consolidated Transportation Program for fiscal years 1990 to 1995. The project is funded for project planning and some design work.

This study began in December, 1984. There were two Alternates Public Meetings held in October, 1985. In 1989, this project was delayed to determine whether major changes were necessary to accommodate elements considered in the Statewide Commuter Assistance Study. It was determined that LRT could be accommodated from the Central Corridor Light Rail Line to Dulany Valley Road with minor adjustments. At the February 28, 1990 public hearing, there were 30 speakers, 14 individual testimonies to recorders and 214 written responses. A consensus of the testimony and written responses was that noise barriers should be erected for all areas that exceed FHWA Noise Guidelines before any roadway construction. A Team Recommendation meeting was held on May 30, 1990. A construction Staging Committee, chaired by Mr. Olsen, has held a series of meetings. They have recently presented their recommendations to the Administrator.

My telephone number is _____

There are four related projects under design or construction within the study limits. MD 43, Whitmarsh Boulevard, is under construction. This includes an interchange with I-695 and Beltway widening west of Harford Road to Putty Hill Avenue. US 1, Belair Road, is tentatively scheduled for advertisement in February, 1991. Improvement of the I-95, Reverse Flow Interchange, is currently under construction with the widening of I-95 from I-695 to MD 43. US 40, Pulaski Highway, Bridge No. 3191 is under design for complete replacement.

The Baltimore Beltway was constructed during the 1950's and early 1960's in accordance with then current design criteria. Little of the existing roadway could meet today's design criteria. This includes interchanges spaced too closely, ramps with too short radii, inadequate superelevation, steep grades, and sharp curves. Nearly all of the study area operates currently at LOS E/F during peak hours. Additionally, operation on some of the side roads causes back-ups onto I-695. With the addition of another lane in each direction by the design year 2015, the roadway level-of-service will approximate current conditions. Currently, the Beltway experienced accident rates at all but 2 interchanges (MD 147 and I-95) higher than the statewide average. The segment of the Beltway from MD 7 to MD 702 is a High Accident Section (HAS) as determined by recent more stringent criteria. This is the only high accident section on the Beltway. Additionally, there are 32 ramps which are High Accident Interchange Ramps (HAIR).

The objectives of this study were to improve safety and capacity of I-695 by adding one lane in each direction and improving operational characteristics of the interchanges. The study extended to the first intersection on crossroads that may influence Beltway operation. Right-of-way impacts were to be avoided or minimized. Design criteria established for this project included:

- Design Speed = 60 MPH
- Correct substandard grades where feasible
- Correct insufficient superelevation where feasible
- LOS D, if possible
- Desirable shoulder width 10'
- Minimum shoulder width of 3-1/2' inside and 7-1/2' outside requires a design exception (see memo 12/27/83)
- Vertical clearance at bridges over I-695: 16' minimum; new construction 16'-9"

3. Alternate 2

Alternate 2 includes the addition of one lane in each direction. The widening will occur from MD 140 to Falls Road in the median, Falls Road to west of MD 147 to the outside, from west of MD 147 to Putty Hill Avenue as part of the proposed MD 43 project, from Putty Hill Avenue to MD 702 in the median. Additionally Alternate 2 would reconstruct the northbound MD 542 to eastbound I-695 ramp from a 25 MPH design speed to 50 MPH design speed. A

total cost of approximate \$135 million is estimated for these improvements. Additionally widening or HOV lane use will be investigated briefly for a subsequent project planning study. A review of areas with substandard superelevation will be completed on a case by case discussion in final design. The SHA Administrator directed obtaining Location/Design approval for Alternate 2.

4. Alternate 2A

Alternate 2A would realign the Beltway, both horizontally and vertically, in the vicinity of the MD 542 (Loch Raven Boulevard/Cromwell Bridge Road) Interchange between Providence Road and Perring Parkway. This area previously had accident rates of over three times, the statewide average; however, following minor improvements the accident rate dropped to a similar rate as other Beltway segments. A total cost of approximately \$29 million is estimated for these improvements. The SHA Administrator directed not to obtain Location/Design approval for Alternate 2A as this alternate is not cost effective.

5. Alternate 2B (U.S. 40 Mainline)

The US 40 bridge is currently being designed for replacement due to structural deficiencies. The existing bridge is located on a crest vertical curve with a 45 MPH design speed. The posted speed currently is 55 MPH. Additionally, the bridge clearance on US 40 will be less than 16'0" when the bridge is widened for the ultimate section. Alternate 2B (US 40 mainline) would raise the crest vertical curve to a 55 MPH design speed. This would require the Beltway to be raised in elevation (a maximum of 7') from east of MD 7 to the CSX bridge (neither bridge would be impacted). This would create long delays and congestion during construction. This area is the only high accident section (HAS) on the Beltway, however, a detailed review of the accident history reveals that the accidents are not associated with the substandard crest vertical curve but with the interchange spacing and configuration. A discussion of the proposed interchange improvements followed but will be discussed in greater detail in subsequent meetings. A total cost of approximately \$4.6 million is estimated for Alternate 2B. The SHA Administrator directed not to obtain Location/Design approval for Alternate 2B.

6. The two additional meetings scheduled for December 20, 1990 and January 8, 1991 will present recommendations to the SHA Administrator for Location/Design approval for the interchange options.



**Maryland Department of Transportation
State Highway Administration**

O. James Lighthizer
Secretary
Hal Kassoff
Administrator

MEMORANDUM

TO: THE FILE

FROM: ROBERT K. SANDERS

DATE: DECEMBER 27, 1990

SUBJECT: CONTRACT NO. 635-101-472
I-695 (BALTIMORE BELTWAY)
MD 140 TO MD 702
PDMS NO. 031113

RE: LOCATION/DESIGN APPROVAL, 2ND MEETING

- The project planning team met on December 20, 1990 to present recommendations to the SHA Administrator for Location/Design approval for I-695 from MD 140 to MD 702. Three meetings have been scheduled. The first meeting was held on November 21, 1990 and selected mainline alternates. The second meeting was held to discuss interchange options from MD 140 to MD 139. The third meeting, scheduled for January 8, 1991, will discuss interchange options from MD 45 to US 40. Present at this meeting were the following:

Hal Kassoff	Administrator, State Highway Administration
C. Robert Olsen	Chief Engineer
Neil Pedersen	Director, Office of Planning, and Preliminary Engineer
Louis Ege, Jr.	Project Planning Division
Robert Houst	Project Planning Division
James Wynn	Project Planning Division
Robert Sanders	Project Planning Division
Scott Holcomb	Project Planning Division
Anthony Capizzi	Highway Design Division
Robert Douglass	Highway Design Division
Richard Harrison	District Engineer, District 4
Darrell Wiles	District 4
Gordon Dailey	Johnson, Mirmiran & Thompson, P.A.
Michael J. Rothenneber	Johnson, Mirmiran & Thompson, P.A.

My telephone number is _____

Memorandum

Location/Design Approval Mtg.

December 28, 1990

Page Two (2)

2. The previous meeting included two unresolved issues. A discussion on additional mainline lanes or HOV lanes, and the US 40 improvements will be discussed during the January 8, 1991 meeting.

3. The following discussion ensued upon each of the proposed interchange improvements from MD 140 to MD 139:

a. Stevenson Road

The improvements at Stevenson Road were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvements. This option could possibly be included as a special project.

b. Greenspring Avenue

The improvements at Greenspring Avenue were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvements. An interim solution of signal timing could be implemented first. This option could possibly be included as a special project.

c. I-83 (JFX) Options A & B

I-83 (JFX) Option B was recommended by the project planning team. The Administrator directed not to obtain Location/Design approval at this time. This option will be addressed within the FONSI but Location/Design approval will not be obtained until either traffic or safety problems occur in the future.

d. I-83 (JFX) Options C & D

In addition to Options C and D, an option from the V.E. Team was presented to the Administrator. No option was recommended by the project planning team. The Administrator directed to obtain Location/Design approval for Option C. Two interim solutions will be investigated. The first solution will investigate if the declaration to I-83 south could be extended through the Falls Road Bridge by restriping. The second solution will investigate a cost to replace the Falls Road Bridge.

e. I-83 (HX)/MD 139 Option A

Option A was recommended by the project planning team. The Administrator directed to obtain Location/Design approval. These improvements will be completed as part of, or following the construction of Type II noise barriers (noise barriers are committed but not funded).



Memorandum
Location/Design Approval Mtg.
December 29, 1990
Page Three (3)

f. I-83 (HX)/MD 139 Option C

Option C was recommended by the project planning team. The Administrator did not direct to obtain Location/Design approval for this option. It could be included as a special project when a problem occurs.

g. I-83 (HX)/MD 139 Option D

Option D was recommended by the project planning team. The Administrator directed to obtain Location/Design approval for this option. Coordination with the proposed LRT station will be required.



**Maryland Department of Transportation
State Highway Administration**

O. James Lighthizer ²⁰
Secretary
Hal Kassoff
Administrator

MEMORANDUM

TO: THE FILE

FROM: ROBERT K. SANDERS

DATE: JUNE 6, 1991

**SUBJECT: CONTRACT NO. 635-101-472
I-695 (BALTIMORE BELTWAY)
MD 140 TO MD 702
PDMS NO. 031113**

RE: LOCATION/DESIGN APPROVAL, 3RD MEETING

- The Project Planning Team meet on June 4, 1991 to present recommendations to the SHA Administrator for Location/Design approval for I-695 from MD 140 to MD 702. Two previous meetings have been held. The first meeting was held on November 21, 1990 and selected mainline alternates. The second meeting was held on December 20, 1990 and selected interchange improvements from MD 140 to MD 139. This meeting was scheduled to discuss interchange options from MD 45 to MD 702. Present at the meeting were the following:

Hal Kassoff	Administrator, State Highway Administration
C. Robert Olsen	Chief Engineer
Robert Douglass	Deputy Chief Engineer
James Wynn	Project Planning Division
Robert Sanders	Project Planning Division
Howard Johnson	Project Planning Division
Scott Holcomb	Project Planning Division
Stephen Drumm	Highway Design Division
Stephen Kouroupis	Highway Design Division
Harvey Muller	Highway Design Division
Thomas Hicks	Office of Traffic
Richard Harrison	District Engineer, District 4
Darrell Wiles	District 4
Andrew Mergenmeier	FHWA
Joyce Curtis	FHWA
Gordon Dailey	GED
Michael J. Rothenheber	Johnson, Mizmiran & Thompson, P.A.

My telephone number is _____

June 6, 1991
Page Two (2)
Memorandum

- 2. The previous meetings included two unresolved issues. These included additional mainline lanes or HOV lanes and an interim typical section under the Falls Road Bridge.

An investigation into a future additional mainline lane or HOV lane beyond the additional lane provided by Alternate 2 was investigated. A feasibility study only was investigated and no detailed engineering was performed. The first option would construct an additional 12' lane beyond the Alternate 2 widening. This would require replacement of all overpasses, right-of-way from over 100 properties even with retaining walls, extensive retaining walls everywhere else, reconstruction of noise walls and retaining walls constructed with Alternate 2, and Section 4(f) land impacts. For these reasons, this option was dropped from further discussion at this time.

The second option would re-stripe proposed Alternate 2 (see attached typical section) to obtain an additional lane. The re-striped lane configuration for both sides would include an 8' outside shoulder (12' along concrete barriers), two - 12' through lanes, two - 11' through lanes, one - 12' HOV/BUS lane, and a 4' inside shoulder adjacent to a median barrier. The two - 11' through lanes would be restricted to no trucks. The one - 12' HOV/BUS lane would be used during peak hours only and would not be a through lane. The effectiveness of an HOV lane was not investigated, only the physical constraints of the typical section. The clearance under bridges and median sign supports maybe a constraint if only the Alternate 2 typical section is considered during design. A study will be performed to determine the impact to increasing the typical section to allow for 10' outside shoulders, 12' lanes, 2' buffer between the HOV lane and through lanes, and a 4' inside shoulder through overpasses. The FHWA representative stated that any change in the typical section would be evaluated in determining approval of the environmental documents. It was noted that all bridges should be designed to include provisions for utilities, variable message signs, and surveillance equipment.

The second outstanding issue involved an interim typical section under the Falls Road Bridge. Staging of the recommended improvements, has Alternate 2 inside widening from MD 140 to I-83 (JFX) as a high priority. This would require the replacement of the Falls Road Bridge which would need additional lengthening for implementation of I-83 (JFX) Option C. A typical section was presented to allow for minimum shoulders under the bridge during the inside widening. This would not require the replacement of the bridge saving \$5.6 million initially. The bridge would be replaced during implementation of I-83 (JFX) Option C. Attached is the interim typical section under the bridge. The Administrator gave approval of the interim option.

3. The following discussion ensued upon each of the proposed interchange improvements from MD 45 to MD 702.

a. MD 45 Option B

The improvements for Option B were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvements.

b. MD 45 Option C

The project planning team did not have a recommendation for this option. The FHWA, in a prior meeting, agreed that this option was not necessary. The Administrator determined that Location/Design approval would not be obtained for this option.

c. MD 146 Option A

The improvements for Option A were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvement.

d. Providence Road

The project planning team did not recommended this option. The Administrator determined that Location/Design approval would not be obtained for this option.

e. MD 141 Option A & B

The improvements for Option A and B were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvements.

f. MD 41 Option E

The project planning team did not have a recommendation for this option. The FHWA, in a prior meeting, agreed that this option was not necessary. The Administrator determined that Location/Design approval would not be obtained for this option.

g. MD 147 Option A

The project planning team did not recommend this option. The Administrator agreed that Location/Design approval would not be obtained for this option.

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June 6, 1991
Page Four (4)
Memorandum

h. MD 147 Option B

The improvements for Option B were recommended by the project planning team. The Administrator directed obtaining Location/Design approval for the proposed improvements.

i. US 1 Option A

The improvements for Option A were recommended by the project planning team. These improvements are being incorporated within the final design contract underway for the US 1 improvements and bridge replacement. The Administrator directed obtaining Location/Design approval for the proposed improvements. Baltimore County Parks and Recreation will be contacted to provide access through Double Rock Park for four properties to avoid a Section 4(f) impact.

j. MD 7/US 40 Option E

Time did not permit discussion on these improvements.



**Maryland Department of Transportation
State Highway Administration**

24
O. James Lighthizer
Secretary
Hal Kassoff
Administrator

MEMORANDUM

TO: THE FILE

FROM: ROBERT K. SANDERS

DATE: JULY 31, 1991

SUBJECT: CONTRACT NO. 635-101-472
I-695 (BALTIMORE BELTWAY)
MD 140 TO MD 702
PDMS NO. 031113

RE: LOCATION/DESIGN APPROVAL, 4TH MEETING

1. A meeting was held on July 30, 1991 at SHA Headquarters for I-695 from MD 140 to MD 702. These previous meetings with the SHA Administrator have been held and direction was given for Location/Design Approval for all areas except interchange improvements from east of I-95 to MD 702. Present at the meeting were the following:

Hal Kassoff	Administrator, State Highway Administration
C. Robert Olsen	Chief Engineer
Robert Douglass	Deputy Chief Engineer
Louis H. Ege, Jr.	Deputy Director, Office of Planning and Preliminary Engineering
Robert Sanders	Project Planning Division
Howard Johnson	Project Planning Division
Bruce Grey	Project Planning Division
Earl Schaffer	Highway Design Division
Stephen Kouroupis	Highway Design Division
Harvey Muller	Highway Design Division
Richard Harrison	District Engineer, District 4
Michael J. Rothenheber	Johnson, Mirmiran & Thompson, P.A.
Joseph Callahan	Johnson, Mirmiran & Thompson, P.A.

2. The meeting was scheduled to receive the SHA Administrator's direction in obtaining Location/Design approval from east of I-95 to MD 702. Presented at the Public Hearing was US 40 Option E (attached). This section of the Baltimore Beltway is the only high accident section (HAS) within the project limits. A review of the accident history revealed the majority of accidents are occurring due to weaving conflicts.

My telephone number is _____

3. Several interim solutions were discussed for weaving conflicts along the outer loop between MD 702 and US 40. The first option would sign vehicles on MD 702 destined to US 40 to use the MD 7 interchange. Concerns were raised that most motorists were local origin/destination and would not obey the signing. A physical barrier to eliminate the weave could be possible. However, a review of the accident data reveals a higher number of accidents in the weave between US 40 and MD 7 than between MD 702 and US 40 on the outer loop. A second option, for traffic along MD 702 destined to US 40 east, would shift the movement to a spur from the loop ramp to westbound US 40. This would require an additional signal along US 40 for the left turn movement. This option was rejected due to the higher number of accidents within the short weave section between the loop ramps.

None of these solutions will be recommended to alleviate the weaving conflicts along the outer loop between MD 702 and US 40.

4. The core team was in disagreement with Option E. Several of the concerns raised about this option were: the inclusions of three new traffic signals on US 40 and the poor level of service; this option does not address the weave on the outer loop between MD 702 and US 40; and the use of Relocated Golden Ring Road to US 40 to supplement denied movements at the MD 7 interchange.

A license plate survey was conducted along MD 695 between US 40 and the split to MD 702 or MD 695. This showed that approximately 1/3 to 1/2 of the traffic is through traffic.

There were three premises used in developing Option E, these included: right-of-way impacts must be minimized; the MD 7 bridge must not be impacted; and the MD 702 interchange would be studied under an independent study.

Four new alternatives were developed incorporating the detailed traffic and accident analysis. Additionally, the restriction of the three premises above was removed. Option 3 was presented as preferred.

5. Option 1 relocated MD 695 to the south. The relocated MD 695 would be a 4-lane divided roadway. It would separate between I-95 and Md 7, follow the transmission lines, overpass MD 7, overpass US 40 just north of Best Products, swing south, run adjacent to Batavia park, and tie-in with the existing MD 695 at the sharp curves near Chesaco Avenue. The existing Md 695 would remain and provide interchanges with MD 7, US 40, and MD 702. A detailed traffic analysis has not been completed, but is anticipated that the relocated roadway would separate approximately one-third of the traffic volumes.

Batavia Park is a gravel excavation pit and not an active park. This option would have approximately 15 displacements. A construction cost of approximately \$55 million is anticipated.

6. Option 2 incorporated the existing alignment, while physical separating MD 695 through vehicles. Between I-95 and MD 7 all through vehicles for MD 695 would be separated into the median (2 lanes each direction) with a concrete barrier. All traffic destined for MD 7, US 40, and MD 702 would be separated to the outside (2 through lanes in each direction). The MD 7 interchange would be reconstructed to a 1/4 cloverleaf with a loop ramp in the southwest quadrant requiring the bridge to be replaced. The US 40 interchange would be reconstructed to a 3/4 cloverleaf, missing a loop ramp in the northwest quadrant. A slip ramp from MD 702 to MD 695 westbound would be provided.

7. Option 3 is very similar to Option 2. This option physical separated the MD 702 through vehicles into the median. The MD 7 and US 40 interchanges would be the same as Option 2. The alignment of this option is more compatible with the existing MD 702 interchange. This option would have reduced cost and environmental impacts than Option 2.

8. Option 4 provided an additional mainline lane to MD 695 as proposed by Alternate 2 at the Public Hearing. The MD 7 interchange would be reconstructed to 1/4 cloverleaf similar to the previous options. The US 40 interchange would be reconstructed to an urban diamond. This would require only one new traffic signal on US 40 (2015 LOS C/E). A slip ramp from MD 702 and MD 695 westbound would be provided. Weaving from the US 40 ramp to MD 702 on the inner loop would create a poor operational section. Three options were presented for this movement; to deny the movement; to physical separate the weave and sign vehicles to MD 7 via Rossville Boulevard and Relocated Golden Ring Road; and to physical separate the weave and provide a loop ramp east of Kelso Drive.

While this option is the lowest cost and least right-of-way impacts of the options; longer and more intense delays are anticipated during construction.

9. Option 3 was presented in a staging of improvements. The first stage would replace Stemmers Run bridge and widen US 40 to six lanes through the intersection with Rossville Boulevard. The second option would replace the US 40 bridge which is structurally deficient. The third stage would widen MD 695 in the median. The fourth stage would construct the slip ramp from MD 702 to MD 695 westbound. The fifth stage would complete the remaining interchange improvements.

10. The SHA Administrator directed obtaining Location/Design approval for Option 3 with some minor changes. The concrete barrier and shoulders separating MD 702 and MD 695 through traffic will not be provided. Additionally, some interim interchange improvements will be investigated.

11. JMT was directed to begin design of Option 3 as discussed.

II. SUMMARY OF ALTERNATIVES

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The State Highway Administration (SHA) has decided to seek Location/Design Approval for: Alternate 2; Stevenson Road Intersection; Greenspring Avenue/MD 133 (Old Court Road) Intersection; I-83 (JFX)/MD 25 (Falls Road) Interchange Option C; I-83 (HX)/MD 139 (Charles Street) Interchange Option A and Option D; MD 45 (York Road) Interchange Option B; MD 146 (Dulaney Valley Road) Interchange Option; MD 41 (Perring Parkway) Interchange Option A and Option B; MD 147 (Harford Road) Interchange Option B; US 1 (Belair Road) Modified Option; MD 7 (Philadelphia Road)/US 40 (Pulaski Highway)/MD 702 (Southeast Throughway) Interchanges Option 3. These improvements are described in Section III. The Selected Build Alternate is supported by Baltimore County.

SUMMARY OF ALTERNATES - I-695 (BALTIMORE BELTWAY) MD 140 TO 702

SELECTED BUILD ALTERNATE	ALTERNATE	LENGTH OF IMPROVED ROADS (MILES)	DISPLACEMENTS				PROPERTIES AFFECTED						RIGHT-OF-WAY REQUIRED (ACRES)						HISTORICAL / ARCHEOLOGICAL	WOODED (ACRES)	MAJOR STREAM CROSSINGS	WETLANDS (ACRES)	100 YEAR FLOOD PLAIN (ACRES)	ESTIMATED COST (MILLIONS 1991)										
			RESIDENTIAL	COMMERCIAL	CHURCHES SCHOOLS	TOTAL	RESIDENTIAL	COMMERCIAL	CHURCHES SCHOOLS	PUBLIC RECREAT'L LANDS	OTHER	TOTAL	RESIDENTIAL	COMMERCIAL	CHURCHES SCHOOLS	PUBLIC RECREAT'L LANDS	OTHER	TOTAL						ENGINEER'G & RIGHT-OF-WAY	CONSTRUCT.	TOTAL								
	Alternate 1: No Build	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
S.B.A.	Alt. 2: MD 140 to Folls Road	3.84	0	0	0	0	6	0	0	0	1	7	3.0	0	0	0	0.6	3.6	0	0	0	0	0	0	0	0	0	2.07	20.54	22.61				
S.B.A.	Stevenson Road Option	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.2	0.15	0.35					
S.B.A.	Greenspring Avenue / MD 133	-	0	0	0	0	1	0	0	0	1	0.5	0	0	0	0	0	0.5	0	0	0	0	0	0	0	0	0.05	0.43	0.48					
	SEGMENT TOTAL	-	0	0	0	0	6	0	0	0	1	7	3.0	0	0	0	0.6	3.6	0	0	0	0	0	0	0	2.32	21.12	23.44						
S.B.A.	Alt. 2: Folls Road to Conrail RR	1.86	0	0	0	0	12	0	0	0	0	12	3.1	0	0	0	0	3.1	0	1.0	0	0.1	0.1	1.44	14.67	16.11								
	I-83 (JFX) / MD 25 Option A	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.7	1	0	0	0.28	4.67	4.95								
	I-83 (JFX) / MD 25 Option B	-	0	0	0	0	2	1	0	0	0	3	2.6	0	0	0	0	2.6	1	4.7	1	0.3	0.1	1.59	19.16	20.75								
S.B.A.	I-83 (JFX) / MD 25 Option C	-	0	0	0	0	2	0	0	0	0	2	8.3	0	0	0	0	8.3	0	0	1	0.9	0.4	0.82	7.14	7.96								
	I-83 (JFX) / MD 25 Option D	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.4	1	0	0.9	0.36	6.06	6.42								
S.B.A.	I-83 (HX) / MD 139 Option A	-	0	0	0	0	4	0	0	0	0	4	0.08	0	0	0	0	0.08	0	0	0	0	0	0.04	0.30	0.34								
	I-83 (HX) / MD 139 Option C	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.03	0.37	0.40								
S.B.A.	I-83 (HX) / MD 139 Option D	0	0	0	0	0	15	4	0	0	0	19	0.02	0.32	0	0	0	0.34	1	0.8	0	0	0	0.97	12.35	13.32								
	SEGMENT TOTAL *	-	0	0	0	0	35	5	0	0	0	40	14.1	0.32	0	0	0	14.42	2	7.2	3	1.3	0.6	5.17	58.66	63.83								
S.B.A.	Alt. 2: Conrail RR to MD 147	6.28	0	0	0	0	4	0	0	0	0	4	4.4	0	0	0	0	4.4	0	4.0	0	0.2	0.6	5.62	60.90	66.52								
	Alt. 2A: Con RR to MD 147 (Reolig.)	6.28	0	0	0	0	6	0	0	0	0	6	6.5	0	0	0	0	6.5	0	9.6	0	0.2	2.9	8.94	98.94	107.88								
S.B.A.	MD 45 Option B	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.2	0	0	0	0.09	1.05	1.14								
	MD 45 Option C	-	0	0	0	0	0	1	0	0	1	2	0	0.04	0	0	0.09	0.13	0	0.3	0	0	0	0.12	1.22	1.34								
S.B.A.	MD 146 Option	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.04	0.55	0.59								
	Providence Road	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.12	1.51	1.63								
S.B.A.	MD 41 Option A & B	-	0	0	0	0	0	1	0	0	0	1	0	0.05	0	0	0	0.05	0	2.0	0	0.1	0	1.05	13.20	14.25								
	MD 41 Option E	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1.6	0	0	0	0.04	0.43	0.47								
	MD 147 Option A	-	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.1	0	0.1	0.2	0.12	1.52	1.64								
S.B.A.	MD 147 Option B	-	0	0	0	0	0	9	0	0	1	10	0	0.45	0	0	0.23	0.68	0	0.1	0	0	0	0.44	1.40	1.84								
	SEGMENT TOTAL **	-	0	0	0	0	4	11	0	0	2	17	4.4	0.54	0	0	0.32	7.36	0	8.3	0	0.4	0.8	7.64	81.78	89.42								
S.B.A.	Alt.2: Putty Hill Ave. to MD 702	4.42	0	0	0	0	1	0	0	0	0	1	1.45	0	0	0	0	1.45	0	6.5	0	0.1	0.3	3.28	34.96	38.24								
S.B.A.	US 1 Modified Option	-	0	0	0	0	0	1	0	0	0	1	0	0.44	0	0	0	0.44	0	0.1	0	0	0	0.37	1.20	1.57								
S.B.A.	MD 7 / US 40 / MD 702 OPTION 3	-	1	2	0	3	3	54	0	0	2	10	1.57	2.80	0	0	5.51	9.90	0	0.4	2	0.3	0	3.76	30.24	34.00								
	SEGMENT TOTAL	-	1	2	0	3	2	5	0	0	1	8	1.52	2.80	0	0	5.51	9.90	0	7.0	0	0.4	0.3	7.41	66.40	73.81								
	GRAND TOTAL OF S.B.A.	16.40	1	2	0	3	45	19	0	0	3	67	20.42	3.62	0	0	6.34	30.45	1	15.1	3	1.7	1.4	20.24	199.08	219.32								

* JFX OPTION D IS NOT INCLUDED WITHIN THE SEGMENT TOTAL

** ALTERNATE 2A: CONRAIL RR TO MD 147 (REALIGNMENT) IS NOT INCLUDED WITHIN THE TOTAL

S.B.A. = SELECTED BUILD ALTERNATE

FIGURE II-1

**III. SUMMARY OF ACTIONS
AND RECOMMENDATIONS**

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A. BACKGROUND

1. Project Location

This section of I-695 (Baltimore Beltway) is located in Baltimore County, north of Baltimore City (see Figure III-1). The segment of the Beltway under study runs circumferentially around the Baltimore Metropolitan area's urban core for approximately 17 miles between MD 140 and MD 702. Major satellite town centers of Owings Mills, Towson and Whitmarsh as well as major Interstate Highways such as I-795, I-83, and I-95 are interconnected with this segment of the Beltway (see Figure III-2).

2. Purpose of the Study

The Baltimore Beltway is perhaps the most important arterial highway in the Baltimore Metropolitan Area. It links several suburban communities and distributes traffic along the major arterial routes into Baltimore City. In addition to the transportation of goods and services and interstate travel, the highway is a highly important commuter route and serves as part of the Defense Priority System.

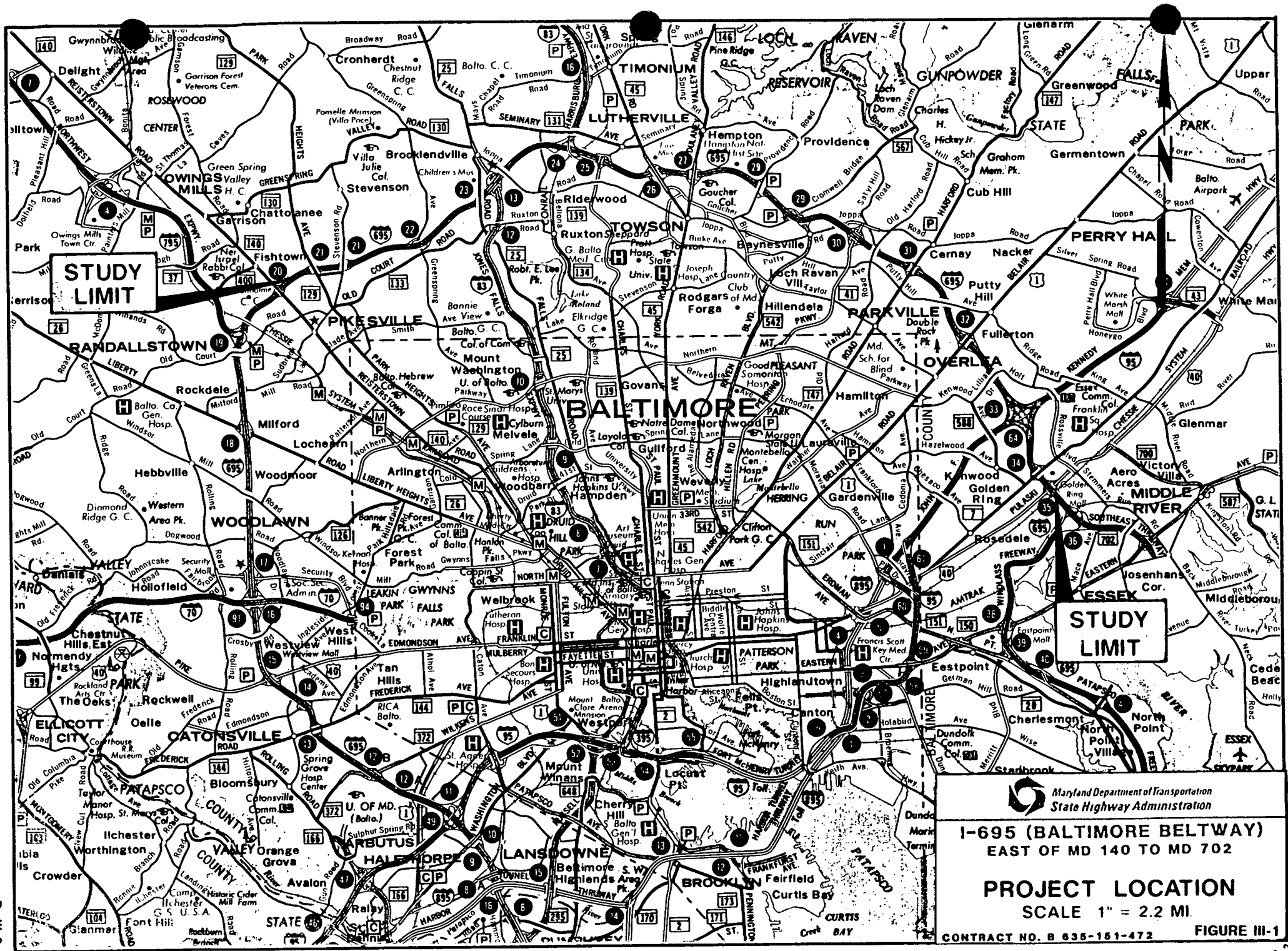
The purpose of this study is to increase capacity and improve the safety of the four and six lane segments of the Baltimore Beltway. This project is being undertaken concurrently with a Project Planning Study on the southwestern portion of the Beltway and would, if both were constructed, provide a nearly continuous eight (8) lane circumferential freeway on the entire interstate portion of the Baltimore Beltway. Currently the Beltway experiences congestion due to capacity constraints and substandard geometrics creating a safety problem. The objectives of the mainline and interchange improvements proposed here are to alleviate existing and future congestion, improve substandard geometrics where possible, and provide for continued safe and efficient operation on the Beltway in the future.

3. Project History

Recognizing prevailing national needs, the Bureau of Public Roads Administration, in cooperation with the American Association of State Highway Officials, adopted the principal network of a new system of national highways in 1947. Four years later the Maryland State Roads Commission formally adopted a proposal by the Baltimore County Office of Planning for the Baltimore County Beltway.

The State's Twelve Year Highway Construction and Reconstruction Program was adopted in April, 1953, and construction of the Beltway began within a year. The first segment opened to traffic in 1955. In 1962, the Beltway was formally dedicated and opened to traffic.

Due to rapid regional economic growth and improved national mobility, traffic volumes soon outgrew the capacity of the four lane expressway. Subsequently, between 1966 and 1971, most of the Beltway was widened to six lanes.



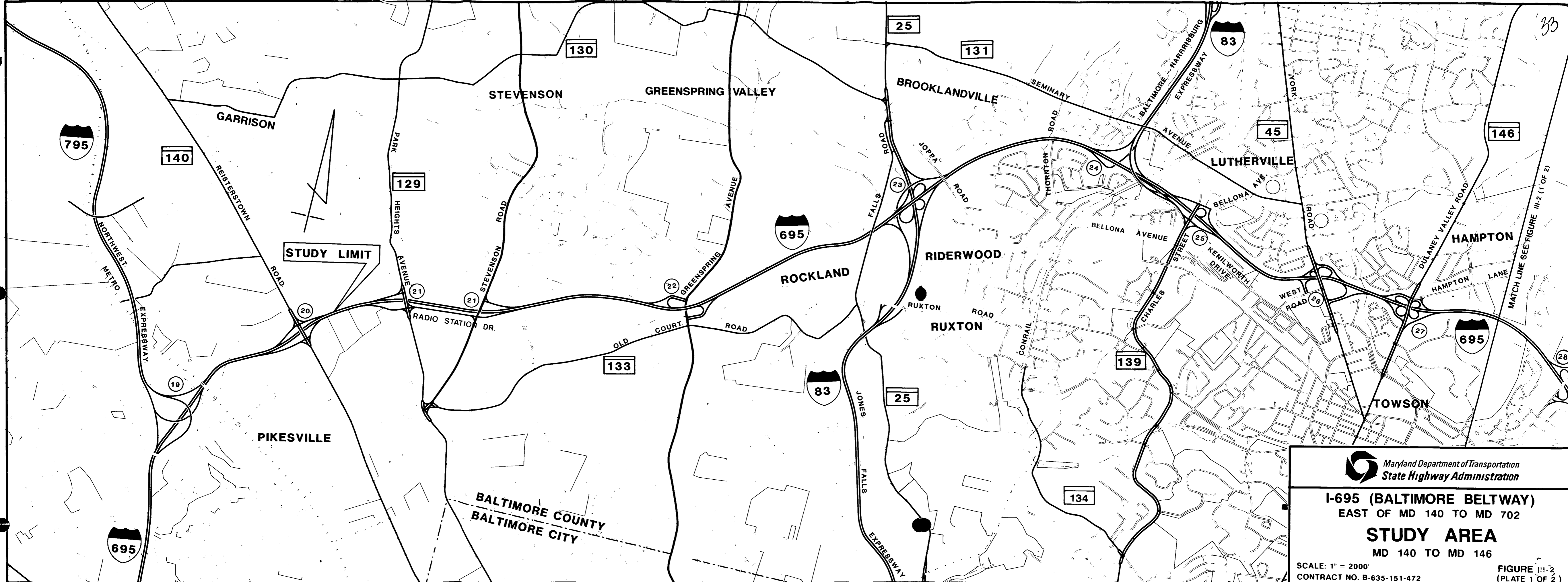
Maryland Department of Transportation
State Highway Administration

I-695 (BALTIMORE BELTWAY)
EAST OF MD 140 TO MD 702

PROJECT LOCATION
SCALE 1" = 2.2 MI.

CONTRACT NO. B 635-151-472 FIGURE III-1

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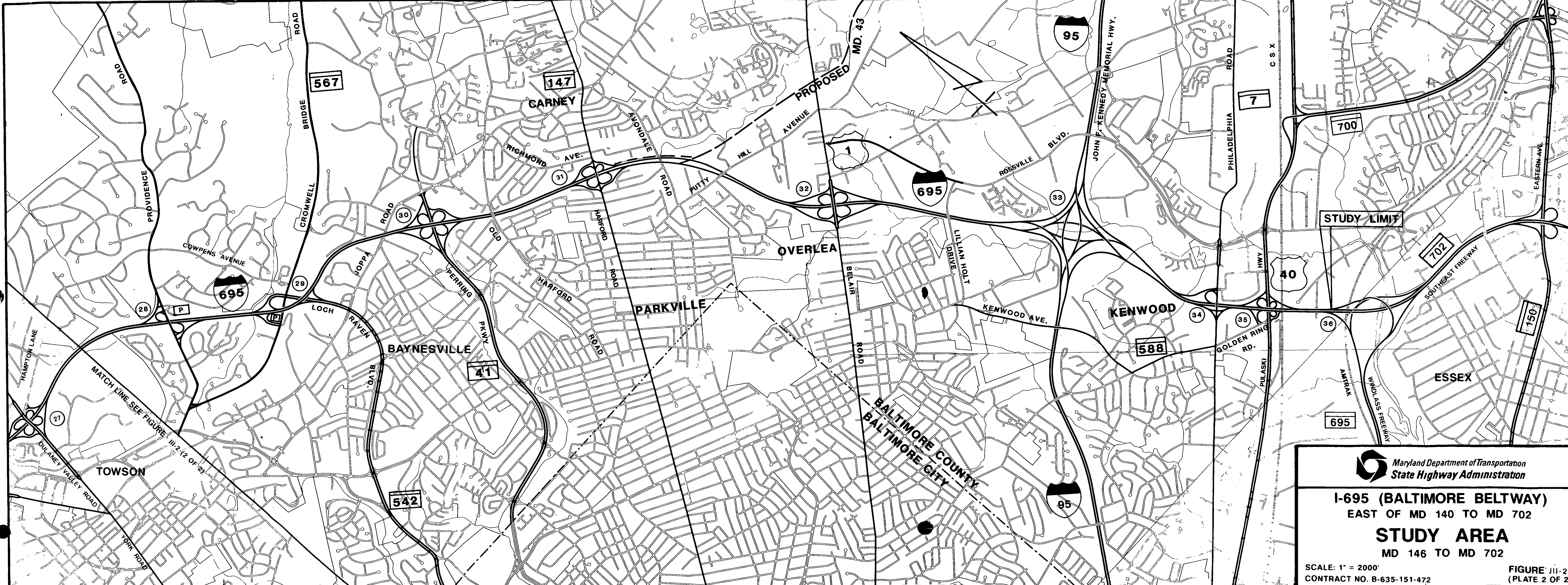




Maryland Department of Transportation
State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD 140 TO MD 702
STUDY AREA
 MD 140 TO MD 146

SCALE: 1" = 2000'
 CONTRACT NO. B-635-151-472

FIGURE III-2
 (PLATE 1 OF 2)




Maryland Department of Transportation
State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD 140 TO MD 702
STUDY AREA
 MD 146 TO MD 702

SCALE: 1" = 2000'
 CONTRACT NO. B-635-151-472

FIGURE III-2
 (PLATE 2 OF 2)

distributor road. Utilizing the eastbound Beltway as a collector-distributor road would remove the weaving condition from the through Beltway traffic and improve traffic flow on the Beltway. The ramps serving the eastbound Beltway would remain essentially as they are today. The loop ramp from the westbound Beltway to Cromwell Bridge Road would be replaced by an outer connection ramp from the westbound Beltway forming an intersection with Cromwell Bridge Road at Cowpens Avenue. The outer connection ramp would have a larger radius than the existing loop ramp and traffic would exit the Beltway under safer conditions. New bridges would be constructed over Loch Raven Boulevard and Cromwell Bridge Road. This was dropped due the high cost, right-of-way impacts, and the major delays that would occur during construction.

c. MD 140 and MD 129 Interchange

The eastbound Beltway segment between Reisterstown Road and Park Heights Avenue experiences congestion due to weaving conditions. The ascending grade at the location tends to inhibit acceleration of some vehicles resulting in difficulty entering the flow of Beltway traffic. Option A proposed an eastbound collector-distributor road through the Reisterstown Road and Park Heights Avenue Interchange. This was dropped because the severity of the weaving condition was not as bad as originally thought, and the high cost.

d. I-83 (JFX) Interchange

Option A proposed to add one lane to the loop ramp for the movement from northbound I-83 (Jones Falls Expressway) to the westbound Beltway (northeast quadrant of the interchange), providing a two lane loop ramp. This was dropped because of the weaving conflict on I-83 (JFX) created by the two lane exit occurring where a loop ramp merges onto I-83 (JFX).

A variation of Option B (See Section III.B.2.f for Option B) was investigated. This would have utilized the existing median of I-83 (JFX) for the two-lane exit to westbound I-695. This option was dropped due to the undesirability of a left side exit, and the additional cost (associated mostly with the retaining walls as the two-lane ramp gained elevation).

A variation of Option B (See Section III.B.2.f for Option B) was investigated that would provide a retaining wall along the Greenspring Valley Historic District (near the Valley Inn). This would have minimized the impacts to the historic district but would not have eliminated it. It was agreed with the State Historical Preservation Officer (SHPO) to provide a fill slope with landscaping rather than the retaining wall.

Another variation of Option B (See Section III.B.2.f for Option B) would have the flyover ramp bridge over Falls Road Bridge instead of under. This was dropped due to the higher cost and visual impact.

Two options were investigated to improve the weave length on eastbound I-695 between the loop ramps. Options to shift the loop ramps were developed. These were dropped due to high cost and only marginally increasing the weave length.

Through the 1970's, interchange modifications and reconstructions were implemented in a few areas, and a series of projects were begun to reconstruct deteriorating bridge decks. Work that began in 1983 to resurface and rehabilitate the Beltway continues today.

This project is currently listed in the Interstate Development and Evaluation Program of the Maryland Department of Transportation's Consolidated Transportation Program FY 1991-FY 1996 for planning and engineering design only. There are currently no funds allocated for right-of-way acquisition or construction. The project is also consistent with the 1986 General Development Plan published by the Baltimore Regional Planning Council (RPC).

B. ALTERNATES

1. Alternates Considered But Dropped Prior to Public Hearing

a. Mainline Build Alternates

Constraints were placed upon the mainline alternates due to the limited available space in the median and the proximity of development contiguous to the Beltway. The mainline build alternates were developed with the basic assumption that right-of-way acquisition would be kept to a minimum. This was accomplished by eliminating safety grading and providing retaining walls where necessary. Mainline sections greater than four lanes in each direction were dropped due to right-of-way constraints.

Transportation Systems Management (TSM) measures such as ramp metering were considered and dropped from further consideration. Ramp metering was not studied in detail because of the lack of suitable parallel roadways in the corridor to receive diverted trips and because of the preponderance of cloverleaf interchanges, which are ill suited for the storage of queued vehicles. Ramp metering would not increase the capacity of the Beltway.

An improvement to correct the substandard vertical grade at Greenspring Avenue was briefly investigated and dropped. This alternate was dropped due to the high cost, and the major delays and congestion that would occur as the Beltway's elevation is changed.

b. Alternate 3A: Realignment at Loch Raven Boulevard with Interchange Modifications

Alternate 3A proposes realigning the Beltway, both horizontally and vertically, in the vicinity of the Cromwell Bridge Road/Loch Raven Boulevard Interchange. The horizontal curves would be flattened which would shift the Beltway alignment northward to reduce the sharpness of the reverse curves. Also the steep grade would be reduced through the area of the relocation. Alternate 3A proposed reconfiguration of the Maryland Route 542 (Loch Raven Boulevard)/Cromwell Bridge Road Interchange in conjunction with the proposed realignment of the Beltway at Loch Raven Boulevard. The Beltway would be realigned and the existing eastbound Beltway roadway would be used as a collector-

A variation to Option C (See Section III.B.2.f for Option C) was investigated for the connection from eastbound I-695 to southbound I-83 (JFX). A two-lane ramp with a 40 MPH design speed was developed to help minimize wetland impacts. This option would have required the westbound I-695 to southbound I-83 (JFX) two-lane ramp to be shifted. This option was dropped due to the increased cost, additional delays and congestion during construction, having a 40 MPH design speed while 50 MPH is desired for an interstate to interstate connection, and only marginally reducing the wetland impacts.

e. I-83 (HX) Interchange

Option B proposed a westbound collector-distributor road along the Beltway for traffic destined to the Harrisburg Expressway. This traffic would exit the Beltway east of Charles Street. The collector-distributor road would bridge over Charles Street and tie into the existing ramp from the westbound Beltway to northbound Harrisburg Expressway. The movement from Bellona Avenue north of the Beltway, to the westbound Beltway would be made with a left turn connection into the loop ramp. The existing loop ramp from northbound Charles Street to westbound Beltway would no longer access the Harrisburg Expressway. To travel from northbound Charles Street to northbound Harrisburg Expressway traffic would turn left at the north end of Charles Street and take the ramp directly to the Harrisburg Expressway. The existing residences and commercial establishments along the Bellona Avenue Ramp would be accessed in a similar manner as today. This was dropped in lieu of Option D.

Several variations of Option B were investigated. The first provided a connection from Bellona Avenue to the loop ramp. This was dropped because the grade differential was too extreme for the connection. Another option allowed for an interlacing of the Beltway and I-83 (N) west of the RR tracks. This minimized the impacts along Bellona Avenue but impacted Seminary Park creating a 4(f) issue. This option was dropped due to the impacts to Seminary Park.

Several variations to Option D (See Section III.B.2.g for Option D) were investigated. An option which provided four through lanes on westbound I-695 was investigated. This would have provided lane balance on I-695. This was dropped because it created greater right-of-way impacts, and provided an overall worse level-of-service. Several variations of shoulder widths through the area of interlacing ramps were investigated. Right-of-way impacts were reduced using minimum shoulder widths. An option which provided a ramp connection from Charles Street to westbound I-695 in the median was developed. This was dropped due to the increased cost, increased delays during construction and the undesirability of the median entrance ramp.

f. MD 45 (York Road) Interchange

Several variations of the eastbound I-695 to southbound York Road via West Road were investigated. One option extended the ramp to connect opposite the adjacent loop ramp. Another option extended the ramp through the Central Voc-Tech Center to Fairmount Avenue. Several options realigned the ramp to eliminate the 90° left turn. These options were all dropped due to right-of-way impacts and costs.

g. MD 146 (Dulaney Valley Road) Interchange

Several variations of the option for westbound I-695 to northbound MD 146 via Hampton Lane were developed. One option provided an additional lane on westbound Hampton Lane. This would have provided a better intersection level-of-service with Dulaney Valley Road but was dropped due to right-of-way impacts. Another option would have provided a one-way roadway (westbound Hampton Lane) through the ramp. This would have provided a free movement from the ramp, provide a better intersection level-of-service, but was dropped due to the strong opposition from the Towson United Methodist Church which was concerned with the lack of access. Another option would extend the ramp across Hampton Lane to Dulaney Valley Road. This option was dropped due to right-of-way impacts.

Option A proposed an eastbound collector-distributor road from west of York Road to east of Dulaney Valley Road. The loop ramp and the outer connection in the southeast quadrant of the Dulaney Valley Road Interchange would be realigned to maintain the design speed of the loop ramp. The bridges at York Road and Dulaney Valley Road proposed for replacement due to Beltway widening would be lengthened to span the collector-distributor road. This option was dropped due to high cost.

h. MD 542 (Loch Raven Boulevard) Interchange

A connection from westbound I-695 to Cromwell Bridge Road at Cowpens Avenue was investigated. This would have provided a safer connection, but was dropped due to the high cost and right-of-way impacts.

i. MD 41 (Perring Parkway) Interchange

Option C proposed to eliminate the loop ramp in the northeast quadrant of the Perring Parkway Interchange (ramp from northbound Perring Parkway to westbound Beltway) with a left turn spur from northbound Perring Parkway into the outer connection from southbound Perring Parkway to westbound Beltway. A left turn lane would be provided in the median of Perring Parkway and another traffic signal would be required. The outer connection from westbound Beltway to northbound Perring Parkway, would be reconstructed improving the horizontal alignment. Option C would be considered in lieu of the westbound collector-distributor road (Option B). This option was dropped in favor of Option B.

Option D proposed to replace the loop ramp in the southwest quadrant of the Perring Parkway Interchange (ramp from southbound Perring Parkway to eastbound Beltway) with a left turn from southbound Perring Parkway onto the outer connection to eastbound Beltway. Another traffic signal would be required. Option D would be considered in lieu of the eastbound collector-distributor road (Option A). This option was dropped in favor of Option A.

j. MD 147 (Harford Road) Interchange

Several variations of the outer connection ramp from westbound I-695 to northbound Harford Road were developed. One option would widen the ramp to two lanes and signal control northbound MD 147 and the ramp. This was dropped due to queuing on northbound MD 147 through the weave area of the loop ramps. Another option provided the same configuration while shifting the signal further north to provide adequate queue distance. This was dropped due to right-of-way impacts.

k. US 1 (Belair Road) Interchange

Several options to provide access rather than requiring four relocations were developed. These all required right-of-way from Double Rock Park. These options were dropped due to the 4(f) issue, Baltimore County Parks and Recreation opposition to the connections, and high cost.

l. MD 7/US 40 Interchange

Several variations for this interchange area was investigated. This included removing the opposite loop ramps then proposed by Option E. This was dropped due to the substandard geometrics. An interlacing scheme between MD 7 and US 40 eastbound was developed. This was dropped due to the high cost. A collector-distributor (C-D) network for eastbound I-695 between MD 7/US 40/MD 702 was developed. This was dropped due to the high cost and right-of-way impacts. Another option would have upgraded Golden Ring Road rather than relocating it. This was dropped because it did not provide an adequate level-of-service.

2. Alternates Presented at the Public Hearing

a. Alternate 1: No-Build

The No-Build Alternate would provide no significant improvements to the study segment of the Baltimore Beltway. Minor improvements to the Beltway and interchanges, such as bridge deck replacements and resurfacing, would occur as part of normal highway maintenance and safety operations. However, these routine maintenance procedures can not measurably affect the ability of the Beltway to accommodate the predicted increase in traffic volumes up to the design year 2015. As traffic volumes continue to increase the duration of peak periods and poor traffic operations would also increase. Travel time, which is directly proportional to congestion, would also increase during peak periods. It can be expected that as the magnitude and duration of congestion increases, the rate of accidents would increase, air quality would degrade due to engine inefficiency, and economic vitality would decline as the highway network becomes less capable of efficiently

delivering goods, service, and customers. The following build alternates were developed on 1"=50' photogrammetry.

b. Alternate 2: Beltway Widening

Alternate 2 consists of adding one lane to the Beltway in each direction. This would provide four through lanes in each direction from MD 140 to I-95 (John F. Kennedy Memorial Highway) and three through lanes in each direction from I-95 to MD 702 (see typical section on P.III-47). The addition of MD 695 from I-95 to MD 702 to the project occurred after the Alternates Public Meeting. The proposed action would significantly increase the capacity of the Beltway within the study area. Except for a few isolated areas the improvement would be contained within the existing right-of-way.

From east of MD 140 (Reisterstown Road) to MD 25 (Falls Road) one additional lane in each direction would be located within the existing 54 foot median. No additional right-of-way would be required for the widening other than areas needed for stormwater management. The MD 25 bridge would be replaced due to the narrow median at this location. The bridge replacement would require a minor right-of-way taking.

One lane in each direction would be added between I-83 (Jones Falls Expressway [JFX]) and I-83 (Harrisburg Expressway [HX]) on the outside of the Beltway. This would provide four (4) through lanes in each direction for I-695 traffic and two (2) lanes in each direction for I-83. A minor amount of right-of-way would be required between Thornton Road and I-83 (HX) on the north side of the Beltway and for stormwater management.

From I-83 (HX) to west of MD 147 (Harford Road), the existing median is 26-28 feet wide. Therefore, the additional travel lanes would be placed outside of the existing roadway. Minimal right-of-way would be required for the improvements and stormwater management. The bridges over the Beltway at MD 139 (Charles Street), MD 45 (York Road), MD 146 (Dulaney Valley Road), Providence Road and Old Harford Road would be replaced.

From west of MD 147 (Harford Road) to Putty Hill Avenue, the widening will be constructed as part of the extension of MD 43 (Whitemarsh Boulevard), which is now under construction. This area is considered outside of the study limits, except for improvements to Harford Road and its interchange as described herein.

From Putty Hill Avenue to MD 702 the median varies between 44 and 54 feet wide. The Beltway widening would occur primarily within the median. Through the I-95 interchange one lane would be added to the mainline in each direction. Additional ramp, deceleration lane, and acceleration lane capacity improvements will be constructed as part of the I-95 widening project from the Beltway to MD 24. Minimal right-of-way would be required for this segment for stormwater management.

Alternate 2 also proposes to reconstruct the interchange ramp from northbound MD 542 (Loch Raven Blvd.) to eastbound I-695 to a higher design speed. This ramp has been identified as a High Accident Interchange Ramp (HAIR) and was the site of a fatal accident in 1987.

Auxiliary lanes are proposed wherever the distance between acceleration and deceleration lanes is less than 1500 feet. These auxiliary lanes would better accommodate merging, diverging and weaving movements.

c. Alternate 2A: Realignment at MD 542

Alternate 2A proposed realigning the Beltway, both horizontally and vertically, in the vicinity of the MD 542 (Loch Raven Boulevard/Cromwell Bridge Road) Interchange between Providence Road and Perring Parkway.

This alternative would only be considered in combination with Alternate 2 outside the limits of this realignment. It should not be thought of as an alternative to Alternate 2 for the entire length of the Study. This alternative alignment improves the I-695 design speed from 50 MPH to 70 MPH.

This alternative would provide a long term improvement to highway safety and traffic operation. The horizontal curves would be eased, shifting the Beltway alignment northward. Also, the steep grade would be reduced through the area of the relocation. New mainline bridges would be reconstructed over Loch Raven Boulevard and Cromwell Bridge Road. An eastbound collector-distributor road would be provided on portions of the existing inner loop roadway and bridges. Dunwoody Road would require relocation.

Traffic operation and especially highway safety would be significantly improved by increases in the horizontal and vertical sight distances and reduction of the grade. The collector/distributor roadway would also improve traffic operation and reduce accident potential by reducing the number of ramp terminal conflict points on the eastbound mainline from four (4) to two (2). These are important improvements considering that this segment of the Beltway previously experienced accident rates of about three times that of the statewide average for highways of similar type. This alternative would also marginally improve highway capacity by reducing the grade.

Maintaining traffic during the lengthy construction period would be complicated. The loss of capacity through the construction period would be complicated. The loss of capacity through the construction area would add to congestion that already exists during peak periods.

d. Stevenson Road Intersection

This option proposes adding a free right turn from northbound Stevenson Road to the connection ramp to eastbound I-695. This improvement would decrease delay by reducing projected intersection congestion.

e. Greenspring Avenue/MD 133 Intersection

This option proposes adding a lane to both MD 133 approaches and the southbound Greenspring Avenue approach to the signalized intersection. This improvement would decrease delay by reducing projected intersection congestion which could influence the operation of the Greenspring Avenue Interchange with the Beltway.

f. I-83 (JFX)/MD 25 (Falls Road) Interchange

Option A would retain the existing geometry but extends the northbound I-83 (JFX) to westbound I-695 acceleration lane beyond the top of the steep westbound Beltway up-grade. This low cost option does not relieve long term traffic problems in the weave area along northbound I-83 (JFX). Option A would required additional lengthening of the Falls Road bridge beyond that required, for Alternate 2.

Option B proposes a two lane flyover ramp with a 50 MPH design speed from northbound I-83 (JFX) to the westbound Beltway. This option increases safety and capacity by enhancing geometrics and eliminates a weave on this interstate to interstate movement. Option B would require another span on the Falls Road Bridge. The intersection of Falls Road and Hillstead Drive would require minor adjustments.

Option C proposes reconstruction of the outer connection from the eastbound Beltway to southbound I-83 (JFX) as a realigned two lane ramp. This option would increase the ramp design speed from 35 to 50 MPH. Improved operation would result, as well as a decrease in accidents.

Option D would widen the existing ramp from the eastbound Beltway to southbound I-83 (JFX) to two lanes rather than realigning the ramp. Both Options C & D include widening southbound I-83 (JFX) southward to the existing truck climbing lane at Falls Road. The bridge carrying I-83 (JFX) over the Jones Falls and Falls Road at Rockland would require widening.

g. I-83 (HX)/MD 139 Interchange

Option A proposes to improve the sight distance of the existing eastbound Beltway to northbound I-83 (HX) ramp by increasing the outside shoulder width. This would improve the safety and efficiency of the exit by increasing the sight distance on the directional ramp.

Option C proposes a southbound auxiliary lane along Charles Street from the Beltway interchange through the Kenilworth Drive/Bellona Avenue intersection. The left turn bay for turning movements from southbound MD 139 to Kenilworth Drive would be lengthened. This option would alleviate weaving problems and decrease delay due to intersection congestion.

Option D proposes construction of an exit from the westbound Beltway to both I-83 (HX) and MD 139 (Charles Street) beginning east of Charles Street. This option involves major reconstruction of the northern portion of the Charles Street interchange. This two lane ramp would deliver traffic to an exit ramp to Charles Street and then continue under the reconstructed Charles Street bridge to I-83 (HX). This would eliminate the unacceptable weave between MD 139 and I-83 (HX) on the Outer Loop. There would be significant traffic disruption during the ramp construction and during reconstruction of the Charles Street Bridge. It is anticipated that the proposed Towson Light Rail Transit Station will be completed prior to this project, which would require the relocation of the access road.

None of these Options are alternatives to one another.

h. MD 45 (York Road) Interchange

Option B proposes replacing the loop ramp in the northwest quadrant with a signalized left turn spur to southbound York Road on the westbound Beltway exit ramp. This improvement would eliminate a very substandard weaving area on the westbound Beltway at the expense of increasing delay on York Road. The improvement would also improve the alignment of the southbound to westbound outer connection ramp.

Option C proposes improvements to the West Road/Beltway ramps and the West Road/York Road intersection. The movements to and from the Beltway and West Road would be made free flowing by adding intersection channelization and minor widening. This would alleviate back-ups onto the Beltway. Option C is not a alternative to Option B.

i. MD 146 Interchange

Option A proposes a two-lane, free flowing, outer connection from westbound Beltway to northbound MD 146 via Hampton Lane. Traffic exclusive to Hampton Lane would yield to ramp traffic. This would improve travel efficiency by eliminating unnecessary stops for most motorists.

This option also includes an improved turning radius on the eastbound to southbound outer connection at Dulaney Valley Road.

j. Providence Road Interchange

The Providence Road Bridge over the Beltway would be lengthened and widened under Alternate 2. Additional improvements at this location would address traffic operation along Providence Road. From Breezwick Road/Southwick Drive to just north of the Park-and-Ride lot, Providence Road would be widened to four continuous lanes to allow for left turning movements. This would minimize most turning conflicts and improve travel efficiency.

k. MD 41 (Perring Parkway) Interchange

Options A and B, propose collector-distributor roads on both the eastbound and westbound Beltway, respectively. These improvements would remove weaving conflicts from the through roadways. Both options would require additional bridges over Perring Parkway. This could be constructed with minor disruption to traffic.

Option E addresses the weave problem on northbound MD 41 between the on ramp from the westbound Beltway and Joppa Road intersection. This option proposes to widen the ramp to two lanes, widen northbound MD 41 to three lanes, and signal control the ramp and northbound MD 41. This would eliminate the unsafe weave, but would create an additional signal on MD 41.

1. MD 147 (Harford Road) Interchange

Option A proposes to eliminate the loop ramp in the northeast quadrant by providing for a left turn on Harford Road. The outer connection ramp in this quadrant would then be realigned to facilitate traffic flow onto northbound Harford Road. This would eliminate an unsafe weave on the Beltway, but would create an additional signal on MD 147.

Option B proposes to widen Harford Road to five lanes from just south of Putty Hill Avenue to the existing five lane section at Second Avenue. This would also include eliminating access to Grendon Lane. A short replacement street would be constructed between Grendon Lane and Edgewood Avenue through an unimproved lot. Left turns would be prohibited from the southbound outer connection ramp to California Avenue. The center turn lane would alleviate left turning conflicts on MD 147.

m. US 1 (Belair Road) Interchange

This option proposes to widen Belair Road to provide a southbound auxiliary lane from the eastbound Beltway to southbound US 1 ramp through the signalized intersection at Fowler Avenue and the Bel Air Beltway Plaza Shopping Center. Left turns would be prohibited from the southbound connection ramp to Overton Avenue. This would eliminate an unsafe weave on U.S. 1. Two business and two residential relocations would be required due to access denial.

n. MD 7/US 40 Interchange

Improvements at these interchanges propose to remove the loop ramps in the southeast and northwest quadrants of US 40. Left turns at the signalized intersections would replace these movements. MD 702 would be accessed by a left turn from northbound US 40 via a loop ramp. Additional improvements would relocate Golden Ring Road toward the south, widen US 40 to six lanes from Relocated Golden Ring Road to Rossville Boulevard, and eliminate the eastbound on ramp from MD 7. These improvements would operate as a system to improve the Beltway and US 40 operational and safety problems associated with lack of highway capacity and substandard geometric design. These improvements would alleviate unsafe weaves on eastbound I-695 between MD 7 and US 40 and between US 40 and MD 702.

3. Selected Build Alternates

a. Modifications of Alternates Following Public Hearing

Following the Public Hearing, several modifications of alternates were investigated. The investigation was completed in response to comments received at the Public Hearing, and comments received from various agencies.

At the Greenspring Avenue/MD 133 Intersection two modifications were investigated. The first modification would shift the roadway widening to the north. This would impact and unimproved property and avoid several residential properties. The second modification would provide a left turn bay for northbound Greenspring Avenue.

A variation of I-83 (JFX)/MD 25 (Falls Road) Interchange Option C was investigated. This modification would have provided a 40 MPH design speed instead of 50 MPH. The two lane ramp would be shifted further to the east to minimize wetland impacts. This would require that the I-83 (JFX) mainline and one other ramp to be reconstructed. Additionally new wetland sites would be impacted.

A modification of MD 45 (York Road) Interchange Option C was investigated. This modification would have created a redundant movement by constructing a spur from the loop ramp in the southeast quadrant. The spur would provide a second movement to southbound MD 45. This would require another traffic signal on MD 45.

Several modifications at MD 147 (Harford Road) Interchange were investigated to help facilitate the northbound directional ramp merge. Two options would signal control northbound MD 147 and the directional ramp. Another option would reduce northbound MD 147 to one lane to allow the directional ramp to be a lane add instead of a merge.

A modification of the US 1 Interchange Option was investigated. The modified option would provide an auxiliary lane from the southbound directional ramp to a right turn only at the intersection with Fowler Avenue/Bel Air Beltway Plaza. The southbound directional ramp would be reconstructed. The radius at the merge area would be reduced from $\pm 350'$ (35 MPH) to $100'$ (under 20 MPH). This reduction in radius will increase the weave section along US 1 by $\pm 100'$. Additionally it will provide a small separation between the ramp and a driveway (access to two commercial properties and two residential properties) which previously was not available. The auxiliary lane between the ramp and intersection would be aligned where the existing southbound right lane is, requiring US 1 to be shifted to the east. The shift of the roadway was provided so that no right-of-way would be required from Double Rock Park. The tie-in with Overton Avenue will be shifted slightly ($\pm 85'$) to the south due to the shift of US 1.

Four new options were investigated for the MD 7/US 40/MD 702 interchanges. The first option would relocate MD 695 from east of I-95 to the sharp curves near Chesaco Avenue. The four-lane relocated roadway would cross MD 7 just north of MD 588, cross US 40 north of Best, cross the CSX railroad, before swinging south to tie-in at the sharp curves near Chesaco Avenue. Relocated MD-695 would carry through traffic and existing MD 695 would remain and provide access for local traffic to MD 7, US 40 and MD 702. The 4 lane relocation would be south of the existing roadway for a length of 3.5 miles. The second and third options would provide a collector-distributor (C-D) from east of I-95 to MD 702. Option 2 proposed to make MD 695 an express movement between I-95 and MD 702. MD 695 would be physically restricted to the interior lanes, without an opportunity to interchange at MD 7, US 40, or MD 702. The exterior lanes would function as a collector-distributor (C-D) system, and would be designated as MD 702 from east of I-95 to MD 702, for traffic desiring to interchange at MD 7, US 40, and MD 702. Option 3 proposed an identical roadway configuration, but with MD 702 being made the physically restricted express movement, and MD 695 being made the locally interchanging roadway. Both options would reconstruct the MD 7 interchange to a 1/2 cloverleaf and the US 40 interchange would be reconstructed to a 3/4 cloverleaf. A slip ramp from MD 702 to MD 695 westbound would also be provided. Option 4 would provide four through lanes in each direction, reconstruct the MD 7 interchange to a 1/2 cloverleaf and would reconstruct the US 40 interchange to an urban diamond.

b. Selected Build Alternates

A complete description of the alternates precedes this section. Mapping for the Selected Build Alternate is shown on P.III-18 through P.III-36. Specific design characteristics of the Selected Build Alternate is described on P.III-46 through P.III-50.

The selected build alternate for the Beltway mainline is Alternate 2. This consists of adding one lane to the Beltway in each direction (see typical section on P.III-47). While this will provide additional capacity to the Beltway, it is anticipated that by the design year 2015 the Beltway will be operating at LOS E and in some areas LOS F during peak hours. As discussed previously, additional through lanes beyond Alternate 2 were dropped due to right-of-way impacts and costs.

The selected build alternate includes the following interchange options:

- ◇ Stevenson Road Intersection. This option proposes to add a free right turn from northbound Stevenson Road to the connection ramp to eastbound I-695.
- ◇ Greenspring Avenue/MD 133 (Old Court Road) Intersection. This will include the modifications as investigated following the Public Hearing. This option will provide additional through lanes and left turn lays at the intersection. While this intersection is anticipated to operate at LOS E in both the AM and PM peak hours, any additional improvements would create substantial right-of-way impacts.
- ◇ I-83 (JFX)/MD 25 (Falls Road) Interchange Option A and B. The no-build alternate was selected in lieu of these improvements. The no-build alternate and Option A will not meet the forecasted operational needs for the connection between northbound I-83 (JFX) and westbound I-695. While Option B will meet the forecasted operational needs, it impacted the Greenspring Valley Historic District, had public opposition and was costly.
- ◇ I-83 (JFX)/MD 25 (Falls Road) Interchange Option C. The modifications investigated following the Public Hearing will not be included with the selected build alternate. Option C as presented at the Public Hearing was selected. This will provide a two-lane ramp between eastbound I-695 and southbound I-83. The diverge from eastbound I-695 is anticipated to operate at LOS F/F (AM/PM peak hours) in the design year. While this is not preferable, it is an improvement over the no-build alternate. An additional mainline lane would be required for a better level of service.
- ◇ I-83 (HX)/MD 139 (Charles Street) Interchange Option A. This will improve the sight distance of the connection ramp from eastbound I-695 to northbound I-83 (HX).
- ◇ I-83 (HX)/MD 139 (Charles Street) Interchange Option C. The no-build alternate was selected in lieu of these improvements. The no-build was selected due to a lack of current operational problems and a doubt of future operational problems.

- ◇ I-83 (HX)/MD 139 (Charles Street) Interchange Option D. This will reconstruct the exit from westbound I-695 to northbound I-83 (HX).
- ◇ MD 45 (York Road) Interchange Option B. This will replace the loop ramp in the northwest quadrant with a signalized left turn spur to southbound MD 45. An additional signal on MD 45 may increase delays slightly.
- ◇ MD 45 (York Road) Interchange Option C. The no-build alternate was selected in lieu of these improvements. Improvements to West Road will be completed by a developer in agreement with Baltimore County.
- ◇ MD 146 (Dulaney Valley Road) Interchange Option. This will provide a two-lane, free flowing, outer connector from westbound I-695 to northbound MD 146.
- ◇ Providence Road Interchange. The no-build alternate was selected in lieu of these improvements. The no build alternate was selected because no operational problems were forecasted at this location.
- ◇ MD 41 (Perring Parkway) Interchange Option A and Option B. This would provide a collector-distributor roadway on both the eastbound and westbound beltway respectfully. While the merge/diverge from the C-D roadways are anticipated to operate at LOS F in the design year, these options will remove LOS F weaves from the Beltway. To improve the merge/diverge level of service would require additional mainline lanes to the Beltway.
- ◇ MD 41 (Perring Parkway) Interchange Option E. The no-build alternate was selected in lieu of these improvements. The no-build alternate was selected due to a lack of current operational problems and a doubt of future operational problems.
- ◇ MD 147 (Harford Road) Interchange Option A. The no build alternate was selected in lieu of these improvements. The no-build alternate was selected due to the wetland impacts, and additional operational concerns Option A would have created to MD 147. The no-build alternate will retain an anticipated LOS F weave between the loop ramps in the design year.
- ◇ MD 147 (Harford Road) Interchange Option B. This will widen MD 147 to five lanes between Putty Hill Avenue and Second Avenue.
- ◇ US 1 (Belair Road) Interchange. The modified option investigated following the Public Hearing was selected. This will reconstruct the directional ramp to southbound US 1 and Overton Avenue, and provide an auxiliary lane along southbound US 1 between the directional ramp and the intersection of Fowler Avenue/Bel Air Beltway Plaza. No right-of-way will be required from Double Rock Park, requiring a slight shift of US 1 eastward. The modified option would not require the denial of access along southbound US 1 that was required with the Public Hearing option. Therefore the access to Double Rock Park maintenance facilities will not be affected and the two commercial and the two residential displacements are not required. The intersection of US 1 with Fowler Avenue/Bel Air Beltway Plaza is anticipated to operate at LOS A/F (AM/PM peak hour) in the design year. An additional southbound lane through the intersection would be required to improve the PM peak hour level of service to LOS D.

The realignment of the directional ramp and the extension of the auxiliary lane on southbound US 1 was developed to address operation and safety concerns at the residential and commercial entrances along southbound US 1 between I-695 and Fowler Avenue. Should safety problems become apparent at these locations in the future, other alternatives will be investigated.

- ◇ MD 7/US 40/MD 702 Interchange Option 3 is the selected option in lieu of MD 7/US 40 Option E. Option 3 provides the necessary four through lanes for capacity requirements instead of three through lanes Option E proposed. Additionally Option 3 addresses substandard weaves at MD 7 and MD 702 which were not addressed with Option E. Option 3 provides a 3/4 cloverleaf at US 40 requiring one additional signal on US 40, while Option E provided a 1/2 cloverleaf at US 40 requiring two additional signals on US 40 and greater delays.

The selection of Option 3, as part of the selected build alternate, does not create any significant impacts and does not substantially change impacts from the Environmental Assessment and Public Hearing.

c. Phased Construction



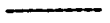





This project includes approximately 17 miles of I-695 and 18 interchanges. Due to the size of this project and the associated high cost, a phasing of construction is anticipated. Additionally a Project Planning Study is being conducted for the southern portion of I-695 from I-70 to west of MD 170 for widening and interchange improvements. A priority system for all of the improvements is being established. Several specific phasing techniques were discussed for the project.

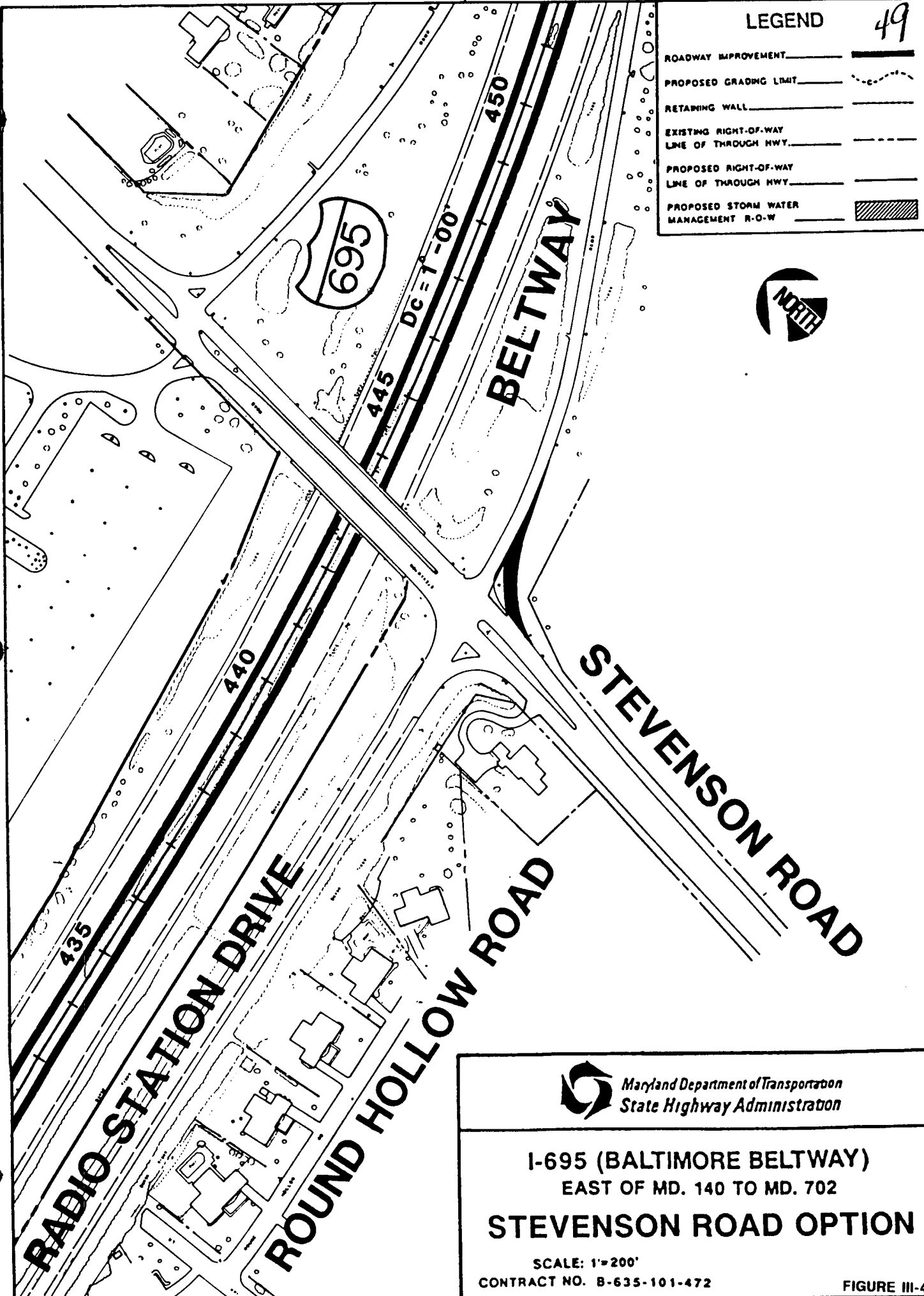
The construction of Alternate 2 from MD 140 (Reisterstown Road) to MD 25 (Falls Road) would construct an additional mainline lane in the median. This would require the MD 25 (Falls Road) bridge to be replaced. The I-83 (JFX)/MD 25 Interchange Option C would require an incremental increase in the bridge length. It was determined that Alternate 2 would be constructed first with minimum shoulders (4' median, 7 1/2' outside) under the bridge to avoid replacement. The bridge would then be completely replaced with the construction of I-83 (JFX)/MD 25 Interchange Option C.

The MD 7/US 40/MD 702 Interchange Option 3 could be constructed in stages. The first stage would widen the Stemmers Run bridge and provide six through lanes on US 40 through the Rossville Boulevard intersection. The second stage would eliminate the loop ramp in the northeast quadrant. This movement would be replaced with a left turn spur on US 40. The third stage would replace the MD 695 bridge over US 40 to the ultimate section. This bridge is structurally deficient and has a life expectancy of 5-10 years. The fourth stage would widen MD 695 in the median to provide an additional lane in each direction. The fifth stage would widen MD 695 to the outside and complete the remaining interchange improvements.

LEGEND

49

- ROADWAY IMPROVEMENT _____ 
- PROPOSED GRADING LIMIT _____ 
- RETAINING WALL _____ 
- EXISTING RIGHT-OF-WAY _____ 
- LINE OF THROUGH HWY. _____ 
- PROPOSED RIGHT-OF-WAY _____ 
- LINE OF THROUGH HWY. _____ 
- PROPOSED STORM WATER MANAGEMENT R-O-W _____ 



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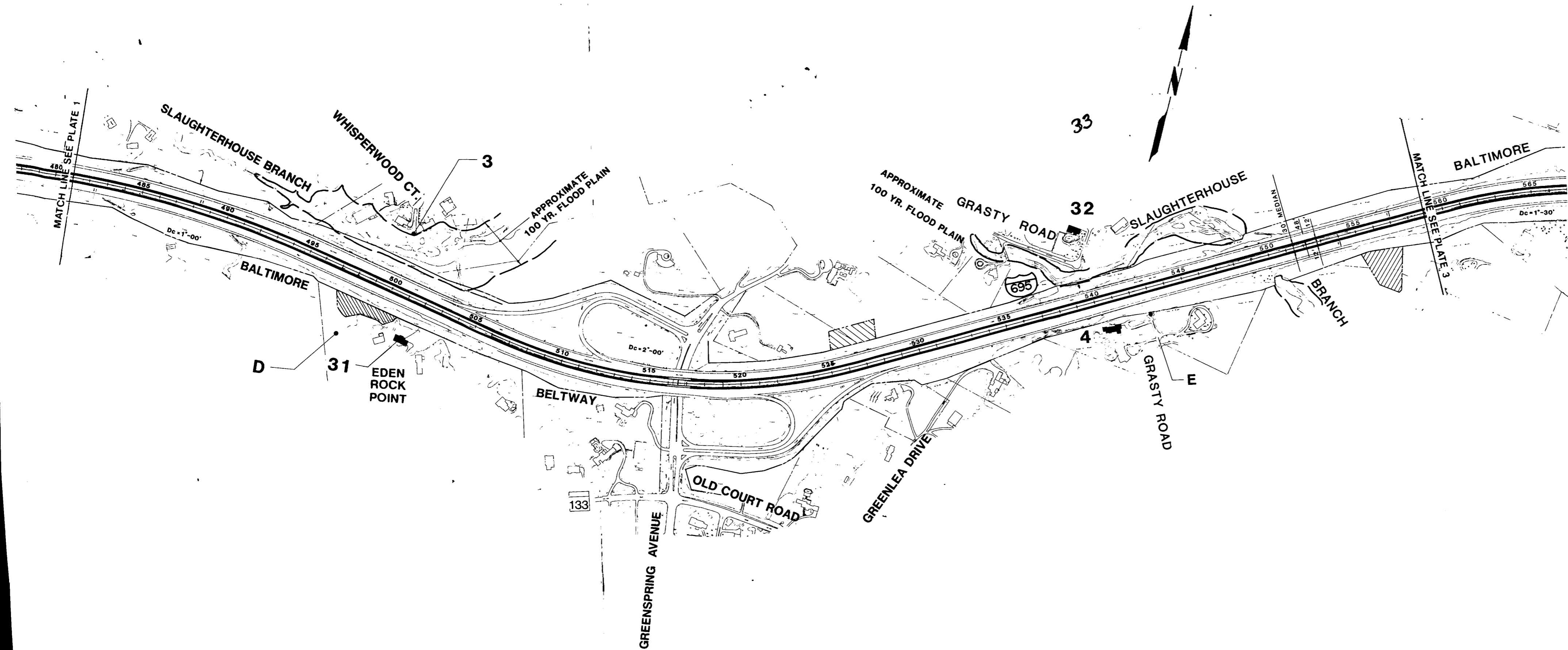
I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702

STEVENSON ROAD OPTION

SCALE: 1"=200'
CONTRACT NO. B-635-101-472

FIGURE III-4

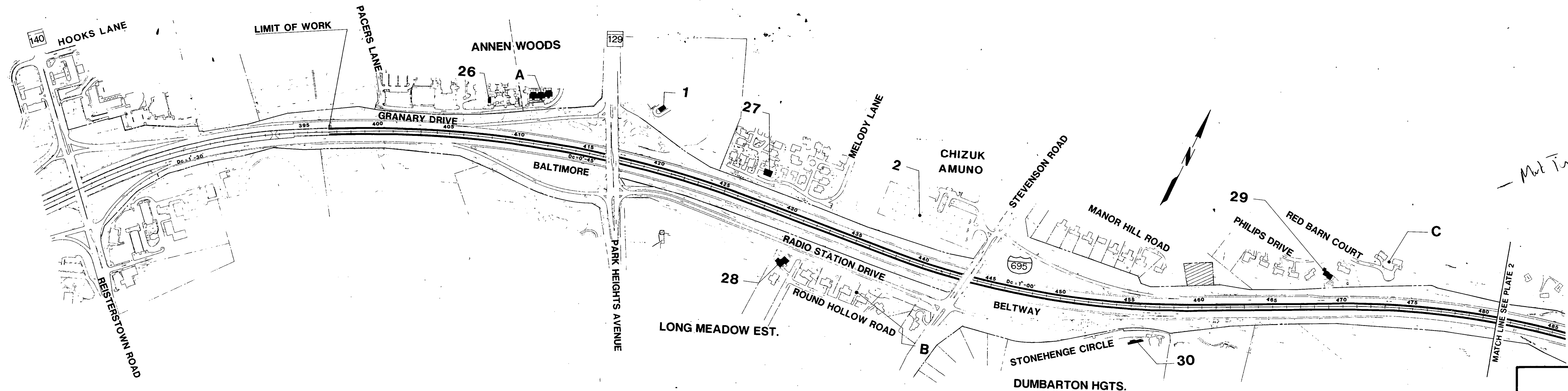
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PROPOSED GRADING LIMIT.....	~~~~~
RETAINING WALL.....	- - - - -
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	—————
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	—————
PROPOSED STORM WATER MANAGEMENT R-O-W.....	▨▨▨▨▨
NOISE RECEPTOR.....	11 ———
AIR QUALITY RECEPTOR.....	A ———




 Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (E. OF STEVENSON RD. TO E. OF GREENSPRING AVE.)
 SCALE: 1"=400' FIGURE III-3
 CONTRACT NO. B-635-151-472 (PLATE 2 OF 9)

ROADWAY IMPROVEMENT.....	—————
PROPOSED GRADING LIMIT.....	~~~~~
RETAINING WALL.....	-----
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	-----
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	-----
PROPOSED STORM WATER MANAGEMENT R-O-W.....	▨
NOISE RECEPTOR.....	11
AIR QUALITY RECEPTOR.....	A



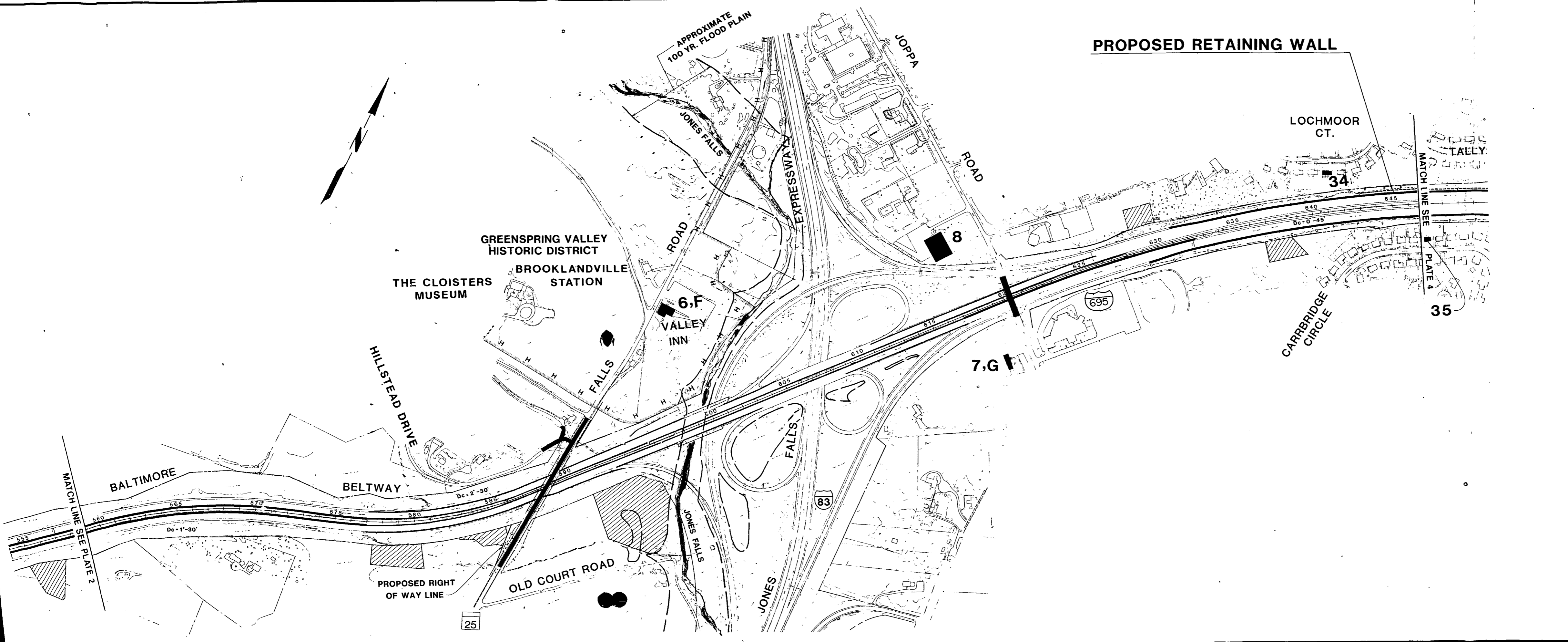
Not To Scale



I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF MD 140 TO EAST OF STEVENSON RD.)
 SCALE: 1"=400' FIGURE III-3
 CONTRACT NO. B-635-151-472 PLATE 1 OF 9

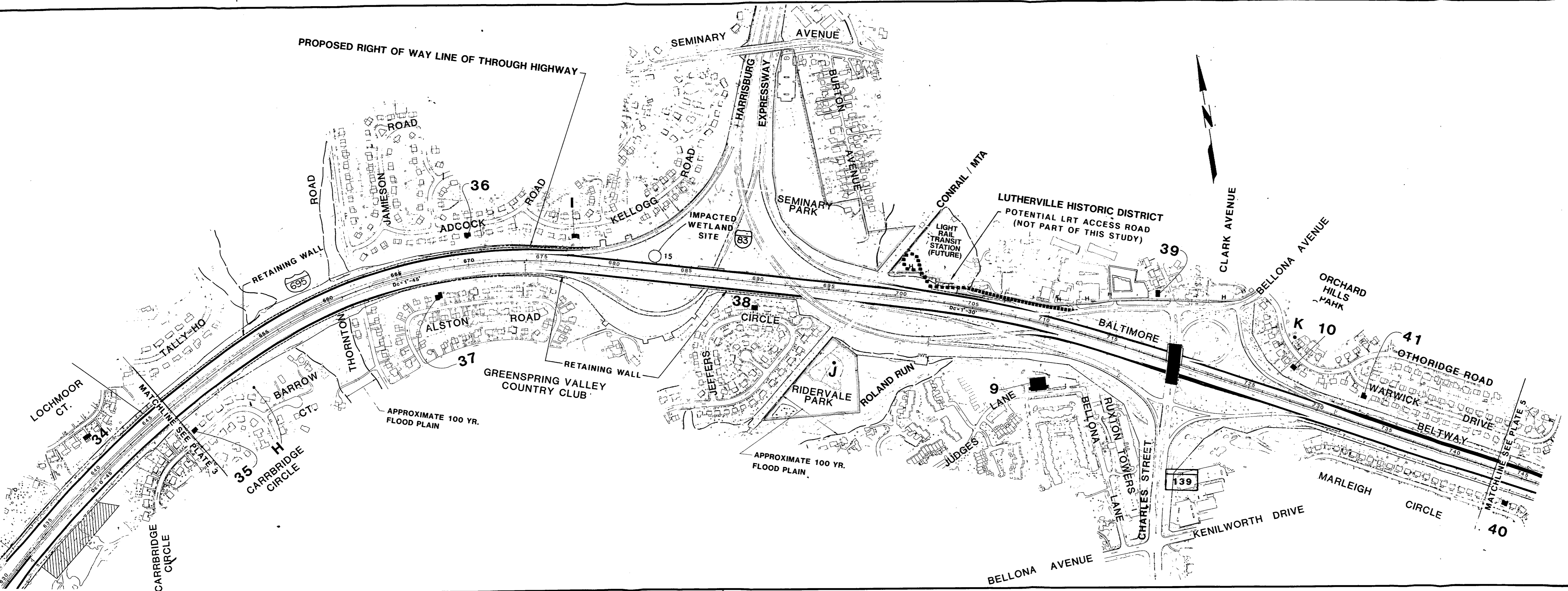
ROADWAY IMPROVEMENT.....	—
PROPOSED GRADING LIMIT.....	—
RETAINING WALL.....	—
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED STORM WATER MANAGEMENT R-O-W.....	▨
NOISE RECEPTOR.....	11 —
AIR QUALITY RECEPTOR.....	A —

PROPOSED RETAINING WALL



I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF GREENSPRING RD TO EAST OF I-83 (JFX))
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

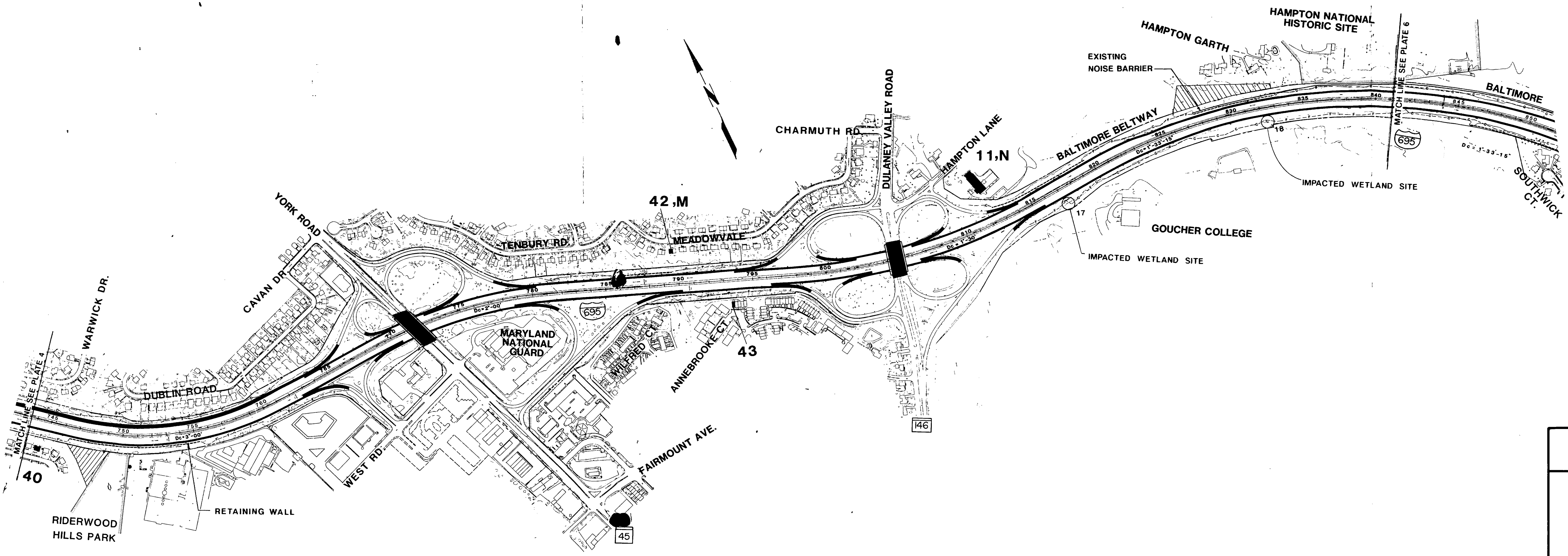
ROADWAY IMPROVEMENT	
PROPOSED GRADING LIMIT	
RETAINING WALL	
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY	
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY	
PROPOSED STORM WATER MANAGEMENT R-O-W	
NOISE RECEPTOR	
AIR QUALITY RECEPTOR	



I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF I-83 (JFX) TO EAST OF MD 139)
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

FIGURE III-3
 (PLATE 4 OF 9)

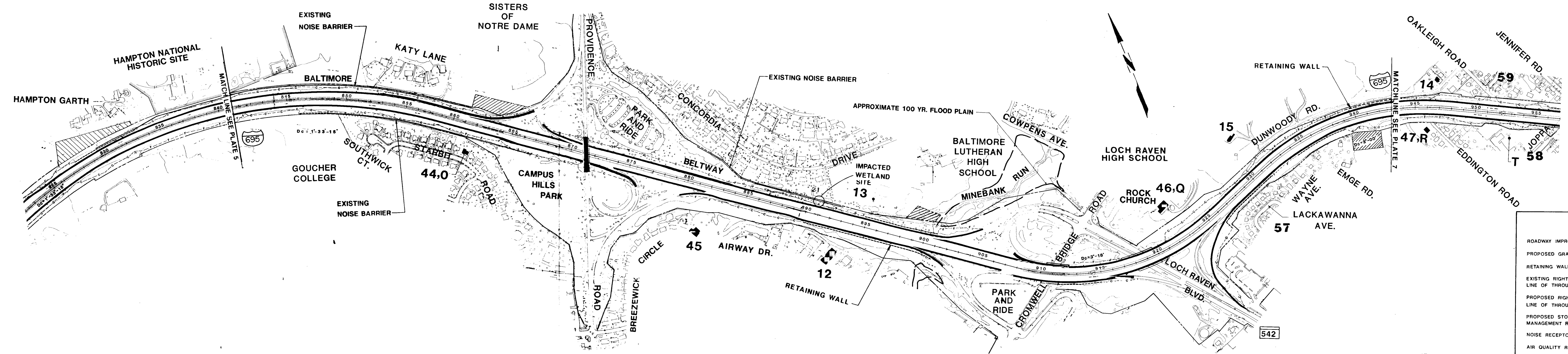
ROADWAY IMPROVEMENT	
PROPOSED GRADING LIMIT	
RETAINING WALL	
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY	
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY	
PROPOSED STORM WATER MANAGEMENT R-O-W	
NOISE RECEPTOR	
AIR QUALITY RECEPTOR	



Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF MD 139 TO EAST OF MD 146)
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

FIGURE III-3
 (PLATE 5 OF 9)



LEGEND

ROADWAY IMPROVEMENT.....	—————
PROPOSED GRADING LIMIT.....	—————
RETAINING WALL.....	—————
EXISTING RIGHT-OF-WAY.....	—————
LINE OF THROUGH HWY.....	—————
PROPOSED RIGHT-OF-WAY.....	—————
LINE OF THROUGH HWY.....	—————
PROPOSED STORM WATER MANAGEMENT R.O.W.....	—————
NOISE RECEPTOR.....	11 —■
AIR QUALITY RECEPTOR.....	A —●

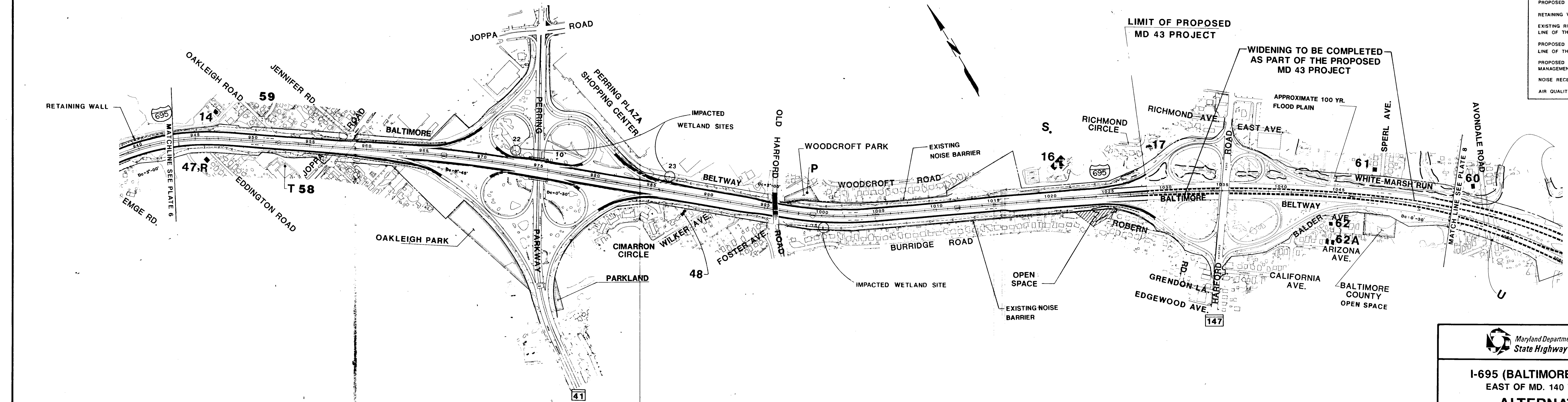

 Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF MD 146 TO EAST OF MD 542)

SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

FIGURE :II-3
 (PLATE 6 OF 9)

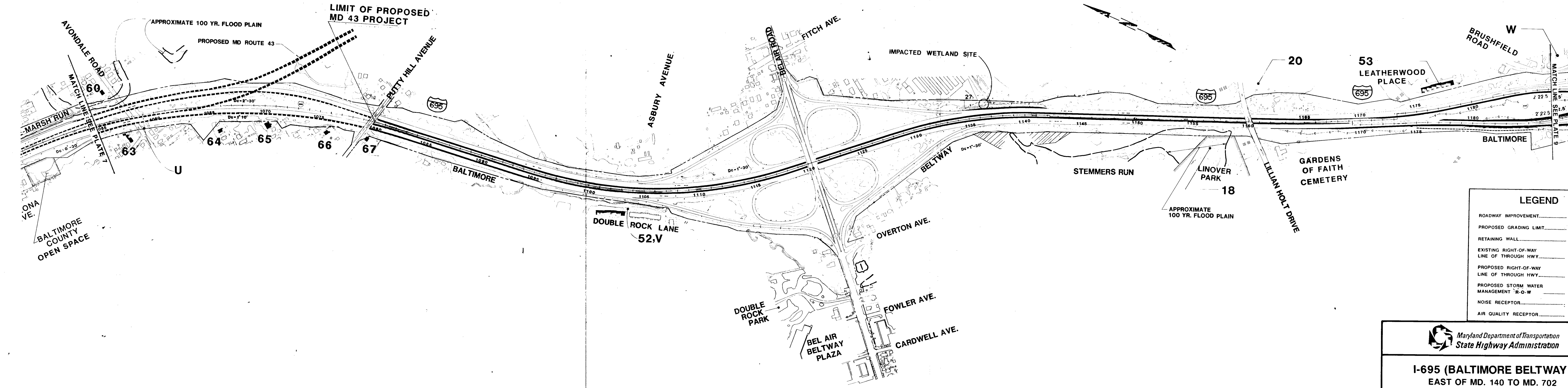
ROADWAY IMPROVEMENT.....	—
PROPOSED GRADING LIMIT.....	—
RETAINING WALL.....	—
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED STORM WATER MANAGEMENT R.O.W.....	▨
NOISE RECEPTOR.....	11 —
AIR QUALITY RECEPTOR.....	A —




 Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
ALTERNATE 2
 (EAST OF MD 542 TO EAST OF MD 147)
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

FIGURE III-3
 (PLATE 7 OF 9)



LEGEND

ROADWAY IMPROVEMENT.....	—————
PROPOSED GRADING LIMIT.....	-----
RETAINING WALL.....	-----
EXISTING RIGHT-OF-WAY.....	-----
LINE OF THROUGH HWY.....	-----
PROPOSED RIGHT-OF-WAY.....	-----
LINE OF THROUGH HWY.....	-----
PROPOSED STORM WATER MANAGEMENT R-O-W.....	-----
NOISE RECEPTOR.....	11 —■
AIR QUALITY RECEPTOR.....	A —●

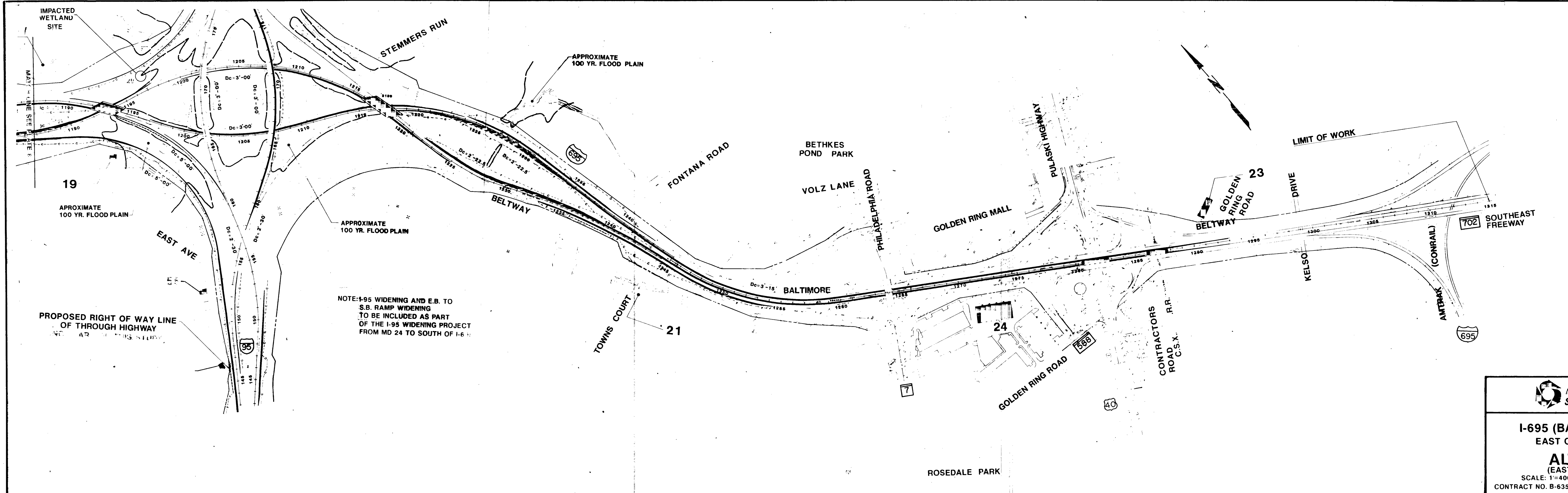
Maryland Department of Transportation
State Highway Administration

I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702

ALTERNATE 2
(EAST OF MD 147 TO EAST OF US 1)

SCALE: 1"=400'
CONTRACT NO. B-635-151-472

FIGURE III-3
(PLATE 8 OF 9)



LEGEND 58

ROADWAY IMPROVEMENT	—
PROPOSED GRADING LIMIT	- - -
RETAINING WALL	- · - · -
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY	- - - - -
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY	— · — · —
PROPOSED STORM WATER MANAGEMENT R-O-W	▨
NOISE RECEPTOR	11
AIR QUALITY RECEPTOR	A

NOTE: I-95 WIDENING AND E.B. TO S.B. RAMP WIDENING TO BE INCLUDED AS PART OF THE I-95 WIDENING PROJECT FROM MD 24 TO SOUTH OF I-695

PROPOSED RIGHT OF WAY LINE OF THROUGH HIGHWAY

Maryland Department of Transportation
State Highway Administration

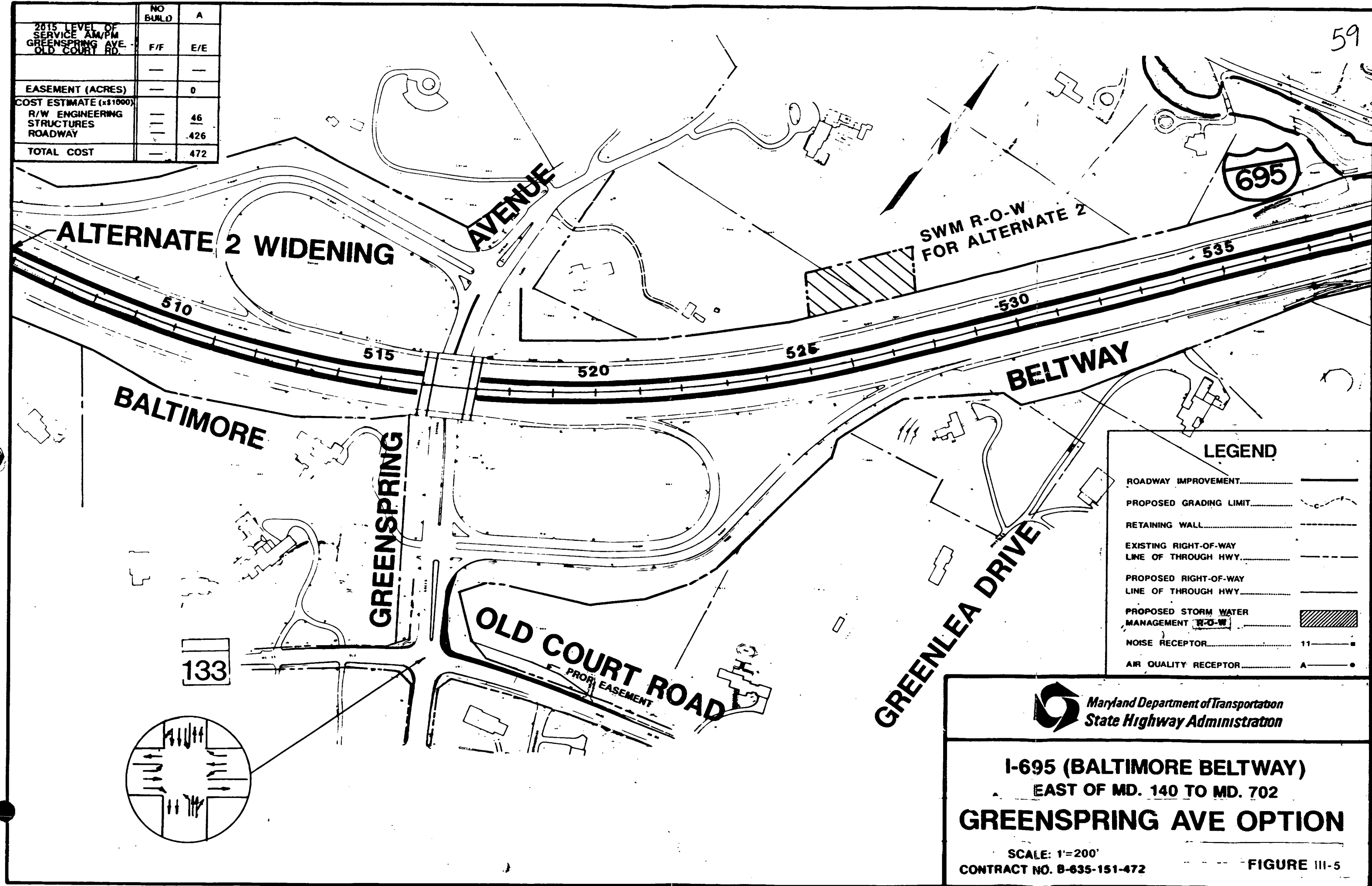
I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702

ALTERNATE 2
(EAST OF US 1 TO MD 702)

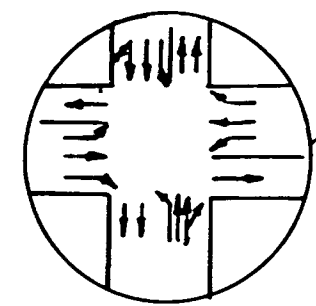
SCALE: 1"=400'
CONTRACT NO. B-635-151-472

FIGURE 11-3
(PLATE 9 OF 9)

2015 LEVEL OF SERVICE AM/PM GREENSPRING AVE. OLD COURT RD.	NO BUILD	A
	F/F	E/E
EASEMENT (ACRES)	—	0
COST ESTIMATE (x\$1000)		
R/W ENGINEERING	—	46
STRUCTURES	—	426
ROADWAY	—	
TOTAL COST	—	472



133



LEGEND

ROADWAY IMPROVEMENT	—————
PROPOSED GRADING LIMIT	- - - - -
RETAINING WALL	—————
EXISTING RIGHT-OF-WAY	—————
LINE OF THROUGH HWY.	—————
PROPOSED RIGHT-OF-WAY	—————
LINE OF THROUGH HWY.	—————
PROPOSED STORM WATER MANAGEMENT R-O-W	▨▨▨▨▨
NOISE RECEPTOR	11 ———
AIR QUALITY RECEPTOR	A ———


Maryland Department of Transportation
State Highway Administration

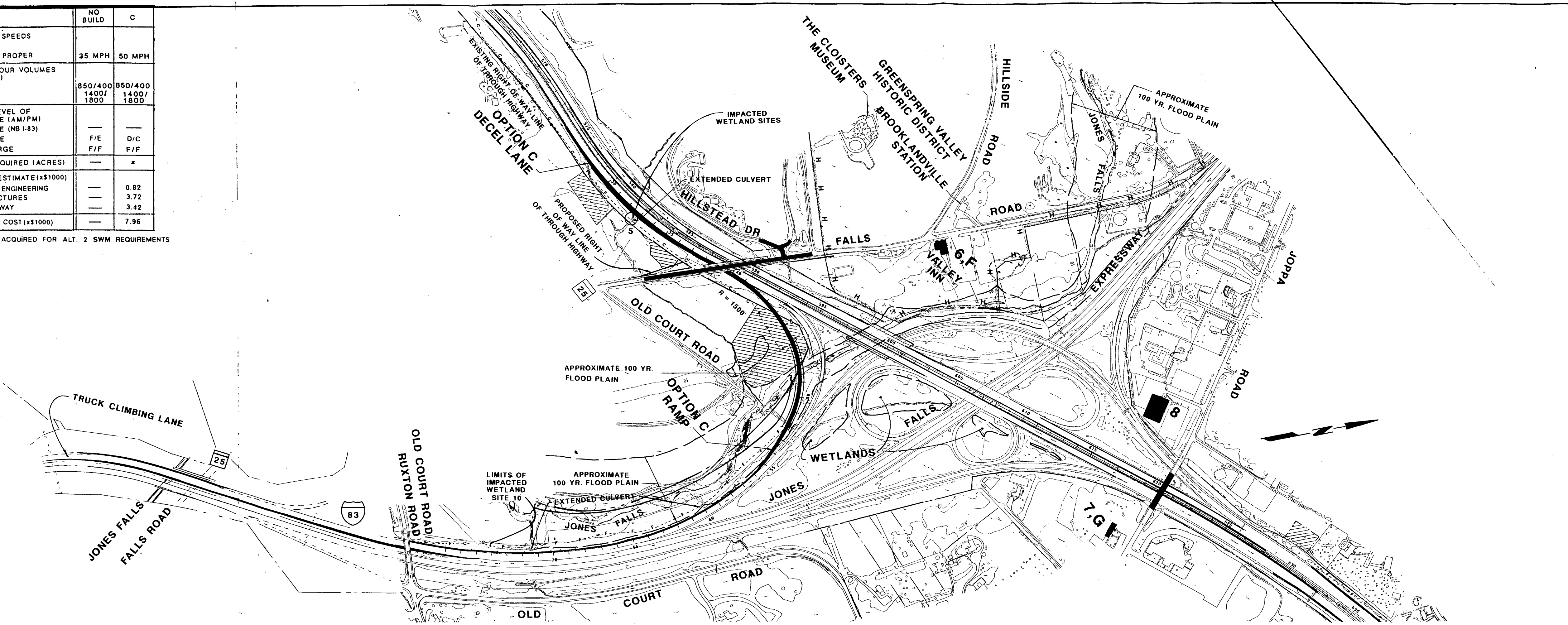
I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702
GREENSPRING AVE OPTION

SCALE: 1"=200'
 CONTRACT NO. B-635-151-472

FIGURE III-5

	NO BUILD	C
DESIGN SPEEDS		
RAMP PROPER	35 MPH	50 MPH
PEAK HOUR VOLUMES (AM/PM)		
1985	850/400	850/400
2015	1400/1800	1400/1800
2015 LEVEL OF SERVICE (AM/PM)		
WEAVE (NB I-83)		
MERGE	F/E	D/C
DIVERGE	F/F	F/F
R/W REQUIRED (ACRES)		*
COST ESTIMATE (x\$1000)		
R/W ENGINEERING		0.82
STRUCTURES		3.72
ROADWAY		3.42
TOTAL COST (x\$1000)		7.96

* R/W ACQUIRED FOR ALT. 2 SWM REQUIREMENTS



LEGEND	
ROADWAY IMPROVEMENT.....	—
PROPOSED GRADING LIMIT.....	—
RETAINING WALL.....	—
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	—
PROPOSED STORM WATER MANAGEMENT R-O-W.....	▨
NOISE RECEPTOR.....	—
AIR QUALITY RECEPTOR.....	—
HISTORIC DISTRICT.....	H


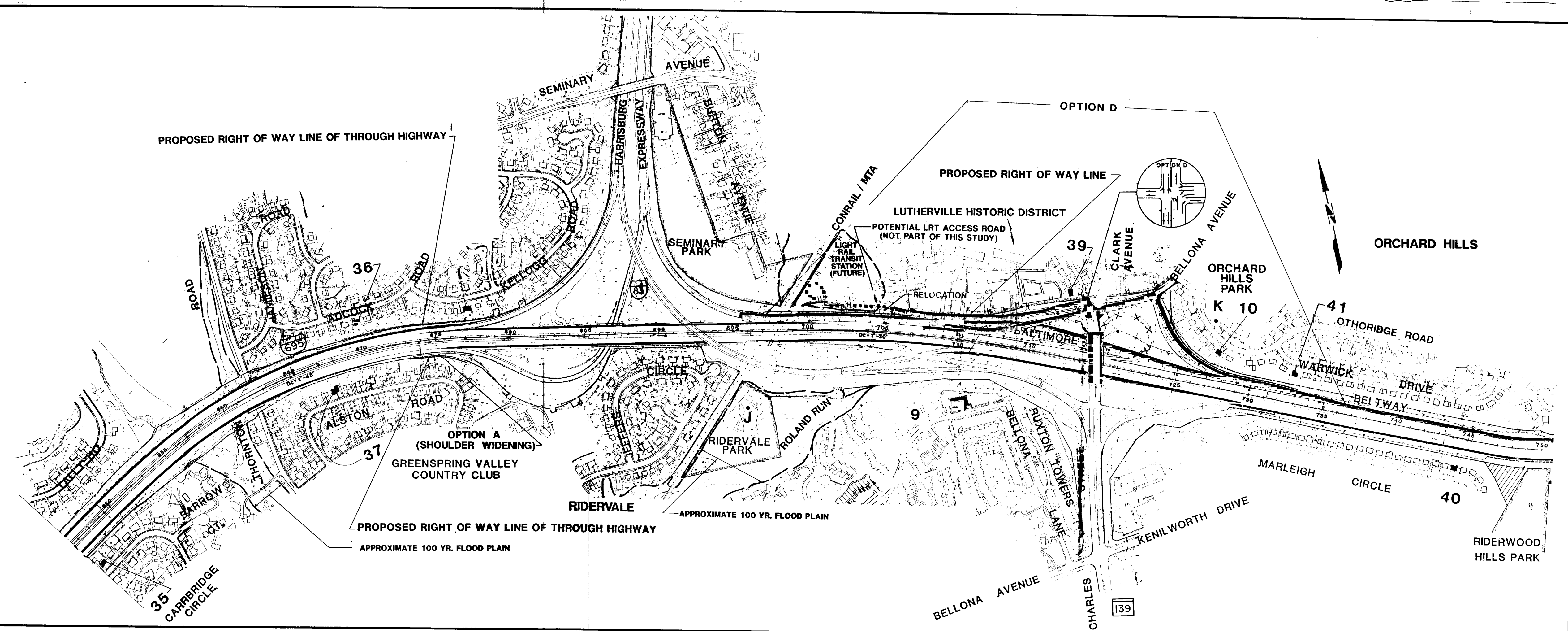

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
I-83 (JFX) OPTION C
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

FIGURE III-6

	NO BUILD	A	NO BUILD	D
DESIGN SPEED				
GORE AREA	36 MPH	50 MPH	—	—
RAMP PROPER	36 MPH	36 MPH	—	—
C-D ROADWAY	—	—	—	60 MPH
PEAK HOUR VOLUME (AM/PM) W.B. MAINLINE E. OF I-83				
1988	—	—	5200/ 5875	3550/ 3850
2016	—	—	5550/ 7500	5550/ 6000
2016 LEVEL OF SERVICE (AM/PM)				
WEAVE	—	—	F/F	—
MERGE	—	—	—	—
DIVERGE	F/F	F/F	—	D/C
INTERSECTION	—	—	C/E	D/E
R/W REQUIRED (ACRES)	—	0.1	—	0.5
RELOCATION	—	—	—	1
COST ESTIMATE (x\$1000)				
R/W RELOC. & EGR STRUCTURES	—	35	—	974
ROADWAY	—	300	—	12,348
TOTAL COST (x\$1000)	—	335	—	18,021



LEGEND	
ROADWAY IMPROVEMENT.....	
PROPOSED GRADING LIMIT.....	
RETAINING WALL.....	
EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....	
PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....	
PROPOSED STORM WATER MANAGEMENT R.O.W.....	
NOISE RECEPTOR.....	
AIR QUALITY RECEPTOR.....	
HISTORIC DISTRICT.....	

Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
I-83 (HX) / MD 139
OPTIONS A & D
 SCALE: 1"=400'
 CONTRACT NO. B-635-151-472

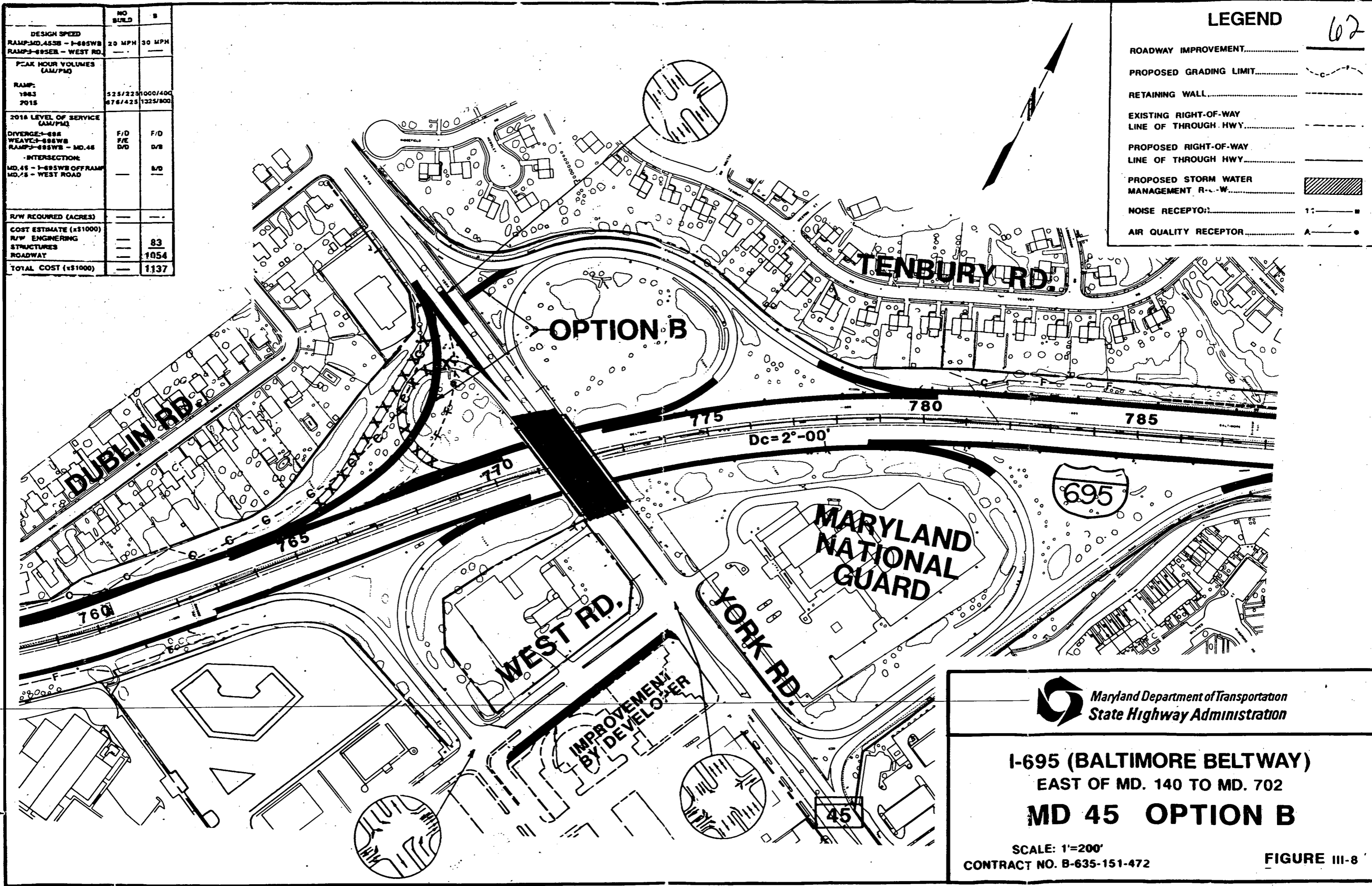
FIGURE III-7

DESIGN SPEED	NO BULD	B
RAMP-MD.45WB - I-695WB	20 MPH	30 MPH
RAMP-695EB - WEST RD.		
PEAK HOUR VOLUMES (AM/PM)		
RAMP:		
1983	525/225	1000/400
2015	676/425	1325/800
2018 LEVEL OF SERVICE (AM/PM)		
DIVERGE-695	F/D	F/D
WEAVE-695WB	F/E	D/B
RAMP-695WB - MD.45	D/D	
INTERSECTION		
MD.45 - I-695WB OFF RAMP		B/D
MD.45 - WEST ROAD		
R/W REQUIRED (ACRES)		
COST ESTIMATE (+\$1000)		
R/W ENGINEERING		83
STRUCTURES		1054
ROADWAY		
TOTAL COST (+\$1000)		1137

LEGEND

- ROADWAY IMPROVEMENT.....
- PROPOSED GRADING LIMIT.....
- RETAINING WALL.....
- EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED STORM WATER MANAGEMENT R.-.W.....
- NOISE RECEPTOR.....
- AIR QUALITY RECEPTOR.....

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Maryland Department of Transportation
State Highway Administration

**I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702
MD 45 OPTION B**

SCALE: 1"=200'
CONTRACT NO. B-635-151-472

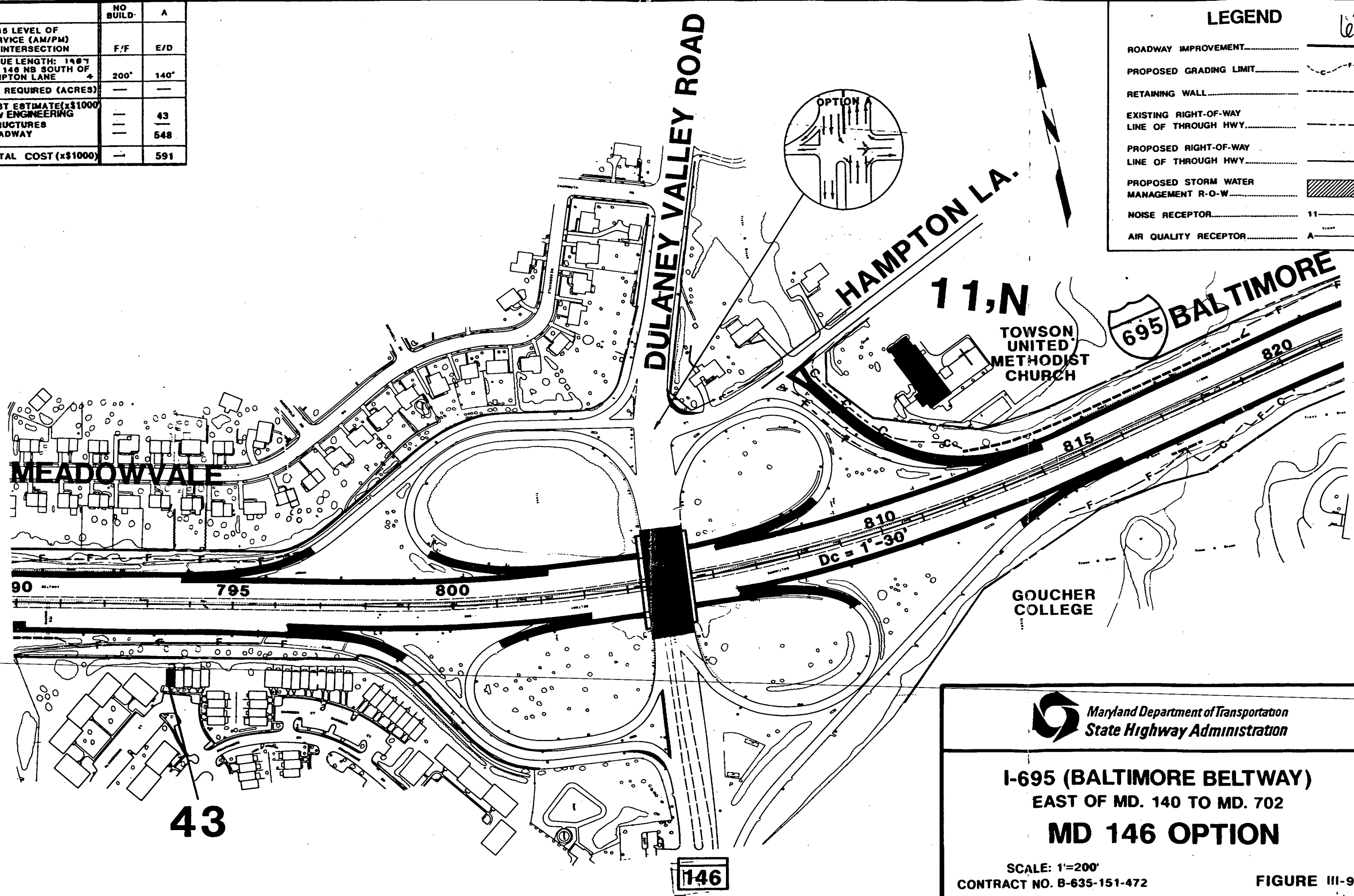
FIGURE III-8

	NO BUILD	A
2015 LEVEL OF SERVICE (AM/PM) INTERSECTION	F/F	E/D
QUEUE LENGTH: 1407 MD. 146 NB SOUTH OF HAMPTON LANE	200'	140'
R/W REQUIRED (ACRES)	—	—
COST ESTIMATE(x\$1000)	—	43
R/W ENGINEERING STRUCTURES ROADWAY	—	548
TOTAL COST (x\$1000)	—	591

LEGEND

63

- ROADWAY IMPROVEMENT.....
- PROPOSED GRADING LIMIT.....
- RETAINING WALL.....
- EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED STORM WATER MANAGEMENT R-O-W.....
- NOISE RECEPTOR.....
- AIR QUALITY RECEPTOR.....

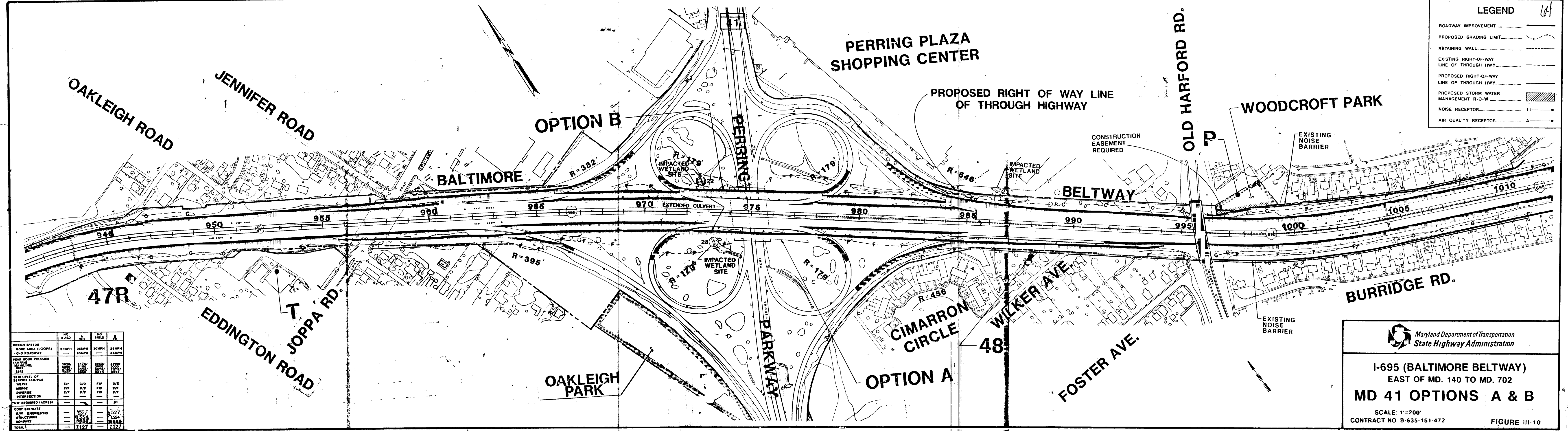


**I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702
MD 146 OPTION**

SCALE: 1"=200'
CONTRACT NO. B-635-151-472

FIGURE III-9

146



LEGEND

- ROADWAY IMPROVEMENT.....
- PROPOSED GRADING LIMIT.....
- RETAINING WALL.....
- EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED STORM WATER MANAGEMENT R-O-W.....
- NOISE RECEPTOR.....
- AIR QUALITY RECEPTOR.....

	NO. BUILT	A	NO. BUILT	B
DESIGN SPEEDS (ONE AREA (LOOPS) C-D ROADWAY)	30MPH	25MPH	30MPH	25MPH
PEAK HOUR VOLUMES (AM/PM)	3336/3178	3178/3336	3336/3178	3178/3336
2018 LEVEL OF SERVICE (AM/PM)	E/F	C/D	F/F	D/E
WEAVE	F/F	F/F	F/F	F/F
DIVERGE	F/F	F/F	F/F	F/F
INTERSECTION				
R/W REQUIRED (ACRES)				91
COST ESTIMATE (\$/M ENGINEERING)	527	527	527	527
STRUCTURES	554	554	554	554
ROADWAY	5500	5500	5500	5500
TOTAL	7127	7127	7127	7127

Maryland Department of Transportation
 State Highway Administration

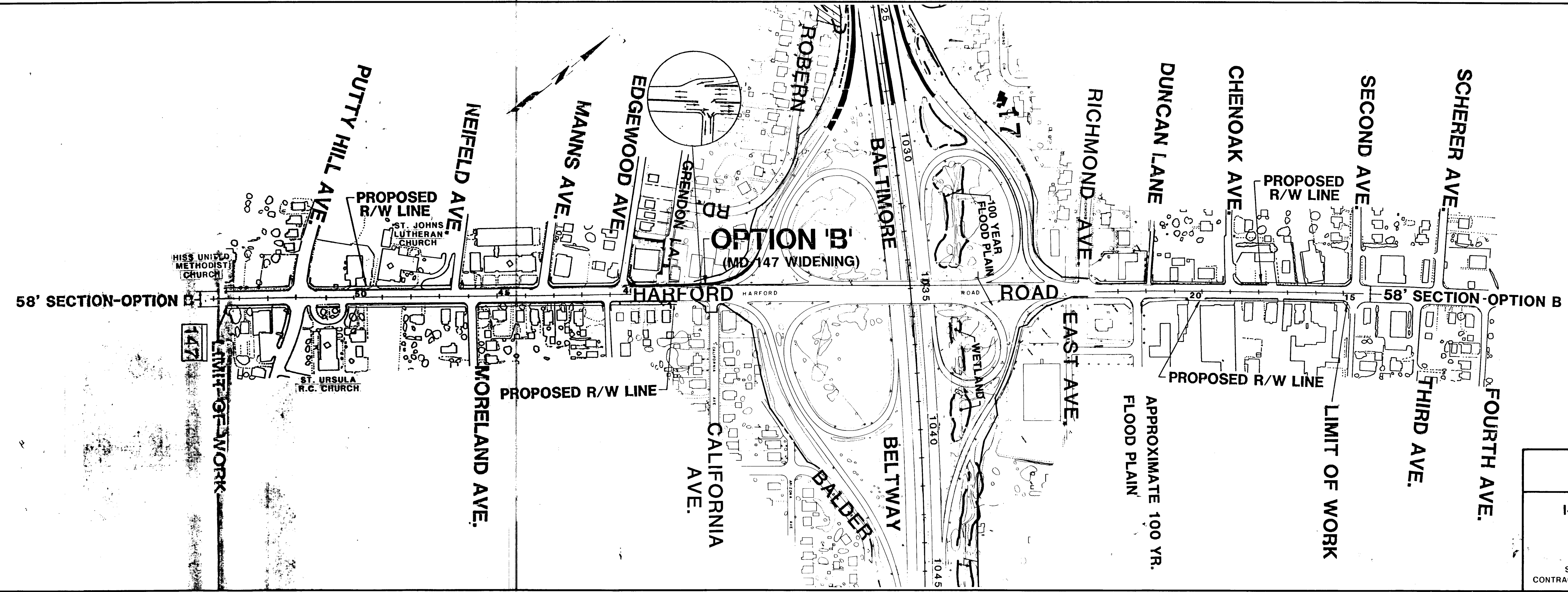
I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
MD 41 OPTIONS A & B

SCALE: 1"=200'
 CONTRACT NO. B-635-151-472

FIGURE III-10

LEGEND 65

- ROADWAY IMPROVEMENT.....
- PROPOSED GRADING LIMIT.....
- RETAINING WALL.....
- EXISTING RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED RIGHT-OF-WAY LINE OF THROUGH HWY.....
- PROPOSED STORM WATER MANAGEMENT R-O-W.....
- NOISE RECEPTOR..... 11
- AIR QUALITY RECEPTOR..... A




OPTION B
(MD 147 WIDENING)

58' SECTION-OPTION B

58' SECTION-OPTION B

	NO BUILD	B
R/W REQUIRED (ACRES)	—	0.7
COST ESTIMATE (x\$1000)		
R/W ENGINEERING	—	441
STRUCTURES	—	1399
ROADWAY	—	
TOTAL COST (x\$1000)	—	1840



 Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD. 140 TO MD. 702
MD 147 OPTION B

SCALE: 1"= 200'
 CONTRACT NO. B-635-151-472

FIGURE III-11

OVERLEA

**DOUBLE
ROCK
PARK**

**STEMMERS
RUN**

695

De-1-30

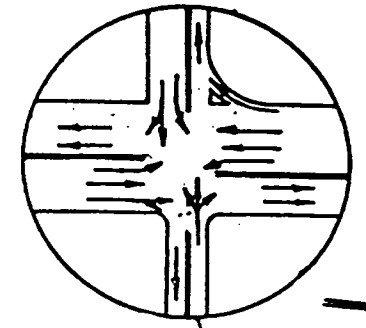
BRIDGE TO BE
REPLACED BY
MD 43 PROJECT.
THIS INCLUDES A
SHIFT OF THE BRIDGE
FOR M. O. T. AND
IMPROVEMENTS TO
US 1 NORTH
OF THE BRIDGE.

LEGEND

lele

- ROADWAY IMPROVEMENT _____
- PROPOSED GRADING LIMIT _____
- RETAINING WALL _____
- EXISTING RIGHT-OF-WAY
LINE OF THROUGH HWY. _____
- PROPOSED RIGHT-OF-WAY
LINE OF THROUGH HWY. _____
- PROPOSED STORM WATER
MANAGEMENT R-O-W _____
- NOISE RECEPTOR _____
- AIR QUALITY RECEPTOR _____

	NO BUILD	BUILD
2016 LEVEL OF SERVICE (AM/PM) WEAVE: I-695B EXIT RAMP- FOWLER AVE.	—	C/D
INTERSECTION: U.S. RTE. 1-FOWLER AVE.	A/F	A/F
R/W REQUIRED (ACRES)	—	0.44
COST ESTIMATE (x\$1000)		
R/W ENGINEERING	—	0.37
STRUCTURES	—	—
ROADWAY	—	1.20
TOTAL COST (x\$1000)	—	1.57



**BEL AIR
BELTWAY
PLAZA**

CARDWELL AVE.

FOWLER AVE.

PROPOSED R-O-W

OVERTON AVE

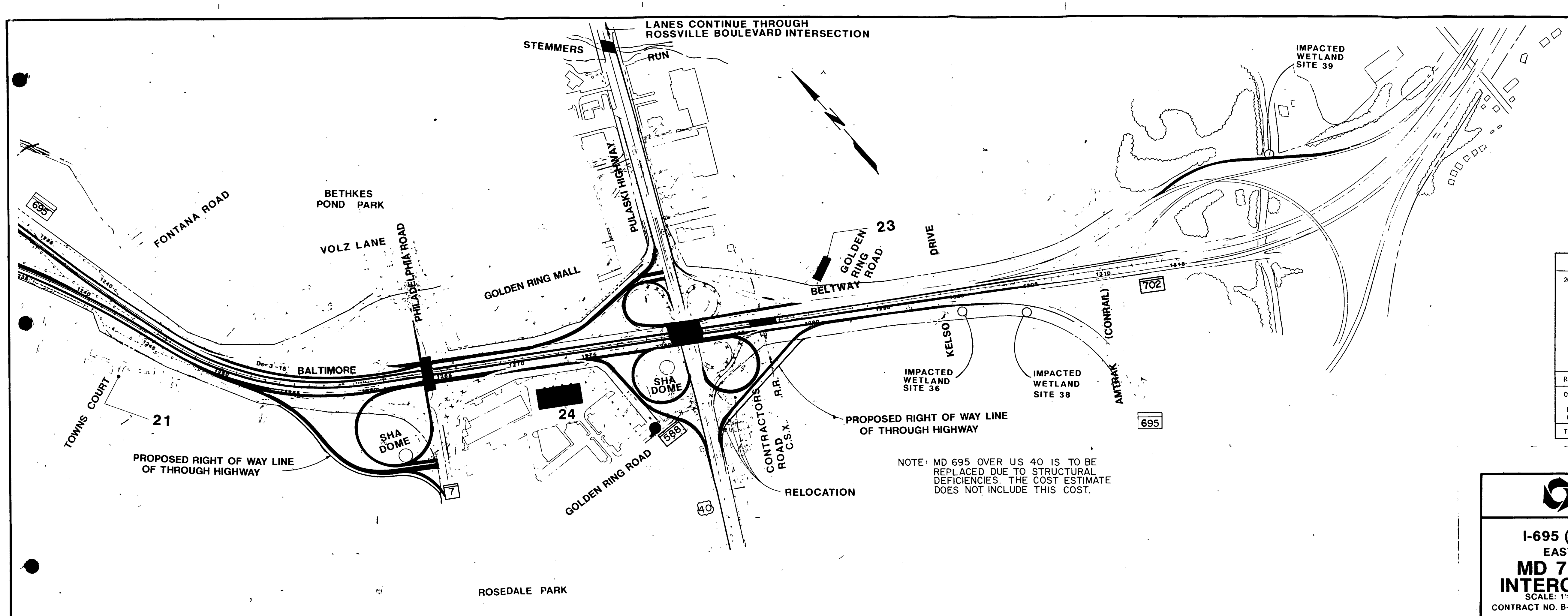
BELAIR ROAD



**I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702
U.S. 1 MODIFIED OPTION**

SCALE: 1"=200'
CONTRACT NO. B-635-151-472

FIGURE 111-12



LANES CONTINUE THROUGH
ROSSVILLE BOULEVARD INTERSECTION

LEGEND

- ROADWAY IMPROVEMENT.....
- PROPOSED GRADING LIMIT.....
- RETAINING WALL.....
- EXISTING RIGHT-OF-WAY
LINE OF THROUGH HWY.....
- PROPOSED RIGHT-OF-WAY
LINE OF THROUGH HWY.....
- PROPOSED STORM WATER
MANAGEMENT R-O-W.....
- NOISE RECEPTOR..... 11
- AIR QUALITY RECEPTOR..... A

	NO BUILD	OPTION 3
2015 LEVEL OF SERVICE		
WEAVE:		
MD 695 EB BETWEEN MD 7 & US 40	E/D	C/B
MD 695 EB BETWEEN US 40 LOOPS	F/F	E/E
MD 695 EB BETWEEN US 40 & MD 702	E/E	E/E
MD 695 WB BETWEEN MD 702 & US 40	E/F	-
MD 695 WB BETWEEN US 40 LOOPS	E/E	-
MD 695 WB BETWEEN US 40 & MD 7	D/C	D/D
INTERSECTIONS:		
MD 695 RAMP AT MD 7	A/D	C/D
US 40 AT RAMP	-	B/C
R/W REQUIRED (ACRES)	-	-
COST ESTIMATE (x \$1000)		
R/W & ENGINEERING	-	\$ 3.76
CONSTRUCTION	-	\$ 30.24
TOTAL COST (x \$1000)	-	\$ 34.00

NOTE: MD 695 OVER US 40 IS TO BE
REPLACED DUE TO STRUCTURAL
DEFICIENCIES. THE COST ESTIMATE
DOES NOT INCLUDE THIS COST.



**I-695 (BALTIMORE BELTWAY)
EAST OF MD. 140 TO MD. 702
MD 7/ US 40/ MD 702
INTERCHANGE OPTION 3**

SCALE: 1"=400'
CONTRACT NO. B-635-151-472

FIGURE III-13

4. Service Characteristics of the Selected Alternate

a. Traffic Summary

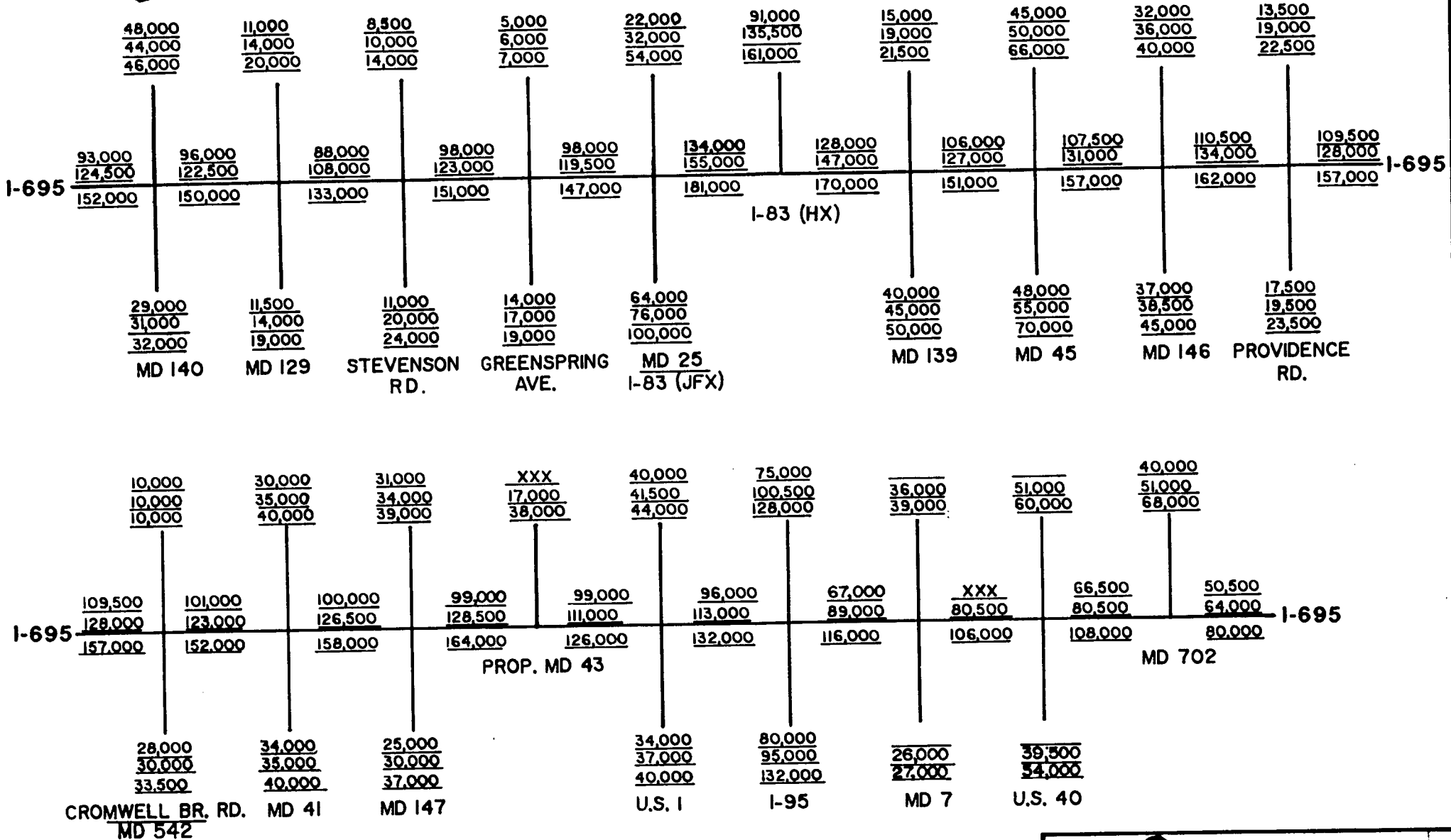
Regional development patterns in the Baltimore Metropolitan area have changed recently from decades of largely centralized residential development to spreading residential growth and extensive commercial and industrial growth. This has resulted in traffic patterns that are more diffused and less hub-centralized. From 1960 through 1980, the number of workers in the Baltimore region commuting from a suburban place-of-work increased by 145 percent. Currently more than one-half of all commuting trips in the area have both suburban origins and destinations.

During the period from 1967 to 1987, motor vehicle registrations in Baltimore County grew from 242,237 to 549,217, an increase of 127 percent, and Baltimore City registrations grew from 247,276 to 309,350, despite a declining population. This trend, which directly relates to an increase in roadway vehicles, together with regular annual increases in statewide gross vehicle miles travelled, indicates continued travel growth in the study area.


The Baltimore Regional Planning Council predicts that from 1990 to 2010 nearly 75 percent of all new commuter trips will be from a suburban site to a suburban site.

Based on current traffic growth rates in the region and nationwide trends, traffic volumes on the Baltimore Beltway are expected to continue increasing over time. Current average daily traffic (ADT) volumes on the Beltway vary from 152,000 vehicles between I-83 (Jones Falls Expressway) and I-83 (Harrisburg Expressway) to 72,000 vehicles east of I-95. These volumes are projected to increase 40 to 50 percent by the design year 2015. Figure III-14 shows the 1984 traffic figures which were used to project traffic to the years 1995 and 2015. The forecasted traffic was obtained by analyzing current trends and reviewing approved masterplans for future land use.


These transportation trends and commuting patterns have created capacity and safety problems on the existing Beltway. If the projected travel demand created by planned regional growth is to be handled in a safe and efficient manner, improvements must be made to the Beltway to increase its capacity. The Baltimore Beltway is the only circumferential highway in the region. Other proposed or recently completed transportation improvements such as I-795 (Northwest Expressway), the Owings Mills Metro line, and MD 43 (Whitemarsh Boulevard) and the Central Corridor Light Rail Line serve different corridors and will not reduce the Beltway's capacity requirements.



LEGEND: 1984 ADT
 1995 ADT
 2015 ADT


 Maryland Department of Transportation
 State Highway Administration

I-695 (BALTIMORE BELTWAY)
 EAST OF MD 140 TO MD 702
AVERAGE DAILY TRAFFIC



CONTRACT NO. B 636-161-472 FIGURE III-14

Level of Service Analysis

Table III-1 lists existing and design year roadway segments that are worse than a level-of-service E capacity for the Beltway mainline and interchanges. Level-of-service (LOS) is dependent upon highway geometry, highway capacity, and traffic characteristics and volumes. The Transportation Research Board's HIGHWAY CAPACITY MANUAL, defines freeway level-of-service as follows:

- o LOS A: Free flow.
- o LOS B: Stable flow; the presence of others in the traffic stream begins to be noticeable.
- o LOS C: Stable flow; the presence of others in the traffic stream begins to significantly affect interactions.
- o LOS D: High density, stable flow; the presence of others in the traffic stream begins to severely affect speed and freedom to maneuver.
- o LOS E: Operating conditions at or near the capacity level. All speeds are reduced to a low, but relatively uniform value.
- o LOS F: Forced or breakdown flow. Frequent stoppages.

Level-of-service E has been selected as the minimal design objective for this project. As can be seen from Table III-1, capacity improvements are required based on today's existing condition. After construction, the Beltway will operate adequately (LOS = E or better) since the constraining bottlenecks (primarily mainline sections) will be eliminated. However, with continued growth in the Baltimore Metropolitan Area, it is expected that even with the proposed improvements, the Beltway will not be able to adequately handle design year, peak period, travel demands.

Certain portions of the Beltway are projected to operate at or near level-of-service F in the design year of 2015 with mainline improvements. Notwithstanding these future congested conditions, overall traffic flows would improve substantially over "No Build" conditions. Table III-2, below, indicates how travel time for selected journeys would improve if the Beltway were widened. Also, in the peak periods, as shown in Table III-3, the duration of congestion would diminish if the Beltway were widened.

Although, under the build conditions there would not be a significant decrease in peak period congestion over existing conditions, under the No-Build, the congested period and delay times would increase by approximately fifty (50) percent.

Table III-1
LEVEL OF SERVICE SUMMARY

EXISTING ROADWAY FAILURE (LOS=F)

- o I-695 from MD 140 to I-83 (JFX)
AM Peak eastbound
PM Peak westbound
- o I-695 from MD 45 to Providence Road
PM Peak eastbound
- o I-695 from Providence Road to U.S. 1
PM Peak eastbound
- o I-695 from MD 146 to MD 147
AM Peak westbound
- o I-695 from US 1 to MD 7
PM Peak eastbound
- o I-695 from I-95 To MD 7
PM Peak both directions
- o I-695 from U.S. 40 to MD 702
PM Peak both directions
- o MD 139 from Kenilworth Avenue to I-695
AM and PM Peaks
- o MD 146 at I-695
PM Peak northbound
- o MD 41 from I-695 to Joppa Road
PM Peak northbound
- o MD 147 from Baltimore City Line through Joppa Road
PM Peak both directions
- o US 1 from Baltimore City Line to Putty Hill Avenue
PM Peak both directions
- o US 40 from I-695 to Rossville Boulevard
PM Peak both directions

2015 ALTERNATE 1 (NO-BUILD) ROADWAY FAILURE (LOS=F)

- o I-695 from MD 140 to MD 702
AM and PM both directions

In addition to existing crossroad problems cited above:

- o Greenspring Avenue at Old Court Road
PM Peak northbound
- o West Road at MD 45
AM and PM Peaks
- o Providence Road at I-695
AM Peak

2015 ALTERNATE 2 (BUILD) ROADWAY FAILURE (LOS=F)

- o I-695 from I-795 to I-83 (JFX)
PM Peak both directions
- o I-695 from MD 45 to MD 43
AM Peak westbound
PM Peak eastbound

Table III-2

PEAK PERIOD TRAVEL TIME COMPARISON
(Minutes)

<u>Origin</u>	<u>Destination</u>	<u>1985</u>	<u>2015 No-Build</u>	<u>2015 Build</u>
I-83(JFX)	I-795	7	22	13
I-795	MD 45 (Towson)	19	32	19
I-795	I-95(JFK)	32	64	38
I-83 (JFX)	MD 45 (Towson)	10	12	7
I-95 (JFK)	MD 45 (Towson)	13	32	19
MD 702 (Essex)	MD 45 (Towson)	16	33	20

Table III-3

PEAK PERIOD DURATION*
(Hours)

	<u>1985</u>	<u>No-Build</u>	<u>Build</u>
AM Peak Period	2.2	3.2	2.4
PM Peak Period	3.0	4.5	3.9

* defined as worse than level-of-service 'D/E'.

b. Accident Summary

Interstate 695, from MD 140 to MD 702, experienced a total of 1,750 accidents during the three year period 1987 through 1989. The average accident rate for the study section was 80.4 accidents for every one hundred million vehicle miles of travel (accidents/100MVM). This accident rate was significantly higher than the statewide average of 74.2 accidents/100MVM for similarly designed highways under state maintenance during that same time period. The three (3) year total accident experience for the study section is listed in Table III-4, below, by severity, year, and rate. The statewide average rate for this type of design highway is listed for comparison.

Table III-4

BELTWAY ACCIDENTS 1987-1989

Severity	1987	1988	1989	Total	Accident Rate/ 100MVM	Statewide Average/ 100MVM
Fatal Accidents	1	3	4	8		
Injury Accidents	194	227	181	602		
Property Damage Only	<u>182</u>	<u>219</u>	<u>211</u>	<u>612</u>		
Total Accidents	377	449	396	1222	80.4 *	74.2

* Significantly Higher Than Statewide Rate

During 1988, the mainline roadway section that included U.S. 40 and MD 702 was classified as a high accident section (HAS). Additionally, the mainline roadway sections at MD 146 (Dulaney Valley Road) and Greenspring Avenue were classified as Section Accident Experience (SAE).

During the period 1987 to 1989 all but two (2) of the interchange areas within the study segment (MD 147 and I-95) experienced collision rates significantly higher than the statewide rate for similar design highways.

The highest accident rates (198.6 and 218.3) occurred at the MD 7/US 40 and the MD 702 interchanges, respectively. The MD 542/Cromwell Bridge Road interchange area, which formerly had the highest collision rate on the interstate route, experienced a lesser rate than the Greenspring Avenue, I-83/Falls Road, I-83/MD 139, MD 146, Providence Road, U.S. 1, MD 7/US 40 and MD 702 interchange areas during the period 1987 to 1989. This reduction in collision rate is probably attributable to widening of the I-695 bridge over MD 542 and roadway resurfacing at this location.

The collision types that significantly exceeded the statewide average rates were rear end, fixed object, and sideswipe. These types of collisions are generally associated with traffic congestion, weaving conflicts, substandard vertical and horizontal geometrics, and substandard clear zone areas. All of these conditions currently exist along the Beltway. Trucks were not involved in an inordinately high number of accidents.

Several interchange ramps within the Study limits were identified as High Accident Interchange Ramps (HAIR) for the 1987-1989 period:

- o Greenspring Avenue: EB off*
- o I-83 (JFX): EB to SB off, SB to EB on.
- o I-83 (HX): EB to NB off*, SB to EB on, WB to NB off, SB to WB on.
- o MD 139: EB off, NB to WB on*
- o MD 146: WB to SB off.
- o MD 542: NB to EB on, WB off.
- o MD 41: EB to SB off*, EB to NB off*
- o MD 147: EB to SB off*, EB to NB off*, WB to NB off*, NB to WB on, NB to EB on
- o US 1: EB to SB off*, SB to WB on*, EB to NB off*
- o I-95: EB to SB off, SB to EB on, WB to SB off, NB to WB on.
- o US 40: EB to SB off*, NB to EB on, WB to SB off*, EB to NB off*

* - HAIRs with substandard geometrics.

High Accident Interchange Ramps are so designated because five or more accidents have occurred upon them within a three-year period.

Under a No-Build alternate, the unacceptable level of congestion and resulting accident rates would continue to exist. If the highway remains unchanged, the number of accidents would be expected to rise as daily traffic volumes and resultant vehicle conflicts increase.

With the implementation of the proposed addition of a lane of travel in each direction of I-695, highway capacity will be increased. Additionally, it is anticipated that the extra lane could result in an approximately fifty (50) percent immediate reduction in the number of rear end accidents. The accident rate would be expected to grow back to approximately today's level by the design year under the Build condition. However, if the Beltway were not widened, frequency of accidents (especially rear end collisions that are associated with congestion) would increase as the duration of peak period congestion increases.

Capacity and safety improvements at high accident interchanges, where reasonable, would serve to greatly reduce accident potential both after construction and through the 2015 design year.

c. Operational Analysis

The combination of travel demand exceeding capacity, substandard geometric conditions, and accident (or near miss) occurrences serve to seriously affect operation, resulting in less than efficient performance on the Beltway, major arterial cross roads, and interchanges between both. The following areas have been defined as deficient and measures are proposed to address these spot problems:

- o Greenspring Avenue: Traffic currently queues in the PM peak period on Greenspring Avenue southbound from the Old Court Road intersection through the unsignalized intersection with the eastbound Beltway ramps. As travel demand between the eastbound Beltway and projected residential land uses to the south grows, the potential will exist for future back-ups onto the Beltway mainline. The selected build alternate includes improvements to the Greenspring Avenue/MD 133 (Old Court Road) intersection to help alleviate these operational problems. While this intersection is anticipated to operate at LOS E in both the AM and PM peak hours, any additional improvements would create substantial right-of-way impacts.
- o I-83 (JFX): Ramps providing movements between I-83 (JFX) and I-695 to the west are not up to standards typically found in Maryland for the Interstate-to-Interstate movements. The northbound JFX to westbound I-695 movement is a loop ramp with a 20 MPH design speed. The Selected Build Alternate does not address this substandard geometrics because no safety or traffic operational problems exist today due to the geometrics. Option B which addressed possible long term operational concerns was dropped due to public opposition, impacts to the Greenspring Valley Historic District, and high costs.

The eastbound I-695 to southbound JFX directional ramp has a 35 MPH design speed that diverges from I-695 off a 5% downgrade and right on to a bridge. The combination of poor geometrics and travel demand for this Interstate-to-Interstate movement does not meet driver expectation and has created a high accident interchange ramp (HAIR). The selected build alternate (Option C) will improve these substandard geometrics and provide additional capacity. The diverge from eastbound I-695 is anticipated to operate at LOS F/F (AM/PM peak hour) in the design year. While this is not preferable, it is an improvement over the no-build alternate. An additional mainline lane would be required for a better level of service.

- o I-83 (HX): The eastbound to northbound two lane ramp that was reconstructed in the 1970's from its original trumpet form, is dangerous being a high accident interchange ramp (HAIR) (21 accidents in the last 3 years). The ramp's reverse curves do not have sufficient tangent for necessary superelevation runoff (design speed = 40 mph). The right side shoulder nearly disappears, at the diverge point, into a concrete barrier that in combination with curve and grade results in a horizontal obstruction reducing design speed to less than 35 mph. This movement is heavily travelled by over 30,000 vehicles per day. The selected build alternate (Option A) will provide additional sight distance (to nearly 50 MPH) through the curve to help reduce the high accident rate.

- o MD 45 Weave Along Westbound I-695: This weaving section is only 400' long and with heavy weaving volumes during the PM peak period results in sporadic disruptions to the mainline flow. The selected build alternate (Option B) will eliminate this substandard weave section.
- o MD 45 Off-ramp from Eastbound I-695: During the AM peak period traffic queues back onto the Beltway due to poor circulation on West Road. During the PM peak period, congestion on West Road results in significant motorist delay. Improvements to West Road will be completed by a developer in agreement with Baltimore County.
- o MD 146 Off-ramp from Westbound I-695: Similar to the situation on West Road, poor circulation on Hampton Lane causes PM peak back-ups onto the Beltway. The signal at MD 146 and Hampton Lane that creates this rolling back-up also results in queuing on northbound MD 146 that regularly extends beyond the cloverleaf weave area. The selected build alternate includes improvements to alleviate the operational problems.
- o MD 147 (Harford Road): The entire roadway from the Baltimore City Line to north of Joppa Road experiences operational difficulties due to friction from street parking, difficulty in negotiating around left turning vehicles, and congested signalized intersections. The duration of congestion is day-long, six days a week. The intersection with Putty Hill Avenue and Joppa Road operate at LOS 'F' during both AM and PM peak hours. During PM peaks, traffic queued at these intersections back up along Harford Road to the Beltway interchange. The Beltway cloverleaf interchange reduces service due to lack of merge lanes for the outer connection ramps merging onto MD 147, crossing traffic from side streets, and quickly turning left turns off of MD 147. During PM peaks, traffic congestion on Harford Road results in back-ups onto the Beltway at the EB to NB loop ramp and the WB to NB outer connection. The selected build alternate (Option B) will provide five lanes along MD 147 from Putty Hill Avenue to Joppa Road to reduce operational problems.
- o US 1 (Belair Road): Similar to MD 147, US 1 experiences undesirable operating conditions from the Baltimore City Line to north of Perry Hall. The US 1 problem, however, tends to be more a case of travel demand exceeding mainline and intersection capacity. The eastbound I-695 to southbound merge onto U.S. 1 is particularly troublesome due to a lack of a merging lane and motorist attempting to turn left from the outer connection into Overton Avenue. This merge area is a high accident interchange ramp (HAIR) location. The selected build alternate will provide an auxiliary lane from the eastbound directional ramp along southbound US 1 to the intersection of Fowler Avenue/Bel Air Beltway Plaza. The auxiliary lane will help reduce accidents currently occurring due to the lack of merge area. The directional ramp will have a 100' radius (under 20 MPH) just prior to US 1. Advance warning devices will be employed approaching the curve.

Two access points are located along the auxiliary lane (one access point to the maintenance facilities at Double Rock Park and one access point combines to two commercial properties and two residential properties). As both access points generate low traffic volumes, this is not anticipated to be a problem. Vehicles ingressing/egressing from Overton Avenue along northbound US 1 will be required to cross the deceleration lane from northbound US 1 to eastbound I-695. Due to the low volumes on Overton Avenue, this is not anticipated to be a problem.

The realignment of the directional ramp and the extension of the auxiliary lane on southbound US 1 was developed to address operational and safety concerns at the residential and commercial entrance along southbound US 1 between I-695 and Fowler Avenue. Should safety problems become apparent at these locations in the future, other alternatives will be investigated.

- o I-95 (John F. Kennedy Memorial Highway): Current operational difficulties at this major fork interchange are being addressed by another study. Construction is underway.
- o MD 7/US 4/MD 702: Several deficiencies occur along and adjacent to this segment of freeway which was never designed to be part of the Beltway. These include:
 - Inadequate weave distances between most all ramps, especially between US 40 and MD 702.
 - Substandard ramp terminal and ramp proper radii resulting in design speeds as low as 25 mph and 15 mph, respectively.
 - Inadequate capacity on US 40 and on roadways between US 40 and MD 7.

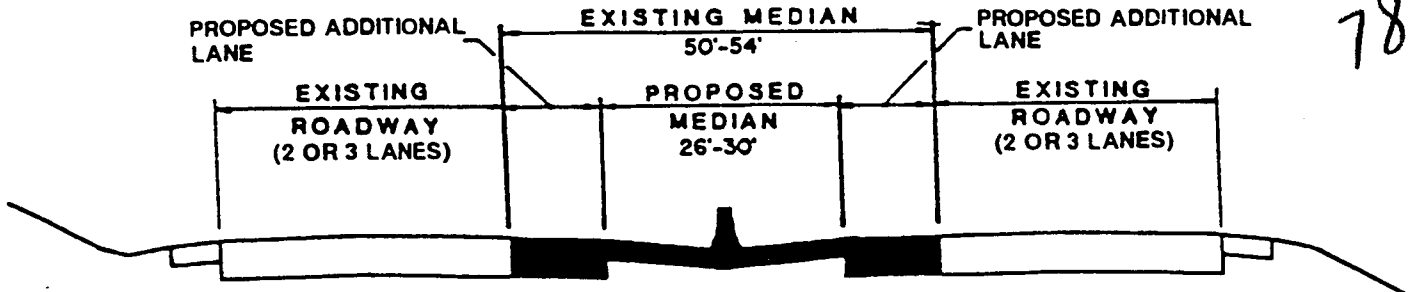
The selected build alternate (Option 3) provides increased capacity and geometrics to address these deficiencies.

5. Design Characteristics of the Selected Alternate

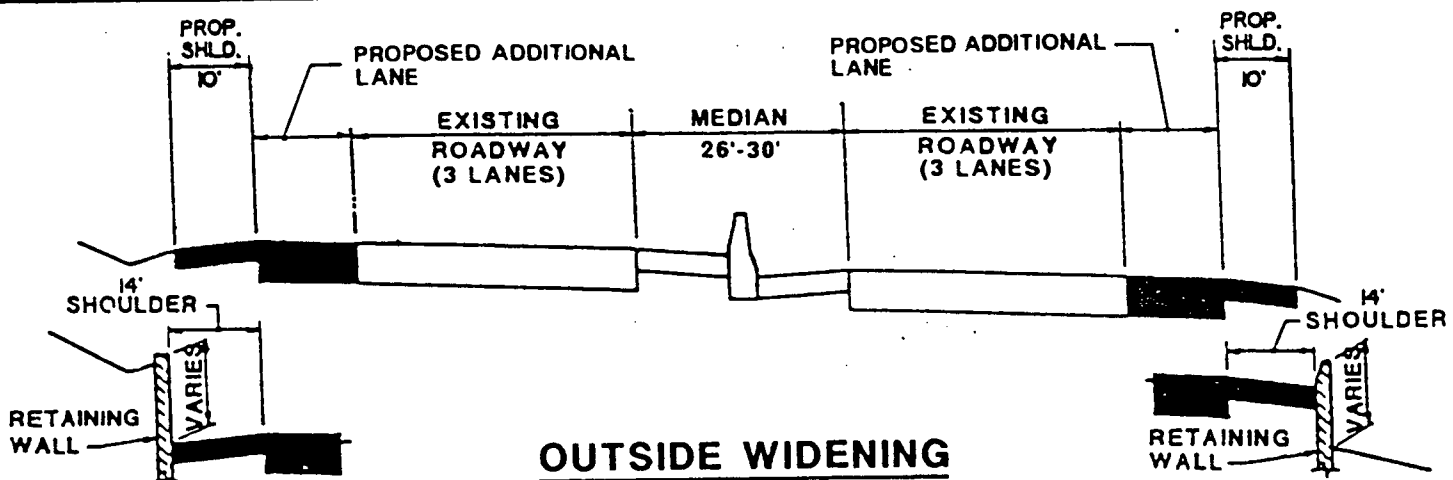
a. Alternate 2: Beltway Widening

Alternate 2 consists of adding one lane to the Beltway in each direction. Constraints were placed upon the mainline alternate due to the limited available space in the median and the proximity of development contiguous to the Beltway. The mainline build alternate was developed with the basic assumption that right-of-way acquisition would be kept to a minimum. This was accomplished by eliminating safety grading and providing retaining walls where necessary (See Figure III-15). This project also includes a resurfacing of the existing mainline lanes, if needed, during the widening. Several superelevated section have substandard cross slopes to today's standards. During final design these areas will be investigated for improvement. Auxiliary lanes are proposed wherever the distance between acceleration and deceleration lanes is less than 1500 feet. These auxiliary lanes would better accommodate merging, diverging and weaving movements.

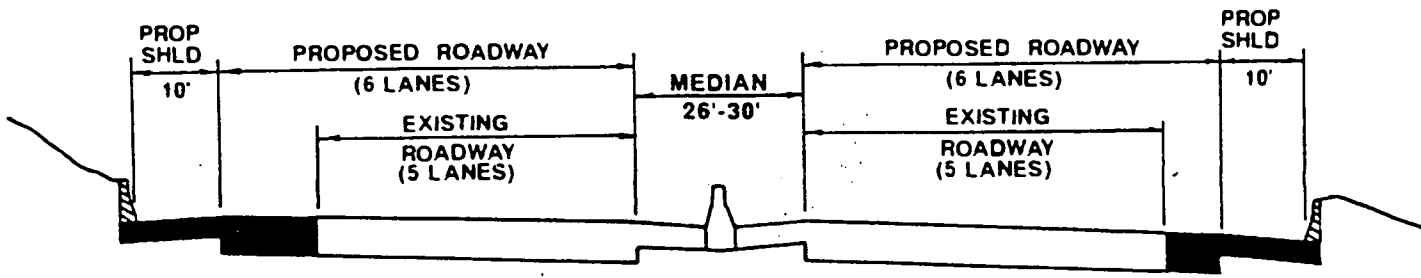
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INSIDE WIDENING
MD 140 TO I-83 (JFX)
MD 147 TO MD 702



OUTSIDE WIDENING
I-83 (HX) TO MD 147



I-83 (JFX) TO I-83 (HX)

THE DIMENSIONS SHOWN ARE FOR THE PURPOSE OF DETERMINING COST ESTIMATES AND ENVIRONMENTAL IMPACTS AND ARE SUBJECT TO CHANGE DURING THE DESIGN PHASE.



I-695 (BALTIMORE BELTWAY)
MD 140 TO MD 702

TYPICAL SECTIONS
ALTERNATE 2

B 635-101-472

High Occupancy Vehicle Lanes

This document addresses the construction of an additional lane in each direction on the Beltway. These additional lanes, which were originally designed for general purpose use, could be converted to HOV's as a transportation demand management measure. Further, the improved mainline cross section and overpassing structures have been designed so as not to preclude the future re-striping of this facility for further capacity expansion. Any additional capacity provided on this facility in the future, would be subject to the appropriate environmental analysis.

Light Rail Transit

This document addresses the construction of an additional lane in each direction on the Beltway. A study was conducted to determine the engineering feasibility of providing a light rail transit (LRT) line connecting potential LRT lines with population centers via I-695. Two lines were investigated. The first line (Towson Connector) would originate from the Central Light Rail Line (CLRL) east of the Jones Falls and would terminate in Towson. The second line (Whitemarsh Connector) would originate in the median of MD 41 (Perring Parkway) and terminate in the median of proposed MD 43 (Whitemarsh Boulevard).

The two study corridors were suggested by the Statewide Commuter Assistance Study as possible future transit lines. The study was not developed to determine ridership, need, or a preferred alignment. The study was performed to determine if major planning revisions would be needed to incorporate the LRT, and to provide input for the Statewide Commuter Assistance Study.

The Study concluded that the roadway improvements selected along the Beltway would not preclude the future inclusion of either LRT lines. Modifications would be required, such as the placement of retaining walls, noise walls, and bridge reconstructions. With scheduling of both the roadway improvements and LRT lines not established, these modifications to the roadway plans may be incorporated to accommodate the LRT lines.

b. Stevenson Road Intersection

The free right turn would be a single lane with a 25 MPH design speed.

c. Greenspring Avenue/MD 133 Intersection

Southbound Greenspring Avenue would require two lanes through the intersection which must be reduced to one lane following the intersection. The northbound Greenspring Avenue left turn lane would be 10'. The left turns on MD 133 would be 10' and the inside through lanes would be 11'. Eastbound MD 133 would require two lanes through the intersection and reduce to one lane following the intersection. All widening on MD 133 would be to the northern side.

d. I-83 (JFX)/MD 25 (Falls Road) Interchange Option C

A two lane ramp with a 50 MPH design speed would be provided. The ramp would bridge the Jones Falls and its' tributary.

e. I-83 (HX)/MD 139 (Charles Street) Interchange Option A and Option D

Option A would widen the outside shoulder to 14' at the diverge area from eastbound I-695 to northbound I-83 (HX). This will increase the horizontal sight distance to nearly 50 MPH at this location.

Option D would provide a three lane exit to MD 139 and I-83 (HX). The outside lane would exit to MD 139 and the two remaining lanes would continue to I-83 (HX). A 50 MPH design speed would be provided. Three lanes would be provided along I-695 from the diverge area until a lane add from MD 139 occurs (approximately 1/2 mile). A separate service roadway would be provided for access west of MD 139 which currently uses the ramp. A future Light Rail Transit Station is proposed at the end of the service roadway.

f. MD 45 (York Road) Interchange Option B

Option B would reconstruct the northwest directional ramp to a 30 MPH diverge and 50 MPH merge onto I-695. A two lane left turn spur ramp in the northeast quadrant would be provided.

g. MD 146 (Dulaney Valley Road) Interchange Option

Option A would widen the directional ramp to two lanes prior to the intersection with Hampton Lane. The directional ramp would be free flow and Hampton Lane would have a stop condition, northbound MD 146 would be re-striped to allow for three through lanes through the intersection.

h. MD 41 (Perring Parkway) Interchange Option A and Option B

The collector-distributor (C-D) roadways would be a single lane with a 65 MPH design speed. The merges and diverges would be reconstructed to tie-in with C-D roadways.

i. MD 147 (Harford Road) Interchange Option B

MD 147 would be widened to five lanes. This will tie-in with the existing five section to the north at Second Avenue and a proposed five lane section under design for the intersection with Putty Hill Avenue. The typical section will provide 11' lanes and a curbed outside edge of roadway. A sidewalk will be constructed where widening impacts an existing sidewalk. The widening will be split to both sides of the existing roadway. A physical restriction between the southbound directional ramp and California Avenue will be provided. Access will be denied along southbound MD 147 to Grendon Lane. This will require that Grendon Lane be closed prior to MD 147. A service road connection between Edgewood Avenue and Grendon Lane would be provided.

j. US 1 (Belair Road) Interchange

The modified option would provide an auxiliary lane from the eastbound I-695 directional ramp to southbound US 1. The auxiliary would become a right turn only into the Bel Air Beltway Plaza. The directional ramp would be reconstructed and a 100' radius would be provided just prior to US 1. Advance warning devices will be employed approaching the curve. The typical section along southbound US 1 would include two

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through lanes, the auxiliary lane, and a left turn pocket at Overton Avenue and at Fowler Avenue. The lane widths would be 13' for the auxiliary lane and 12' for the remaining lanes near the directional ramp. The lane widths would be reduced to 11' at the box culvert over Stemmers Run.

The typical section along northbound US 1 would provide two through lanes. The lane widths would be 11' at the intersection with Fowler Avenue and would taper out to 12'. A deceleration lane to eastbound I-695 would begin at the Stemmers Run box culvert.

The typical section would provide a curbed outside edge of roadway. A curbed median would be provided across the Beltway Bridge to the intersection with Overton Avenue.

The improvements will require no land from Double Rock Park. See Section III.B.7.d (P.III-72 for a complete discussion on the Park).

k. MD 7/US 40/MD 702 Interchange Option 3

Option 3 would provide four through lanes for MD 695. The new loop ramp at MD 7 would have a 30 MPH design speed. The three reconstructed loop ramps at US 40 would have a 25 MPH design speed. The slip ramp from MD 702 to MD 695 westbound would be constructed primarily on bridge.

6. Design Exceptions of the Selected Alternate

In order to obtain final approvals of the Selected Alternate, design exceptions of current AASHTO standards will be required. The Baltimore Beltway was designed in the 1950's and 1960's, and since that time several revisions to AASHTO Standards have occurred. This description does not constitute the request for the exceptions, but instead documents the types of exceptions that may be required for the selected alternate.

A. Mainline

Current AASHTO standards allow for a 3% preferable (4% maximum) grade and 60 mph design speed for the Beltway. Two locations along the Beltway currently do not meet these requirements: Greenspring Avenue and Loch Raven Boulevard.

The existing vertical grade east of Greenspring Avenue is 5%. A study was performed to reduce the grade to current standards. It was determined due to costs, impacts, and extensive delays during construction, this reconstruction was not cost effective. Similarly at MD 542, Loch Raven Boulevard, it was determined that reducing the +5% grade to current standards was not cost effective.

The vertical crest curve of the Beltway at US 40 meets 45 mph under current AASHTO standards. A study was performed to adjust the vertical crest curve design criteria to increase it's design speed. A review of the accident data revealed no correlation between the current accidents and the substandard vertical crest curve. It was determined that due to the costs, impacts, extensive delays during construction, and no correlation of safety problems to the vertical crest curve, this reconstruction was not cost effective.

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The proposed typical section does not provide for a clear zone (safety grading) within areas of outside widening. A 10' outside shoulder (14' adjacent to retaining walls) would be provided. It was determined that the extensive impacts to right-of-way and associated cost did not warrant the placement of a clear zone.

Several superelevated sections have substandard cross slopes under current AASHTO standards. During final design these areas will be investigated for improvement. The insufficient superelevation occurs at 16 locations throughout the study area (see Table III-5).

B. Bridge Clearance

A preliminary investigation of the existing and proposed bridge clearances was performed (see Table III-6). The investigation included both horizontal and vertical clearances. All bridges which must be reconstructed will be designed to meet AASHTO standards.

A review of the proposed horizontal clearances revealed on bridge which may not meet AASHTO standards. The proposed horizontal clearance of I-83 (HX) southbound ramp to eastbound I-695 (Ramp C) over I-695 (bridge #3203) is 8' along the median of eastbound I-695. The median of westbound I-695 meets AASHTO standards. Providing the minimum acceptable horizontal clearance of 10' would require three spans of the flyover ramp to be reconstructed. It was determined due to the high cost of the reconstruction of the bridge, this reconstruction was not cost effective.

A review of the proposed vertical clearances revealed four bridges which may not meet current AASHTO standards. An attempt will be made during final design to maintain or improve these vertical clearance. The following order of priority will be used to evaluate bridge clearance:

- o Provide desirable AASHTO clearance
- o Provide minimum AASHTO clearance
- o Maintain existing clearance

The following bridges over I-695 may not meet vertical clearance criteria: MD 129 (bridge #3147); MD 139 (bridge #3160); and I-95 southbound over I-95 northbound (bridge #3183). The following underpass of I-695 may not meet vertical clearance criteria: US 40 (bridge # 3191).

Due to impacts that would be incurred, roads with substandard vertical clearance are not proposed to be reconstructed to maintain clearance or increase clearance to standard requirements. Where an existing vertical clearance is less than or equal to current requirements, this clearance will be maintained by milling the pavement prior to placing the overlay. Where the existing clearance is greater than the standard requirements, that clearance will not be reduced to substandard.

C. Ramps

Many of the interchanges along the Beltway were designed in the 1950's and 1960's. With several revisions to AASHTO standards occurring since then, there are many violations of current AASHTO standards. An investigation of existing interchange directional and loop

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ramps revealed 38 instances of substandard geometries. While some ramps will be improved by the selected alternate, many will not due to cost and right-of-way impacts (see Table III-7).

US 1 Modified Option will reconstruct the directional ramp from eastbound I-695 to southbound US 1. The directional ramp currently has no merge area which has contributed a high accident interchange ramp (HAIR) location. The lack of merge area is compounded by an access point (to two commercial properties and two residential properties) within 25'. The approach radius of the directional ramp will be reconstructed from a $\pm 350'$ radius (35 MPH) to a 100' radius (under 20 MPH). AASHTO minimum criteria is 360' radius. An auxiliary lane would be provided from the directional ramp to a right turn only at the intersection of Fowler Avenue/Bel Air Beltway Plaza. The reduction in radius will increase the weave section by $\pm 100'$. Additionally it will provide a small additional separation between the ramp and the access point. By providing the auxiliary lane and increasing the separation between the directional ramp and access point, these improvements will potentially decrease the accident rate. Advance warning devices will be employed approaching the 100' radius curve.

The realignment of the directional ramp and the extension of the auxiliary lane on southbound US 1 was developed to address operational and safety concerns at the residential and commercial entrances along southbound US 1 between I-695 and Fowler Avenue. Should safety problems become apparent at these locations in the future, other alternatives will be investigated.

I-695 from I-95 west to the project limit is on the 26,000 Mile Priority Network (Network established by Department of Defense, State, and FHWA to meet the most urgent national defense needs). Proposed vertical clearance design exception (including exceptions which do not upgrade existing deficiencies) are to be sent to the FHWA Washington Office for coordination with the Military Traffic Management Command (in accordance with the FHWA May 11, 1990, Memorandum from the Associate Administrator for Engineering and Program Development).

TABLE III-5
AASHTO DESIGN EXCEPTIONS REQUIRED FOR MAINLINE

I-695 MAINLINE LOCATION	DESIGN EXCEPTION REQUIRED	REQUIRED TO MEET AASHTO MINIMUM CRITERIA	JUSTIFICATION FOR DESIGN EXCEPTION
1. EAST OF GREENSPRING AVENUE	MAINLINE VERTICAL GRADE + 5%	4% MAXIMUM 3% PREFERRED	IT WAS DETERMINED DUE TO COSTS, IMPACTS, AND EXTENSIVE DELAYS DURING CONSTRUCTION, RECONSTRUCTION OF THE BELTWAY AT THIS LOCATION WAS NOT COST EFFECTIVE.
2. MD 542 LOCH RAVEN BOULEVARD	MAINLINE VERTICAL GRADE + 5%	4% MAXIMUM 3% PREFERRED	IT WAS DETERMINED DUE TO COSTS, IMPACTS, AND EXTENSIVE DELAYS DURING CONSTRUCTION, RECONSTRUCTION OF THE BELTWAY AT THIS LOCATION WAS NOT COST EFFECTIVE.
3. US 40	VERTICAL CREST CURVE AT 45 MPH	VERTICAL CREST CURVE AT 60 MPH	AS THERE WAS NO CORRELATION OF CURRENT ACCIDENTS TO THE SUBSTANDARD VERTICAL CREST CURVE. IT WAS DETERMINED DUE TO COSTS, IMPACTS AND EXTENSIVE DELAYS DURING CONSTRUCTION THAT RECONSTRUCTION OF THE BELTWAY AT THIS LOCATION WAS NOT COST EFFECTIVE.
4. STUDY LIMITS	NO CLEAR ZONE PROVIDED	VARIING 20' TO 30'	IT WAS DETERMINED THAT THE EXTENSIVE IMPACTS TO RIGHT-OF-WAY AND ASSOCIATED COST DID NOT WARRANT THE PLACEMENT OF A CLEAR ZONE.
5. PARK HEIGHTS AVENUE	S.E. RATE = 0.016	S.E. RATE = 0.021	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
6. STEVENSON ROAD	S.E. RATE = NORMAL CROWN	S.E. RATE = 0.027	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
7. GREENSPRING AVENUE	S.E. RATE = 0.040	S.E. RATE = 0.045	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
8. MD 25	S.E. RATE = 0.050	S.E. RATE = 0.051	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
9. JOPPA ROAD (WEST)	S.E. RATE = 0.0208	S.E. RATE = 0.021	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
10. EAST OF MD 45	S.E. RATE = 0.050	S.E. RATE = 0.055	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
11. MD 45	S.E. RATE = 0.025	S.E. RATE = 0.045	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
12. MD 146	S.E. RATE = 0.025	S.E. RATE = 0.045	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
13. PROVIDENCE ROAD	S.E. RATE = 0.025	S.E. RATE = 0.037	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
14. MD 542	S.E. RATE = 0.040 E B	S.E. RATE = 0.055	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
15. JOPPA ROAD (EAST)	S.E. RATE = 0.050	S.E. RATE = 0.055	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
16. OLD HARFORD ROAD	S.E. RATE = 0.040	S.E. RATE = 0.045	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
17. AVONDALE ROAD	S.E. RATE = 0.030	S.E. RATE = 0.037	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
18. PUTTY HILL ROAD	S.E. RATE = 0.023 E B	S.E. RATE = 0.027	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
19. US 1	S.E. RATE = 0.030	S.E. RATE = 0.037	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.
20. EAST OF US 1	S.E. RATE = 0.033	S.E. RATE = 0.037	DURING FINAL DESIGN, THESE AREAS WILL BE INVESTIGATED FOR IMPROVEMENTS.

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TABLE III-6

AASHTO DESIGN EXCEPTIONS FOR BRIDGE CLEARANCE

BRIDGE LOCATION	DESIGN EXCEPTION REQUIRED	REQUIRED TO MEET AASHTO MINIMUM CRITERIA	JUSTIFICATION FOR DESIGN EXCEPTION
1. I-83 (HX) S.B. RAMP TO I-695 E.B. (RAMP C) (BRIDGE # 3203)	8' MEDIAN SHOULDER ON I-695 E.B.	10' MEDIAN SHOULDER	TO PROVIDE 10' MEDIAN SHOULDER WOULD REQUIRE THREE SPANS OF THE FLYOVER RAMP TO BE RECONSTRUCTED. IT WAS DETERMINED DUE TO THE HIGH COST, THIS RECONSTRUCTION WAS NOT COST EFFECTIVE.
2. MD 129 (BRIDGE # 3147)	15'-10" VERTICAL CLEARANCE	16'-0" VERTICAL CLEARANCE	IT WAS DETERMINED NOT TO BE COST EFFECTIVE TO REPLACE THIS BRIDGE TO OBTAIN ADDITIONAL 2" VERTICAL CLEARANCE.
3. MD 139 (BRIDGE # 3160)	15'-8 1/2" VERTICAL CLEARANCE W.B.	16'-0" VERTICAL CLEARANCE	IT WAS DETERMINED NOT TO BE COST EFFECTIVE TO REPLACE THIS BRIDGE TO OBTAIN ADDITIONAL 3 1/2" VERTICAL CLEARANCE.
4. I-95 S.B. OVER I-95 N.B. (BRIDGE # 3183)	15'-2" VERTICAL CLEARANCE	16'-0" VERTICAL CLEARANCE	IT WAS DETERMINED NOT TO BE COST EFFECTIVE TO REPLACE THIS BRIDGE TO OBTAIN ADDITIONAL 10" VERTICAL CLEARANCE.
5. US 40 (BRIDGE # 3191)	15'-9" VERTICAL CLEARANCE	16'-0" VERTICAL CLEARANCE	IT WAS DETERMINED NOT TO BE COST EFFECTIVE TO REPLACE THIS BRIDGE TO OBTAIN ADDITIONAL 3" VERTICAL CLEARANCE.

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TABLE III-7

AASHTO DESIGN EXCEPTIONS REQUIRED FOR RAMPS

RAMP LOCATION	DESIGN EXCEPTION REQUIRED	REQUIRED TO MEET AASHTO MINIMUM CRITERIA	JUSTIFICATION FOR DESIGN EXCEPTION
1. W.B. OFF LOOP TO GREENSPRING AVENUE	R = 140'	R = 175'	UPGRADING THE RAMP WOULD REQUIRE APPROXIMATELY 1 ACRE OF WOODED RESIDENTIAL PROPERTY, DISPLACE TWO SHEDS, AND COST APPROXIMATELY \$ 2,600,000.
2. E.B. OFF LOOP TO GREENSPRING AVENUE	R = 135'	R = 175'	UPGRADING THE RAMP WOULD REQUIRE APPROXIMATELY 1 ACRE OF WOODED RESIDENTIAL PROPERTY, AND WOULD PUSH THE RAMP INTO THE INTERSECTION OF GREENSPRING AVE. WITH OLD COURT RD.
3. W.B. ON RAMP FROM GREENSPRING AVENUE	R = 200'	R = 360'	ASSOCIATED WITH NOTE # 1.
4. E.B. ON RAMP FROM GREENSPRING AVENUE	R = 250'	R = 360'	ASSOCIATED WITH NOTE # 2.
5. I-83 (JFX) N.B. TO W.B. LOOP	R = 145'	R = 175'	UPGRADING RAMP WOULD REQUIRE RECONSTRUCTION OF A FLYOVER RAMP AND DIRECTIONAL RAMP, AND WOULD REQUIRE ACQUISITION OF COMMERCIAL RIGHT-OF-WAY.
6. E.B. LOOP TO I-83 (JFX) N.B.	R = 145'	R = 175'	UPGRADING RAMP WOULD DISPLACE ONE RESIDENCE AND ONE SHED, REQUIRE RECONSTRUCTION OF A TWO-LANE DIRECTIONAL RAMP, AND COST APPROXIMATELY \$ 3,300,000.
7. MD 45 N.B. LOOP TO W.B.	R = 149'	R = 175'	UPGRADING RAMP WOULD IMPACT FOUR RESIDENTIAL PROPERTIES AND REQUIRE RECONSTRUCTION OF THE DIRECTIONAL RAMP.
8. W.B. ON RAMP FROM MD 45 S.B.	R = 250'	R = 360'	MD 45 OPTION B UPGRADES THE EXISTING RADIUS FROM 100' TO 250'. TO UPGRADE THE RAMP FURTHER WOULD IMPACT RESIDENTIAL PROPERTIES.
9. W.B. OFF RAMP TO MD 45 N.B.	R = 223'	R = 360'	THIS SUBSTANDARD RADIUS IS REMOVED FROM THE BELTWAY EXIT TERMINAL. UPGRADING REMAINING RAMP WOULD RESULT IN IMPACTS TO THREE RESIDENTIAL PROPERTIES AND REDUCE THE ALREADY CRITICALLY SHORT DISTANCE BETWEEN THE RAMP AND THE INTERSECTION WITH CRAVEN DRIVE.
10. MD 45 N.B. OFF RAMP TO E.B.	R = 118'	R = 360'	THIS SUBSTANDARD RADIUS IS REMOVED FROM THE BELTWAY ENTRANCE TERMINAL. UPGRADING RAMP WOULD RESULT IN DISPLACING ONE COMMERCIAL IMPROVEMENT AND SEVERAL PARKING SPACES.
11. E.B. LOOP TO MD 45 N.B.	R = 145'	R = 175'	THIS SUBSTANDARD RADIUS IS REMOVED FROM THE BELTWAY TERMINAL. IT WAS DETERMINED NOT TO BE COST EFFECTIVE TO RECONSTRUCT THE RAMP FOR AN INCREASE OF 3 MPH.
12. E.B. OFF RAMP TO MD 146 S.B.	R = 50'	R = 360'	THIS SUBSTANDARD RADIUS IS REMOVED FROM THE BELTWAY EXIT TERMINAL. UPGRADING THE RAMP WOULD DISPLACE ONE RESIDENCE IMPACT ANOTHER RESIDENCE AND COMMUNITY SWIMMING POOL, AND COST APPROXIMATELY \$ 2,400,000.
13. MD 146 S.B. LOOP TO E.B.	R = 110'	R = 175'	UPGRADING THE RAMP WOULD DISPLACE 18 - 24 CONDOMINIUM UNITS AND COMMUNITY POOL.
14. E.B. LOOP TO MD 146 N.B.	R = 110'	R = 175'	UPGRADING THE RAMP WOULD REQUIRE APPROXIMATELY THREE ACRES FROM GOUCHER COLLEGE AND COST APPROXIMATELY \$ 2,200,000.
15. MD 146 N.B. LOOP TO W.B.	R = 115'	R = 175'	UPGRADING THE RAMP WOULD DISPLACE TWO RESIDENCES, IMPACT ONE CHURCH, AND REQUIRE HAMPTON LANES TO BE RELOCATED.
16. W.B. LOOP TO MD 146 S.B.	R = 100'	R = 175'	UPGRADING THE RAMP WOULD DISPLACE TEN RESIDENCES.
17. W.B. LOOP TO PROVIDENCE ROAD	R = 170'	R = 175'	UPGRADING THE RAMP WOULD REQUIRE APPROXIMATELY FOUR ACRES FROM A PRIVATE SCHOOL INCLUDING AN ATHLETIC FIELD, AND COST APPROXIMATELY \$ 1,900,000.
18. CROMWELL BRIDGE RD. LOOP ON TO E.B.	R = 110'	R = 175'	UPGRADING RAMP WOULD DISPLACE ONE COMMERCIAL ESTABLISHMENT, IMPACT PARKING SPACES FROM A HOTEL, IMPACT THE 100 YEAR FLOOD PLAIN, AND COST APPROXIMATELY \$ 4,200,000.
19. W.B. DIRECTIONAL TO MD 41 N.B.	R = 180'	R = 360'	UPGRADING RAMP WOULD RESULT IN A DEGRADATION OF AN ALREADY INADEQUATE WEAVE AREA BETWEEN THE RAMP AND JOPPA ROAD.
20-23. ALL LOOP RAMPS AT MD 147	R = 110' TO 150'	R = 175'	UPGRADING LOOP RAMPS WOULD RESULT IN MAJOR IMPACTS AND DISPLACEMENTS TO RESIDENCES AND COMMERCIAL ESTABLISHMENTS IN ALL FOUR QUADRANTS.

TABLE III-7

AASHTO DESIGN EXCEPTIONS REQUIRED FOR RAMPS

RAMP LOCATION	DESIGN EXCEPTION REQUIRED	REQUIRED TO MEET AASHTO MINIMUM CRITERIA	JUSTIFICATION FOR DESIGN EXCEPTION
24-27 ALL DIRECTIONALS AT MD 147	R = 130' TO 150'	R = 360'	ASSOCIATED WITH NOTE # 20 - # 23.
28-31 ALL LOOP RAMPS AT US 1	R = 150'	R = 175'	UPGRADING LOOP RAMPS WOULD CREATE A 4(f) PARK IMPACT AND WETLAND IMPACTS IN THE SOUTHWEST QUADRANT, AND RESIDENTIAL AND COMMERCIAL DISPLACEMENTS IN THE OTHER QUADRANTS.
32. US 1 S.B. DIRECTIONAL TO W.B.	R = 290'	R = 360'	UPGRADING THE RAMP WOULD REQUIRE RESIDENTIAL AND COMMERCIAL DISPLACEMENTS.
33. W.B. DIRECTIONAL TO US 1 N.B.	R = 290'	R = 360'	UPGRADING THE RAMP WOULD REQUIRE RESIDENTIAL AND COMMERCIAL DISPLACEMENTS.
34. US 1 N.B. DIRECTIONAL TO E.B.	R = 320'	R = 360'	UPGRADING THE RAMPS WOULD REQUIRE RESIDENTIAL DISPLACEMENTS.
35. W.B. DIRECTIONAL TO US 40 N.B.	R = 220'	R = 360'	UPGRADING RAMP WOULD DISPLACE A U-HAUL WAREHOUSE AND COST APPROXIMATELY \$ 7,000,000.
36. W.B. LOOP TO US 40 S.B.	R = 160'	R = 175'	UPGRADING RAMP WOULD IMPACT A PARKING GARAGE FOR GOLDEN RING MALL.
37. E.B. DIRECTIONAL TO US 40 S.B.	R = 220'	R = 360'	UPGRADING RAMP WOULD DISPLACE A STORMWATER MANAGEMENT POND AND PARKING SPACES, AND WOULD COST APPROXIMATELY \$ 4,400,000.
38. US 40 S.B. DIRECTIONAL TO W.B.	R = 230'	R = 360'	ASSOCIATED WITH NOTE # 36.

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7. Environmental Consequences of the Selected Alternates

An Environmental Assessment (FHWA-MD-EA-90-02-D) was approved by the Federal Highway Administration on January 23, 1990. This section discusses the potential environmental impacts associated with the Selected Build Alternate. Minimization and/or avoidance of impacts has been a primary goal in development of the build alternates.

a. Socio-Economic and Land Use

The transportation benefits of a Build Alternate would enhance the quality of life in the communities surrounding the Beltway. The Build alternates would not divide existing neighborhoods and existing school districts would not change.

Some access would be changed with two (2) of the interchange alternates. Under Option B at Harford Road the movement from the eastbound Beltway to California Avenue would be physically prohibited. Also direct access between Edgewood Avenue and Harford Road on the west side would be controlled and replaced by a short street connecting Grendon Lane and Edgewood Avenue. Only a minor portion of trips would be affected and alternative travel paths although sometimes circuitous, are available. At US 40, the proposed improvement at Golden Ring Road and MD 588 would change access. Existing Golden Ring Road would become a cul-de-sac. Immediate access between southbound US 40 and existing Golden Ring Road would be eliminated, resulting in minor access changes to several businesses and the new Rosedale Post Office (21237) located on Golden Ring Road. These businesses would have the same access to MD 7.

During construction of any of the Build alternatives (widening or interchanges), the neighborhoods adjacent to the Beltway would experience some adverse effects. During peak travel periods the existing number of lanes of traffic on I-695 would be maintained in accordance with detailed Maintenance of Traffic plans developed to assure as safe and efficient a flow of traffic as possible. However, because commuters and other Beltway users would experience traffic delays during construction, drivers may seek alternate routes through neighborhoods. In addition, those who live next to the construction areas would experience short-term increases in noise, and decreases in air quality (including higher levels of dust). Access from neighborhoods to the nearby Beltway may be hampered during construction of interchange options. Access to neighborhood services, such as shopping centers, may also be temporarily diminished during certain portions of construction. During bridge reconstruction, pedestrian paths across these bridges may be closed for short periods of time. Two bridges over I-695, MD 25 and Old Harford Road, may be closed to traffic during reconstruction either in their entirety or in part. MD 25 could be detoured via Old Court Road to either Joppa Road or Greenspring Avenue or I-83 (JFX) could be used. Old Harford Road may be reconstructed resulting in either a one-way system or a signal controlled two-way system. If a detour is required traffic could be detoured to Putty Hill Avenue then to either MD 41 or MD 147 to Joppa Road. The closing of either bridges, if required, will be

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coordinated with emergency facilities.

Because the Baltimore Beltway has been operational since the early 1960's, few or no substantial impacts to community facilities are expected to occur along the study corridor.

Each of the interchange build options would promote safety and circulation in the study area. As noted in the previous section, access to the Rosedale Post Office on Golden Ring Road would be slightly changed under the US 40 improvement proposed. No community facilities will be permanently separated from the area it serves by any proposed improvements.

During construction, the response time for police and fire services would increase as a result of additional roadway congestion associated with reduced Beltway capacity. Emergency response time would be increased if existing bridges at Falls Road and Old Harford Road would be totally closed during construction and emergency vehicles must use other routes (see correspondence section P.V-152 to P.V-155). In the long term, however, response time is expected to decrease over the No-Build condition due to less congested traffic patterns.

No residences would be taken by Alternate 2, no relocations will be required, and housing values are not expected to drop. No impacts to income or employment are anticipated.

The only commercial displacements due to interchange options would occur at US 40 (Pulaski Highway). Option 3 at MD 7/US 40/MD 702 would displace two commercial properties due to the reconstruction of the US 40 interchange in the southwest quadrant.

There would be no adverse impact to adjacent, established communities. There would be one mobile homes displaced at US 40 (Pulaski Highway). There should be no problem in acquiring affordable residential replacement properties.

Relocation of the individuals and families displaced by the project will be accomplished in accordance with the "Uniform Relocation Assistance and Land Acquisition Policies Act of 1970" as amended in 1987 (See Appendix). The relocation will be satisfactorily completed within a 6 to 12 month period, and in a timely, orderly and humane manner. The required acquisitions can be accomplished with minimal impact to the economic well-being of the project area and those directly affected.

As survey of the local real estate rental and sales market indicates that there is sufficient comparable replacement housing for the dislocated families. There should not be any major difficulties in relocating the affected families. Enough housing appears to be available in the area so there would be no adverse impact on neighborhoods into which the affected families will move. No significant change in population density or distribution is anticipated. No other federal, state or local projects are foreseen which would affect the supply and availability of needed replacement housing. None of the replacements are known to be handicapped.

TITLE VI STATEMENT

It is the policy of the Maryland State Highway Administration to ensure compliance with the provisions of Title VI of the Civil Rights Act of 1964, and related civil rights laws and regulations which prohibit discrimination on the grounds of race, color, sex, national origin, age, religion, physical or mental handicap in all State Highway Administration program projects funded in whole or in part by the Federal Highway Administration. The State Highway Administration will not discriminate in highway planning, highway design, highway construction, the acquisition of right-of-way, or the provision of relocation advisory assistance. This policy has been incorporated into all levels of the highway planning process in order that proper consideration may be given to the social, economic, and environmental effects of all highway projects. Alleged discriminatory actions should be addressed to the Equal Opportunity Section of the Maryland State Highway Administration for investigation.

The Build Alternates may positively affect study area income and employment if new jobs in the study area (created by better access or project construction) are gained by area residents, or if improved access increases land values. The increasing employment opportunities in the study area projected by the Regional Planning Council will be supported by this project's improvements to general access and local circulation.

Short term impacts due to construction are expected to be minor, consisting of disturbances in the study area promoting an unattractive working and commuting environment.

In general, the widening of the Baltimore Beltway supports the county's land use strategy for growth and development. Improvements at interchanges would enhance access to the surrounding area. This, in combination with other factors, would attract commercial and industrial development near interchanges. If properly managed through zoning and other tools, the areas between major interchanges would be freed for development as residential and other community uses, thus supporting the centralized development pattern the County promotes.

Short term land use impacts would include:

- o Restricted access to properties during construction.
- o Use of sites adjacent to the Beltway for storage of construction vehicles, equipment, and supplies. Most likely, all need for storage could be satisfied by the space available in the "loops" or open areas within interchanges. Construction easements at some places may temporarily restrict its usual land use.

b. Natural Environment

Topography, Geology, Soils, Farmlands

Long term impacts due to the widening (Alternate 2) would be minimal, particularly where the additional lanes would be constructed in the existing median. Where construction would be to the outside of existing pavement, changes in topography would occur, including cuts, fills, and construction of stormwater management basins. No major cuts or fills and no contact with bedrock is anticipated.

Impacts to soils under Alternate 2 (conversions to pavement) are minor as the widening remains in SHA right-of-way and the extent is small.

For the mainline widening, the potential conversion of prime farmland soil units and farmland soils of statewide importance outside the current right-of-way to pavement would be approximately 4 and 10 acres respectively. The Farmland Protection Policy Act (FPPA) requirements regarding coordination have been fulfilled and the Soil Conservation Service has indicated that due to zoning the FPPA does not apply. All of the impacted farmland is zoned for development and does not carry a high priority for protection.

Short term potential impacts to land resources would be erosion, downstream sedimentation, and construction related cuts and fills. Mitigation such as revegetation, as specified in the Maryland Water Resources Administration's Standards and Specifications for Soil Erosion and Sediment Control, 1983, will minimize these impacts and be included in contract specifications. Control of sedimentation and careful construction work on streambanks is particularly important to reduce water quality effects and soil loss. Strict enforcement of an erosion and sediment control plan approved by the Maryland Department of the Environment (MDE) will be adhered to.

Vegetation and Wildlife

There are no known threatened or endangered species in the study area or impacted by this project.

Long term impacts to vegetation and wildlife due to the widening (Alternate 2) would be the reduction of trees and shrubs in the corridor by approximately 11.5 acres. Species taken would be primarily ornamental and within the right-of-way. This clearing would remove all trees from the median strip as well as many from the roadside. Additional clearing outside the right-of-way may result from construction of stormwater management basins. The impacts are expected to be minimal since cleared areas will remain grassed as much as possible, and any species utilizing them would be very tolerant of disturbance. Reforestation of an appropriate acreage and location, under the definitions of Natural Resources Article Section 5-103, will be coordinated with the Maryland Department of Natural Resource during final design. The nature of the habitat (as an ecotone) would not change and the proximity of suburban development to which they could move would minimize impacts to birds and small mammals. Clearing

adjacent to streams would be kept to a minimum to reduce shading of the water and would be mitigated by replantings where possible.

The reduction of habitat and vegetation under Alternate 2 would, by increasing the impervious surface, impact aquatic species by reducing the filtering benefits of the area between the pavement and the water course. Filtering of pollutants such as oil, grease, sediment, and salts would therefore be less. This is not a change as much as an incremental impact, however, as the Beltway currently introduces these pollutants into area surface waters. Additionally, it is a goal of the project design to manage for water quality via basins, trenches, and other stormwater management devices. The resulting minor impacts to water quality would be concentrated in Lake Roland via Jones Falls, and in Stemmers Run due to the drainage patterns of the Beltway. Presence of brown trout in Jones Falls dictates certain types of stormwater management to preserve their habitat. Particularly, measures which allow stormwater runoff to pond and potentially rise in temperature would be avoided. In the Jones Falls drainage, infiltration would be via subsurface trenches dispersed throughout the watershed. Conventional detention basins (24 hour) would be used to avoid the potential of ponded water being flushed into the stream by a subsequent storm event.

Other more traditional stormwater management measures would be used elsewhere in the project. These would be located, when feasible, within interchange areas.

Impacts of the interchange options are generally minor. The culvert extensions or new culvert construction would be at I-83 (JFX) Option C, MD 41 Options A and B, and the US 40 Option. Reforestation of an appropriate acreage and locations, as defined in the Natural Resources Article Section 5-103, will be coordinated with the Maryland Department of Natural Resources during final design of any interchange options chosen.

Short term impacts to vegetation and wildlife include disturbance and soil erosion. Sediment can impact aquatic organisms by reducing the clarity of the water, and burying fish eggs and bottom dwellers. Mitigation measures, such as construction scheduling to comply with water quality standards, adherence to an approved Erosion and Sedimentation Control Plan, and quick revegetation would be followed to avoid such impacts. Design and construction of stormwater management measures prior to roadway construction would also alleviate these construction related impacts (as well as addressing long-term concerns). During construction near Jones Falls a double line of silt fences and management measures similar to the Chesapeake Bay Initiatives would protect this particularly important stream from sedimentation impacts. A waterway construction permit will be acquired prior to any instream work. Further mitigation by enhancement or in-kind replacement of corridor wetlands would also benefit area wildlife.

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Streams, Groundwater, Wetlands and Floodplains

Long term impacts to streams due to Alternate 2 are related to both the quantity and quality of runoff. Alternate 2 would result in 53 acres of new impervious surface. Approximately 34 percent would provide runoff ultimately to Jones Falls, 30 percent to Stemmers Run, and the remainder to other corridor streams. The resultant increase in volume and flow rate at any point would be incremental over existing conditions. In addition, higher traffic volumes will increase the oil, grease, and sediment on the Beltway, and the additional lane would require use of more salt and other deicing agents. Widening in the median is likely to cause a greater impact than on the outside since all vegetation which may have filtered roadway washoff would be removed and the wash off is drained directly into the receiving streams. However, water quality classifications are not expected to change as a result of Alternate 2, and the stormwater management measures provided would alleviate these impacts.

Stormwater management will provide, as required by state and county regulations, measures necessary to maintain post-development peak discharges for a 24 hour, 2 and 10 year frequency storm event at a level that is equal to or less than the respective pre-development rates. Additionally, particularly in Jones Falls, the downstream discharge peak for the 100 year frequency storm would not be increased. The goal in all areas is no net increase to the receiving stream. The order of preference for stormwater management measures utilized is as follows:

1. Infiltration of runoff on-site
2. Flow attenuation by use of open vegetated swales and natural depressions.
3. Stormwater retention structures.
4. Stormwater detention structures.

Impacts to groundwater due to Alternate 2 are associated with surface water quality and are expected to be negligible due to natural filtering by vegetation and topsoil.

Wetlands, Floodplains

In accordance with Executive Order 11990 (Protection of Wetlands) and Section 404 of the Clean Water Act, wetlands within the study area have been identified and the impacts produced by the proposed improvements have been quantified. The majority of the wetlands identified in the study area are found in direct association with streams that intersect the Beltway, with the remainder being areas that have developed due to the highway or other altered hydrologic conditions. Field investigations were held on March 8 and 30, 1988 and delineation was completed using the "Classification of Wetlands and Deepwater Habitats of the United States" by Lewis M. Cowardin (U.S.F.W.S.).

Alternate 2

Impacts to wetlands under Alternate 2 include partial takings of nine (9) areas (a total of approximately 0.25 acres) as well as increased

roadway runoff to all wetlands immediately adjacent to I-695. Table III-5 provides a listing of wetland impacts for Alternate 2. The runoff impacts are similar to those described under the impacts to streams although filtration through stormwater management basins will have occurred prior to discharge. Locations for stormwater management basins, to be determined as design progresses, would be outside wetland boundaries.

Wetland Site 15 (Figure III-3, P.III-21)

Wetland Site 15 is located just east of the gore area of wetbound I-695 and southbound I-83N (HX). The improvement in this area is the addition of one outside lane on the westbound Beltway. The resultant impact is approximately .070 acre of wetland taken for new earth fill.

Avoidance - The median can not accomodate the required widening. A mainline shift to avoid this site would impact the backyards of the Thornleigh Community along Alston Road and require the reconstruction of the existing median. A retaining wall designed to avoid this very minor take (\$146,000) would not be cost effective.

Minimization - No safety grading is proposed for this project (20' - 24' is required). Additionally, a guard rail with 2:1 slopes will be employed at this site.

Wetland Site 17 (Figure III-3, P.III-22)

Wetland site 17 is located at the end of the on-ramp of northbound MD 146 to eastbound I-695. The improvement in this area is the addition of one outside lane to the eastbound Beltway. The resulting impact is approximately .014 acres of wetland taken for new excavation and fill.

Avoidance - The median can not accommodate the required widening. A mainline shift to avoid this site would impacts the Hampton National Historic Site (a 4 (f) issue), and would require a reconstruction of the existing median. A retaining wall designed to avoid this very minor take (\$155,000) would not be cost effective.

Minimization - A retaining wall is proposed at the box culvert to avoid replacement of the box culvert. This will help minimize wetland impacts and retain use of the existing box culvert. No safety grading is proposed for this project (20' - 24' is required).

Wetland Site 18 (Figure III-3, P. III-22)

Wetland Site 18 is located approximately 1500' from the on-ramp of northbound MD 146 to eastbound I-695, and east of Wetland Site 17. The improvement in this area is the addition of one outside lane to I-695 for eastbound traffic. The resultant impact is approximately .026 acres of wetland taken for new earth fill.

Avoidance - The median cannot accomodate the required widening. A mainline shift to avoid this site would impact the Hampton National Historic Site (a4(f) issue) and would require a reconstruction of the

existing median. A retaining wall designed to avoid this very minor take (\$101,000) would not be cost effective.

Minimization - No safety grading is proposed for the project (20' - 24' is required).

Wetland Site 21 (Figure III-3, P.III-23)

Wetland Site 21 is located approximately 440' west of the gore point of the westbound on-ramp from Cromwell Bridge Road to westbound I-695. The improvement in this area is the addition of one outside lane to the outer loop (westbound traffic) of I-695. The resultant impact is approximately .024 acres of wetland taken for new earth fill. In addition, this proposed improvement will require a minor rerouting of the tributary to Minebank Run.

Avoidance - The median can not accomodate the required widening. A mainline shift to avoid the site would impact Wetland Site 19 (Minebank Run) and would require a reconstruction of the existing median. A retaining wall designed to avoid this very minor take (\$127,000) would not be cost effective.

Wetland Sites 22 and 23 (Figure III-3, P.III-24)

Wetland Site 22 is located in the northwest loop of the cloverleaf interchange at I-695 and MD 41 (Perring Parkway). Wetland Site 23 is located on the northside of the directional ramp from westbound I-695 to northbound MD 41. The improvement in this area is the addition of one outside lane to the outer loop (westbound traffic) of I-695. The resultant impact is approximately .003 acres and .012 acres of wetlands respectfully for new earth fill. These wetland sites would incur additional impacts with MD 41 (Perring Parkway) Interchange Option B.

Avoidance - The median can not accommodate the required widening. A mainline shift to avoid this site would impact Wetland Sites 20, 24 and 28, would require a reconstruction of the existing median, and would require the complete reconstruction of the MD 41 (Perring Parkway) interchange. A retaining wall designed to protect Wetland Site 22 would not be cost effective (\$11,000). A retaining wall will not avoid impacts to the Wetland Site 23.

Minimization - No safety grading is proposed for this project (20' - 24' is required). Additionally, a guard rail with 2:1 slopes will be employed at these sites.

Wetland Site 24 (Figure III-3, P.III-24)

Wetland Site 24 is located approximately 60' east of the Old Harford Road overpass of I-695. The improvement in this area is the outside widening of eastbound I-695 by one lane. The resultant impact is the taking of approximately .084 acres of wetland (as well as the stream channel) from a tributary associated with Whitemarsh Run, for new earth fill and excavation.

Avoidance - The median can not accommodate the required widening. A mainline shift to avoid the site would impact Wetland Sites 22 and 23, would impact Woodcraft Park (a 4 (f) issue), would require a reconstruction of the existing median and would require reconstruction of the MD 41 (Perring Parkway) interchange. A retaining wall will not avoid impact to this site.

Minimization - No safety grading is proposed for this project (20' - 24' is required).

Wetland Site 27 (Figure III-3, P.III-25)

Wetland Site 27 is located just east of the off-ramp to northbound US 1 from I-695. The improvement in this area is to provide a deceleration lane to northbound US 1. Currently there is no deceleration lane. The mainline widening is occurring within the median in this area. The resultant impact is approximately .009 acres of wetland for new earth fill.

Avoidance - The median will be utilized by the mainline widening. A shifting of the beltway to provide the deceleration lane is not feasible. A retaining wall designed to protect this very minor area (\$147,000) would not be cost effective.

Minimization - No safety grading is proposed for this project (20' - 24' required). A guard rail with 2:1 slopes will be employed at this site.

Wetland Site 29 (Figure III-3, P.III-26)

Wetland Site 29 is located on the northside of the ramp to northbound I-95 from eastbound I-695. The improvement in this area is the widening of the ramp to two lanes. The resultant impact is approximately .009 acres of wetland from the tributary associated with Stemmers Run for new earth fill.

Avoidance - To provide the widening on the south side would require a reconstruction of eastbound I-695 and an extension of a box culvert. A retaining wall will not avoid impacts to the site.

Minimization - No safety grading is proposed for this project (20' - 24' is required).

Pursuant to Executive Order 11990, efforts were made to avoid and minimize harm to wetlands within the project corridor. As previously discussed, there are no practicable alternatives to the proposed construction and take of wetland areas. The No-Build Alternate does not address the traffic needs for increased capacity, operations, and safety for I-695. A Section 404 Permit (COE), Non-Tidal Wetland Permit (DNR) will be required to fill wetlands in the project area. A suitable wetland mitigation plan will be developed during the project's final design phase and will be coordinated with appropriate permitting and resource agencies.

Impacts to floodplains due to Alternate 2 would be encroachment on six corridor streams. New impervious surface is an incremental addition to

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existing Beltway stream crossings and total paved area of study area watersheds. These encroachments would not measurably impact the floodplain values nor induce development. Increased flooding is not anticipated. A finding in accordance with EO 11991 is not required. A waterways construction permit reviewed and approved by the Maryland Department of Natural Resources would be required for options impacting floodplains. None of the impacts to floodplains are significant as defined by risk of loss of life, disruption of emergency service, or impacts to beneficiary floodplain values.

Interchange Options

Impacts to wetlands from the interchange options include partial takings of eight (8) wetland areas. Table III-5 provides a listing of the takes for the interchange options. In each case, wetland takings are required to adequately serve traffic and safety concerns. Roadway design has minimized encroachment through careful pier placement, particularly in the Jones Falls area. In keeping with E.O. 11990, measures to minimize harm, such as careful construction management and flagging of wetland boundaries will be incorporated into project specifications.

A comparison of wetland impacts reveals that the Jones Falls watershed will experience the greatest impacts with new stream crossings and large areas of new roadway surface. This watershed is quite sensitive due to its established wetland and trout population. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance, including use of snow fencing to mark areas, a double row of silt fencing and straw bales intercepting runoff, immediate removal of dredged material from the floodplain, and revegetation of disturbed areas with natural vegetation.

I-83 (JFX) OPTION C (Figure III-6, P.III-29)

Improvements associated with I-83 (JFX) Option C would impact two wetland sites (5 and 10). A total of 0.916 acres of wetland impacts would occur. All of the wetlands are associated with the Jones Falls. This improvement is a two-lane ramp from eastbound I-695 to southbound I-83 (JFX).

Wetland Site 5

Wetland Site 5 is situated adjacent to the inner loop of the Beltway. This improvement requires the extension of an existing culvert headwall in the stream channel. The resultant impact is approximately 0.003 acres of wetland.

Wetland Site 10

Wetland Site 10 is located approximately 200' south of I-695 and is immediately adjacent to the southbound I-83 (JFX) ramps from I-695. This site surrounds the Jones Falls. The resultant impact is 0.913 acres of wetland. The impacted area includes a bridge crossing of the Jones

Falls in which the entire area under the bridge was considered as impacted wetland.

Avoidance: The No-Build Option is the only avoidance of these two wetland sites. The No-Build Option does not meet the transportation needs in the design year 2015.

Minimization: A guard rail with 2:1 slopes will be employed at both wetland sites. The impacted area includes a bridge crossing the Jones Falls in which the entire area under the bridge was considered as impacted wetland (approximate 0.5 acres). This could be reduced in Final Design to just the pier location. A retaining wall along a section of Wetland Site 5 would not be cost effective (\$36,000). I-83 (JFX) Option D is an alternate to I-83 (JFX) Option C through Wetland Site 10. Option D would reduce the wetland impact by approximately 0.4 acres. However, Option D would reduce the design speed of this two-lane ramp from 50 MPH to 35 MPH which does not meet the design criteria for this important interstate to interstate movement. Another option, V.E. Option is also an alternative to I-83 (JFX) Option C. The V.E. Option would reduce the wetland impact by approximately 0.7 acres. However, the V.E. Option would reduce the design speed of this two-lane ramp from 50 MPH to 40 MPH which does not meet the design criteria for this interstate to interstate movement. Additionally, the V.E. Option would have difficulties in maintaining traffic as the ramp is constructed and I-83 (JFX) is shifted. A retaining wall to minimize impacts to Site 10 would not be cost effective (\$1,365,000).

MD 41 (PERRING PARKWAY) OPTION A (Figure III-10, P.III-33)

Improvements associated with MD 41 (Perring Parkway) Option A would impact Wetland Site 28. The resultant impact is approximately 0.025 acres of wetlands. The improvements include a C-D road for the eastbound Beltway.

Wetland Site 28

Wetland Site 28 is located in the southwest guardrant of the cloverleaf interchange of MD 41 (Perring Parkway) and I-695. The improvements require the extension of an existing culvert headwall in the stream channel.

Avoidance: The No-Build Option is the only avoidance to these improvements.

Minimization: A guard rail with 2:1 slopes will be employed at this site.

MD 41 (PERRING PARKWAY) OPTION B (Figure III-10, P.III-33)

Improvements associated with MD 41 (Perring Parkway) Option B would impact Wetland Site 22 and 23. A total of approximately 0.067 acres of wetlands would be impacted. The improvements include a C-D road for the westbound Beltway. Both of these wetland sites would be impacted by Alternate 2 (mainline widening), the impacted area is in addition to

impacts associated with Alterante 2.

Wetland Site 22 and 23

Wetland Site 22 is located in the northwest loop of the cloverleaf interchange at I-695 and MD 41 (Perring Parkway). Wetland Site 23 is located on the northside of the directional ramp from westbound I-695 to northbound MD 41. A resultant impact of 0.014 acres for Wetland Site 22 and 0.060 acres for Wetland Site 23.

Avoidance: A retaining wall designed to avoid impacts to Wetland Site 22 would not be cost effective (\$20,000). Wetland Site 23 can not avoid impacts with either Alternate 2 (mainline widening) or Option B.

Minimization: A guard rail with 2:1 slopes will be employed at these sites.

MD 7/US 40/MD 702 Option 3 (Figure III-13, P.III-36)

Improvements associated with these improvements would impact Wetland Sites 36, 38, and 39. A total impact of 0.309 acres would occur.

Wetland Site 36

Wetland Site 36 is located just south of the MD 695 overpass of Kelso Drive. Impacts to this wetland result from the US 40 interchange option. The resultant impact is approximately 0.010 acres of wetland associated with a tributary to Stemmers Run, taken for new earth fill.

Wetland Site 38

Wetland Site 38 is located just south of the eastbound MD 695 split to MD 702. The resultant impact is approximately 0.236 acres of wetlands taken for new earth fill.

Wetland Site 39

Wetland Site 39 is located on the northern side of the MD 695/MD 702 Interchange. The resultant impact is 0.063 acres. The impacted area was measured as the total area under the proposed bridge crossing of the Northeast Creek.

Avoidance: The No-Build would avoid these wetland sites but does not meet the transportation needs in the design year. A shift away from Sites 36 and 38 to avoid these sites would impact other wetland sites. A retaining wall to avoid Wetland Sites 36 and 38 would cost approximately \$550,000 and was deemed not cost effective.

Minimization: A guardrail with 2:1 slopes will be employed at Wetland Sites 36 and 38. The impacted area for Wetland Site 39 was taken as the area under the proposed bridge crossing of Northeast Creek. During final design this impacted area maybe reduced to the area under the bridge.

Pursuant to Executive Order 11990, efforts were made to avoid and minimize harm to wetlands within the project corridor. As previously discussed, there are no practicable alternatives to the proposed construction and take of wetland areas. The No-Build Alternate does not address the traffic operational needs. A Section 404 Permit (COE); Non-Tidal Wetland Permit (DNR) will be required to fill wetland in the project area. A suitable wetland mitigation plan will be developed during the project's final design phase and will be coordinated with appropriate permitting and resource agencies.

Encroachment on area floodplains due to interchange improvements is minimal and not anticipated to affect floodplain values or benefits.

Short term impacts to water resources and associated wetlands and floodplains would be erosion and sedimentation during construction. Construction activities, may, additionally, increase the likelihood of fuel or contaminants entering area waterways. If this occurs, mitigation will be implemented.

Mitigation, if properly applied, would alleviate almost all anticipated impacts. Adherence to Maryland Standards and an Erosion and Sedimentation Control Plan would reduce the sediment and pollutant load reaching corridor streams, wetlands, and groundwater. Strict enforcement of an erosion and sediment control plan approved by the Maryland Department of the Environment (MDE) will be adhered to. The stormwater management basins which are designed as part of the widening and therefore part of the interchange and relocation options serve to control both quantitative and qualitative stormwater effects. The filtering of runoff through vegetated swales is one of the most effective measures of qualitative control. Settling ponds such as the proposed stormwater management basins would improve water clarity and promote groundwater recharge. Local flow increases and erosion impacts are mitigated through the use of rocks and gravel at culvert edges.

CONCEPTUAL MITIGATION

A total of 1.575 of wetland impacts are anticipated for the Selected Alternates. The Jones Falls has the largest impact with approximately 0.916 acres (I-83 [JFX] Option C). The Jones Falls is quite sensitive due to the occurrence of a "wild" brown trout population that extends from its headwater above Greenspring Valley Road and continues inside the Baltimore Beltway. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance (including a double row of silt fencing, and straw bales intercepting runoff) and revegetation of disturbed areas with natural vegetation.

The Jones Falls, Slaughterhouse Branch, Stemmers Run, Whitemarsh Run, and Meinebank Run all appear to be good mitigation sites. In identifying potential mitigation sites for the preliminary phase, the following hierarchy by ascending order will be used:

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1. Within Watersheds
 2. Hydric Soils (considered primarily for grading and sources of hydrology)
 - A. 0' - 1'
 - B. 1.5' - 2.5'
 - C. 3.0' - 4.0'
 3. Land Use
 - A. Agriculture
 - B. Pasture
 - C. Cropland
 - D. Strip Mines, Quarries, and Gravel Pits
 - E. Barren Land
 - F. Shrub and Brush Rangeland
 - G. Transitional Areas
 4. Existing wetlands and location to existing wetlands
 5. Location to stream channels
 6. Within right-of-way
 7. Floodplains
 8. Slopes
 - A. 0 - 3%
 - B. 3 - 10%
 9. Size of located wetland mitigation site
 - A. 0 - 9 acres
 - B. 10+ acres

Effects on Coastal Resources

Consistency with the Maryland Coastal Zone Management Program has been a goal of this study. The concerns of the program have been a consideration throughout the development of this project, and coordination has been undertaken with the Coastal Resources Division (CRD), Tidewater Administration, Maryland Department of Natural Resources (DNR). A representative of DNR participated in the wetlands field review, and Coastal Resource impacts were discussed.

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TABLE III-8

WETLAND IMPACTS FOR ALTERNATE 2 AND INTERCHANGE OPTIONS

SITE	ALTERNATE	DESCRIPTION	CLASSIFICATION	IMPACTED ACREAGE
5	INT. OPT. C	JONES FALLS	R2UB3	.003
10	INT. OPT. C.	JONES FALLS	PF01A	.913
15	2	I-83/I-695	PEM5A	.070
17	2	HAMPTON BRANCH	PF01A/R2UB3	.014
18	2	HAMPTON BRANCH	PF01A	.026
21	2	LOCH RAVEN BLVD. I/C	R2UB2	.024
22	INT. OPT. B	PERRING PKWY. I/C	PF01A	.014
22	2	PERRING PKWY.	PF01A	.003
23	INT. OPT. B	PERRING PKWY. I/C	PEM5A/PF01A	.060
23	2	PERRING PKWY.	PEM5A/PF01A	.012
24	2	OLD HARFORD RD.	PEM5A	.084
27	2	BELAIR RD. I/C	PF01A	.009
28	INT. OPT. A	PERRING PKWAY	PF01A	.025
29	2	I-95/I-695	PEM5A	.009
36	INT. OPT. 3	KELSO DRVIE	PF01A	.010
38	INT. OPT. 3	KELSO DRIVE	PEM5A	.236
39	INT. OPT. 3	MD 702	R2UB2/R2SB1	.063
			TOTAL	1.575

c. Cultural Resources

Under Alternate 2 the widening would not impact any significant cultural resources. The State Historic Preservation officer (SHPO) concurred with the determination of No Effect for the widening in Alternate 2.

Option D of the I-83(HX) interchange proposal would require 0.5 acre of right-of-way from commercial properties in the Lutherville Historic District. This land, a 700' long strip taking, is presently used for access to commercial establishments for which access will be maintained. The closest National Register site to this improvement (**The Creighton Springhouse**) is over 300' away. The SHPO has made a determination of no adverse effect for this alternative due to the nature of the improvement. Section 4(f) does not apply to these properties because neither the buildings nor properties are individually historic, are not an integral part of the historic district in which they are located, and do not contribute to the factors which make the district historic.

One potential archeological site has been identified in the study area. This site located west of Hampton Mansion possibly would be disturbed by excavation for a stormwater management basin. The location and size of the basin will be determined during final design. The Maryland Historic Trust has determined that Phase I archeological investigations are not warranted for this site because they did not retain sufficient integrity for testing due to extensive disturbances connected with Beltway construction. (See Coorespondence P.V-143 to V-145).

In the short term, visitors to the Hampton Mansion Historic Site and the Valley Inn may experience increased noise and dust for short periods of time. A permanent noise barrier has been constructed along the Hampton Mansion Historic Site (prior to the mainline widening). The Greenspring Valley and Lutherville Historic Districts may see similar short-term disturbances and access disruption. These possible short term impacts will not effect the use of the historical resources.

The SHPO has made a determination of no adverse effect of the project on the historical resources. (See Coorespondence P.V-134 to P.V-142). Coordination is on-going with the Advisory Council regarding the no adverse effect determination for Lutherville Historic District and Greenspring Valley Historic District. The Advisory Council's concurrence in the no adverse effect determination is anticipated. A Section 4(f) is not warrented for the historical resources within this project. This project will not effect the use of any of the historical resources.

Based upon the above; visual, noise, and aesthetic impacts to the three sites will be minor, of short duration, and does not interfere with elements of the sites which make them eligible for the National Register and will have no substantial impacts to those qualities. Therefore, constructive use doe not apply.

d. Parks and Recreation

No direct impacts to parks and recreation areas are anticipated with Alternate 2. MD 41 (Perring Parkway) Interchange Option B would have temporary construction easement at Woodcroft Park. (See Figure III-10, P.III-33). Approximately 0.5 acres would be used for

construction activities over one construction season. An earth retaining barrier would avoid permanent taking from the parkland. Construction activities would not interfere with the existing access, playground, or tot lot and would be separated by a fence. Construction equipment will not be allowed to cross the park but will access via the Beltway. During, construction, the existing noise barrier would be removed and then replaced once the earth retaining wall is completed. The site temporarily used would be reseeded after construction and restored to its original condition. These improvements do not constitute a Section 4(f) impact. A Section 4(f) does not apply to a temporary occupancy (including those from a right-of-way entry, construction and other temporary easements and other short-term arrangements) where the officials having jurisdiction over the protected resource agree that the temporary occupancy: a) will be short duration, b) does not result in change of ownership, c) does not result in any temporary permanent adverse change to the activities, features, or attributes of the site, and d) involves only minor amounts of land. Correspondence with Baltimore County Department of Parks and Recreation (P.V-162 to V-164) indicates concurrence with these provisions.

US 1 (Belair Road) Interchange Option would require no land from Double Rock Park (see Figure III-12, P.III-35). Double Rock Park currently has an access entrance to maintenance facilities from US 1 and from the Bel Air Beltway Plaza (approximately 150' away across from Fowler Avenue). These improvements do not effect the activities of the park or the public access which is via Avondale Road to Texas Avenue.

In the short term, visitors to some parks near I-695 might be exposed to the view of construction activities. In addition, park sites may experience increased noise and dust for short periods of time. The construction will not interfere with the activities of the parks.

Based upon the above; visual, noise, and aesthetic impacts to the parks will be minor, of short duration, and does not interfere with elements of the parks which make them parks and will have no substantial impacts to those qualities. Therefore, constructive use does not apply.

e. Air Quality

The objective of this analysis is to compare the carbon monoxide (CO) concentrations estimated to result from the traffic volumes and roadway configurations of each alternate with the State and National Ambient Air Quality Standards (S/NAAQS). The NAAQS and SAAQS are identical for CO; 35 parts per million (PPM) for the maximum 1-hour period (40 mg/m³) and 9 PPM for an average one hour period within the maximum consecutive 8-hour period (10 mg/m³).

A microscale CO dispersion analysis for 1-hour and 8-hour CO concentrations resulting from automobile emissions was conducted. All calculations were performed for 1995 (year of completion) and 2015 (design year). The emission factors were calculated using the Environmental Protection Agency (EPA) third generation Mobile Source Emissions Model (MOBILE 3) computer program with credit for a vehicle inspection and maintenance program. Line source CO dispersion estimates were calculated using the third generation California Line Source Dispersion Model (CALINE 3).

A comparison of the predicted concentrations with the ambient air quality standards indicates that Alternate 2 would result in an improvement of the air quality of the area over the No-Build Alternate. Under the No-Build (Alternate 1), three minor violations of the 1 hour standard would occur in the 2015 design year. No violations of the 1-hour or 8-hour CO standard would occur due to implementation of Alternate 2.

The construction phase of the proposed project has the potential of impacting the ambient air quality through fugitive dust from grading operations and materials handling. The State Highway Administration has addressed this possibility by establishing Standard Specifications for Construction for Materials, which specifies procedures to be followed by contractors involved in state work.

The Maryland Air Management Administration was consulted to determine the adequacy of the Specifications in terms of satisfying the requirement of the Regulations Governing the Control of Air Pollution in the State of Maryland. The Administration found that the specifications are consistent with the requirements of these regulations. Therefore, during the construction period, all appropriate measures (Code of Maryland Regulations 26.11.06.03 D) will be taken to minimize construction impacts on the air quality of the area.

Copies of this Air Quality Analysis Report for I-695 from MD 140 to MD 702 have been provided to the Maryland Air Management Administration and the United States Environmental Protection Agency for review and comment.

A conformity analysis was completed by the Baltimore Regional Council of Governments in September, 1991. The Federal Highway Administration made a determination of conformity between the TIP and the SIP for attaining air quality standards in November, 1991.

The Air Management Administration and the United States Environmental Protection Agency reviewed the analysis and found them to be satisfactory. Copies of their comments can be found in Section V.

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TABLE III-9
 MAXIMUM 1-HOUR PREDICTED CO CONCENTRATIONS (PPM)
 (INCLUDING BACKGROUND CONCENTRATION)

Site	Use	Location	1995		2015	
			No-Build	Build	No-Build	Build
A	Residential	Annen Woods	17.0	14.5	22.3	18.7
B	Residential	Long Meadow Estates	14.5	12.4	18.4	15.5
C	Residential	Cranwood	20.7	17.3	26.9	22.3
D	Residential	Eden Roc	21.8	18.3	28.8	23.8
E	Residential	Grasty Road	26.9	22.3	36.0*	29.6
F	Commercial	Valley Inn	15.9	13.8	19.3	17.7
G	Religious	Towson 7th Day Adventist Church	20.3	17.2	26.0	21.8
H	Residential	Thornleigh	27.0	22.8	33.7	28.1
I	Residential	Wellington Valley	18.8	18.3	26.6	22.4
J	Residential	Rivervale	26.8	22.6	35.9*	30.0
K	Residential	Orchard Hills	21.4	18.1	29.8	24.9
L	Residential	Riverwood Hills	17.7	14.8	23.5	19.5
M	Residential	Green Ridge	18.5	15.6	25.7	21.5
N	Religious	Towson Methodist Church	17.5	13.1	24.2	17.5
O	Residential	Campus Hills	20.3	17.3	27.7	23.3
P	Recreational	Woodcroft Park	23.5	20.5	33.7	28.8
Q	Religious	Rock Church	18.0	15.4	25.0	21.1
R	Residential	Baynesville	25.0	21.4	35.3*	29.9
S	Residential	Woodcroft Park	18.5	15.7	26.3	22.0
T	Residential	Methodist Church Joppa Road	19.0	17.3	30.3	25.9
U	Residential	Ridge Grove	22.9	19.5	33.8	28.7
V	Residential	Parktown Apartments	19.4	16.4	26.6	22.1
W	Residential	Town and Country Apartments	12.3	10.6	18.0	15.2

* 35 PPM Standard Exceeded

TABLE III-10
MAXIMUM 8-HOUR PREDICTED CO CONCENTRATIONS (PPM)
(INCLUDING BACKGROUND CONCENTRATION)

<u>Site</u>	1995		2015	
	<u>No-Build</u>	<u>Build</u>	<u>No-Build</u>	<u>Build</u>
A	4.4	4.0	5.1	4.5
B	3.9	3.6	4.3	3.9
C	4.7	4.2	5.6	4.9
D	4.9	4.4	5.9	5.1
E	5.7	5.1	7.1	6.1
F	3.9	3.6	4.2	4.0
G	5.1	4.6	6.1	5.3
H	5.7	5.1	7.0	6.1
I	4.4	4.3	5.6	5.0
J	4.5	4.1	5.4	4.7
K	5.1	4.5	6.1	5.4
L	4.6	4.2	5.5	4.8
M	4.9	4.4	5.9	5.2
N	4.8	4.1	5.8	4.7
O	5.1	4.5	6.1	5.4
P	5.4	4.9	6.8	6.1
Q	4.5	4.1	5.4	4.8
R	5.7	5.1	7.2	6.3
S	4.6	4.2	5.7	5.0
T	5.1	4.6	6.3	5.6
U	5.4	4.9	7.0	6.2
V	4.7	4.3	5.4	4.8
W	4.4	4.0	5.0	4.4

Note: No site exceeds the 9 PPM standard.

f. Noise Quality

Noise abatement criteria for various land uses have been established by the Federal Highway Administration (FHWA) in 23 CFR 772. The noise abatement criterion for land uses occurring in the project corridor, (Category B), is 67 dBA Leq. The Leq or "Equivalent Sound Level" is the energy averaged sound level for a given time period; for this analysis the Leq is predicted for a one (1) hour period. Future year (2015) noise levels for the project area were predicted using the Federal Highway Administration Traffic Noise Prediction Model (FHWA-RD-77-108). The STAMINA 2.0/OPTIMA Barrier Cost Reduction Procedure version of the FHWA model was used.

According to the procedures described in 23 CFR 772 noise impacts occur when predicted traffic noise levels for the design year approach or exceed the noise abatement criterion prescribed for a particular land use category, or when the predicted traffic noise levels are substantially higher than existing noise levels (in Maryland this is a 10 dBA increase). This analysis (utilizing Category B land uses), was completed in accordance with federal procedures and evaluated in accordance with State Highway Administration noise policy and guidelines. Under SHA policy, once an impact has been identified (the FHWA criteria of 67 dBA, approached or exceeded in this case), the following factors are evaluated to determine whether mitigation is reasonable and feasible.

- o Whether an effective and feasible method is available to attenuate the noise.
- o Whether the cost of noise mitigation is reasonable for those receptors that are impacted - approximately \$40,000 per impacted and protected residence.
- o Whether the mitigation is acceptable to affected property owners.

A total of 21 of the 43 NSAs retained for detailed study are currently experiencing Beltway traffic generated noise levels which equal or exceed the 67 dBA Leq FHWA noise abatement criterion for residential land uses.

Predicted noise levels for Alternate 2 (Mainline Beltway Widening), also reflect "worst case" or LOS "C" traffic conditions in the corridor (as well as changed number of roadway lanes). As shown in Table III-11, predicted noise levels increase over No Build levels and range from 1 to 6 dBA for Alternate 2. The effect of Alternate 2 over the ambient condition range up to 10 dBA. A total of 36 of the 43 receptors would equal or exceed 67 dBA Leq and therefore be impacted.

As detailed in Table III-11, construction of any of the interchange options would have little or no effect on noise levels at the NSA's receptors as predicted under Alternate 2. Therefore the noise barriers investigated by Alternate 2 need not be revised. The MD 146 Interchange improvements would increase the predicted noise level build alternate by 1 dBA but would not effect the noise barrier investigated by Alternate 2.

The State Highway Administration adopted their Type II Noise Abatement Program, in conjunction with Federal legislation, to provide relief from existing noise levels for residential areas and Public Institutions adjacent to

existing major highways. To date the following Type II projects are either approved or constructed in the project area:

- I-695: Perring Parkway to Harford Road (Constructed)
- I-695: Providence Road (EBR) (Constructed)
- I-695: Hampton/Concordia Drive (Constructed)
- I-695: York Road/Dulaney Valley Road
- I-695/I-83: Thorton/Seminary Road
- I-695: Charles Street/York Road

Various noise abatement measures were initially considered for receptors approaching or exceeding the 67 dBA noise impact criterion under a build alternate. Neither a lane shift, construction of earth berms, nor acquisition of buffer zones were feasible for any of the NSA's due to right-of-way constraints. Depressing the new lanes on all or part of the Beltway was not feasible because of the need to maintain continuity with the existing facility. Rerouting of through trucks was not feasible as I-695 is a major part of the interstate system on the Northeast Corridor and banning trucks would constitute a severe economic burden. A truck ban on portions of the facility would direct truck traffic onto local streets. Because of these constraints, noise barriers appear to be the most feasible means of reducing noise in the project area.

Noise walls were analyzed for 35 of the 36 NSA's in which impacts were projected to occur in order to determine the feasibility and estimate the cost of such mitigation. A barrier protecting Hampton Mansion has been constructed as a Type II project and is therefore not included in this analysis. A barrier at NSA 39 was not analyzed because of site restrictions. A noise wall at this location would not have been effective because it would have to be segmented by numerous driveways. Offset barriers to compensate for the breaks in the wall are not feasible due to right-of-way impacts and they would impact the Lutherville Historic District creating a Section 4(f) impact.

Barriers were analyzed as to achieve a 7 to 10 dBA reduction in predicted noise levels where feasible. Table III-11 lists the location of NSA's analyzed. Also included is the ambient noise level, predicted noise levels with the no-build and build condition. Noise barriers were analyzed for the affected NSA's. For the purpose of this document, the total construction cost of any noise barrier is assumed to be \$16 per square foot, based on recently completed barrier projects. All impacted receptors receiving a 5 dBA or greater reduction in noise levels are considered to be protected.

The evaluation of the 36 NSA's impacted by this project is detailed below. Note that some sites are protected by one barrier and other sites have been split and two barriers evaluated.

NSA 1-Beth El Synagogue: NSA 1 will have a predicted 2015 noise level of 67 dBA. There is a 4 dBA difference between predicted Build and No-Build levels. A barrier of 440' in length by 22' in height with a total cost of \$154,880 was analyzed. This barrier would provide a 6 dBA reduction for the synagogue. The cost per residence is \$30,976. A church is considered to be equivalent to 5 residences. This barrier will be considered further during final design.

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NSA 4-Grasty Road): NSA 2 will have a predicted 2015 noise level of 71 dBA. There is a 2 dBA difference between predicted Build and No-Build levels. A barrier of 1000' in length by 24' in height with a total cost of \$384,000 was analyzed. This barrier would provide a 10 dBA reduction for 3 residences. The cost per residence is \$128,000. This barrier is not considered reasonable.

NSA 8-Recreational Facility off Joppa Road: NSA 8 will have a predicted 2015 noise level of 68 dBA. There is 7 dBA difference between predicted Build and No-Build levels. A barrier of 945' in length by 10'-18' in height with a total cost of \$151,200 was analyzed. This barrier would provide a 4 dBA reduction for 1 recreation area. The cost per residence is \$20,160. This barrier will be considered further during final design.

NSA 10 and 41-Othoridge Road and Warwick Drive: NSA 10 and 41 will have a predicted 2015 noise level of 73 dBA. There is a 10 dBA and 11 dBA difference between predicted Build and No-Build levels, respectfully. A barrier of 4330' in length by 12-22' in height with a total cost of \$985,280 was analyzed. This barrier would provide a 7 dBA reduction for 78 residences. The cost per residence is \$12,632. This barrier will be considered further during final design.

NSA 11-Towson United Methodist Church: NSA 11 will have a predicted 2015 noise level of 71 dBA. There is a 9 dBA difference between predicted Build and No-Build levels. A barrier of 990' in length by 20'-26' in height with a total cost of \$376,800 was analyzed. This barrier would provide a 7 dBA reduction for 1 church. The cost per residence is \$75,360. This barrier is not considered reasonable.

NSA 12-Cromwell Valley Apartments: NSA 12 will have a predicted 2015 noise level of 68 dBA. There is a 4 dBA difference between predicted Build and No-Build levels. A barrier of 1790' in length by 10'-14' in height with a total cost of \$332,806 was analyzed. This barrier would provide a 4 dBA reduction for 33 residences. The cost per residence is \$10,085. This barrier will be considered further in final design.

NSA 13-Lutheran High School: (Residential): This site has a barrier recently constructed through the State Highway Administration's Type II noise abatement program.

NSA 13-Lutheran High School (Recreational): NSA 13 will have a predicted 2015 noise level of 71 dBA. There is a 6 dBA difference between predicted Build and No-Build levels. A barrier of 840' in length by 25' in height with a total cost of \$336,000 was analyzed. This barrier would provide a 6 dBA reduction for 1 recreation area. The total cost per residence is \$84,000. This barrier is not considered reasonable.

NSA 14-Dunwoody Road (East): NSA 14 will have a predicted 2015 noise level of 71 dBA. There is a 2 dBA difference between predicted Build and No-Build levels. A barrier of 1185' in length by 20' in height with a total cost of \$379,200 was analyzed. This barrier would provide a 6 dBA reduction for 17 residences. The total cost per residence is \$22,306. This barrier will be considered further during final design.

NSA 18-Linover Park: NSA 18 will have a predicted 2015 noise level of 69

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dBa. There is a 9 dBA difference between predicted Build and No-Build levels. A barrier of 1110' in length by 16' in height with a total cost of \$284,160 was analyzed. This barrier would provide a 7 dBA reduction for 1 park. The total cost per residence is \$142,080. This barrier is not considered reasonable.

NSA 19-Trump's Mill Road: NSA 19 will have a predicted 2015 noise level of 68 dBA. There is a 6 dBA difference between predicted Build and No-Build levels. A barrier of 830' in length by 10' in height with a total cost of \$132,800 was analyzed. This barrier would provide a 3 dBA reduction for 2 residences. The total cost per residence is \$66,400. This barrier is not considered reasonable.

NSA 20-Recreational facility off Lillian Holt Drive: NSA 20 will have a predicted 2015 noise level of 67 dBA. There is a 1 dBA difference between predicted Build and No-Build levels. A barrier of 1600 sq. ft. in size (triangular in shape to account for terrain) with a total cost of \$25,600. This barrier would provide a 6 dBA reduction for 1 recreation area. The total cost per residence is \$9,143. This barrier will be considered further in final design.

NSA 21-Fordcrest Road: NSA 21 will have a predicted 2015 noise level of 69 dBA. There is a 8 dBA difference between predicted Build and No-Build levels. A barrier of 1125' in length by 25'-29' in height with a total cost of \$492,800 was analyzed. This barrier would provide a 7 dBA reduction for 19 residences. The total cost per residence is \$25,937. This barrier will be considered further in final design.

NSA 26-Granary Drive: NSA 26 will have a predicted noise level of 68 dBA. There is a 1 dBA difference between predicted Build and No-Build levels. A barrier of 1000' in length by 15' in height with a total cost of \$240,000 was analyzed. This barrier would provide a 5 dBA reduction for 13 residences. This total cost per residences is \$18,462. This barrier will be considered further in final design.

NSA 29-Phillips Drive: NSA 26 will have a predicted noise level of 68 dBA. There is a 1 dBA difference between predicted Build and No-Build levels. A barrier of 1000' in length by 15' in height with a total cost of \$744,080 was analyzed. This barrier would provide a 5 dBA reduction for 13 residences. This total cost per residence is \$41,338. This barrier will be considered further in final design.

NSA 30-Stonehenge Circle: NSA will have a predicted 2015 noise levels of 75 dBA. There is a 1 dBA difference between predicted Build and No-Build levels. A barrier of 1330' in length by 10'-12' height with a total cost of \$226,560 was analyzed. This barrier would provide a 10 dBA reduction for 21 residences. A total cost per residence is \$10,789. This barrier will be considered further in final design.

NSA 31-Eden Roc Court: NSA 31 will have a predicted 2015 noise level of 70 dBA. There is a 3 dBA difference between predicted Build and No-Build levels. A barrier of 780' in length by 24' in height with a total cost of \$299,520 was analyzed. This barrier would provide a 4 dBA reduction for 3 residences. A total cost per residences is \$99,840. This barrier is not considered reasonable.

NSA 32-Grasty Road: NSA 32 will have a predicted 2015 noise level of 67 dBA. There is no difference between predicted Build and No-Build levels. A barrier of 1295' in length by 14'-20' in height with a total cost of \$361,600 was analyzed. This barrier would provide a 6 dBA reduction for 5 residences. A total cost per residence is \$72,320. This barrier is not considered reasonable.

NSA 34-Lochmoor Court: NSA 34 will have a predicted 2015 noise level of 73 dBA. There is a 5 dBA difference between predicted Build and No-Build levels. A barrier of 1740' in length by 10'-18' in height with a total cost of \$409,600 was analyzed. This barrier would provide a 8 dBA reduction for 28 residences. A total cost per residence is \$14,629. This barrier will be considered further in final design.

NSA 35-Carrbridge Circle: NSA 35 will have a predicted 2015 noise level of 73 dBA. There is a 5 dBA difference between predicted Build and No-Build levels. A barrier of 1200' in length by 15'-24' in height with a total cost of \$427,680 was analyzed. This barrier would provide a 7 dBA reduction for 26 residences. A total cost per residences is \$16,449. This barrier will be considered further in final design.

NSA 36-Adcock Road: This site has been proposed for a barrier under the State Highway Administration's Type II Noise Abatement Program. This barrier would provided on 8 dBA reduction. The cost per resident is \$14,084.

NSA 37 and 38-Alston Road and Jeffers Circle: This site has been proposed for a barrier under the State Highway Administration's Type II Noise Abatement Program. This barrier would provide a 10 dBA reduction. The cost per resident is \$14,091.

NSA 39-Charles Street: NSA 39 will have a predicted 2015 noise level of 70 dBA. There is a 4 dBA difference between predicted Build and No-Build noise levels. A barrier at this location would cross driveways, precluding access, and was therefore is considered unreasonable. (See discussion on P.III-78).

NSA 40-Marleigh Circle: This site has been proposed for a barrier under the State Highway Administration's Type II Noise Abatement Program. This barrier would provide a 7 dBA reduction. The cost per resident is \$9,956.

NSA 42-Meadowvale Road: This site has been proposed for a barrier under the State Highway Administration's Type II Noise Abatement Program. This barrier would provide a 7 dBA reduction. The cost per resident is \$12,187.

NSA 43-Annebrook Court: NSA 43 will have a predicted 2015 noise level of 75 dBA. There is a 1 dBA difference between predicted Build and No-Build levels. A barrier of 570' in length by 14'-22' in height with a total cost of \$180,211 was analyzed. This barrier would provide a 10 dBA reduction for 16 residences. A total cost per residences is \$11,263. This barrier will be considered further in final design.

NSA 43-West of Longquarter Branch: This site has been proposed for a barrier under the State Highway Administration's Type II Noise Abatement Program. This barrier would provide a 10 dBA reduction. The cost per resident is \$11,263.

NSA 46-Rock Church: NSA 46 will have a predicted 2015 noise level of 70 dBA. There is no difference between predicted Build and No-Build levels. A barrier of 500' in length by 10' in height with a total cost of \$80,000 was analyzed. This barrier would provide a 5 dBA reduction for 1 church. A total cost per residence is \$16,000. This barrier will be considered further in final design.

NSA 47-Eddington Road: NSA 47 will have a predicted 2015 noise level of 73 dBA. There is a 3 dBA difference between predicted Build and No-Build levels. A barrier of 1150' in length by 10'-16' in height with a total cost of \$259,840 was analyzed. This barrier would provide a 7 dBA reduction for 7 residences and 1 church. A total cost per residences is \$21,653. This barrier will be considered further in final design.

NSA 47-Wayne Avenue: NSA 47 will have a predicted 2015 noise level of 73 dBA. There is a 3 dBA difference between predicted Build and No-Build noise levels. A barrier of 1405' in length by 15' in height with a total cost of \$337,200 was analyzed. This barrier would provide a 9 dBA reduction for 17 residences. A total cost per residences is \$19,835. This barrier will be considered further in final design.

NSA 52-Double Rock Lane: NSA 52 will have a predicted 2015 noise level of 70 dBA. There is a 1 dBA difference between predicted Build and No-Build noise levels. A barrier of 1060' in length by 20' in height with a total cost of \$339,200 was analyzed. This barrier would provide a 3 dBA reduction for 31 residences. A total cost per residence is \$10,942. This barrier will be considered further in final design.

NSA 55 and 56-East Avenue: NSA 55 and 56 will have a predicted 2015 noise level of 67 dBA and 72 dBA. There is a 2 dBA and 1 dBA difference between predicted Build and No-Build total cost of \$139,200 was analyzed for these sites. This barrier would provide a 6 dBA reduction for 10 residences. A total cost per residence is \$13,920. This barrier will be considered further in final design.

NSA 57-Wayne Avenue; NSA 58-Eddington Road; and NSA 59-Oakleigh Rd: Noise analysis for NSA 57, 58 and 59 vicinities were prepared for "Report on Noise Barrier Feasibility I-695: From Lock Raven Boulevard to Perring Parkway." (Revised June, 1989). Barriers of 1500' in length by 13.5' in height (NSA 57), 2170' in length by 16" in height (NSA 58), and 1450' in length by 17' in height with a total cost of \$1,273,520 was analyzed. This barrier would provide noise reduction for 55 residences and a cost per residences is \$23,155. This barrier will be considered further in final design.

NSA 60-Proposed MD 43; Noise analysis for NSA 60 includes 9 receptor sites that were prepared for "Noise Analysis for MD 43 at I-695" (May 1, 1987). Under the State Highway Administration's Type II Noise Abatement Program, two NSA's were analyzed. Balder Road/Arizona Avenue will have a predicted 2010 noise level of 71 dBA. There is a 2 dBA difference between predicted Build and No-Build noise levels. A barrier of 1,200' in length by 24' in height with a total cost of \$460,800 was analyzed. This barrier would provide a 10 dBA reduction for 20 residences. A total cost per residence is \$23,040. California Avenue will have a predicted 2010 noise level of 68 dBA. There is a 2 dBA drop between the predicted Build and No-Build noise levels. A barrier

of 600' in length by 26' in height with a total cost of \$249,600 was analyzed. This barrier would provide a 10 dBA reduction for 1 residences. A total cost per residence is \$249,600.

Land uses that would be sensitive to vehicular noise would also be sensitive to construction noise. Although construction is a short-term phenomenon, it can cause significant noise impacts. Additionally, it is likely that some construction would take place at night to avoid severe traffic impacts. The extent and severity of the noise impact would depend upon the phase of construction and the noise characteristics of the construction equipment in use. Construction would have a direct impact on receptors located close to the construction site and would have an indirect impact on receptors located near roadways whose traffic flow characteristics are altered due to rerouting of traffic from the construction site.

As with any major construction project, areas around the construction site are likely to experience varied periods and degrees of noise impact. This type of project would probably employ the following pieces of equipment that would likely be sources of construction noise:

- o Bulldozers and Earth Movers
- o Graders
- o Front End Loaders
- o Dump and Other Diesel Trucks
- o Compressors

Maintenance of construction equipment will be regular and thorough to minimize noise emissions because of inefficiently tuned engines, poorly lubricated moving parts, poor to ineffective muffling systems, ect.

Noise barriers have been analyzed for this project. Based upon the information available, it appears that barriers at 18 locations may be reasonable and feasible and maybe approved for construction. A final decision on each barrier will be made during the design phase of the project when more detailed design information will be developed.

Several noise barriers are expected to be in place prior to Beltway construction, including one at the Hampton Mansion Historic Site. With the exception of a few spot locations where noise barriers will need to be relocated, these barriers will help to mitigate construction noise.

**TABLE III-II
SUMMARY OF NOISE ANALYSIS
ALTERNATE 2**

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NOISE SENSITIVE AREA (b) AFFECTED	USE	LOCATION	AMBIENT NOISE LEVEL	PREDICTED NOISE LEVEL NO-BUILD ALTERNATE	PREDICTED NOISE LEVEL BUILD ALTERNATE	PREDICTED NOISE LEVEL WITH BARRIER	LENGTH / HEIGHT (h) OF BARRIER (feet)	COST OF BARRIER (d) (\$)	NUMBER OF RESIDENCES PROTECTED	COST PER RESIDENCE (e) (\$)
1	Religious	Beth El Synagogue	63 (a)	66	67	61	440/22	\$ 154,880	1 church	\$ 30,976
2	Religious	Chizuk Amuno Synagogue	66 (a)	63	64	-	-	-	-	-
3	Residential	Wisperwood Court Residences (Halcyon Gate)	62 (a)	62	63	-	-	-	-	-
4	Residential	Grasty Road Residences (South)	69 (a)	70	71	61	1000/24	\$ 384,000	3	\$ 128,000
6	Commercial (k)	Valley Inn Restaurant	60	64	65	-	-	-	-	-
7	Religious	Towson 7th Day Adventist Church	62 (a)	65	66	-	-	-	-	-
8	Recreational	Private Recreational Area off Joppa Road (Brooklandville)	61 (a)	68	68	64	945/10-18	\$ 151,200	1 rec. area	\$ 20,160
9	Residential	Bellona Lane Residences (Rivervale)	61 (a)	64	65	-	-	-	-	-
10	Residential	Orthoridge Road Residences (Orchard Hills)	63 (a)	72	73	66	500/15	\$ 120,000	7	\$ 17,143
11	Religious	Towson United Methodist Church (Hampton)	62 (a)	69	71	64	990/20-26	\$ 376,800	1 church	\$ 75,360
12	Residential	Cromwell Valley Apartments	64 (a)	67	68	64	1790/10-14	\$ 332,806	33	\$ 10,085
13	Residential	Concordia Drive Residences (Hunt Crest Estates)	65 (a)	70	71	65	1400/25	\$ 560,000	15	\$ 37,333
13	Recreational	Lutheran High School Athletic Field	65 (a)	70	71	65	840/25	\$ 336,000	1 rec. area	\$ 84,000
14	Residential	Dunwoody Road Residences (East) (Oakleigh)	69	69	71	65	1185/20	\$ 379,200	17	\$ 22,306
15	Residential	Dunwoody Road Residences (West)	61 (a)	62	63	-	-	-	-	-
18	Recreational	Linover Park (Linhigh)	60 (a)	68	69	62	1110/16	\$ 284,160	1 park	\$ 142,080
19	Residential	Trumps Mill Road Residences (Kenwood Park)	62 (a)	66	68	65	830/10	\$ 132,800	2	\$ 66,400
20	Recreational	Private Recreation Area Along Lillian Holt Drive	66 (a)	66	67	61	(g)	\$ 25,600	1 rec. area	\$ 9,143
21	Residential	Fordcrest Road Residences (Kenwood)	61 (a)	68	69	62	1125/25-29	\$ 492,800	19	\$ 25,937
26	Residential	Granery Drive Residences (Fishtown)	67	67	68	63	1000/15	\$ 240,000	13	\$ 18,462
27	Residential	Melody Lane Residences	64	65	66	61	1410/15 (j)	\$ 338,400	10	\$ 33,840
28	Residential	Round Hollow Road Residences	61	62	63	-	-	-	-	-
29	Residential	Phillips Drive and Red Barn Court Residences (Cranwood)	68	68	69	65	3050/14-20	\$ 744,080	18	\$ 41,338
30	Residential	Stonehenge Circle Residences (Long Meadow Estates)	74	74	75	65	1330/10-12	\$ 226,560	21	\$ 10,789
31	Residential	Eden Roc Court Residences (Eden Roc)	67	69	70	66	780/24	\$ 299,520	3	\$ 99,840
32	Residential	Grasty Road Residences (North)	67	66	67	61	1295/14-20	\$ 361,600	5	\$ 72,320
33	Residential	Brickford Lane Residences	61	60	62	-	-	-	-	-
34	Residential	Lockmoor Court Residences (Boxwood)	68	72	73	65	1740/10-18	\$ 409,600	28	\$ 14,629
35	Residential	Carrbridge Circle Residences (Thornleigh)	68	72	73	64	1200/15-24	\$ 427,680	26	\$ 16,449
36	Residential	Adcock Road Residences	72	74	75	67	2400/12-18	\$ 535,200	38	\$ 14,084
37	Residential	Alston Road Residences (Thornleigh)	71	76	77	67	1800/12-16	\$ 408,640	29	\$ 14,091
38	Residential	Jeffers Circle Residences	63 (a)	69	70	64	950/10-15	\$ 168,000	18	\$ 9,333
39	Residential	Charles Street Residences (North)	66	69	70	(c)	-	-	-	-
40	Residential	Marleigh Circle Residences (Riverwood Hills)	68	71	72	65	2000/14	\$ 448,000	45	\$ 9,956
41	Residential	Warwick Avenue and Dublin Road Residences (Orchard Hills)	72	72	73	66	3765/12-22	\$ 865,280	71	\$ 12,187
42	Residential	Tenbury Road and Meadowvale Road Residences (Green Ridge)	70	72	73	64	2515/8-20	\$ 559,040	44	\$ 12,705
43	Residential	Wilfred Court Residences West of Longquarter Branch (Towson Park)	74	74	75	65	570/14-22	\$ 180,211	16	\$ 11,263
43	Residential	Annebrooke Court Residences East of Longquarter Branch	74	74	75	65	1230/14-22	\$ 388,877	41	\$ 9,485
45	Residential	Breezewick Circle Residences	61	63	64	-	-	-	-	-
46	Religious	Rock Church	70	69	70	65	500/10	\$ 80,000	1 church	\$ 16,000
47	Residential	Eddington Road Residences (Baynesville)	70	71	73	67	1150/10-16	\$ 259,840	7, 1 church	\$ 21,653
47	Residential	Wayne Avenue Residences (Baynesville)	70	71	73	64	1405/15	\$ 337,200	17	\$ 19,835
52	Residential	Double Rock Lane Residences (Parktowne)	69	69	70	67	1060/20	\$ 339,200	31	\$ 10,942
53	Residential	Glendower Court Residences	64	65	67	61	1610/10-24	\$ 556,000	24, 1 rec. area	\$ 22,063
55	Residential	East Avenue Residences (Kenwood Park)	65	66	67	61	870/10	\$ 139,200	10	\$ 13,920
56	Residential	East Avenue Residences (Kenwood Park)	71	70	72	64	(i)	(i)	(i)	(i)
57	Residential	Wayne Avenue Residences (Baynesville) (k)	-	-	-	-	1500/13.5	\$ 1,273,520	55	\$ 23,155
58	Residential	Eddington Road Residences (Baynesville) (k)	-	-	-	-	2170/16	\$ 1,273,520	55	\$ 23,155
59	Residential	Oakleigh Road (Oakleigh) (k)	-	-	-	-	1450/17	\$ 1,273,520	55	\$ 23,155
60	Residential	Avondale Road (Outer Loop)	64	63	64	-	-	-	-	-
61	Residential	Sperl Avenue	63	63	64	-	-	-	-	-
62	Residential	Balder Avenue (Ridge Grove) (L)	69	69	70	61	-	-	-	-
62A	Residential	Arizona Avenue (Ridge Grove)	59	58	59	52	1200/24	\$ 460,800	20	\$ 23,040
63	Residential	California Avenue (Ridge Grove)	67	67	67	58	600/26	\$ 249,600	1	\$ 249,600
64	Residential	Victory Avenue	66	65	66	-	-	-	-	-
65	Residential	Fearne Avenue	65	65	64	-	-	-	-	-
66	Residential	Glenroy Avenue	67	66	66	-	-	-	-	-
67	Residential	Putty Hill Road (Inner Loop)	70	71	68	-	-	-	-	-

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**TABLE III-II
SUMMARY OF NOISE ANALYSIS (CONT.)
INTERCHANGE OPTIONS**

INTERCHANGE OPTION	NOISE SENSITIVE AREAS AFFECTED	USE	LOCATION	AMBIENT NOISE LEVEL	PREDICTED NOISE LEVEL NO-BUILD ALTERNATE	PREDICTED NOISE LEVEL BUILD ALTERNATE	CHANGE FROM BUILD ALTERNATE 2	NUMBER OF RESIDENCES PROTECTED	EFFECT ON ANY PROPOSED NOISE BARRIER; OTHER COMMENTS
Greenspring Avenue/MD Route 133	none								
I-83 (JFX) - C	none								
I-83 (HX)/Charles Street - A	38	Residential	Jeffers Circle Residences	63 (a)	69	69	-1	18	Insignificant effect on barrier
	39	Residential	Charles Street Residences (North)	66	69	69	-1	-	
	40	Residential	Marleigh Circle Residences (Riverwood Hills)	68	71	71	-1	45	
I-83 (HX)/Charles Street - D	38	Residential	Jeffers Circle Residences	63 (a)	69	70	0	18	None (c) None
	39	Residential	Charles Street Residences (North)	66	69	(f)	(f)	-	
	40	Residential	Marleigh Circle Residences (Riverwood Hills)	68	71	72	0	45	
MD 45 - B	none								
MD 146	11	Religious	Towson United Methodist Church (Hampton)	62(a)	69	72	+1	1 Church	Insignificant effect on barrier.
MD 41	none								
MD 147	none								
US 1	none								
MD 7/US 40/MD 702	none								

Notes :

(a) Measurement location is not the same as prediction location. Prediction location was changed to either (1) reflect the worst case noise level, or (2) represent an area of use within the noise sensitive area.

(b) Only 43 NSA's of 56 original sites are listed here. Those removed were outside the study area, not sensitive uses, or protected by existing conditions. The noise analysis performed for NSA's 57 through 67 were performed by subsequent studies and included as additional information.

(c) A barrier at this location would cross driveways, precluding access, and was therefore not considered.

(d) \$ 16 per square foot.

(e) This cost effectiveness measure also includes equivalent values as follows:
1 church = 5 residences; 1 school = 10 residences; every 125 feet along the improvement of park or area in active recreational use = 1 residence.

(f) Existing ramp also serves as local access road. The interchange option would move the ramp traffic further from the receptor and at a lower elevation. Although not estimated, noise impact at the receptor would likely be reduced.

(g) This barrier (1600 square feet) is triangular to account for terrain.

(h) Barrier height may vary with terrain and receiver elevation conditions.

(i) Included in analysis for NSA 55.

(j) Extension of barrier protecting NSA 1.

(k) These three receptors were analyzed for "Report on Noise Barrier Feasibility I-695: from Loch Raven Boulevard to Perring Parkway." The three sites were analyzed as a composite area.

(l) A continuous barrier for NSA 62 and NSA 62A was analyzed.

IV. PUBLIC HEARING COMMENTS

IV. PUBLIC HEARING COMMENTS

A combined Location/Design Public Hearing was held on February 28, 1990 at Loch Raven Senior High School in Towson, Maryland. The purpose of the Hearing was to present the results of the engineering and environmental studies completed for the Beltway and Interchange improvements. Following the presentation by the SHA Project Planning Team, 28 individuals made public statements and another 15 individuals gave individual testimonies.

The following is a summary of the public statements made during the combined Location/Design Public Hearing and the responses offered by SHA. A complete Hearing transcript is available for review in the Project Development Division Offices, State Highway Administration, 707 North Calvert Street, Baltimore Maryland 21202. Written comments received during or after the Hearing are discussed in the correspondence section of this document.

Okay, at this time I am going to go through the speaker's list and ask those people, in the order that they signed up to speak, to come forward. When I call your name, would you please come forward to the podium to my left with the microphone and clearly state your name, address, and affiliation, if any. Eugene Bandy? Yes, sir?

MR. BANDY:

I'm Gene Bandy. I represent--do you want the address--I'm sorry, you want name?

MR. OLSEN:

Just name, address, and affiliation for the record.

MR. BANDY:

Name, address. It's Gene Bandy. The address is 30 Tenbury Road, Lutherville, Maryland. And I'm with the Dulaney Valley Improvement Association, and I'd like to thank you very much for this opportunity to speak.

We support the Type II noise barriers, especially

Response:

The SHA adopted their Type II Noise Abatement Program in conjunction with Federal legislation, to provide relief from existing noise levels for residential areas and public institutions adjacent to existing major highways. To date the following Type II projects are either approved or constructed in the project area:

I-695: Perring Parkway to Harford Road (constructed)

I-695: Providence Road (EBR) (constructed)

I-695: Hampton/Concordia Drive (constructed)

I-695: York/Dulaney Valley Road

I-695/I-83: Thorton/Seminary Road

I-695: Charles Street/York Road

Based on the noise analysis study completed to date, the SHA has determined that Type I noise abatement measures are reasonable and feasible and will be considered during final design at 18 location.

1 the one between Dulaney Valley and York Road, and we would
2 like it to come before the widening and it would be money
3 that is well spent. Thank you.

4 MR. OLSEN:

5 Thank you. Les Graef?

6 MR. GRAEF:

7 Les Graef, Executive Director of the Towson
8 Development Corporation. We are a private, non-profit busi-
9 ness community organization, headquartered in Towson at 29 West
10 Susquehanna Avenue.

11 We also support the widening of the Beltway to
12 four (4) lanes. We think that the growth of the corridor,
13 with the population increase and the increased travel demand
14 do, in itself, demand that this widening occur to maintain
15 our quality of life in this corridor.

16 I do have some written comments here that I would
17 submit to you in greater detail but would, at this point,
18 mention only that we're concerned that the additional traffic
19 light to be placed on York Road north of the Beltway, with the
20 elimination of the ramp in the northwest quadrant causes us
21 some concern, and we would ask that the State Highway group
22 work with us to see what can be done to minimize that
23 conflict point.

Response:

1. Alternate 2 was selected to widen the Beltway to four-lanes in each direction.
2. MD45 Option B was selected. This will place an additional signal on MD45. Coordination is occurring with the Towson Development Corporation.

1 We're also interested in working with the State
2 Highway Administration, as a matter of protecting the capacity
3 of both York Road and Dulaney Valley Road to the Town Center
4 and would look forward to you having a priority designation
5 for study of those two (2) corridors so that even as the Belt-
6 way traffic increases, we will be able to have adequate handling
7 of traffic coming to the Town Center. Thank you.

8 MR. OLSEN:

9 Thank you, Mr. Graef. Mary Lipa?

10 MS. LIPA:

11 Gentlemen and Beltway neighbors. My name is Mary
12 Lipa. I live on Cromwood Road, which is between Perring Park-
13 way and Loch Raven Boulevard, and my affiliation is that I'm a
14 human being and I live in this area. According to the State
15 Highway Administration, one of their definitions of the
16 Beltway is that it's Baltimore County's main street. I
17 think, depending on whom you speak to, would depend on what
18 their conception of Main Street happens to be. I live in an
19 area where I can sit on my back porch and watch the traffic
20 on the Beltway. I'm wondering, where is this traffic coming
21 from in the morning and the afternoon? My perception is that
22 it's coming from Harford County down I-95 to 695, perhaps from
23 Route 24 or Mountain Road, which is Route 152. If this be the

Response:

1. An outer beltway was not considered as part of the project.
2. See the response on Noise Abatement on P. IV-1.

1 case, has there been a feasibility study to find out if a
2 road between I-95 and I-83 can be constructed as an outer
3 beltway? If this would be inappropriate, as the study may
4 indicate, how about having commuter buses purchased by the
5 State with Park-and-Rides put into the areas where the heavy
6 traffic flows, have one lane of the existing Beltway as a
7 bus/commuter lane when the necessity arises.

8 One of my main concerns, because of where I live,
9 is the fact that we have no sound barriers. I live six (6)
10 houses from the Beltway. We have been told, "Yes, you most
11 definitely are in an area where we may have a sound barrier."
12 May we have them? "No, we may not because when the study was
13 done, there was a mistake made." There is no funding for our
14 sound barriers and not only that, there is no guarantee that
15 when the construction is done, we will get them. Environ-
16 mental impact? Yes.

17 Are we going to have to be protected by OSHA and
18 have our children and our back yards wearing earphones and
19 filter masks without sound barriers? That's what it will
20 come to. When my child is in the back yard, I cannot hear
21 her. There have got to be other alternates. Perhaps widen-
22 ing the Beltway is the best way to do it. If that be the
23 case, after other studies are made, so be it. However, give

1 us our sound barriers. This is a necessity for us. We must
 2 have them. We are more than willing to cooperate, if this be
 3 the case.

4 By the way, the Joppa Road bridge is not mentioned
 5 in this study. Mr. Honeywell did not refer to the Joppa
 6 Road bridge in any of his explanations. There is nothing
 7 like lying in the bed in the morning and hearing an empty
 8 tractor/trailer going across the Beltway at 2:00 A.M. It
 9 really gets the old heart pumping, believe me.

10 If 2 is approved and the roadwork begins, let our
 11 area be where it begins. Put our barriers up first, and
 12 let's do it now. Put some of that \$135 million to work next
 13 year. This is just the way it happens to be, but what's going
 14 to happen in the year 2015, when the traffic is the same as
 15 it is now after the Beltway has been widened? Are we going
 16 to add another lane? Perhaps another? There are other
 17 alternatives.

18 State Highways, help us. Put up our barriers and
 19 do it now, and then begin to build to your heart's content on
 20 your side of the wall. Thank you.

21 MR. VEIT:

22 Ed Veit, 1732 Dunwoody Road, 21234. I am Project
 23 2-A. First of all and foremost, I am for Alternate 1, No-Build.

Response:

1. The No-Build alternate was not the selected alternate because it did not address the operational or safety concerns.

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1 As I read what you want to do with the Beltway, if
2 you want to take people and put them from Point A to Point C,
3 and relocate people at Point B, in between I would rather
4 see you relocate people in Point C and tell them to move
5 closer to work. I checked the Beltway on my way out, there
6 was no back-up. It only backed up about an hour a day, and if
7 the people would leave earlier for work, it would take care
8 of it.

9 I dare say we would have a different type of crowd
10 here if this were a people project, with as much money
11 involved. It would be screaming about what's going to happen
12 to my tax money, if it would involve health care, education,
13 facilities for the aged or homeless, drug program, teen preg-
14 nancy or teen suicide--this is just simply a highway. Spend
15 a \$135 million anyway you please.

16 I think you should tell us the cost flat-out
17 rather than than tell us where to look in our booklets.

18 Anyway, the other thing, if this were to be funded
19 by property taxes, I think you would have even a different
20 crowd here with an altogether different sentiment. I am
21 for No-Build. Thank you.

22 MR. OLSEN:

23 Thank you. Mindy Hanlon?

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1 MS. HANLON:

2 Hi, my name is Mindy Hanlon. I live at 1760 Dun-
3 woody Road in Parkville. Any my comments pertain to Alternate
4 2, as it affects 695 between Perring Parkway and Loch Raven
5 Boulevard, and also Alternate 2A which calls for realignment
6 horizontally and vertically of the Beltway in the same area.

7 In regard to Alternate 2, addition of a fourth
8 (4th) lane is needed. I watch the back-up from my home every
9 morning and again in the evening. I believe one of the lanes
10 should be designated a carpool commuter lane. This concept
11 has worked well in other cities and could work well here,
12 too. I hope the addition of a lane will not have to change
13 current right-of-ways. The disturbance caused by 24-hour a
14 day resurfacing a few years ago was horrible and since our
15 area was excluded from the noise abatement program begun in
16 1985, we have no means of relief from the disturbances this
17 project will create.

18 We have been told that walls in our area had to
19 wait for future funding. We were outraged when a wall
20 was placed past Hampton Mansion which was not on the original
21 plan. We have since learned, through reading the complaint
22 book available at the Loch Raven Library, that the Department
23 of the Interior requested the wall so that tourists could hear

Response:

1. Alternate 2 was the selected alternate. This selection does not preclude the future re-stripping of the facility for further capacity expansion such as HOV lanes.
2. See the response on Noise Abatement on P. IV-1.
3. Alternate 2A was not selected.

1 guides talk about the greenhouses and gardens on the mansion
2 grounds. Now, our outrage is compounded because we found
3 that the needs of a few tourists were placed far above the
4 needs of so many residents.

5 Mr. Honeywell has assured me that walls will be
6 placed at the time of lane construction. I request that the
7 wall construction precede the road construction so that we
8 may have the relief we so desperately need. I cannot carry
9 on a conversation anywhere in my yard without having to
10 shout.

11 We hope you will remedy our situation as soon as
12 possible.

13 In regard to Alternate 2A, if adopted, this plan
14 will completely disrupt my family's life as well as my
15 neighbors' lives. A ravine in the woods adjoining my property
16 will be back-filled, damaging fox and deer habitat as well
17 as the stream. The amount of dirt and noise pollution from
18 the heavy equipment moving up and down my road constantly
19 will be unacceptable. The potential danger to my children
20 is the forefront of my concern. Dump trucks, cranes, and
21 bulldozers look neat from afar. Having them on my street
22 would be a tremendous safety hazard.

23 Combine all this with a \$31 million price tag,

1/25

1 and you will agree Alternate 2A is unacceptable. I hope that
 2 the State can, instead, appropriate that amount for the educa-
 3 tion budget. Thank you.

4 MR. OLSEN:

5 Okay, thank you. Donna Spicer?

6 MS. SPICER:

7 Donna Spicer, 8719 Eddington Road. I live in the
 8 area between Loch Raven and Perring Parkway, but I think there
 9 are a lot of you that will agree with some of the things I
 10 have to say.

11 I do not believe the people affected by this
 12 widening have been and possibly will not be totally informed
 13 on the true consequences of this proposal. Letters to resi-
 14 dents announcing this meeting and an ad placed in the
 15 Evening Sun state "material summarizing study results, maps,
 16 and public comments will be available at listed locations."
 17 This material was either not available, difficult to view,
 18 or incomplete. After reviewing the material we could find,
 19 the general consensus that the State Highway Administration
 20 does not seriously consider the problems of the people living
 21 adjacent to 695.

22 While moving traffic efficiently and safely is
 23 important, no less important are the consequences the people

Response:

1. Alternative means of transportation were not part of this study.
2. See the response on Noise Abatement on P.IV-1..

12/10

residing

1 in the area will have to live with 24 hours a day. Periodic
2 widening only delays the time commuters will have to face
3 alternative means of transportation. Your widening proposal
4 will supposedly improve safety and movement only a limited
5 time, possibly until the year 2015. What after 2015?

6 The solution is not to keep widening roadways for
7 an estimated sixty percent (60%) increase in traffic. The
8 solution is to reduce the traffic. Public transportation
9 will only be accepted when alternative travel becomes more
10 convenient than using your own vehicle.

11 There is concern for air quality and noise levels
12 today. What of tomorrow, with traffic and worse conditions?
13 It seems the Highway Administration is more concerned with
14 moving vehicles through our area than the environment we have
15 to live in. Detrimental effects of noise on mental, emotional
16 physical health and quality of life are well documented.
17 Sound barriers are only capable of reducing the noise levels
18 5 to 10 decibels. If traffic increases as predicted, noise
19 levels behind those walls will again be unbearable. The
20 residents of my community left your 1985 October meeting
21 on this same topic, with false impressions that we would be
22 included in the sound abatement program by 1990. Not until
23 last Spring did we discover that we had been overlooked.

1 Only after much work and pleading were we recognized as highly
 2 impacted and feasible. Then, when asking for protection from
 3 the danger and harrassment we live with today, the answer
 4 we received--"No funds available." How does the Highway
 5 Administration justify finding funds to increase the traffic
 6 and worsen the problem?

7 Alternate 2A includes one (1) barrier, but you
 8 state is not cost-effective. Alternate 2 includes only the
 9 possibility of four (4) Type I barriers. Considering the area
 10 involved, if constructed as proposed and using all four (4)
 11 walls, again people will be left totally unprotected, not only
 12 from today's noise levels, but from planned and expected
 13 increases. Do we again hear empty promises? Will we again
 14 end up with no noise abatement? Will we again hear "No funds
 15 available"? Are we again to leave with false impressions?
 16 No-build until fully informed and we receive honest and
 17 complete answers to all our questions.

18 MR. OLSEN:

19 Okay, thank you. Robert Wilford?

20 MR. REUTER:

21 Good evening. My name is Bob Reuter. And I am
 22 Chairman of the Architectural and Transportation Barriers
 23 Committee of Baltimoreans Against Disability Discrimination.

Response:

1. Alternative means of transportation were not part of this study.
2. The selected alternate does not preclude the future re-stripping of the facility for further capacity expansion such as HOV lanes.

1 And Vice President of the Baltimore Area Transit Association,
2 the Citizens Non-Connected with the MTA or any other Govern-
3 ment Agency Citizens Group. I am also, by profession, a
4 professional transportation engineer and I have had the unique
5 experience in the past of having to sit at that table. I
6 know what they're up to. If you can't dazzle them with
7 your brilliance, baffle and confuse them with your bull-shit.

8 I heard here tonight claim that there was no impact
9 on disabled citizens by this highway. Since over seventy per-
10 cent (70%) of the disabled citizens of the Baltimore
11 Metropolitan Area do not drive, I cannot possibly imagine
12 how this will not impact on disabled citizens. It is the
13 draining of funds from the treasury to benefit a few, and
14 blatantly discriminate against seventy percent (70%) of the
15 disabled population of the Baltimore Metropolitan Area.

16 The MTA claims there are no funds available for
17 lifts for buses. Baltimore County claims there are no funds
18 for County Ride. No funds--but they find funds for people to
19 drive. He says there is a minority participation--dut-dut-
20 dut-dut--where's the minority participation for the disabled?
21 If you have your house taken by the State Roads Administration
22 and you have disabled-related improvements in your house,
23 they're not considered in replacement housing. You're on your

1 own. Same old you-know-what.

2 The Baltimore Metropolitan Area is interdicted to
3 reduce the air pollution in this area. Studies have shown,
4 repeatedly, that Baltimore exceeds the allowable standards for
5 hydrocarbons, for carbon monoxide, and other oxidate pollut-
6 ants. Yet, studies have repeatedly proven, time after time
7 after time, that highways operate on the X + 1 theory. X is
8 the number of lanes and capacity of the highway. There will
9 always be X + 1 number of vehicles trying to use them no
10 matter how big or how many highways you build.

11 Highways always exceed one hundred percent (100%)
12 of the design capacity, irregardless of their design capacity.
13 The bigger you build them, the more people try to get on them.
14 All you are doing is creating your own demand to build more
15 highways.

16 There is almost as much money in this little bit
17 of highway, one lane on the Beltway, part-way around as in the
18 entire Baltimore light rail improvement project, and the cost
19 per lane mile is higher. Does this say something about our
20 priorities? One line of light rail will carry the capacity
21 of sixteen (16) lanes of highway. That one light rail system
22 is going to exceed the Beltway in capacity by four (4) times
23 and cost less.

1 This particular project will take thirty-one (31)
2 acres of land from our available property tax base--the light
3 rail project will take two (2). Twenty-two acres of these are
4 woodlands, 0 on the light rail project. Seventy-six (76)
5 properties versus 0 for the light-rail project. Why is it
6 there is so much ripping, roaring hullabaloo and everything
7 else you want to call it over how bad the light-rail project
8 is and no complaints about the environment. I've head one
9 person so far talk about the damage to the environment. I've
10 heard about the wetlands, but not yet. I've heard all of
11 these things--not yet spoken. They've got a little report
12 that we can't even get. The MTA light rail project produced
13 four (4) inches of project environment effects report on the
14 light rail project and still there are complaints. Where
15 are those complaints on this environmental disaster?

16 Where will, in this project, be the bus lanes or
17 the carpool lanes which carry the equivalent? One bus
18 carries the equivalent of forty-six (46) automobiles.
19 If you took one lane off the existing highway and made it
20 an exclusive bus lane, you would exceed the capacity improve-
21 ments of the entire project.

22 Where is the room on here for the in-the-planning
23 stage light rail line from the current north/south line to

1991

1 downtown Towson? I thought Les Graef would talk about that
2 from Towson Improvements; he did not, and I don't understand
3 why because we have had a lot of meetings with him.

4 In any case, where is the access for downtown
5 Towson to public transit? They're going to build a large
6 transit center in downtown Towson and no way to get to it.
7 Think ahead. Be sure and think ahead. That way you will be
8 able to go back later and tear it all down and build it
9 again.

10 There is no cross-county transit alternative
11 included, and there is no transportation system management
12 alternative improvement which is required in every public
13 transit improvement hearing. You have to have a TSM, a
14 Transportation Systems Management. None here. This project
15 cost would buy--the MTA opened a bid on new buses a week ago.
16 Using the prices from that bid opening, the cost of these
17 lane widening improvements would purchase nine hundred and
18 ninety-one (991) MTA buses. That would certainly move a
19 heck of a lot of cars and a lot of people, yet move the cars
20 off the highway and the people where they have to go.

21 State Highway Administration? Yes, do something
22 constructive. Give the money to the MTA.
23

1 MR. OLSEN:

2 Thank you, Mr. Reuter. Mr. Wilford?

3 MR. WILFORD:

4 My name is Robert Wilford. I live at 8729 Emge
5 Road. There is no affiliation. However, I believe I speak
6 for a group of neighbors on Lackawanna Avenue, Wayne Avenue,
7 and Emge Road, and we are concerned about that portion of the
8 proposed project that would include the area between Loch
9 Raven Boulevard and Perring Parkway.

10 Many of the comments that I had prepared for
11 tonight, after I spoke with Mr. Honeywell last week, and with
12 the representative from Mr. Gisriel's office who will speak
13 later, a lot of my comments will be yielded to he, and I think
14 we concur with his opinions.

15 However, a couple of points have arisen tonight,
16 now, of which I have growing concern, and one is the under-
17 standing of the status of the study for the Type II barriers
18 for that area along 695 between Loch Raven and Perring Park-
19 way. When you said earlier, and most appropriately, that many
20 of the specific questions would be answered at the maps,
21 several of my neighbors prior to this meeting at that map,
22 got information that directly contradicts some of the corre-
23 spondence we have in writing from your office. That concerns

Response:

1. See the Noise Abatement response on P. IV-1.

6/21

1 me as to the status of that project.

2 And speaking for the record, of course, provided
3 there is no Type II barrier in place when you begin this
4 construction, I would like to see some assurances that the sound
5 barriers will be constructed in conjunction with the improve-
6 ments.

7 The entire environmental impact portion, in the
8 meetings that I've been involved in since last June with your
9 department, we keep coming back to noise abatement and treat-
10 ing it as a nuisance issue. And, I, for the record, want to
11 stress that I believe it is also a structural issue. I live
12 close enough to the Beltway now that the traffic vibrations
13 are causing dishes to vibrate off my kitchen sink, and we
14 have--the three years we've lived in that house--have had to
15 patch the walls twice.

16 After my conversation with Mr. Honeywell last
17 week, where he explained to me some of the studies that have
18 been done relative to vibration damage in projects of this type,
19 I had the opportunity to request from the State of Illinois
20 information that contradicts or may supplement some of your
21 own studies. I ask that that be, in my possession tonight,
22 so I could make that available to you. It is not, and as soon
23 as I get it, I will give it to you.

1 But I want to underscore to the State Department
2 of Transportation that we shouldn't treat noise abatement
3 strictly as a nuisance issue--that I can't be the only property
4 suffering serious structural damage, and now you're suggesting
5 that you're going to bring that vibration and all of that
6 traffic even closer to my back door. So, when you consider
7 the sound wall, whether you do it in Type II when funding
8 becomes available, or in Type I with this project, for
9 heaven's sake do it. Thank you.

10 MR. OLSEN:

11 Thank you, Mr. Wilford. Norris Lankford?

12 MR. LANKFORD:

13 My name is Norris Lankford, with home residence at
14 2310 West Joppa Road. I speak as a citizen. Members of the
15 Project Planning Team, Maryland citizens--I wish to provide
16 some observations which follow my review of your Baltimore
17 Beltway study contract number B 635-101-472.

18 In general, it appears that considerable increase
19 in handling capacity for Beltway traffic will result from
20 your Beltway widening Alternative 2. And, additionally,
21 safety should be enhanced by the ramp and interchange modi-
22 fications. The most significant other features of the
23 proposal are the minimal additional right-of-way requirements

Response:

1. Additional capacity by double decking the Beltway was not part of this study.

1 and negligible effects on existing improvements and the
2 environment with the exception of noise.

3 On page 6 of the report, it is predicted that the
4 proposed improvements will provide significant enhancement
5 until around the year 2015, when the east/west traffic at
6 peak may equal or exceed present conditions. While support-
7 ing the alternative 2 proposal, and believing that the
8 anticipated traffic increase will occur, I wish to offer
9 supplementary ideas to the concerns providing future capacity
10 for handling east/west traffic. This improvement, after all,
11 is designed to handle the east/west traffic portions on the
12 northern section of the Beltway.

13 Now, these concerns are expressed in this project
14 report at the top right of page 6, at the end of the section
15 on Alternative 2. The focus of concern is the statement that
16 in most areas it is not feasible to consider mainline sections
17 greater than four (4) continuous lanes in each direction.
18 I propose, first, that we can provide protection for our land
19 more easily in Maryland, a small state, than California does.
20 The State of California is not only the third largest in the
21 Unon, but has fifteen (15) times more land area of Maryland.
22 In spite of high earthquake potential, recent large earth-
23 quakes and the warnings of engineers, California State Highways

1 have built and plan to rebuild double-decker highways to save
2 land for other purposes. Cost of rebuilding after earth-
3 quakes and even safety are disregarded.

4 It is my proposal that, for the future, the option
5 of partial sections of flyover or double-decking for east/west
6 Beltway be considered and retained in consideration. In
7 Maryland we have zero (0) earthquake risk as well as mild
8 winter weather. Needs of land for housing, agriculture, and
9 other uses are too great to waste space for inappropriate
10 taking of significant right-of-ways for highways. I do not
11 believe this program has a significant acquisition, but
12 you're indicating in the future there would be no other
13 alternative than significant acquisition

14 I wish to have included in Final Design specifica-
15 tions of this Alternate 2, the future double-deck considera-
16 tions. It may be, however, that future east/west traffic
17 needs, suburb-to-suburb and suburb-to-industrial park will
18 be required further north in the form of a sector of an outer
19 beltway. For such a need for future east/west highway
20 transport, a new northern outer beltway sector could be made
21 in two decks to minimize new land acquisition. Whether one
22 (1) deck or two (2), such a northern east/west highway would
23 also mitigate undesirable build levels of service anticipated

1 for this design year peak hours.

2 I wish to commend the planners of this study report
3 on the details of the Alternative 2 proposal which should
4 cover traffic needs for all but the long-term concerns
5 addressed on page 6 and herein. Thank you.

6 MR. OLSEN:

7 Okay, thank you, Mr. Lankford. Vivian Kasper?

8 MS. KASPER:

9 My name is Vivian Kasper. I live at 18 Allenbrook
10 Court which is part of Dulaney Towers. Hopefully I represent
11 some three hundred (300)-odd residents at Dulaney Towers.

12 We have a history much like what I've been hearing
13 from the other folks. Documented in 1985, that we would get
14 noise barriers to be built in the fiscal year '87. We haven't
15 seen anything but we have been told subsequent to that, the
16 State has changed its criteria. We no longer qualify, but
17 we are fortunate, we can get them if we pay half the price,
18 at five hundred dollars (\$500.00) a running foot, we pay half,
19 we can get them.

20 We're a community, in some cases, older folks.

21 We don't have that kind of money that we would pay. We would
22 be unique, then, we would be the only ones in the Baltimore
23 Metropolitan Area that could pay for the privilege of getting

Response:

1. See the response on Noise Abatement on P. IV-1.

1 them. Other ones, if they get them, won't have to pay for
2 them.

3 We just recently went through a resurfacing phase
4 of the Beltway which was promised for 1986, and many, many,
5 many long nights between 11:00 and 3 o'clock in the morning,
6 I shudder to think what's going to be happening when they're
7 going to start doing this--much of it is done in the late
8 night hours when most of us are trying to sleep. It would
9 seem that there should be a restriction and it's interesting,
10 while we're sitting here tonight, I read this little thing
11 in this pamphlet that is out there, indicating that the
12 Federal Highway Administration requires that if there is a
13 noise problem, that the solution from the State agency be
14 practical, reasonable, and accepted to the public. We don't
15 accept it.

16 MR. OLSEN:

17 Thank you. Hugh Meade?

18 MR. MEADE:

19 My name is Hugh Meade. I live at 2204 Fox Hunt
20 Lane, a community called Heatherfield. I'm here this evening
21 representing myself and other neighbors. We're particularly
22 concerned about widening of the Beltway in Alternate 2,
23 between the two (2) I-83s. Right now, I believe it's five

Response:

1. Alternate 1, the No-Build, was not the selected alternate because it did not meet the operational or safety needs.
2. See the response on Noise Abatement on P.IV-1.

1 (5) lanes and it would be expanded to six (6) lanes. I guess
2 what our main concern is, as I've heard expressed here tonight
3 it's the environmental impact, the noise, primarily that we
4 have now. We would expect additional noise by expansion of
5 the Beltway to the extent it is proposed in Alternate 2.

6 So what we'd like to say, and I'd like to say for
7 those of my neighbors and myself, that we would support
8 Alternate 1, which is No Build, unless we could be assured
9 we would get and we qualify for sound barriers to help
10 abate the noise that is now bad, but which we would expect
11 to increase and get much worse.

12 We did ask earlier for sound barriers. We were
13 told that we qualified on the basis of the construction, in
14 terms that we were there before the Beltway; number two--that
15 the noise decibel level was such that also we qualify, but
16 because there were so few of us affected, that was the third
17 criteria that we did admit under the current retrofit program
18 so we're hoping if Alternate 2 is decided upon, at that
19 stretch of the Beltway, that we, in turn, would get some
20 assurances we'd have another chance to press our case for
21 environmental barriers in terms of noise abatement barriers
22 at that particular stretch. That's all I have to say--thank
23 you.

MR. OLSEN:

1 Okay, thank you. James McManus?

2 MR. McMANUS:

3 Good evening. My name is Jim McManus. I live
4 at 10 Lochmoor Court in a neighborhood called Seminary Ridge.
5 It's between the two (2) 83s. My back yard is on the outer
6 loop of the Beltway. In fact, my back yard is practically
7 the Beltway. I've lived in the house for about thirteen (13)
8 years. I'm quite pleased with the neighborhood and the
9 surrounding area. The only problem is the noise level has
10 increased over the last thirteen (13) years and I really
11 want to address what I feel is sort of a reverse discrimina-
12 tion on the people in my neighborhood and surrounding areas.
13 Evidently our lots are too large and the value of our homes
14 is too high to qualify for the noise abatement barriers. And
15 I think this is a form of discrimination. The State of Mary-
16 land has no problem in increasing the assessed valuation of
17 my property over the next three (3) years to where it's just
18 about doubled to what it is now, but yet we're being deprived
19 the noise abatement barriers. I think, I'm not denying these
20 barriers to any other neighborhood, but it seems like if I
21 lived in a more densely populated neighborhood and had a
22 less expensive home, and paid less taxes, I would be eligible
23 for these barriers. So there does seem to be a form of

Response:

1. See the response on Noise Abatement on P.IV-1.

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1 prejudice here, and I think we should address this.

2 I certainly feel that my home deserves these
3 barriers. I knew the Beltway was there when I bought the home,
4 but I didn't know it was going to grow to this tremendous
5 capacity. I didn't know that the registered automobiles and
6 the County, Baltimore County, would double in the '80s, so I
7 think this would certainly create some money in the general
8 transportation fund that could be used for these barriers.
9 If we have twice as many cars in Baltimore County as we did
10 in the '70s, where is this money going? Sure, it's going to
11 build the beltways, but why don't we build the sound barriers
12 before we build the Beltway any wider? Thank you.

13 MR. OLSEN:

14 Thank you. Reese Luger?

15 MR. LUGER:

16 My name is Stephen Luger, and I live at 20 Marcie
17 Woods Court, in a subdivision called Marcie Woods, which is
18 in the Stevenson area.

19 We are very concerned in the neighborhood. We
20 presented a petition to the Maryland Department of Transporta-
21 tion for sound barriers. And we echo the same sentiments that
22 many of the other people in the area have voiced. It is
23 interesting when the decibel levels were measured in our

Response:

1. See the response on Noise Abatement on P. IV-1.

1 neighborhoods that they were using maps where our neighborhood
2 is much closer to the Beltway than the decibel readings that
3 were taken at the Beltway on Area #29.

4 Second of all, we're very concerned that we have
5 the same problems as everyone else did with the resurfacing,
6 at all hours of the night with tremendous noise levels. We
7 ask here, again, that before any more construction takes place,
8 that we request that the sound barriers be put up before any
9 more construction goes. I don't think it's fair that we have
10 to pay interstate--we don't have to take the brunt of all the
11 construction for people to shuttle both interstate and from
12 suburban areas to downtown areas and be put on the back burner.
13 We have been told that we do not qualify for sound barriers
14 despite the fact that decibel levels, which are further away
15 from the Beltway, put us into a higher, a risky area which has
16 already been designated, and we feel that the whole planning
17 purpose of the highway system should take into effect the
18 sound abatement program as an integral part of the whole system,
19 not separately.

20 We have also noticed that into our neighborhood have
21 been coming people who break down^{on} the Beltway, they climb the
22 fences, come into the areas at two o'clock, three o'clock, and
23 four o'clock in the morning, people are coming into our

1 neighborhood requesting help. Well, you just don't know who
 2 these people are coming into the neighborhoods, and we think
 3 there is a safety issue also, that these walls would give us
 4 an extra measure of safety.

5 The main thing that--we're not opposed--our neigh-
 6 borhood is not opposed at all to the widening of the Beltway
 7 if we have the sound barriers, but if we don't have the sound
 8 barriers, we would say we don't want the widening of the
 9 Beltway which will actually almost increase decibel levels by
 10 about fifty percent (50%). Thank you.

11 MR. OLSEN:

12 Thank you. Harvey Greenbaum?

13 UNIDENTIFIED SPEAKER:

14 They left.

15 MR. OLSEN:

16 Okay, thank you. Gill Abramson? Jeff Klose?

17 MR. KLOSE:

18 My name is Jeff Klose. I live between Dulaney
 19 Valley Road and York Road. Most of my comments have already
 20 been covered. The main thing I have to say is--I don't
 21 totally disagree with the widening of the Beltway, but what
 22 I do want is for the noise abatement walls to go up first.
 23 I think that should be the priority. Here we're talking about

Response:

1. See the response on Noise Abatement on P. IV-1.

1 spending \$135 million for road improvement but does that
2 include the noise barriers? I think we're juggling what are
3 our priorities. We start one project--we should finish that
4 project. And the noise abatement walls should go up anywhere,
5 anywhere along that Beltway. I had to put central air in my
6 house. There is no way that I can sleep at night with my
7 windows open. I'm sure everybody that lives near the Beltway
8 experiences the same thing. I'm sure the State or Federal
9 Government doesn't want to absorb the price of my central air,
10 but I had to do it because of that reason, and that's not being
11 smart.

12 I agree with the light rail system because that
13 would remove some of the cars from the Beltway. We could
14 widen that Beltway to fifty (50) lanes each way, and it would
15 not reduce the noise, it would not reduce a lot of the safety
16 hazards on it. So, the main thing right now, I believe, is
17 to put the walls up and then consider widening it. And, put
18 those walls up with the idea of--you can widen it if you want.

19 I called Mr. Honeywell and he informed me that the
20 Beltway is going to be widened in many places if this plan goes
21 through, but some of the existing walls would have to be moved.
22 Now, that is crazy. That's a waste of our money and I don't
23 agree with that at all. Thank you.

1/4/58

1 MR. OLSEN:

2 Okay, thank you. George Arconti?

3 MR. ARCONTI:

4 Good evening. George Arconti from 12 Barrow Court
5 in the Lutherville/Riderwood section of Baltimore County. My
6 property is immediately adjacent to the Beltway. I have some
7 six hundred feet (600') of property that is immediately adja-
8 cent to the Beltway right at the I-83/695 interchange. Since
9 I've been living there, I have had to suffer through the same
10 thing the rest of these people have had to suffer through,
11 which is the noise and the dust, and everything else that goes
12 along with it. I can make a long, eloquent speech like many
13 of you have.

14 Simply--Gentlemen, are you listening? All these
15 people are here about the sound barrier. Pay attention.
16 Thank you.

17 MR. OLSEN:

18 Thank you. Mani Pulimood?

19 Mr. PULIMOOD:

20 Good afternoon. My name is Mani Pulimood. I live
21 at 8 Johnson Mill Road, Baltimore, Maryland. I live in the
22 Ruxton Hill community, which is near the proposed I-83 or
23 Jones Falls, and Maryland 25, Falls Road interchange.

Response:

1. See the response on Noise Abatement on P. IV-1.

Response:

1. I-83 (JFX) Option B was not a selected alternate.

1 I am here to testify against the proposed inter-
2 change at this location. I have written out comments here.
3 I will leave that with you for this turn.

4 I am a registered professional engineer practicing
5 in Maryland for nearly twenty (20) years, and one of my areas
6 of expertise is in environmental engineering and noise control.
7 During the course of my daily commute, you know, on my way to
8 work, I use this intersection to access 695 west, through the
9 existing access ramp from 83, that is, Jones Falls North. I
10 would also like to go on record to state that I cannot recall
11 any instances where there was a back-up due to natural
12 traffic congestion at this location, other than times during
13 road construction at this intersection.

14 The only traffic problem I encounter near this
15 intersection of the Beltway is the natural slowing down of the
16 traffic on the outer Beltway traffic due to the steep upgrade
17 between Exit 23 and 22. In my opinion, the proposed Options
18 A or B at this intersection would not appreciably correct
19 these inherent bottlenecks of the Beltway in this area.

20 I also heard today in the environmental impact
21 study statements made stating that, you know, these proposed
22 changes would affect Jones Falls watershed and also the
23 wetlands in this area. And I was also told that every step

1 will be made to conform. I don't know what you will be
2 conforming to, because you will be breaking to start off with.

3 I am particularly opposed to the proposed Option
4 B which utilizes a high-speed, two (2)-lane flyover ramp from
5 northbound Jones Falls to westbound Beltway, due to its
6 environmental noise impact on the surrounding communities. By
7 elevating a portion of the highway ramp, the natural noise
8 shielding of the existing portion of the highway due to the
9 natural embankment effect and the vegetation will be lost.
10 It is my estimate that the increase in the ambient noise due
11 to this, will be in the order of about six (6) to eight (8)
12 DBA, or more than doubling the noise-intensity level or the
13 perceived noise level from this adjacent community will more
14 than double.

15 I know my community has, in the past, approached
16 the State Highway Department and, you know, they did a study
17 near Old Court Road and I was told that there won't be any
18 funding because the noise level is not that very much at this
19 point in time. What I'm afraid is, if the Option B is to go
20 through and this flyover ramp came about and if the noise
21 impact is very high, and you come back and you come back and
22 add a wall, there is no way you can add a wall, you know, with
23 a flyover lane which is about fifteen hundred feet (1,500')--I

1 don't know how many feet it will be designed to. So we will
2 all be stuck with a very, very high noise problem, and it is
3 my humble opinion that you should drop this option altogether.

4 Thank you very much.

5 MR. OLSEN:

6 Kathy Senior? Janet Bowlander? Nader Gary?

7 MR. GARY:

8 Good evening, ladies and gentlemen. My name is
9 Nader Gary. My address is 2 Roland Court, Ruxton Hill, 21204.

10 We did not hear anything about the cost of this
11 construction. If I'm not mistaken, somewhere along the line
12 it was \$135 million. This is the current price. What would
13 be the real price when it's finished? Maybe it would be
14 better to build a second beltway by 2015 when there will be
15 no opposition and there will be no hearing, I assume, but I
16 have to read what I have prepared.

17 Extension of I-83 north, adding another lane to
18 695 or I-83, building a flyover ramp connecting 83 North to
19 695 South, will not improve the traffic jams in peak hours.
20 Instead, it will have an adverse effect on property values in
21 Ruxton and surrounding area communities, leading to less
22 taxes on these homes, increasing noise, and increased air
23 pollution. The logical solution to the congestion is to improve
the

Response:

1. This study included the interchanging roadways to the first major intersection away from the Beltway to help in circulation.

1 collaterals rather than increase the size of major arteries.
2 If major arteries and collaterals cannot drain well, there will
3 be a congestion in the main artery and arteries. The follow-
4 ing factors play a major role: (1) Exits are not able to
5 drain at 695. (2) Too many stops or traffic lights immedi-
6 ately after exits from 695 exist. (3) Town or city streets
7 are not able to handle the load of cars coming out of major
8 arteries.

9 Example of #1 factor, near York exit from 695 is
10 totally inadequate. It creates a back-up for five (5) or six
11 (6) miles every day between 4:30 and 6:00 P.M. Following
12 exit from 695 toward New York, there is another congestion on
13 95 itself. By adding another lane to 695, this problem will
14 not be solved.

15 Example #2--on York Road, Charles Street, Kenil-
16 worth Avenue, traffic lights and a stop sign slow the traffic
17 down.

18 Example for #3 factor--Traffic lights, local stops
19 and right turns into local businesses such as gas stations,
20 McDonald's, etc., will slow down the traffic.

21 In summary, adding to major arteries, especially
22 on 83-North and South to 695 will not solve the problem.
23 There may be--suggestions are: good exits, good collaterals,

1 good drains, possible transit officers to improve and guide
2 the traffic in peak hours. #5--prohibit commercial vehicles
3 to commute during these peak hours. #6--to increase the
4 number of lanes to major arteries--No increase, I'm sorry,
5 no increase in numbers of lanes.

6 I strongly oppose the proposal to build new roads,
7 especially at the junction of 83 and 695.

8 MR. OLSEN:

9 Okay, thank you. Dr. Burns?

10 DR. BURNS:

11 Good evening. Can you hear me? Are you awake?

12 This is a very boring evening, to me. Lots of talk. I'm here
13 because I live at 502 Hampton Lane. My name is Dr. Howard H.
14 Burns.

15 If you get off the Beltway at Dulaney Valley Road,
16 you have three (3) choices. You can go up Dulaney Valley, you
17 can go down Hampton Lane, or you can go into my driveway.
18 About thirty (30) years ago, they built the wall and a curb
19 and the sewerage and said, "Now, Dr. Burns, we will not take
20 any more of your land." They lie. They now want eight feet
21 (8') in the front of my property, and they want eight feet
22 (8') in the back of my property on Dulaney Valley Road. What
23 they pay me, I'm not interested. What I'm interested in is

1 my beautiful trees. When you get off the Beltway, to Dulaney
 2 Valley Road, you will see my beautiful trees, my forsythia
 3 (two varieties), and my bridal wreath, which is truly beauti-
 4 ful, in my opinion. In the back I have my pine trees. I
 5 love my place. I'm eighty-one (81) years old. I don't want
 6 to lose any more land. As long as I have been here, when the
 7 Beltway was not, Hampton Lane was adjacent to the Goucher
 8 property. There was no Notre Dame Academy. Ten (10) or fifteen
 9 (15) cars went up the lane every day. Today it's a speedway.
 10 My place--sometimes a garbage heap--I hate it.

11 I have yet to see or hear or have access to any
 12 engineer from this department or any other department who came
 13 to my house and said, "Dr. Burns, do you have any suggestion
 14 what we can do with Hampton Lane and Dulaney Valley Road?"
 15 Maybe I do, but they will never ask me, they will confiscate
 16 my land, and I'm sure they'll do it whether I like it or not.
 17 And I think it is absurd and unforgivable. Thank you.

18 MR. OLSEN:

19 Thank you, Dr. Burns. Malcolm Barlow?

20 MR. BARLOW:

21 I'm Malcom Barlow. I live on Robern Avenue. I'm
 22 not sure if that's in South Carney or North Parkville, but it's
 23 not on your map. You go back to the drawing boards.

Response:

1. MD 147 Option B, widening MD 147 to five lanes, was the selected alternate. This alternate was selected to help operational and safety problems along MD 147.

1 I am president and currently the sole member of
2 the Association to Abolish the State Highway Administration.
3 He's smiling over there. You know, some Olsens throw baseballs
4 and some throw the bull. You figure out which is which.

5 I want to specifically talk about the Harford Road
6 project. And, to begin with, I think you've got your
7 geography mixed up. You're talking about north of Putty Hill
8 widening, not south, are you not? What streets are affected
9 by your widening of Harford Road?

10 MR. OLSEN:

11 It's on Harford Road between Putty Hill and Second
12 Avenue, which is north of the Beltway.

13 MR. BARLOW:

14 That's north.

15 MR. OLSEN:

16 Yes, from Putty Hill.

17 MR. BARLOW:

18 South of Putty Hill is toward Baltimore City to
19 Parkville, where the church is.

20 MR. OLSEN:

21 The intersection of Harford and Putty Hill south
22 of the Beltway, and we would go from there north of the
23 Beltway to Second Avenue.

1 MR. BARLOW:

2 North of Putty Hill is toward the Beltway. South
3 of Putty Hill is toward Parkville. So I think you've got your
4 directions mixed up.

5 At any rate, your one proposal to widen Harford
6 Road would be a horrible thing to do. Now, I lived there
7 before the Beltway, so I've been around a little bit, and
8 right now, if you try to get out of any of the side streets,
9 you're taking your life in your hands. They come off that
10 Beltway and down Harford Road--I would say that ninety percent
11 (90%) of the people are exceeding speed limits, seventy-five
12 percent (75%) are exceeded by ten (10) miles per hour or more.
13 We've asked for lights, and the answer is, "We've got to move
14 traffic." You've got lights on Belair Road--four (4) of them,
15 as a matter of fact, between Putty Hill and the Beltway.
16 You've got intersections I'm familiar with on Harford Road,
17 but that's not what I want to talk about.

18 Now, you widen the Beltway, you are increasing the
19 speedway, number one. The second thing you're doing, right
20 now people on Grendon Lane and Harford Road, you have a little
21 bit of a chance of getting out on the highway because when the
22 light changes on Joppa Road, it kind of breaks it up a little
23 bit. You can get out on the road before those kinks coming

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1 off the Beltway get their speed up to fifty (50) miles an
2 hour. But if you move us up to Edgewood, number one--you've
3 got three (3) streets sending cars out where you currently only
4 have one (1) street sending two to one/one. In other words,
5 if you have two (2) cars at each street, one gets out and
6 another gets out. You put them all on one street, you've
7 got a back-up of four (4) cars to get out. You try to get off--
8 I don't know how many people here that live around there or
9 ride a bus. I don't. But I know if you try to get off,
10 cross Harford Road, when you get off the bus at night, you'll
11 never make it. You have to get off at Putty Hill and walk
12 four (4) or five (5) blocks to your street, and then across.
13 Or go up to the Texaco station and try to cross there and,
14 again, you're taking your life in your own hands. So, widening
15 it would make it worse.

16 What bothers me is it doesn't matter apparently to
17 our politicians, about the fact that people on Joppa Road,
18 Harford Road, Belair Road, Loch Raven, whatever--it doesn't
19 matter if they've lived there for thirty (30) or thirty-five
20 (35) years and pays taxes. To hell with them. We've got to
21 widen the road to move these people, these newcomers, out of
22 Harford County downstairs. The hell with you people. This
23 is the big problem. And I'll tell you right now, I think come

55

1 election, some people ought to be moved.

2 Now, as far as the widening, don't blame these
3 fellows, really, it's not them, you know, really. It's higher
4 than them. Widening the Beltway, that's not going to solve
5 anything. As I told you, I lived there before the Beltway,
6 and I can tell you now, they built the Beltway "to relieve the
7 traffic on Joppa Road and Putty Hill," and I can guarantee you
8 that there's at least ten (10) times as much traffic now on
9 those two (2) roads as it was before you built the Beltway.
10 All you're doing is creating a service road for developers--
11 the more damned roads you build, the more traffic you generate.
12 You widen 83, you're going to throw more traffic onto the Belt-
13 way. If you want to solve the problem, extend I-70 across
14 Baltimore City where the hell it was supposed to go. That
15 will take traffic off of the Beltway. And why ^{didn't} they go
16 down there? Because your sweet Governor who was Mayor of
17 Baltimore at the time, blocked it. He blocked it because a
18 few people complained about it going through Leakin Park.
19 Now, you're about as safe walking through Leakin Park as you
20 are in the woods of Nicaragua.

21 And the houses--I know, I used to live across from
22 Leakin Park years ago, thirty-five (35) years ago, I was a kid
23 before I moved to the Parkville area. And the houses that

1 would be torn down, you'd have an urban renewal project out
 2 of it, but the Governor gave in and stopped that highway. It
 3 ends in a field out there by Woodlawn.

4 Now, damn it, if you built that road, through Balti-
 5 more City, you would take off twenty-five percent (25%) of the
 6 truck traffic on that Beltway, I'll guarantee you, and I think
 7 Dick Trainor ought to drink a couple of strong scotches and
 8 go down to the Governor, get his nerve up and go down to the
 9 Governor, and say, "Donnie boy, this is what we've got to do,"
 10 because that's what you're going to have to do to stop it.

11 Thank you.

12 MR. OLSEN:

13 Emory Gross?

14 MR. GROSS:

15 Good evening, my name is Emory Gross. I live at
 16 6317 Holly Lane. I'm here representing Delegate Gisriel. For
 17 you from Highways, that means we represent the area from Exits
 18 25 over to Harford Road, Exit 31. We have a few communities
 19 which have been greatly impacted by the noise of the Beltway.
 20 We're in support of your study. We're in support of you
 21 making the expansions that are necessary on the Beltway, but
 22 we think you must put the noise barriers up first. They've
 23 got to go in before you do the expansion. We've had some help

Response:

1. See the response on Noise Abatement on P. IV-1.

1 from Hal Kassoff along these lines. We would like to make sure
2 that you include in your study the Type II noise barriers.
3 There are several communities in particular tha we're con-
4 cerned with. Starting on the west side is the Orchard Hills
5 community, which is impacted; coming east we have the Dulaney
6 Village people in the north; we have Towson Towers on the
7 south; and the most agregious problem we have in the community
8 is over in the Oakleigh Community Association. This is a
9 community which many members have talked to you earlier. They
10 were impacted when the Beltway first came in because of an
11 oversight prior to your administration. They were missed when
12 it came to putting the noise barriers in, and we really would
13 like to see you take some action on that. Thank you very
14 much.

15 MR. OLSEN:

16 Thank you, Mr. Gross. Dr. Margaret Fine?

17 DR. FINE:

18 Hello, I'm Dr. Fine, and I live at Greenspring
19 Avenue on the Beltway, just about on the Beltway. I've given
20 four (4) pages before to the Court Reporter and, on top of
21 that, I'd like to add about the wonderful trees and the
22 beautiful things that are being destroyed by the pollution,
23 by the fumes from the Beltway; trees that are over two hundred

Response:

1. See the response on Noise Abatement on P. IV-1.
2. The selected alternate includes intersection improvements at Greespring Avenue to provide operational and safety improvements.

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1 (200) years old that I've seen the difference since I've
2 moved there, since I had the house built in 1950, for fifteen
3 (15) years to 1965, before the Beltway began, it was just
4 beautiful. Now the foxes are gone, the bluebirds are gone.
5 The deer seldom come around, and all because of the Beltway.

6 The barriers would help; they wouldn't take away
7 the whole thing, but I see now that the sludge and the other
8 things from the riprap that were put in as a rain drain that
9 comes down Slaughter Road or Slaughter Stream, has polluted
10 the whole area there. I did bring along some water, but,
11 unfortunately, I left it in the car. You can't even see
12 through it any more, it's just destroyed so much of the beauti-
13 ful area there at Greenspring. I have over several thousand
14 feet that go onto the Beltway and I notice that they've been
15 sprayed with some kind of chemical; the vines and the trees
16 there are affected and you tell me that they aren't. When I
17 see what it has done to your old galvanized posts, I know
18 that it's done more than that to the animals and to the
19 environment around there.

20 And I'm definitely against any kind of building
21 there. Thank you.

22 MR. OLSEN:

23 Okay, thank you. That completes the list of

1 regitered speakers. Is there anyone else who would like to
 2 speak at this time? Yes, sir? Please clearly state your name,
 3 address, and affiliation, if any.

4 MR. HOGARTH:

5 Okay, I'd like to know, Mr. Speaker, why is it in
 6 the morning, when we hear the traffic reports, when people are
 7 going across 695, why is it that if a person is stopped on the
 8 road, why is it that traffic is so backed up? I have to work
 9 for an auto parts store, and I go to five (5) locations. I'm
 10 in the middle of the five (5) stores. I go all the way out
 11 to Woodmoor and I go all the way down to Essex. This roadway,
 12 if you build it, I would appreciate it to go up. I saw in the
 13 Greenspring Avenue area, you're going to have a truck lane.
 14 Down in the southwest portion of Baltimore, on 695, there is
 15 a truck lane. That would help eliminate some of the traffic
 16 backed-up in the morning to alleviate the problem, but I'd like
 17 to say, help out the people, do build the walls, but if there
 18 is some way, tell the people at the traffic report, if there's
 19 an accident, don't let the people go and look at the side of
 20 the road. That's what causes the back-ups in the morning.
 21 That's why we have to stay in back-ups for twenty-five (25)
 22 minutes waiting to get from one side of town to the other
 23 because of the back-up. Thank you very much.

Response:

1. See the response on Noise Abatement on P. IV-1.

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1 MR. OLSEN:

2 Thank you. Anyone else who would like to make any
3 comment? Yes, sir?

4 MR. HOGARTH:

5 My name is Frank Hogarth.

6 MR. OLSEN:

7 Please state your name, address, and affiliation.

8 MR. MARCHANTI:

9 My name is Tony Marchanti, and I live at 3021 East
10 Avenue which is off of 147, right behind Doug Griffith, and
11 one of the problems we have is definitely the noise, because
12 it's getting out of control. But, the other problem is where
13 the Beltway was built, the box culverts are all deteriorated
14 now and the maintenance is not quite up-to-par. The area
15 back behind there suffers from bad soil erosion which I put
16 30 tons of rock in myself, and then the State finally did come
17 in and let me tear the fence down, and they decided to put
18 some rock in, too.

19 The stream which is called Whitemarsh Run that runs
20 behind that whole area, where the whole road extends down
21 behind, you can stick your arm up underneath there, four or
22 five feet (4-5') where the water cuts through it, and we
23 filled that all in with rock and that water approximately is

Response:

1. See the response on Noise Abatement on P. IV-1.
2. Erosion and Sediment control measures will be employed with this project.

1 about twelve inches (12") right now, but when it rains, it's
 2 as high as 13-1/2 feet. And with the extra lane that I pre-
 3 sume you're going to build, I mean I don't know where my
 4 property is going to wind up before that's done. I mean it's
 5 definitely a soil erosion problem there, a sediment problem,
 6 and I don't think anybody's addressed it, and I'd like to
 7 bring it up.

8 MR. OLSEN:

9 Okay, thank you. Yes, ma'am? Please state your
 10 name and address and affiliation, if any.

11 MS. DAVIS:

12 My name is Anneka Davis. I live at 2226 Crest Road
 13 in Baltimore City. I am not actually in these specific remarks
 14 speaking for the Baltimore Environmental Center, but I am the
 15 president of the group and at a Board meeting on Monday
 16 night, we all agreed that to do this project at all is a
 17 further continuation of highway building insanity.

18 I believe, in this project as well as many others,
 19 that one should turn off the faucet before you start mopping
 20 your floor. I think, in a small system, the noise is one of
 21 these problems. This is not something that Maryland could
 22 address and it's obviously a long-term thing. I simply put
 23 it out as food for thought. Trucks, particularly, are very

Response:

1. See response on Noise Abatement on P. IV-1.
2. The No-Build alternate does not address the operational and safety needs and was not selected.

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1 badly aerodynamically designed. I mean, if you are driving
2 past a truck on a rainy day, you can see the swirls of water
3 coming out. You know that you don't have good flow lines of
4 air past trucks, and this is one of the sources of the noise.
5 Trucks could be designed to be quieter. I never heard of
6 anybody suggesting making the trucks quieter, only walling
7 off the noise which doesn't work anyway. That's just a
8 thought.

9 As far as the whole business of building highways,
10 that's another much more major source of a problem, and it's
11 the same failure to turn off the faucet. We're never going
12 to solve the problem of highway congestion by building more
13 highways. I mean, this is something highway engineers appear
14 to be the only people who can't recognize that. Most of the
15 people here know that if you build more highways, you will have
16 more cars, and if you have more cars, then there will be the
17 demand to build more highways, and then there will be more cars
18 on them and so it goes.

19 Short of paving over the entire East Coast, which
20 I am sure would not be something the highway people would be
21 adverse to, I think we might as well stop now because we're
22 not going to solve our problems that way.

23 One of the things that troubles me is the just plain

1/23

1 the amount of money that is being used on highways that would
2 more effectively solve the social problems if it were spent in
3 other ways. I fairly recently was at a planners' meeting
4 where Neil Pedersen who is also with the Highway Department,
5 presented a program for the Eastern Bypass and the Western
6 Bypass alternatives. This is supposed to take care of the
7 traffic coming up through the whole Washington/Baltimore
8 corridor and going around Washington and Baltimore. One
9 possibility on the east side. One possibility on the west
10 side. The project makes this one look piddling, it really is
11 enough to curl your hair; it is, the predicted cost, I mean the
12 estimated cost at this point are one and a half billion dollars.
13 I mean that is an awful lot of money.

14 I suggested during the comment period for that
15 meeting, that if that amount of money were taken and used to
16 create a better city in Baltimore, that people would be will-
17 ing to live in, that would lessen the tax rate in Baltimore;
18 because this; of course, is one reason people move out and
19 it is one reason there is all this continuing development,
20 pressure in the counties and it involves a whole lot more
21 than highways. I mean, it's got to be looked at as a problem
22 to be solved, not simply by building more highways, but by re-
23 thinking the whole way that we are developing and expanding

1 or not expanding.

2 If Baltimore City were made livable, so the people
3 stayed there, that would actually take the pressure off the
4 highways because there would be fewer people wanting to be
5 driving a hundred miles. I don't know whether people really
6 enjoy their daily commutes on the Beltway anyway, you know,
7 coming 30, 40, 50 miles in the rush hour highway conditions,
8 but if that same amount of money, the one and a half billion
9 they're talking about for a bypass, with over \$30 million for
10 this, were used on some things that actually increased public
11 livability and the quality of life, I think it would be a lot
12 more effective in solving not only our traffic problem but in
13 something else. Now, I know that is not going to be decided
14 by the Highway people, but it may be decided by us as citizens,
15 writing to our representatives and making some noise about
16 this kind of thing, this business of using the money in this
17 piecemeal, patchwork, noncomprehensive unplanned kind of
18 development. Again, this is a long-term thing, but I think
19 we ought to get started on it now because things are only
20 going to get worse if we don't do some real rethinking of
21 the whole policy of developing out in the counties as we've
22 been doing, and thinking we can solve highway problems by
23 building highways. Thank you.

1 MR. OLSEN:

2 Okay, thank you. The gentleman in the back?

3 Please state your name, address, and affiliation, if any.

4 MR. UDZINSKI:

5 My name is Steven Udzinski, and I live at 3047

6 California Avenue, and I have some concerns with the inter-

7 change at Harford Road. As it stands right now, California

8 Avenue is an extension of the on-ramp and off-ramps of the

9 Baltimore Beltway. Every morning and evening, I have to put

10 up with cars speeding past my house. Many times, I cannot

11 even get into my car as other cars are speeding by, and that

12 is an inconvenience in itself.

13 Now, you want to tell me that you want me to make

14 a left-hand turn across southbound Harford Road during the

15 rush hour traffic in the morning, when everybody and their

16 brother comes down from Harford County. I can't see how that

17 is improving the interchange. And also, there's going to be

18 a traffic light there, as I understand it, and you know, I

19 can't see how that's going to help anything either.

20 Also, on the other end, with Option B, it is a

21 problem of cars making a left-hand turn onto California

22 Avenue from the off-ramp, and I'm happy to see that you're

23 trying to make some kind of improvement there. But, I am

Response:

1. MD 147 Option A was not selected.

2. MD 147 Option B was selected and would widen MD 147 to five lanes between Putty Hill Avenue and Second Avenue.

1 surprised that you're dead-ending Grendon Lane. You're just
2 going to go ahead and take somebody's, you call it, unimproved
3 property, but somebody enjoys that, to have, you know, so you
4 can have access onto Edgewood Avenue. You know, the problem
5 isn't at Grendon Lane, the problem is at California Avenue
6 and the off-ramp.

7 Also, at California Avenue, as you're coming onto
8 Harford Road, looking south, there is a sight distance prob-
9 lem. There is a small hill on the property looking south
10 with also a hedge and the Harford Road is already starting to
11 taper onto the on-ramp going east, and if you try to go past
12 that hedge, you're sticking out into the traffic. And that
13 should be addressed.

14 Also, you want to widen Harford Road, but south-
15 bound Harford Road still has parking on it, which is ridiculous.
16 You know, you have a two (2)-lane roadway and you have parked
17 cars on it, so every night coming home, you see people getting
18 stuck behind these parked cars, you have to try to weave back
19 into traffic and then get back over to get onto the on-ramp.

20 That's about all I have to say. Thank you.

21 MR. OLSEN:

22 Okay, thank you. Yes, sir?

23 MR. LINDSEY:

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1 My name is Bill Lindsey, 125 Dublin Drive in
2 Lutherville.

3 I have before me parts of two (2) letters, one
4 dated back in '84 where the Department of Transportation says
5 the noise barriers would be up between '88 and '89. That came
6 and went. One dated '85 saying they'll be up in '89. That
7 came and went.

8 The thing that really has me concerned is in the
9 old '85 meeting book that we got, when we came here in '85,
10 there was a statement in there "Noise is a major issue due to
11 the extensive residential development in many areas adjacent
12 to I-695. Several locations are already scheduled for Type
13 II noise barriers. The noise impact in other areas will be
14 monitored and analyzed as part of this year's project." Well,
15 that was encouraging. But, in the new book, the one we
16 received coming here, it basically tells you that it's not
17 part of the project any more. You have no concern of it and
18 it's part of the Landscaping Department's concern.

19 It appears that you're taking the whole noise
20 abatement and just pushing it aside, along with the walls,
21 along with all the letters that we get from you, and we cannot
22 believe what you're saying. Thank you.

23 MR. OLSEN:

Response:

1. See the response on Noise Abatement on P. IV-1.

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1 Thank you. Yes, sir?

2 MR. CHRISTOPHER:

3 My name is Robert Christopher. I'm President of
4 the Back River Neck Peninsula Community Association. I see
5 you want to widen the road down to Route 702. Well, I'm very
6 familiar with Route 702, as I'm sure you are. You came down
7 there and you took a lot of trees down at the Chesapeake Bay
8 Critical Area, destroyed a lot of wetlands, and right now,
9 there is still uncontrolled flooding, sediment control is
10 atrocious, and I would submit to you that before you consider
11 these projects, which may be a worthy one in the long run, you
12 complete the projects you started by replacing the wetlands
13 as you said you would do, replacing the trees in the
14 Chesapeake Bay Critical Area, and keeping face with the
15 people. Thank you.

16 MR. OLSEN:

17 Thank you. Yes, ma'am?

18 MS. FLORENDO:

19 My name is Sony Florendo, and I'm one of those
20 whose property will be allocated eventually, and that is
21 7718 Belair Road. The reason I'm here is, I am a minority,
22 I am a woman in business, and when I bought that property
23 five (5) years ago, I wasn't informed by the owner of the

Response:

1. Requests are not related to this project.

Response:

1. U.S. 1 Option has been selected to address the operational and safety concerns.

1 property about the future of what I was buying. I know that
2 a buyer should be aware of what we are doing, but, you know,
3 I felt I was really--this information really hurts me. When
4 I bought the property, I had at least a ten (10)-year project.
5 This involves doing a project for the children, for inter-
6 national understanding. There is a project wherein the
7 Philippino community can share the culture with the rest of
8 the community, both in Maryland and in the adjoining counties.

9 I have big plans for this place. If you are in the
10 neighborhood, you will see that that property is the only one
11 which offers a lot of potential for sharing culture, educa-
12 tion, and many more. I'd like this Maryland State Highway,
13 the State of Maryland, to see this project as a project of
14 interest to the rest of the community. I'd like you to help
15 me devise some way of addressing my problems. I am also aware
16 that that intersection, Exit 32-A, is a dangerous intersection.
17 I have heard many accidents happening there. I look out my
18 window and I see these things happening, but as I look, my
19 husband and I would go around the community. There are other
20 ways to address this, and I need your help.

21 And to those of you who came here tonight to
22 address the sound that is affecting your communities and the
23 environmental effects, I support you.

1 I came in front, at the beginning I felt like this
2 is a selfish motive for me to say, you know, I am interested
3 and I love my property, but after hearing Dr. Burns express
4 his concern about his property, I felt that I have many more
5 things to offer to Maryland. Thank you.

6 MR. OLSEN:

7 Okay, thank you. Yes, sir, in the back? Please
8 state your name and address.

9 MR. PATH:

10 My name is Bob Path and I live in the Longford
11 section of Lutherville. I have a couple of concerns.

12 We heard a couple people tonight from the Perring
13 Parkway area speak, and Loch Raven, and Dulaney Valley Road,
14 talking about the noise abatement wall, when they're going to
15 get it, and everybody's been promised. We in the Longford area
16 have been promised as well, that we're going to get the wall,
17 and I would like to see the State come out with a unified list,
18 have a unified position as to who is going to get the wall,
19 and where we're going to get this wall. I don't know if all
20 of you folks are aware of the original list of twenty-six (26)
21 priorities and where you fit on that original list of nineteen
22 (19) or twenty-six (26). We were number 19. The Hampton
23 Mansion project was like number 14 or 15 which we couldn't

Response:

1. See the response on Noise Abatement on P. IV-1.

1 believe. We've been told that we're next in line, but probably
2 some of you folks in the Perring Parkway area and other areas.
3 have been told that you're next in line.

4 We've had our meetings with Hal Kassoﬀ and Frank
5 Kelley, and everybody else has been out and maybe they're
6 playing all of us against one another, I don't know, but I'd
7 like to know, number one--what the State's position is, and
8 stick to that position.

9 #2--I think we were promised a wall in '87, '88,
10 and '89, and the gentlemen over here said, and the gentleman
11 over there said, it keeps getting pushed back further and
12 further. Now, what you're asking us to do is potentially
13 endure more noise, potentially double the noise with the
14 prospect of never getting the wall. We've seen that already,
15 that we may never get this damned wall, and now we're going
16 to look at doubling the traffic, the noise, the pollution, and
17 everything else with the prospect of never getting it.

18 Thank you.

19 MR. OLSEN:

20 Thank you. All the way in the back, the lady all
21 the way in the back? Please state your name, address, and
22 affiliation.

23 MS. PATRONE:

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1 My name is Charlotte Patrone. I live at 1026 Adcock
2 and I'm with the Longford Community Association. There are
3 a couple of things that have concerned me this evening.

4 #1, I have never heard anybody over there reference
5 any coordination with the Baltimore County Master Plan. You
6 know, are we going to continue the planning on this kind of
7 scale? Do you all coordinate?

8 MR. OLSEN:

9 Absolutely.

10 MS. PATRONE:

11 They've got their ideas of what's happening in
12 Baltimore County. Please, you know, let's not keep breaking
13 down walls and costing the taxpayers dollars because people
14 don't write letters and people don't communicate. I can't
15 understand why I'm looking at this Beltway but yet I'm not
16 looking at anything to do with the Baltimore County Master
17 Plan.

18 MR. OLSEN:

19 If you look at the brochure, you will find that
20 Baltimore County is part of the Project Planning Team and
21 consistent with the Baltimore County Master Plan.

22 MS. PATRONE:

23 But it was not part of the presentation, and it

Response:

1. See the response on Noise Abatement P. IV-1.
2. Mass Transit studies were not part of this study.

1 was not consistently displayed.

2 The other thing is, and Mr. Honeywell, I know that
3 the barriers are not your problem. I know the barriers are
4 not part of anybody's problem over there, and maybe that is
5 the problem. The barriers are in Landscape. Why in the world
6 are they in Landscape? They're not decorative, by any means.
7 Whose great idea was it to put protective abatement barriers
8 in a landscape project? This is part of your problem.
9 You're telling me you've got \$240 million to add a lane, but
10 if somebody fell in my pool in my back yard, they'd drown
11 because I can't hear it. But you've got \$240 to add a lane.
12 You've got such gross lack of foresight that rather than try
13 to initiate mass transportation in the State of Maryland,
14 you're continuing to put cars on the road as if we fund Ford
15 Motor Company. I can't believe it.

16 Yes, these intersections, these interchanges have
17 to be upgraded, they're dangerous. The rights-of-way are too
18 narrow. There's no getting around that. You cannot continue,
19 but for God's sake, you know, let's start looking mass transit,
20 and I don't mean the light rail system. I mean some kind of
21 working system where people are not having it stuffed down their
22 throats. Thank you.

23 MR. OLSEN:

1 Does anyone else want to make any comments?

2 Anyone else?

3 Okay, let the record show that no further verbal
4 comments were offered. Should you desire to extend your
5 comments in writing, to submit exhibits, or to offer written
6 rather than verbal comments, you may do so by letter to the
7 appropriate member of the Project Planning Team as noted in
8 your brochure or by means of forms available on the recep-
9 tionist's table for this purpose. We will hold the formal
10 record open until March 16, 1990 for your written comments.

11 If your questions were not satisfactorily answered
12 this evening, I urge you to contact the Project Manager, Mr.
13 Honeywell, whose mailing address and phone number are con-
14 tained in the brochure. He will attempt to furnish you with
15 a more definitive response.

16 Up to the time that we receive project approval,
17 all information developed in support of the proposed location
18 and design will be available, upon request, at the Baltimore
19 Office of the State Highway Administration for public inspec-
20 tion and copying. Arrangements for such a review should be
21 made through Mr. Honeywell.

22 Thank you for attending tonight's hearing. We
23 appreciate the interest you have shown in this important
project.

INDIVIDUAL TESTIMONY

1
2 DR. GARY:

3 Okay, this is Dr. Nader Gary, residing at 2 Roland
4 Court, Ruxton, Maryland 21204. My phone number is 337-5050.

5 The extension of I-83 North, adding another lane
6 to 695 or I-83, building a flyover ramp connecting 83-North
7 and 695 South will not improve the traffic jams in peak hours.
8 Instead, it will have an adverse effect on property values of
9 Ruxton and the surrounding community, leading to less access
10 on these homes, increasing noise and increased air pollution.

11 The logical solution to the congestion is to
12 improve the collaterals rather than increase the size of the
13 major arteries. If major arteries and collaterals cannot
14 drain well, there will be a congestion in the main artery.
15 The following factors play a major role: (1) Exits are not
16 able to drain out of 695. (2) Too many stops or traffic lights
17 immediately after exit from 695. (3) Town or city streets are
18 not able to handle the load of cars coming out of major
19 arteries.

20 Example of #1 factor is New York exit from 695 is
21 totally inadequate. It creates a back-up for five (5) or
22 six (6) miles every day between 4:30 P.M. to 6:00 P.M.
23 Following exit from 695 towards New York, there is a congestion

Response:

1. I-83 (JFX) Option was not a selected build alternate.
2. This study included the first major intersection away from the Beltway interchanges.

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1 on 95 itself. By adding another lane to 695, this problem
2 will not be solved.

3 Example #2 factor--on York Road, Charles Street,
4 and Kenilworth Avenue, traffic lights and stop lights slow the
5 traffic down.

6 Example for #3--traffic lights, local stops, and
7 right-turns into local businesses, gas stations, McDonald's,
8 etc., will slow down the traffic.

9 In summary, adding to major arteries, especially
10 on 83-North and South, to 695, will not solve the problem.
11 There has to be other solutions, which I list as below:
12 #1--good exits, good collaterals meaning the connection between
13 the major arteries should be sufficient. #3--good drains out
14 of 695. #4--possible transit officers to guide the traffic
15 in peak hours. #5--prohibit commercial vehicles to commute
16 during peak hours. #6--no increase in number of lanes to
17 major arteries.

18 I strongly oppose the proposal to build new roads,
19 especially at the junction of 83 and 695. Thank you.

20 MR. CHILDERS:

21 I'm Jack Childers, C-H-I-L-D-E-R-S. I live at 8344
22 Tally Ho Road, Lutherville, Maryland 21093. This is just
23 north of the Beltway between Thornton and Joppa Roads. We've

Response:

1. See the response on Noise Abatement on P. IV-1.

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1 lived there since 1971. When we moved there, the Beltway
2 was six (6) lanes. It is now increased to ten (10) and the
3 proposed improvements of which I'm speaking will take it to
4 twelve (12). Not only that, each lane carries more vehicles
5 than it did in 1971 and the vehicles that it carries are
6 larger than they were in 1971 and noisier.

7 Despite the fact we knew the Beltway was there in
8 1971, we did not know it would go from six (6) to twelve (12)
9 lanes and we would like to make a plea for a noise abatement
10 barrier on the north side of the Beltway between Thornton and
11 Joppa.

12 MR. PORTER:

13 My name is Wayne R. Porter. I live at 8339 Tally
14 Ho Road, Lutherville.

15 I feel that attention should be directed to page 9,
16 that is the area of I-83, Jones Falls Expressway, to I-83, the
17 Harrisburg Expressway, which states "Environment impacts are
18 subject to change during the Design Phase." Over the past
19 twenty (20) years, the Beltway has been widened from two (2)
20 lanes to the existing five (5) roadway lanes and proposed six
21 (6) roadway lanes. The Seminary Ridge community along this
22 stretch, has consistently been told by the State Highway
23 Administration that it does not qualify for noise abatement

Response:

1. See the response on Noise Abatement on P. IV-1.

1 barriers because the Beltway was here before the development.
2 While that may be true, it is fact that the noise level has
3 already been increased by one hundred fifty percent (150%)
4 and the proposed widening will result in a two hundred percent
5 (200%) increase in noise level.

6 Traffic noise has made our yards virtually worthless
7 because we are held prisoners within our homes in our efforts
8 to try to shut out some of the noise. The State Highway
9 Administration Bureau of Landscape Architecture reports that
10 the entire I-83 JFX to I-83 Harrisburg Expressway segment is
11 now identified as a noise-impacted area. However, there is a
12 lack of funds and when funds do become available, time of
13 which is unpredictable, prior approved areas will be accommo-
14 dated first even though the noise impact in those areas is
15 not as severe as between the two (2) I-83s.

16 A December 13, 1989 article in The Baltimore Sun
17 identified this area as the number 1 traffic hot-spot on the
18 Beltway, with 175,000 cars moving through every day--that's
19 more than two (2) per second, twenty-four (24) hours a day.
20 Only ten feet (10') from our property.

21 I feel that Alternate 2, Beltway widening must
22 include noise abatement barriers between I-83, Jones Falls
23 Expressway and I-83 Harrisburg Expressway. Thank you for

1 the opportunity to present this.

2 MR. CLAPPERTON:

3 My name is Gilbert Clapperton. I live at 8338
4 Tally Ho Road. In the eighteen (18) years that I've lived in
5 Seminary Ridge, I have seen the Beltway increase in size and
6 traffic volume considerably, and I believe that with the pro-
7 posed increase in size, that noise abatement barriers are a
8 must. Thank you.

9 DR. FINE:

10 For the record, State Roads Commission, et al.,
11 Dear Sirs: For the record and for appeal purposes, I am
12 opposed to any widening of the 695 road highway that would
13 cause me to lose additional land. In addition, the very sug-
14 gestion of allowing more traffic so close to my residence is
15 extremely unhealthy and devastating to my health and my real
16 estate value. I have been subjected to undue hardships due
17 to the State Roads highway projects from the very first day
18 in 1965 when this major noise and air pollution began. Prior
19 to this date, the quiet sylvan beauty of my residence was a
20 joy to behold. There were deer and birds of many kinds and
21 a quietness that caused us to build our home there in the
22 first place back in 1950.

23 Now I am plagued with the extreme, continuous

Response:

1. See the response on Noise Abatement on P. IV-1.

Response:

1. See the response on Noise Abatement on P. IV-1.
2. Air quality analysis are included in the study process.

1 noise all day and all night from this heavy traffic. There
2 is a gradual hill before the Greenspring exit and the trucks
3 on 695 change gears about that level with my property. I have
4 had studies done to record the decibels incurred. All show
5 a noise level above and beyond any normal highway findings.
6 In addition, the loss of privacy, the reason for building out
7 here in Baltimore County in the first place has diminished
8 with each additional lane enlargement. I have suffered through
9 two (2) major fires and two (2) numerous acts of vandalism and
10 numerous thefts which all occurred since the Beltway has been
11 here in operation. I have come home to my house being burglar-
12 ized and narrow escape from being held captive myself. All
13 this has occurred, I allege, because the State Roads Commission
14 has not protected me from the criminal element who look for
15 places to rob by allowing a roadside lane to be level with
16 my home and not supplying plantings or trees or prevent a
17 clear view from my home.

18 No effort to put up any barriers or view protection
19 devices have ever been made here. I hold the State Roads Com-
20 mission, their engineers, the safety-protection divisions,
21 directly responsible for allowing cars to have clear access
22 to my private home and invasion of my privacy of view of the
23 695. Naturally, when the police show up, the intruders either

1 lie to the officers or fane reading a map or say they're wait-
2 ing for someone, but nothing is done. This fear has followed
3 the loss of over a half a million dollars in furniture,
4 clothes, artifacts, property that has gone away because this
5 condition, due to the Beltway, has only increased, not
6 decreased in travel.

7 The exhaust fumes from the trucks and the cars, the
8 constant accidents that occur, and the carbon monoxide fumes
9 which have increased each year tend to cause tension and
10 breathing problems that should never have been allowed in the
11 first place. My health has been in jeopardy and continues to
12 be in jeopardy from this main highway with very minimal super-
13 vision on exhaust fumes and noise factors. I cannot recall
14 ever seeing anyone from the State Roads Commission taking
15 note of these dangerous levels of poisonous gases at my
16 residence or any other level for noise.

17 If this is not enough to cause my property real
18 estate value to depreciate over eighty percent (80%), then
19 there is the sign of the Greenspring Avenue exit, Exit 22, which
20 gives light all through the night to the access of my property
21 and causes me the loss of even more privacy and sleep.

22 In addition, the chemical spraying of the natural
23 vines and trees on the Beltway side with poisonous toxic

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1 elements unknown to me have caused my trees and vines on my
2 side of the property to die and never come up again.

3 Again, my air is violated, the State Roads Commis-
4 sion has never once alerted me to the time and date of the
5 toxic attack of the vegetation or my breathing of poisonous
6 substances or prevention of same. In addition, whenever the
7 State Roads Commission decides to have some men cut down the
8 weeds or trees along my property and the Beltway, there are
9 incidents when these men relieve themselves by the wire
10 fencing along my property. No arrangements seem to be made
11 to curtail these disgusting practices by the State Roads
12 Commission workers.

13 In addition to this, these same workers try to
14 tease or disturb my guard dogs which is another invasion of
15 my privacy. At no time has anyone even suggested landscaped
16 planning to avoid even a little of the noise or the continued
17 view of cars racing and trucks grinding under too much
18 tonnage. No baffle dividers of concrete or barriers of any
19 nature have appeared there at Greenspring property as I have
20 seen at many, many other sites along the Beltway. When I
21 asked for these barriers to be installed, I was told due to
22 the density factor and so few people living there, it was not
23 economically feasible. Well, possibly litigation should be

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1 even more expensive. There must be some way the State Roads
2 Commission can do better than they have in the past. I have
3 lost many acres to eminent domain in the name of progress, and
4 only \$40,000 was offered for over eleven (11) acres, and my
5 legal fees on top of that. This is Big Brother at his most
6 disgusting worst.

7 Thank you very much for considering my suggestions.
8 I appreciate your time.

9 MS. LUGER:

10 I'm Reesa Luger, 20 Marcie Woods Court, Baltimore
11 21208. I'm here because our house is extremely close to the
12 Beltway. The noise level is terrible. We would like to have
13 a decibel level physically taken in our back yard. From
14 looking at the maps, it looks as though the decibel levels have
15 been taken on Phillips Drive in an area that is older than our
16 development, and not as close to the Beltway. We feel that
17 we are closer to the Beltway and that the noise level would
18 be higher. The noise level, I'm being told, has already
19 exceeding your acceptable limits of 67. They're saying that
20 we are at 68, expecting to be 69, and we feel that we're even
21 higher because we are closer to the Beltway than you were
22 for the testing.

23 We also have had serious problems because whenever

Response:

1. See the response on Noise Abatement on P. IV-1.

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1 cars break down on the Beltway, people seem to think that
2 they should jump the fence and come into our neighborhood to
3 use the telephone. Our neighborhood is totally full of
4 children. Unfortunately, I would like to be a good citizen
5 but in the world the way it is today, I feel it is unaccept-
6 able to have strangers coming into our neighborhood. I feel
7 that the sound barrier wall not only would deter the sound but
8 it would block our neighborhood from the Beltway and provide
9 major safety.

10 . In the summer, the noise is so loud that it is
11 impossible to go outside to have a barbecue or anything like
12 that. The other problem is, we have heard that there were some
13 studies done for the environment showing the people that live
14 close to major highway systems have increased colds and
15 illness, and we would like you to get some information on
16 that for us. I have filed a petition with all of our neighbors'
17 names with Cynthia Simpson for Noise Abatement. She promised
18 me the petition would be taken to the proper place.

19 Thank you for your help.

20 MR. LUGER:

21 My name is Stefan Luger. I live at 20 Marcie Woods
22 Court in Pikesville. I am very concerned about the noise
23 level that is going to be occurring when the Beltway is

Response:

1. See the response on Noise Abatement on P. IV-1.

25

1 widened in our area. You currently will be going from three
2 (3) lanes in either direction to four (4) lanes in either
3 direction. We feel that the noise being produced now is so
4 totally unacceptable and with the addition of these lanes, it
5 will be much higher. The decibel readings that were taken
6 at the NSA 29 which is the decibel reading area is not repre-
7 sentative of our neighborhood, and from what we understand
8 the closer you are to the Beltway the sound volume increases logo-
9 rhythmically. So, we would like to have a testing in our
10 neighborhood.

11 We do request a sound barrier be added to our
12 neighborhood for safety reasons as well as noise level.
13 Currently when you are in the yard in the neighborhood, it
14 sounds as if there is a constant running of water, like a
15 waterfall. During the summers, it is impossible to talk to
16 anybody else in the neighborhood until there is a lull in
17 traffic.

18 I would also be suspect of the decibel reading
19 levels that were made in the neighborhood because I understood
20 they are taken only on a 15-minute basis. It depends at what
21 time of day these levels are taken, the sound will vary
22 according to the time of day and also the season of the year.
23 And if it's rush hour traffic or whether the speeds are up to

1 where they should be or determining whether there is a lull
2 in the period or what day of the week it is, the sound
3 levels can vary dramatically, so we don't know if a 15-minute
4 test is adequate to judge the entire character of the sound
5 being produced by the traffic. Thank you.

6 MS. GREENBAUM:

7 My name is Selene Greenbaum. I live at 18 Marcie
8 Woods Court, zip code 21208. I'm here also with my neighbors
9 to testify that the sound from the Beltway that is generated
10 every day is incredibly loud, that I cannot walk outside my
11 house and enjoy the outside conditions even in the summer
12 because of the amount of noise that the Beltway gives off.
13 And I probably could give you a traffic report because I can
14 look outside of my sliding glass doors in my living room and
15 tell you the cars I can see and recognize. That's how close
16 the Beltway is to my house.

17 I feel, as do most or all of the people in my
18 neighborhood that a barrier would significantly help our living
19 conditions, not only from the noise standpoint but also from
20 the safety standpoint. I, myself, have a four-year-old
21 son whom I will not let play outside in the back yard by himself
22 or with a friend, basically because I'm afraid someone will
23 get stuck on the Beltway and will jump over the fence, which

Response:

1. See the response on Noise Abatement on P. IV-1.

1 has been done in the past, several times, or on many occasions
2 actually, and God knows what they could do. In the past,
3 I guess it was two years ago. I inquired to Paula Hollinger
4 about the safety issue, not even the noise, and they added
5 two (2) rows of barbed wire on top of the already existing
6 6-foot chain-link fence. I can tell you that there have been
7 many incidents since then that people have still come over the
8 Beltway because the barbed wire does not--it was not put up
9 in a long enough stretch. They put it up, I guess it was too
10 short of an area. But, even so, I do not think that that
11 deters people when they see that the houses are right there
12 and it is a convenient way to get a phone call made and to get
13 help from being stranded on the Beltway. I think that that
14 alone, that the wall would shield, just by sight, the neighbor-
15 hood, would be incredibly helpful to us.

16 And then back to the noise issue, I think it would
17 make our standard of living quite better in all respects. I
18 hope that something will be done. And I also hope that again
19 our neighbors who were previously speaking, mentioned that the
20 testing that had been done previously was not even done in
21 our neighborhood, and we are closer to where the specific test
22 area is in relation to the Beltway and I think it should be
23 done in our neighborhood and that we should be notified when

1 the person is going to be out there or have some kind of
 2 letter stating that they were actually out there and at what
 3 time. I appreciate your help. Thank you.

4 MR. GREENBAUM:

5 Harvey M. Greenbaum. I would like to direct this
 6 to somebody's attention in reference to the barrier by my
 7 house at the Beltway at Marcie Woods Court. I feel that the
 8 noise level is very loud, constant from early in the evening
 9 to early in the morning is very disturbing.

10 Also, from a safety point, we have a lot of people
 11 who break down, they hop over the fence, which is not high
 12 enough or there is not enough barbed wire to keep the people
 13 out and I feel that a barrier would serve a dual purpose, for
 14 the noise and for the safety factor. I would appreciate what-
 15 ever you can to help us out. Thank you.

16 MR. MATZ:

17 This is Richard Matz. I'm testifying as to the
 18 noise level and security issues in and around Marcie Woods
 19 Court, in the subdivision called Marcie Woods. Noise levels
 20 are at this point very high, in our opinion. At certain times
 21 trucks and other vehicles are exceedingly high. We have also
 22 experienced security problems, even as shortly as two weeks
 23 ago, there was a fire on the Beltway which looked like it was

Response:

1. See the response on Noise Abatement on P. IV-1.

Response:

1. See the response on Noise Abatement on P. IV-1.

1 going to spill down the hill into our woods and cause us
2 serious problems; luckily nothing happened.

3 There have been occasions when people have stopped
4 on the Beltway and climbed the fence and knocked on the door
5 of our house in the middle of the night, which is not so good
6 either. But, we feel the noise as well as the security issue
7 warrants having a wall put up.

8 We would also like tests redone in our neighborhood
9 because we feel the tests that were done previously were done
10 in 1986 or so and the maps that are now being used for the
11 exhibits do not even show the houses in our neighborhood.

12 I think that concludes my testimony.

13 MR. YEN:

14 This is Nelson Yen on Tally Ho Road. My back yard
15 exists next to 695 only 110 feet away. Once three (3) guys
16 crossed the fence to my home. They tried to make a telephone
17 call in the midnight, it was a cold rainy day. Everybody
18 at our house was scared.

19 Another thing, the noise--getting more noise every
20 year because of the traffic increase in lots and no way to
21 be quiet. We can't sit in the yard, so that's what we need
22 to know--do you have the project for the noise barrier on
23 Tally Ho Road? My house is number 8337 Tally Ho Road, so you

Response:

1. See the response on Noise Abatement on P. IV-1.

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19D

1 look on the map, on the south tip of the Tally Ho Road very
2 close to 695. Your further consideration would be appreci-
3 ated very much. Thank you.

4 MS. JORDAN:

5 My name is Carolyn Jordan. I live at 7207 North
6 Charles Street in Lutherville, that's Charles Street, north
7 of the Beltway just above Bellona Avenue. And I'm concerned
8 about the traffic at that intersection. I understand with
9 the new proposal all traffic coming up North Charles Street
10 will make a left and go onto the Beltway.

11 What I would like you people to be aware of the
12 traffic there. For people coming (A) from the 7200 block North
13 Charles Street and trying to go left on Bellona Avenue, or
14 southbound on North Charles Street. And, secondly, to remem-
15 ber the people that will be coming from the new AAA complex
16 which is on Bellona Avenue just west of North Charles Street.
17 They, too, will find it almost impossible to cross Charles
18 Street on Bellona Avenue, heading east at certain times of the
19 day. This is an intersection that has many traffic accidents,
20 and I only foresee that it will have more in the future if all
21 traffic would have to go onto the Beltway or 83 by going left
22 from North Charles Street.

23 That's all I have to say.

Response:

1. I-83 (HX)/MD 139 (Charles Street) Option D is a selected build alternate. This option addresses the intersection of Charles Street and Bellona Avenue to improve operations and safety.

1 MS. PATRONE:

2 My name is Charlotte Patrone. I'm with the Long-
3 ford Community Association. I think the plans that are pro-
4 posed in the pamphlets and during the speech are shortsighted.
5 They are not progressive. They continue to inundate the
6 area with car traffic; they don't look to mass transit and
7 they certainly don't look to the residents of the area. They
8 don't consider the burden.

9 I also feel the way this has been presented, it
10 is quite obvious that the sound barriers are being held up by
11 the design of the widening of the road and that is abominable.
12 We continue to live with noise, vibration, and dirt until you
13 can make up your minds as to what design you will end up with,
14 if in fact any of it since it's really a moot point, "we have
15 no funding."

16 Thank you.

17 MS. BOATWRIGHT:

18 Yes, my name is Joyce Boatwright, Mrs. Lee Boat-
19 wright, and I live at 1930 Old Court Road, and that's Ruxton
20 21204. And I would just like to express my great concern about
21 the noise that we're hearing right now, and I would like to
22 invite anyone on the committee or in the group that would like
23 to hear the noise at my house right now, but I would also like

Conference Reporting Service • 301-768-5918

Response:

1. This study did not include mass transit.
2. See the response on Noise Abatement on P. IV-1.

Response:

1. See the response on Noise Abatement on P. IV-1.
2. I-83 (JFX) Option B was not a selected build alternate.

192

1 to say that I am greatly opposed to Option B on the I-83 JFX
2 Exit 25, it's called, the Falls Road interchange. It's Option
3 B, would be the one that they recommend a flyover ramp with
4 a fifty (50)-mile per hour design speed for northbound I-83,
5 JFX to the westbound Beltway. This option, it's going to create
6 even more noise and again it concerns me because it would be
7 located so much higher than if we can possibly get some of
8 those protective walls.

9 I would just like them to seriously consider not
10 doing this Option B.

11 MR. MAC CARRON:

12 My name is John MacCarron. I live at 14 Barrow
13 Court in Towson. I live at the southwest corner of the inter-
14 section of Thornton Road and 695. We do not have a sound
15 barrier currently nor are we in the plans to get one. There-
16 fore, I am opposed to the project in its entirety because the
17 current level of traffic noise, pollution--both air and noise--
18 is not acceptable and all indications are that it can only
19 worsen and there is insufficient assurance in this plan, and
20 it's a polite way of putting it, that we are going to get
21 anything in the way of noise barriers in my neighborhood.
22 Thank you.
23

Response:

1. See the response on Noise Abatement on P. IV-1.

1 STATE OF MARYLAND)
2)

3 I, the undersigned, Notary Public in and for the
4 State of Maryland, do hereby certify that the within named,
5 individuals appeared before me at the time and place herein
6 set out; and further, having been duly sworn before me,
7 according to law, was interrogated by counsel.

8 I FURTHER CERTIFY that this hearing was recorded
9 electronically by me and then transcribed from tapes to the
10 within typewritten transcript in a true and accurate manner.

11 I FURTHER CERTIFY that I am not of counsel to any
12 of the parties, nor am I an employee of counsel, nor any
13 relation to any of the parties, nor in any way interested
14 in the outcome of this action.

15 AS WITNESS, my hand and Notarial Seal this 26th
16 day of March, 1990.

17
18 *Robert M. Cannon*
19 Notary Public
20

21
22 My commission expires 7-1-90.
23

IV-77

194

V. CORRESPONDENCE

196

V. CORRESPONDENCE

The following presents the written comments received during or subsequent to the Combined Location/Design Public Hearing (held February 28, 1990). Originals of these correspondence are available for review in the Project Development Division Offices, State Highway Administration, 707 North Calvert Street, Baltimore Maryland 21202. Oral comments received during the Hearing are presented in Section IV of this document.

- A. Written Comments Received Subsequent to the Combined Location/Design Public Hearing
- B. Elected Officials
- C. Agency Coordination

V. CORRESPONDENCE

A. Written Comments Received Subsequent to
the Combined Location/Design Public Hearing

February 2, 1990

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

Office of Planning and Preliminary Eng.
State Highway Administration
Baltimore, Maryland 21203-0717

Attn: Mr. Neil J. Pedersen, Dir.

Dear Sir:

Although I am unable to be present at the hearing on February 28, 1990, I would like to be placed on your mailing list.

Since I own the building at 9008 Harford and run my business from this location, I would like to be kept up to date on the projects in and around this area.

Thank you for your cooperation.

Sincerely,

Ronald A. Sautter

RS/is

Response:

- MD 147 (Harford Road) Option B was selected. This will widen Harford Road to five lanes.

NAME DR. BERNARD EISENBERG DATE 2/18/90

PLEASE PRINT ADDRESS 8000 IUY LANE

CITY/TOWN BALTO STATE MD ZIP CODE 21208

I/We wish to comment or inquire about the following aspects of this project:

Please construct sound barrier along area between Parkgate ave and Stevenson Rd as soon as possible. Thank you. Air barrier air pollution would be appreciated. I approve of the change at Stevenson Rd.

Response:

1. The SHA adopted their Type II Noise Abatement Program in conjunction with Federal legislation, to provide relief from existing noise levels for residential areas and public institutions adjacent to existing major highways. To date the following Type II projects are either approved or constructed in the project area: I-695, Perring Parkway to Harford Rd (constructed); I-695, Hampton/Concordia Drive (constructed); I-695, Providance Rd (EBR) (constructed); I-695, York/Dulaney Valley Rd; I-695/I-83, Thorton/Seminary Rd; and I-695, Charles St/York Rd. Based on the noise analysis completed to date, the SHA has determined that Type I noise abatement measures are

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already reasonable and feasible and will be considered during final design at 18 locations.

199

Response:

1. See response on Noise Abatement on P. V-2.

PROJECT DEVELOPMENT
 STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472 FEB 22 10 30 AM '90
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME TORRE E. HERRERA DATE 2-17-90

PLEASE PRINT ADDRESS 3039 BALDER AVE.

CITY/TOWN BALTIMORE STATE MARYLAND ZIP CODE 21234

I/We wish to comment or inquire about the following aspects of this project:

- 1- NOISE ABATEMENT WALL -
- 2- DETOURS WHILE CONSTRUCTION BET. TOWSON - I. 95

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. A C-D network was considered in Stage I and dropped due to operational problems.
2. The Providence Road Option has not been selected.
3. A staging of mainline and interchange improvements has not been finalized.

STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

CONTRACT No. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME JAMES THOMPSON DATE 2/21/90

PLEASE PRINT ADDRESS 7 LOMBARDY PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

- DISAPPOINTED EMPHASIS PLACED ON MAINLINE CAPACITY RATHER THAN RECTIFYING THE MAJOR INTERCHANGE PROBLEMS IN TOWSON AREA, LIKE WAS DONE @ US40; WITHOUT MAJOR INTERCHANGE MODIFICATION AND REDUCTION IN # OF ACCESS POINTS THROUGH TOWSON, I695 WILL ONLY BE AN BLAME RATHER THAN 6 LANE PARKING LOT @ PEAK!
- A COLLECTOR-DISTRIBUTOR LANE THROUGH MD45 + MD146 (PERHAPS TO MD139 + EASTWARD TOO) WOULD BE HIGHLY DESIREABLE IN LIEU OF MAINLINE LANE.
- MD139, MD45 + MD146 INTERCHANGES ARE COMPROMISED BY HAVING LOCAL TRAFFIC INJECTED INTO THEIR MOVEMENTS UNNECESSARILY; THIS MUST BE RECTIFIED BEFORE ADDING MAINLINE CAPACITY (SEE SCHEMES)
- LACK OF LOCAL SUPPORT SYSTEM CONTINUITY CAUSES MANY RESIDENTS TO USE I695 FOR SHORT TRIP PURPOSES; MINOR REALIGNMENTS (IE CHARLTON/HAMPTON, VALLE SEMINARY OR JOPPA) WITH ASSOCIATED TOPICS IMPROVEMENTS WOULD HELP
- UNLESS PROVIDENCE IS STRIPED FOR 4 LANES SOUTH OF BREEZEWICK, A 3 LANE SECTION THROUGH INTERCHANGE SHOULD BE MORE COST EFFECTIVE
- THE JFX INTERCHANGE PROBLEM HAS BEEN EXACERBATED BY FAILURE TO CONNECT THE N.W. EXPRESSWAY AND WALBASH AVE AS ORIGINALLY PLANNED
- FIRST PRIORITY IS SAFETY + MAKE INTERCHANGE IMPROVEMENTS BEFORE ANY MAINLINE CAPACITY IMPROVEMENTS IN FUTURE; QUESTION MASSIVE INVESTMENT + OVER-RELIANCE ON I FACILITY

Please add my/our name(s) to the Mailing List.*

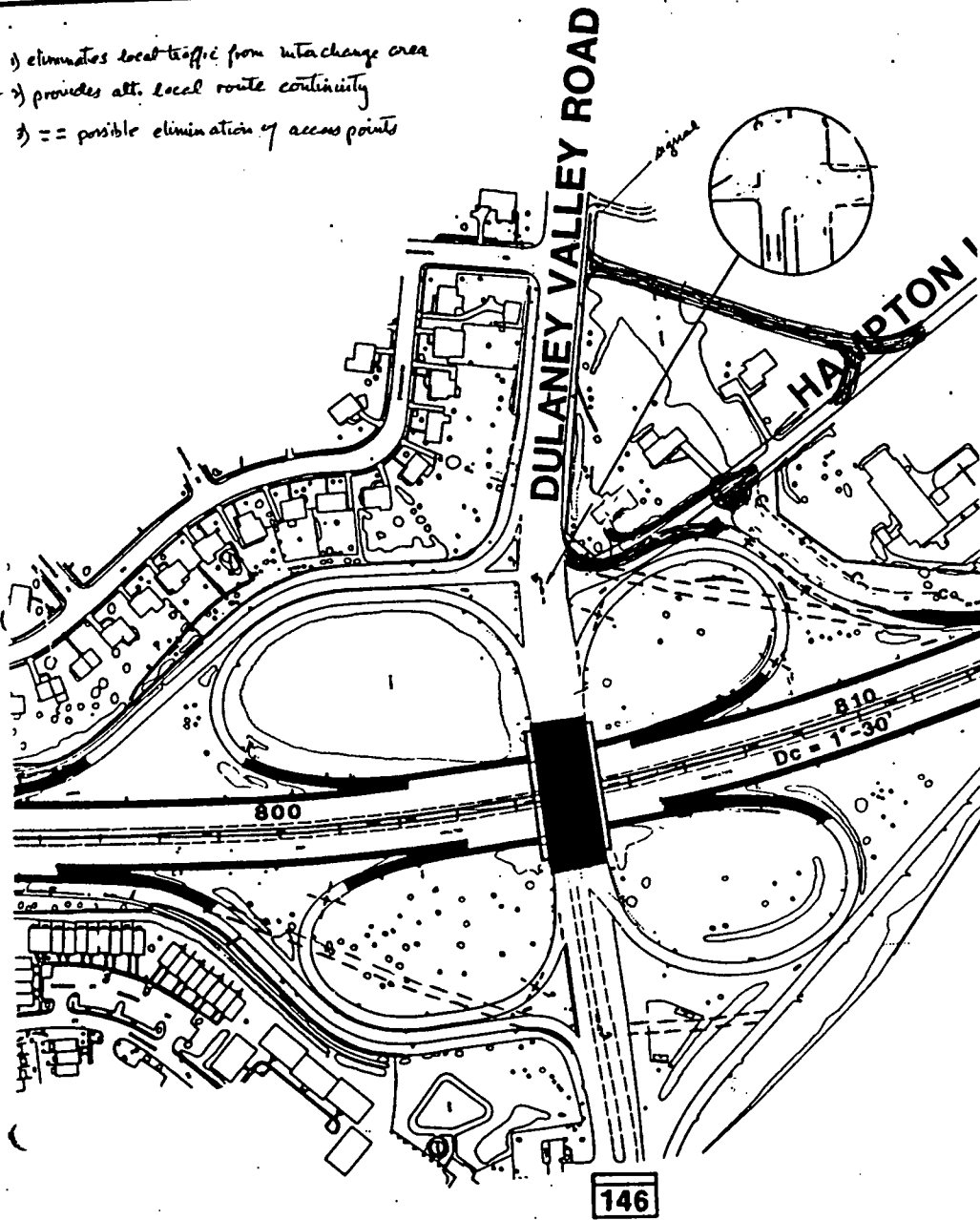
Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

attachments
 cc FHWA
 OPPE

4
 199

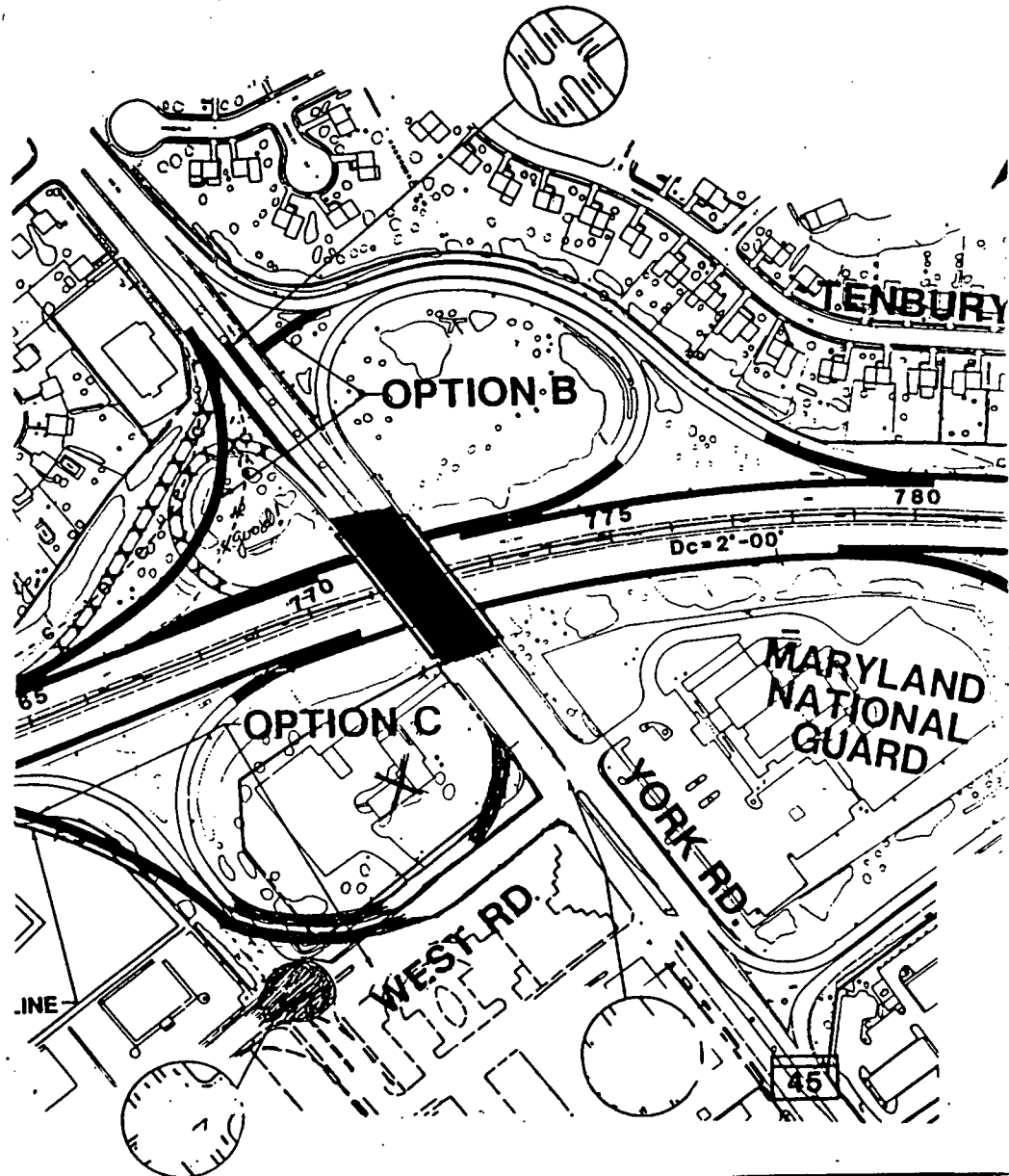
- 1) eliminates local traffic from interchange area
- 2) provides alt. local route continuity
- 3) == possible elimination of access points



Response:

1. A similar option was investigated and dropped due to right-of-way impacts.

cul de sac (eliminates local traffic from interchange area) and/or connect 2 lanes relocated w/ road to York @ Fairmount (provides local route continuity)



Response:

1. This option was also looked at by the Planning Team and dropped due to traffic operations on West Road.

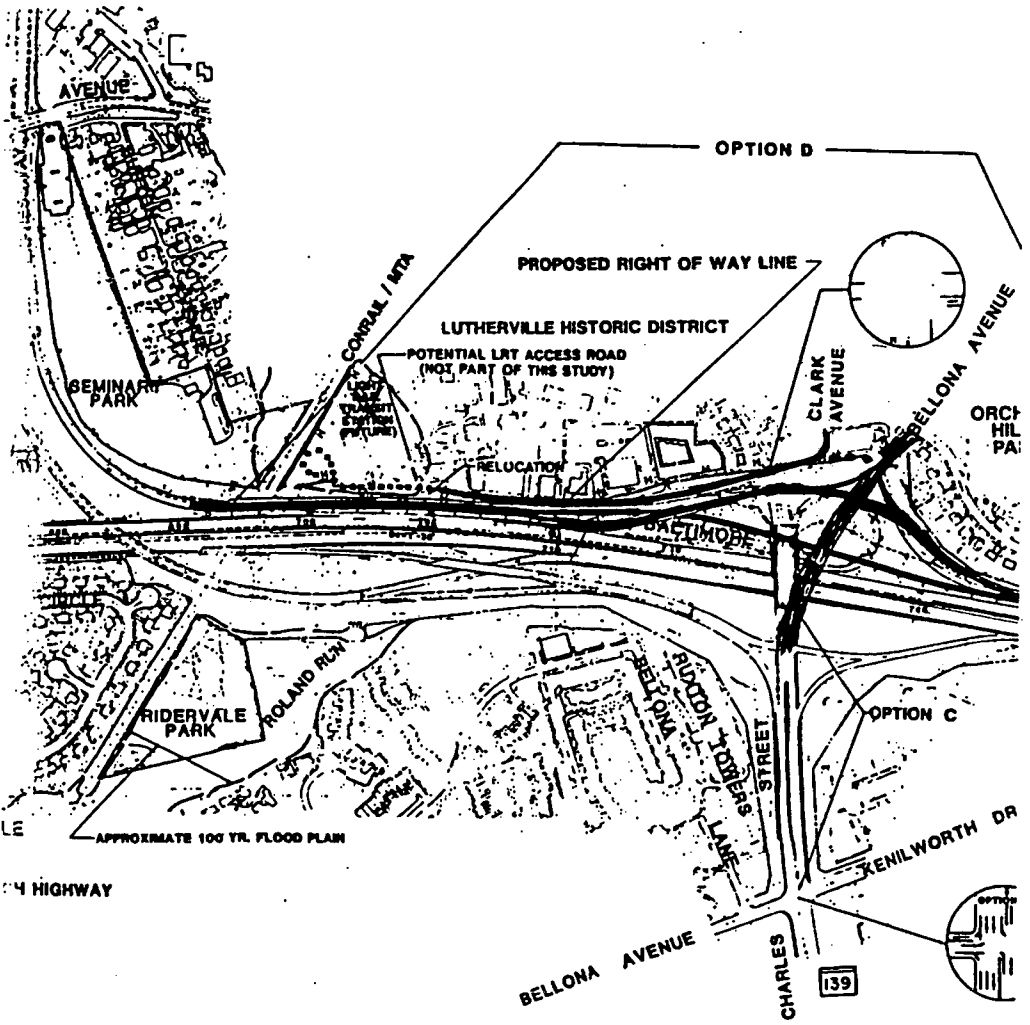
2000

Response:

1. This option was investigated and dropped due to public opposition and impacts to the Lutherville Historic District.

Response:

1. Alternate 2A was not selected.



- 1) orientates Charles to Bellona (eliminates major dogleg + establishes route continuity)
- 2) separates local service traffic from interchange area (via Clark/Lincoln to Bellona - widen Bellona to Lincoln; or direct connection to Charles Rd? ...)
- 3) eases traffic maintenance problem during const.

PROJECT DEVELOPMENT DIVISION
 STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

FEB 23 9 46 AM '90

CONTRACT No. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Charles L. Jackson DATE 2/17/90
 PLEASE PRINT ADDRESS 123 Dublin Drive
 CITY/TOWN Lutherville STATE Md. ZIP CODE 21093

I/We wish to comment or inquire about the following aspects of this project:

Page 29 of Study - Summary of Alternatives
- I-677 Beltway Md. MD to 702

ALTERNATE 2. Council Res. to MD 147
indicates 4.4 Acres of Residential Right-
Of-way is required.

What properties along Dublin Rd (Pg 137-17)
or Worshik Drive are involved
in land acquisition and
what is the distance (in feet) (additional)
required outside the existing Right of way.
for these properties.

In other words: How much closer are
the New Lines going to be to existing
property lines and exactly how much
VDZ does near roads will we lose?

- Please add my/our names to the Mailing List.
 Please delete my/our names from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Also, what happened to our Sound Barrier Wall - still no
Funding?

(5)
 231

Called him about these questions 2/16/90
PM

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

(H) 825-2560
(O) 785-7171

NAME Jeffrey P. Klose DATE 2/21/90

PLEASE PRINT ADDRESS 213 Meadowvale Road

CITY/TOWN Lutherville STATE Md ZIP CODE 21093

I/We wish to comment or inquire about the following aspects of this project:

I am going to attend the meeting at Loch Raven High School on the 28th of February but I would like a question answered anyway. As you can see I live on Meadowvale Road which is between York and Dulaney Valley Roads. I cannot understand why we do not have the noise abatement wall yet! Our neighbors between Providence Rd and Dulaney Valley have just had their wall completed. Their houses are much further away from the beltway than ours. Our neighborhood houses are closer together than Hampton's. I am not saying they should not get the wall but I was lead to believe it was based on soundings taken in our back yards. I know our back yards were louder than theirs. We still don't have the wall and now you want to widen the roads. Your excuse for us not having the wall was no funds. How can you come up with 135 million dollars to do these improvements? I am a very frustrated person

Please add my/our names to the Mailing List.*

Please delete my/our names from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

I checked this block before I noticed the star. Sorry!

would be greatly appreciated. Thank you for listening. Respectfully,
Jeff P. Klose

Response:

- 1. See Response on Noise Abatement on P.V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME George W. Black, Jr. DATE _____

PLEASE PRINT ADDRESS 3 Southerly Ct. #201

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

The crossing of traffic on westbound I 695 exiting to southbound Dulaney Valley Rd. and northbound Dulaney Valley Rd. traffic entering westbound I 695 has been a dangerous situation for a long time and is not relieved by MD 146 Option. The same is true on the south side of I 695 at this interchange. With the expansion of Four Seasons Center and the new construction in Towson proper, this dangerous interchange will only get worse.

A new configuration of this interchange should be designed to eliminate the crossing traffic on both sides of I 695. The result would be improved traffic flow, uninterrupted, on I 695 and easier entrance and exit for Dulaney Valley Rd traffic. Most of all, the chance of a fatal incident should be greatly reduced.

Please add my/our names to the Mailing List.*

Please delete my/our names from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. A C-D network through Towson was investigated and dropped due to traffic operations.

V-6

30
7

Response:

1. This has been completed.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCK RAVEN SENIOR HIGH SCHOOL

NAME MARK A. SHAPIRO DATE 2-22-90

PLEASE PRINT

ADDRESS 3417 MANOR HILL RD

CITY/TOWN BALTO STATE MD ZIP CODE 21208

I/We wish to comment or inquire about the following aspects of this project:

When will block top be put on the area of I-695 between Stevenson Road in Pikesville and Green Spring Ave.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

already on list SHAM019 B 4 08 P2 30

(8)

Response:

1. See response for Noise Abatement on P. V-2.
2. Air quality analysis has determined that the selected build alternate has lower CO emissions than the no-build.

State Roads Commission et al. February 22nd, 1990
Re: Hearing on Feb. 28th, 1990..

Dear Sirs,
In the record and for appeal purposes, I am opposed to any widening of the #695 road/Highway that would cause me to lose additional land.

In addition, the very suggestion of allowing more traffic so close to my residence is extremely unhealthy and devastating to my health and my real estate value.

I have been subjected to undue hardship due to the State Roads Highway projects from the very first day in 1945, when this major noise and air pollution began.

Prior to this date, the quiet suburban beauty of my residence was a joy to behold. There were deer and birds of many kinds and the quietness that caused us to build our home here in the first place back in 1950.

Now I am plagued with the extreme, continuous noise all day and all night from this heavy traffic. There is a gradual hill before the Green Spring exit and the trucks on #695 change gears about level with my property. I have had studies done to record the decibels incurred, and all show a noise level above and beyond any normal highway findings.

In addition, the loss of privacy, the reason for building out here in Baltimore County in the first place, has diminished with each additional lane enlargement.

I have suffered three ^{major} fire and numerous acts of vandalism and numerous thefts, which have all occurred since the Beltway has been in operation.

I have come home to my home being burglarized and narrowly escaped being held captive myself.

All this has occurred, I allege, because the

(9)

203

② State Roads Commission has not protected me from the criminal element who look for places to rob, by allowing the road side lane to be level with my home and not supplying plantings or trees to prevent a clear view into my home.

No effort to put up any barriers or ~~barriers~~ protection devices have ever been made here.

I hold the State Roads Commission, their engineers, their safety protection divisions, directly responsible for allowing cars to have clear access to my private home and invasion of my privacy by ^{view}.

Naturally, when the police show up the intruders either lie to the officers or feign pleading a ^{mistake} and say they are waiting for some one, and ~~nothing~~ ^{nothing} in fact. This fear which followed the loss of over 1/2 million dollars in furniture, clothes, artifacts and property has not gone away because this condition due to the Beltway has only increased not decreased in travel.

The exhaust fumes from the trucks and cars and the constant accidents that occur and the carbon monoxide fumes which have increased each year, tend to cause tension and breath problems that should never been allowed in the first place.

My health has been in jeopardy and continues to be in jeopardy from this main highway, with very minimal supervision on exhaust fumes and noise factors.

I can not recall ever seeing anyone from the State Roads Commission taking note of these dangerous levels of poisonous gases at my residence level, or the noise.

If this is not enough to cause my property's real estate value to be depreciated over 80%, then there is the sign of Green Spring exit (Exit 22) which gives light all thru the day and night to the access of my property and causes me the loss of each moment of privacy and sleep.

③ In addition, the chemical spraying of the natural vines and trees on the Beltway side, with poisonous toxic elements unknown to me, have caused my trees and vines on my side of my property, to die and never come up again.

Again my air is violated and the State Roads Commission has never once alerted me as to the time or date of this toxic attack on the vegetation or my breathing of poisonous substances/pollution.

In addition, when ever the State Roads Commission decides to have some men cut down the weeds or trees along my property line and the Beltway, there are incidents where these men select themselves by the wire fencing along my property, no arrangement seems to have been made to curtail this disgusting practice by the State Roads Commission workers.

In addition to this, these same workers try to chase and disturb my guard dogs, which is another invasion of my privacy.

At no time has anyone even suggested landscape planning to avoid even a little of the noise or the ~~unpleasant~~ continual view of cars racing and trucks grinding under too much tonnage. No buffer/dividers of concrete or barriers of any nature have appeared here at Green Spring as I have seen at many, many other sites along this Beltway.

When I asked for these barriers to be installed, I was told that due to the 'density factor' and so few persons living here, it was not economically feasible. Well, possibly litigation could be even more expensive.

There must be some way that the State Roads Commission can do better than they have in the past.

I have lost many, many acres to eminent

Response:

- 1. No response is required.

41.



United States Department of the Interior

OFFICE OF THE SECRETARY
WASHINGTON, D.C. 20240



FEB 12 1990

FEB 15 1990

ER 90/117

Mr. A. Porter Barrows
Division Administrator
Federal Highway Administration
711 West 40th Street, Suite 220
Baltimore, Maryland 21211

Dear Mr. Barrows:

This is in regard to the request for the Department of the Interior's comments on the Draft Environmental Statement concerning I-695 (east of SR-140 to west of SR-702), Baltimore County, Maryland.

This is to inform you that the Department will have comments, but will be unable to reply within the allotted time. Please consider this letter as a request for an extension of time in which to comment on the statement.

Our comments should be available about late March.

Sincerely,

Jonathan P. Deason
Jonathan P. Deason
Director
Office of Environmental Affairs

cc: Mr. Neil J. Pedersen
Director
Office of Planning and Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

domain is the name of "Progress" - Only \$40,000.00 was offered for over 11 acres and then my legal fees on top of that - this is Big Brother in his ugliest disgusting worst.

And now you propose to take even more land away from me plus adding nothing but more noise, more pollutants, more view for thieves to view my home and my privacy, more exhausts to destroy the good air that the trees which you have destroyed worked so hard to purify.

I am disgusted with your lack of concern for the taxpayers and private property.

I find your actions greedy and ill advised. I can not find one good reason why the State Roads Commission is entitled to the huge amount of money attributed to you by the politicians for what you accomplish.

Respectfully,
Dr. M. B. Lina

Box # 2974
Baltimore, Md.
21203

RECEIVED

ARCHITECTURE DIVISION

10

25

6-V

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

FEB 28 11 45 AM '90

NAME ARTHUR A. HACHTEL, JR DATE 2/22/90

PLEASE PRINT ADDRESS 8909 HARFORD Road.

CITY/TOWN BALTO. STATE MD ZIP CODE 21234

I/We wish to comment or inquire about the following aspects of this project:

What plans have been made for mass transit between Belair & Hunt Valley. There is no way today to get from Belair Road to Towson, or Hunt Valley unless you drive.

The widening of Harford Road is of interest to me. How much property is to be taken? From which side of the road? Who will pay for sidewalks? Will parking be prohibited? Will our taxes be affected?

The big question is why should we, the property owner & tax payer have to pay for these changes because people want to move to Belair or Hunt Valley? Raise the taxes for those people to pay for these changes.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Already on List HAC002 B 4 08 P2

Response:

1. Mass transit is not part of this study.
2. MD 147 Option B will widen to both sides and replace any sidewalk impacted.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Dr. M. B. Fine DATE 2/23/90

PLEASE PRINT ADDRESS Box #2274

CITY/TOWN Balto. STATE Md. ZIP CODE 21203

I/We wish to comment or inquire about the following aspects of this project:

Please contact the Landscape Chief for a copy of the letter I sent to his attention, about the list of reasons that I am against any more use of my land.

The Landscape Chief just proposed to be the name to whom I sent my objections - my objections were directed to all of the State Highway Administration and to be read into the minutes of the hearing coming on Feb. 28th, 1990.

Please see the Certified registered letter mentioned

Box #2274
Balto, Md.
21203

Respectfully
Dr. M. B. Fine

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Already on List FINM004 B 4 08 P2

Response:

1. Dr. Fine's comments precede this page.

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12

Response:

1. Traffis operations did not require these improvements 15 years ago.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME TREVOR W. SMITH DATE 2/23/90

PLEASE PRINT ADDRESS 1214 HARWALL RD.

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21207

I/We wish to comment or inquire about the following aspects of this project:

WHY WASN'T THIS PROJECT DONE 15 YEARS AGO?

V-11

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mr. & Mrs. Wayne R. Porter DATE 2-24-90

PLEASE PRINT ADDRESS 8339 Tally-Ho Road

CITY/TOWN Lutharville STATE Maryland ZIP CODE 21093

I/We wish to comment or inquire about the following aspects of this project:

Attention should be directed to Page 9, I-83 (JFX) to I-83 (HX) which states "Environment impacts .. are subject to change during the design phase". Over the past 20 years, the Beltway has been widened from 2 lanes to the existing 5 Roadway Lanes and is now proposed to be widened to 6 Roadway Lanes on either side. The Seminary Ridge Community along this stretch has consistently been told by the SHA that it does not qualify for NOISE ABATEMENT BARRIERS because the Beltway was here before the development. While that may be true, it is fact that the NOISE LEVEL has already been increased by 150% and the proposed widening will result in a 200% increase in NOISE LEVEL. Traffic noise has made our yards virtually worthless because we are held prisoners within our homes in our efforts to shut out some of the noise.

The SHA Bureau of Landscape Architecturs reports that the entire I-83 (JFX) to I-83 (HX) segment is now identified as a NOISE IMPACTED AREA; however, there is a lack of funds and, when funds do become available (time unpredictable), prior-approved areas will be accommodats first even though the noises impact is not as severe as between the two I-83's. A 12-13-89 article in The Baltimore Sun identified this area as the No. 1 Traffic Hot Spot on the Beltway with 175,000 cars moving through every day (more than 2 per second 24 hours a day), only 10 feet from our property.

Alternate 2 - Beltway Widening MUST INCLUDE NOISE ABATEMENT BARRIERS between I-83 (JFX) and I-83 (HX).

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochurs through the mail are already on the project Mailing List.

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February 27, 1990

Mr. Donald G. Honeywell, Project Manager
Maryland State Highway Administration
707 N. Calvert Street
Baltimore, MD 21202

RE: ROUTE 695 ROAD IMPROVEMENTS
CHARLES STREET-BELLONA AVENUE INTERCHANGE

Dear Mr. Honeywell:

Thank you very much for meeting with me yesterday regarding the above and its impact on the office buildings located at 1300-1306 Bellona Avenue. As you know, we represent the owners of this property, Beltway Investors General Partnership, and are writing to express our opinions in lieu of attending the public hearing scheduled for February 28, 1990.

We understand that two modifications to Bellona Avenue are under study as they relate to the widening of Route 695, access ramps to 695, and the addition of a light rail transit station proposed by MTA. We understand that SHA has no particular jurisdiction over the MTA plans regarding the light rail transit station, except as it relates to the access road which will be the new Bellona Avenue directly adjacent and in front of the four office buildings owned by Beltway Investors General Partnership.

Please be advised that none of the plans under review and consideration, including the MTA light rail transit station, will have a positive effect on our property. These plans, in some form, will require the taking of portions of our property and will result in significantly increased traffic flows much of which will be transit buses.

Notwithstanding our overall feelings, we are writing to strongly protest one version of the plans under consideration in favor of another version. Specifically, the "lane continuity" approach proposed by the Federal Highway Administration would have the most detrimental effect on our property requiring large takings of land and rendering two of the four office buildings unusable. Once again, we strongly protest this version of the plan and, respectfully, request that you do not consider it further.

Real Estate

7 East Rutwood Street, Suite 1200, Baltimore, Maryland 21202, (301) 752-4285
FAX: (301) 576-9031

Mr. Donald G. Honeywell
February 27, 1990
Page Two

If we are to accept revisions to Bellona Avenue, we would, therefore, support the plan which places the new Bellona Avenue substantially south of the plan proposed by FHA and one which essentially keeps the primary portions of the existing Bellona Avenue in tact.

We hope this letter will be useful for your purposes. Please do not hesitate to contact me directly with any other information with respect to these road improvements. Again, many thanks for your time yesterday.

Very truly yours,

W. C. PINKARD & CO., INC.

David R. Frederick
Executive Vice President

DRF/cg

Response:

1. The FHWA "lane continuity" is I-83 (HX) Option D with four through lanes versus three through lanes. The three through lanes option was selected.

V-12

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Response:

- I-83 (JFX) Option C was selected. Neither Option A or B was selected.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Tina Cornwell DATE 2/28/90

PLEASE PRINT ADDRESS 1932 Old Court Rd
CITY/TOWN Baltimore STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I-83 (JFX) Options A, B, C, D
I support option A and either C or D as
improvements to the interchange, preferably D.
Option B would severely limit our
privacy. The noise level and visible traffic
would lower our property level and limit our
enjoyment of our land. Air quality is
already a concern. We already have a noise fence from the JFX.
Therefore I oppose Option B.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- See the response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Steven McClines DATE 2/28/90

PLEASE PRINT ADDRESS Beaver Bank Circle 945
CITY/TOWN Towso STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I think this whole thing is pretty stupid, I know you
will not listen to me because I am very young. But here are
me through after school I walk home on Cornwell
bridge road and one time I saw 7 deer hiding trapped
between the beltway and exit at the intersection. When I
ride my bus to school we go by work men working on the
walls near Lutheran High school this blocks our bus and
makes us late to school. I agree about the walls but you
can't take 12 crans and tractor and squeeze them in
one place. I do not live very near the beltway I live about
a half a mile away and I can still here the noise.
The smell when I walk home is horrible I try to get
out of that area quick. I like to ride my bike and can't
ride on weekdays because of the traffic from the
beltway. Thank you

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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V-13

Lutherville Community Association

P/O Box 6
Lutherville, Maryland 21093

28 February 1990

Mr. Louis H. Ege, Jr.
State Highway Administration
Baltimore, MD 21202

Dear Mr. Ege:

We want to thank you and Mr. Pedersen for sending the Association (and to me personally) the Environmental Assessment and the green booklet for this public hearing.

I concur in the assessment statement, page IV-12 "The SHPD has made a determination of no adverse effect ..." Quite the opposite. The SHA had made every effort to address the concerns of the Lutherville Community Association and has rejected the flyover. The proposed alternate D is quite acceptable and it would seem that Option D "modified" is required for real safe traffic flow.

It might be noted that the Baltimore County Historic District is considerably smaller than the Federal District. The County district is confined to the contiguous boundaries of the truly historic properties. The Creighton Springhouse is historic but outside County boundaries.

Since Option D "modified" was proposed we have been told by MTA that the beltway stop for light rail has been deferred and probably will never be built. Perhaps the sister departments of SHA and MTA need to communicate further on this option.

For real accuracy in the documents submitted to us I would like to point out several things:

There is a historic black congregation church on Bellone Ave. that should appear on Figure 1-5. Also St. John's Methodist on Seminary Ave. just west of Bellone was omitted from 1-5.

The country club which lies along the edge of the ramp from eastbound beltway to northbound I-83 is simply Valley Country Club (do not confuse it with something in Greenspring Valley).

Please keep us informed as progress continues. We might want to meet and confer about the impact of option D Modified. Right now it seems appropriate.

Sincerely,

Robert W. Gifford

515 Spring Ave.
Lutherville

Response:

1. I-83 (HX) Option D has been selected.

MEMBERS OF THE PROJECT PLANNING TEAM

MY NAME IS NORRIS LANKFORD WITH HOME RESIDENCE AT 2310 WEST JOPPA RD.

I WISH TO PROVIDE SOME OBSERVATIONS WHICH FOLLOWED MY REVIEW OF YOUR BALTIMORE BELTWAY STUDY CONTRACT NUMBER B63S-101-472.

IN GENERAL IT APPEARS THAT CONSIDERABLE INCREASE IN HANDLING CAPACITY FOR BELTWAY TRAFFIC WILL RESULT FROM YOUR BELTWAY WIDENING ALTERNATIVE 2 AND, ADDITIONALLY, SAFETY SHOULD BE ENHANCED BY THE RAMP AND INTERCHANGE MODIFICATIONS.

THE MOST SIGNIFICANT OTHER FEATURES OF THE PROPOSAL ARE THE MINIMUM ADDITIONAL RIGHT OF WAY REQUIREMENTS AND NEGLIGIBLE AFFECTS ON EXISTING IMPROVEMENTS AND THE ENVIRONMENT.

THE PROPOSED BELTWAY WIDENING FROM ROUTE 140 TO ROUTE 702 IS FOR THAT SECTION OF THE BELTWAY CARRYING EAST TO WEST AND WEST TO EAST TRAFFIC. ON PAGE 6 OF THE REPORT IT IS PREDICTED THAT THE PROPOSED IMPROVEMENTS WILL PROVIDE SIGNIFICANT IMPROVEMENTS UNTIL AROUND THE YEAR 2015 WHEN THIS EAST- WEST TRAFFIC AT PEAK MAY EQUAL OR EXCEED PRESENT CONDITIONS.

WHILE SUPPORTING THE ALTERNATIVE 2 PROPOSAL AND FINDING THE ANTICIPATED TRAFFIC INCREASE REASONABLE, I WISH TO OFFER SUPPLEMENTARY IDEAS TO THE CONCERNS FOR FINDING FUTURE CAPACITIES FOR HANDLING EAST- WEST TRAFFIC. THESE CONCERNS ARE EXPRESSED IN THIS PROJECT REPORT AT THE TOP-RIGHT OF PAGE 6 AT THE END OF THE SECTION ON ALTERNATIVE 2 FOCUS OF CONCERN IS A STATEMENT THAT IN MOST AREAS IT IS "NOT FEASIBLE TO CONSIDER MAINLINE SECTIONS GREATER THAN 4 CONTINUOUS LANES IN EACH DIRECTION."

I PROPOSE, FIRST, THAT EVENTUALLY WE CAN PROVIDE PROTECTION FOR OUR LAND MORE EASILY IN MARYLAND, A SMALL STATE, THAN CALIFORNIA DOES. THE STATE OF CALIFORNIA IS NOT ONLY THE THIRD LARGEST IN THE UNION BUT HAS MORE THAN FIFTEEN TIMES THE LAND AREA OF MARYLAND. IN SPITE OF HIGH EARTHQUAKE POTENTIAL, RECENT LARGE EARTHQUAKES, AND THE WARNINGS OF ENGINEERS, THEY HAVE BUILT AND PLAN TO REBUILD DOUBLE DECKER HIGHWAYS TO SAVE LAND FOR OTHER PURPOSES. COST OF REBUILDING AND EVEN SAFETY IS DISREGARDED.

IT IS MY PROPOSAL THAT, FOR THE FUTURE, THE OPTION OF PARTIAL SECTIONS OF FLYOVER OR DOUBLE DECKING FOR EAST- WEST BELTWAY BE CONSIDERED AND RETAINED IN CONSIDERATION.

Response:

1. An outer Beltway or double decked Beltway was not part of this study.

IN MARYLAND WE HAVE ZERO EARTHQUAKE RISK AS WELL AS MILD WINTER WEATHER. NEEDS OF LAND FOR HOUSING, AGRICULTURE, AND OTHER USES ARE TOO GREAT TO WASTE SPACE FOR INAPPROPRIATE TAKING OF SIGNIFICANT NEW RIGHT OF WAYS FOR HIGHWAYS. I WISH TO HAVE INCLUDED IN FINAL DESIGN SPECIFICATIONS OF ALTERNATIVE TWO THE FUTURE DOUBLE DECK CONSIDERATIONS.

IT MAY BE, HOWEVER, THAT FUTURE EAST WEST TRAFFIC NEEDS (SUBURB TO SUBURB AND SUBURB TO INDUSTRIAL PARK) WILL BE REQUIRED FURTHER NORTH IN THE FORM OF A SECTOR OF AN OUTER BELTWAY. EVEN IF THIS IS A MORE LIKELY NEED FOR FUTURE EAST- WEST HIGHWAY TRANSPORT, A NEW NORTHERN OUTER BELTWAY SECTOR COULD BE MADE IN TWO DECKS TO MINIMIZE NEW LAND ACQUISITION. WHETHER ONE DECK OR TWO, SUCH A NORTHERN, EAST- WEST HIGHWAY WOULD ALSO MITIGATE UNDESIRABLE BUILD LEVELS-OF-SERVICE ANTICIPATED FOR THE DESIGN YEAR PEAK HOURS.

AGAIN I WISH TO COMMEND THE PLANNERS OF THIS STUDY REPORT ON THE DETAILS OF THE CURRENT ALTERNATIVE TWO PROPOSAL WHICH SHOULD COVER ALL BUT THE LONG TERM CONCERNS ADDRESSED ON PAGE 6 AND HEREIN.

RESPECTFULLY SUBMITTED,

Norris B. Lankford

NORRIS B. LANKFORD

FEBRUARY 28, 1990

Response:

1. See response for Noise Abatement on P. V-2.

PETITION

We, the residents of Marcie Woode, Pikesville, Maryland, are concerned about the proposed addition of two lanes to the Baltimore Beltway adjacent to our development. Increasing the number of lanes in the Beltway will have a direct and adverse affect on the amount of traffic and the noise level emanating from Beltway traffic adjacent to our development, as well as the safety of our residents. There have been instances where drivers whose cars have broken down on the Beltway adjacent to our development have crossed the fence and trespassed on to our property. We oppose the addition of additional lanes to the Beltway adjacent to Marcie Woode. Further, in the event that additional lanes are constructed, we respectfully demand that a noise barrier be erected to curtail the increased noise level from increased traffic flowing on an increased number of lanes as well as to ensure the safety of those of us living in Marcie Woods.

Name	Address
<i>John W. Steyer</i>	<i>20 Marcie Woods Ct</i>
<i>Wesley August</i>	<i>20 Marcie Woods Ct.</i>
<i>Michael J. Bayen</i>	<i>3908 REDSPIRE LANE</i>
<i>John D. ...</i>	<i>3308 Redspire Lane - 21208</i>
<i>Paul Bloom</i>	<i>3305 Redspire Lane 21208</i>
<i>John ...</i>	<i>3105 Redspire Lane 21208</i>
<i>Cathy Kalish</i>	<i>3302 Redspire Lane 21208</i>
<i>Joe ...</i>	<i>1 MARCIE WOODS CT 21208</i>

V-15

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PETITION

We, the residents of Mercie Woods, Pikesville, Maryland, are concerned about the proposed addition of two lanes to the Baltimore Beltway adjacent to our development. Increasing the number of lanes in the Beltway will have a direct and adverse affect on the amount of traffic and the noise level emanating from Beltway traffic adjacent to our development, as well as the safety of our residents. There have been instances where drivers whose cars have broken down on the Beltway adjacent to our development have crossed the fence and trespassed on to our property. We oppose the addition of additional lanes to the Beltway adjacent to Mercie Woods. Further, in the event that additional lanes are constructed, we respectfully demand that a noise barrier be erected to curtail the increased noise level from increased traffic flowing on an increased number of lanes as well as to ensure the safety of those of us living in Mercie Woods.

<u>Name</u>	<u>Address</u>
<i>Ann Taylor</i>	5 Mercie Woods Ct 21208
<i>David Hill</i>	" " " "
<i>Bill J. Lamb</i>	7 Mercie Woods Ct. 21208
<i>Mindy Hays</i>	7 Mercie Woods Ct 21208
<i>Brian H. Hoff MD</i>	16 Mercie Woods Ct 21208
<i>Megan J. Hoff</i>	16 Mercie Woods Ct 21208
<i>Wendy</i>	15 Mercie Woods Ct 21208
<i>Patricia</i>	15 Mercie Woods Ct 21208
<i>Kathleen M. Matz</i>	19 Mercie Woods Ct. 21208

Response:
1. See previous page.

PETITION

We, the residents of Mercie Woods, Pikesville, Maryland, are concerned about the proposed addition of two lanes to the Baltimore Beltway adjacent to our development. Increasing the number of lanes in the Beltway will have a direct and adverse affect on the amount of traffic and the noise level emanating from Beltway traffic adjacent to our development, as well as the safety of our residents. There have been instances where drivers whose cars have broken down on the Beltway adjacent to our development have crossed the fence and trespassed on to our property. We oppose the addition of additional lanes to the Beltway adjacent to Mercie Woods. Further, in the event that additional lanes are constructed, we respectfully demand that a noise barrier be erected to curtail the increased noise level from increased traffic flowing on an increased number of lanes as well as to ensure the safety of those of us living in Mercie Woods.

<u>Name</u>	<u>Address</u>
<i>Elina V. Matz</i>	19 Mercie Woods Ct. 21208
<i>Toni J. Sapier</i>	3 Mercie Woods Ct 21208
<i>Margaret J. Zimm</i>	2 Mercie Woods Ct. 21208
<i>Janet L. Zimm, M.D.</i>	2 Mercie Woods Ct 21208
<i>Charles J. Zimm</i>	6 Mercie Woods Ct 21208
<i>David R. Zimm</i>	3304 Lakeside Lane 21208
<i>Wendy L. Zimm</i>	3 Mercie Woods Ct. 21208
<i>Ellen H. Zimm</i>	18 Mercie Woods Ct. 21208
<i>Henry M. Zimm</i>	18 Mercie Woods Ct 21208

Response:
1. See previous page.

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V-16

Response:

1. Coordination with Towson Development Corporation has continued.



TOWSON
DEVELOPMENT
CORPORATION *a nonprofit community improvement organization*

29 West Susquehanna Avenue • Suite 10 • Towson, MD 21204 • (301) 811-7271

TESTIMONY BY
TOWSON DEVELOPMENT CORPORATION
at Beltway Widening Public Hearing
February 28, 1990

Towson Development Corporation with its 55 member Board and 11 member Executive Committee wishes to go on record as favoring, in general SHA's widening of the Beltway to 4 lanes in each direction in the vicinity of Towson Towncenter.

Our corporation has over the past ten plus years been in regular touch with the appropriate County and State transportation planners regarding highway and transit improvements in our area. We have had our transportation and transit committees review proposals by DOT, SHA, MTA and others and feel that we are in a good position to comment upon this current Beltway proposal.

Towson towncenter is prospectively destined to double in size over the next twenty years. As other communities in the drainage (access) shed of the Beltway also take on additional growth, it is clear that we must have a Beltway that can accommodate expanding vehicular traffic needs. Therefore, we are in full accord with the widening of the Beltway, the so-called "fourth laning". We see that maximizing the capacity potential of existing roadway rights-of-way, along with new fixed guideway transit, as offering the best prospects of keeping pace with increased travel demands in Baltimore's growing suburbs. In light of our support for the Beltway widening improvement, we pledge to work with you both to find the appropriate timely funding of the project as well as integrating the community's working around the construction interruptions and complications.

We have some comments about the detailed design as presented. By the way, we consider the four interchanges from Charles Street to Providence Road as the feeder access gateways to Towson's towncenter. We'd like to see the design planning for roadways feature or emphasize Dulaney Valley as the primary ingress roadway for Beltway travelers from the east and York Road for travelers from the west.

Relative to the Charles Street interchange area, we feel that there should continue to be an access road from Charles Street to the prospective Beltway Station for the Northern Central Light Rail project. While there may be delays in building that station, we see it as the bus connector station for Towson towncenter. So we expect SHA will continue to show a road connector to that future transit station.

In regard to the York Road interchange, we understand why in Option B there is proposed replacement of the northwest loop ramp with a signalized left turn spur in the northeast quadrant. However, this causes us a great deal of concern! We're worried that the addition of a traffic light at this spot on York Road further exacerbates the traffic conditions on York Road. We ask you to share expected traffic loading on that new crossover of York Road and its impact so that we may be fully briefed on the difficulty it may cause.

We see no complications with the Dulaney Valley and Providence Road interchanges.

Basically our primary interest is in having a Beltway whose interchanges work well and to also have roadways of adequate capacity that feed and drain the interchanges well. In our area, we're very worried about York and Dulaney Valley Roads being sized sufficiently to handle traffic loads ten to twenty years from now. Therefore, we ask SHA to put on its priority study agenda a comprehensive assessment of access problems southerly from the Beltway into our towncenter Loop Road system. Please let us know when this might be scheduled.

Finally, we ask that SHA sign the Beltway to alert drivers coming to Towson towncenter that they have a choice of several interchanges to get to Towson. We think you should install signs which say "Towson - Next 3 (or 4) Interchanges". We'll work with you on specific wording and placement.

Thank you,

Jerry Rescigno
Jerry Rescigno and

Alan Shecter
Alan Shecter

JR:AS:rs

V-17

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NADER G. GARY, M.D., P.A.
DISEASES & SURGERY
OF THE EYE

Diplomate of American
Board of Ophthalmology

Fellow of American
Academy of Ophthalmology

NADER G. GARY, M.D.
DANA TAYLOR, O.D.

100 N. BROADWAY
BALTIMORE, MD 21231
342-4900

4000 ANNAPOLIS RD.
BALTIMORE, MD. 21227
789-0303

Diplomate of American
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Academy of Ophthalmology

NADER G. GARY, M.D.
DANA TAYLOR, O.D.

NADER G. GARY, M.D., P.A.
DISEASES & SURGERY
OF THE EYE

100 N. BROADWAY
BALTIMORE, MD 21231
342-4900

4000 ANNAPOLIS RD.
BALTIMORE, MD 21227
789-0303

February 27, 1990

Maryland Department of Transportation
State Highway Administration

Extension of I83 North-adding another lane to 695 or I83, building a flyover ramp connecting 83 North and 695 South will not improve the traffic jams in peak hours. Instead, it will have an adverse effect on property values of Ruxton and surrounding communities leading to less taxes on these homes, increased noise and increased air pollution.

The logical solution to the congestion is to improve the collaterals, rather than increase the size of major arteries. If major arteries and collaterals cannot drain well, there will be a congestion in the main artery. The following factors play a major role:

1. Exits are not able to drain out of 695
2. Too many stops or traffic lights immediately after exit from 695.
3. Town or city streets are not able to handle the load of cars coming out of major arteries.

Example of #1 Factor: New York exit from 695 is totally inadequate. It creates a back up for 5-6 miles every day between 4:30 & 6:00 pm. Following exit from 695 towards New York, there is congestion on 95 itself. By adding another lane to 695, this problem will not be solved.

Example for #2 Factor: On York Road, Charles Street and Kenilworth Avenue, traffic lights and stop signs slow the traffic down.

continued.....

Response:


1. I-83 (JFX) Option B was not selected.

Example for #3 Factor: Traffic lights, local stops and right turns into local businesses, gas stations, McDonalds, etc will slow down the traffic.

In Summary: Adding to major arteries, specially on 83 North and South to 695 will not solve the problems. There has to be:

1. Good exits
2. Good collaterals
3. Good drains
4. Possible transit officers to guide the traffic in peak hours.
5. Prohibit commercial vehicles to commute during peak hours
6. No increase in number of lanes to major arteries

I strongly oppose the proposal to build new roads, specially at the junction of 83 and 695.


Nader G. Gary
2 Roland Court
Ruxton, MD 21204

P.S. If the projection is correct, there is a need for a 2nd beltway 5-7 miles outside the present beltway, which economically is more feasible.

81-V

(2)

HIT

Response:

1. Access to California Avenue from E.B. I-695 is via Putty Hill Avenue or East Avenue to Ayondale Avenue or by U-Turn.
2. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT DEVELOPMENT

FEB 28 1990

MAR 1

NAME James F Eagle DATE 2-26-90

PLEASE PRINT ADDRESS 3049 Arizona Ave

CITY/TOWN Baltimore STATE MD ZIP CODE 21234

I/We wish to comment or inquire about the following aspects of this project:

Coming EAST ON I-695 TO 147 (HARFORD RD) HOW CAN I
OR OTHER PEOPLE GET ON CALIFORNIA AVE.
WHAT DO I HAVE TO DO TO MAKE A U-TURN ON HARFORD RD
IF THE RAMP IS REMOVED NORTH OF I-695 I CAN NOT
EVEN MAKE A LOOP ON 695 TO HARFORD RD TO MAKE
A LEFT TURN ON TO CALIFORNIA AVE.

Thank.

James F Eagle

P.S. We NEED NOISE ABATING WALLS
FROM HARFORD RD TO BOWEN RD. SOUTH
SIDE OF I-695

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. MD 147 Option A was not selected due to traffic operations.
2. MD 147 Option B was selected. By Dec. 3/5/90 adding a center turn, safety will be improved for vehicles enter/exiting MD 147

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Joseph PORTERA DATE 3/1/90

PLEASE PRINT ADDRESS 2953 Edgewood Avenue

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21234

I/We wish to comment or inquire about the following aspects of this project:

I am for Option A for route 147 Harford Rd. Interchange,
I am against Option B which proposes to include
eliminating access to Harford Lane. I live on Edgewood Ave
and option B would create extra traffic on Edgewood Ave.
You would be cutting down access not only to Harford Lane
but Robert Rd also. With the off ramp on the proposed
Option B coming up to Harford Lane, automobile making
a left on Edgewood to go north would not have a chance.
You would be putting a great burden on those individuals
living on Edgewood Ave, Harford Lane, and Robert with
the increased amount of vehicles, service vehicles, emergency
vehicles would be at a disadvantage in their response.
One question I have is, why do you always put the
cut before the house, the County lets the builder build
houses on all the lots that are available, and then
they say, we have to widen the roads, why don't you
widen the roads and then build the houses. I agree
I am for Option A. RT-147. Definitely against Option B!

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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VI-19

2/5

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

MANI K. PULIMOOD, P.E.
 EIGHT - JOHNSON MILL ROAD
 BALTIMORE, MARYLAND 21204

(301) 823-2658

PROJECT DEVELOPMENT DIVISION
 MAR 6 3 30 PM '90

NAME Mani K. Pulimood DATE 3-1-90

PLEASE PRINT ADDRESS 8 Johnson Mill Road

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

REF: PROPOSED JFX / FALL ROAD INTERCHANGE:

ENCLOSED IS A COPY OF THE TESTIMONY I GAVE AT THE HEARING FOR YOUR FILES, REGARDING THE ABOVE PROJECT.

I STRONGLY FEEL THE PROPOSED CHANGES AT THIS INTERSECTION, ESPECIALLY OPTION B WILL HAVE A SERIOUS ENVIRONMENTAL NOISE IMPACT TO THE ADJACENT COMMUNITIES.

IN ADDITION TO THE ALREADY STATED IMPACT ON THE JONES FALLS STREAM AND ADJACENT WETLAND, DUE TO THE PROPOSED CONSTRUCTION OF THE RAMP, AND THE ENVIRONMENTAL IMPACT STUDY THAT WAS ALREADY DONE BY YOUR DEPARTMENT.

I WILL APPRECIATE IF YOU WILL KEEP ME UPDATED AS TO THE FUTURE ACTIONS - IF ANY TAKEN ON THIS PROBLEM.

IN THE MEAN TIME SHOULD THERE BE ANY QUESTIONS, REGARDING MY TESTIMONY OR CONCERNS, PLEASE FEEL FREE TO CONTACT ME.

Sincerely,
 Mani K. Pulimood

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

NOTE: I WILL APPRECIATE IF YOU WILL MAIL ME A COPY OF THE ENVIRONMENTAL IMPACT STUDY REPORT AS WELL AS ANY OTHER INFORMATION TO WHICH I AM ENTITLED.

Response:

1. I-83 (JFX) Option B was not selected.

TESTIMONY AGAINST PROPOSED I-83 (JFX) - MD25 (FALL ROAD) INTERCHANGE

My name is Mani K. Pulimood. And I live in close proximity to the above interchange.

I am a Registered Professional Engineer, practicing in Maryland for the past 20 years and one of my areas of expertise is Environmental Engineering and Noise Control.

During the course of my commute to work I use the above intersection and access 605 West through the existing access ramp from B3 (JFX) North. I would also like to go on record to state that I can not recall any back up at this ramp other than times during road construction.

Only traffic problem I experienced near the intersection of the Beltway is the natural slowing down of the entire Beltway traffic due to the steep up-ramp between Exit 23-22. In my opinion the proposed Option A or B at the intersection would not adequately correct the inherent bottle neck of the Beltway at this area.

I am particularly opposed to the proposed Option B, which utilizes a high speed fly over ramp from Northbound JFX to Westbound Beltway. Due to its environmental noise impact on the surrounding communities, by elevating a portion of the highway ramp the natural noise shield of the existing portion of the highway due to embankment and vegetation will be lost. It is my estimate that the increase in ambient noise due to the direct line of sight of the traffic in the immediate environment will be 6-8 dBA; or much more through the noise feedback and perceived noise levels in the adjacent communities. Further noise and environmental noise solutions such as noise walls are impossible to be implemented for higher elevations involved in the case of a flyover ramp. So it is my professional judgement that Option B at the above intersection should be dropped from further consideration.

Mani K. Pulimood February 28, 1990

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Response:

- 1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Willy M. Lombardi DATE 3/5/90
ADDRESS 28 BARDEEN CT
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

PLEASE PRINT

We wish to comment or inquire about the following aspects of this project:

I am definitely opposed to the addition of another lane to both Beltway 695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

- 1. MD 147 Option B was selected resulting in the closing of Grendon Lane. A connection between Grendon Lane and Edgewood Ave. would be constructed.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROD 001

NAME DOMINICK PROVINI DATE _____
ADDRESS 2950 Edgewood Avenue
CITY/TOWN Baltimore STATE Md. ZIP CODE 21234

PLEASE PRINT

We wish to comment or inquire about the following aspects of this project:

Detail design information regarding closing of Grendon Lane and the making of Edgewood Avenue two way traffic. Parking on Edgewood Avenue? Possibility of widening Edgewood Avenue?

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT
 DATE

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

MAR 6 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mrs Paula Bengel DATE 3/5/90

PLEASE PRINT ADDRESS 17 Chiara Ct

CITY/TOWN Towson STATE Md ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

Just opposed to the addition of
another lane to Balto beltway #695
unless barriers are built prior
to construction in all areas
that exceed Federal noise
guidelines

STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

MAR 6 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Eileen S Dezell DATE 3-5-90

PLEASE PRINT ADDRESS 41 Chiara Ct

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

The congestion on the
Baltimore Beltway is an
acknowledged fact. Additional
construction is necessary.
However, please do not
compound the problem by
destroying the life-style of
residents bordering the
Beltway by creating real
noise problems.
Please guarantee that
with the addition of
another lane on the beltway
there will be included
noise abatement barriers for
all areas affected.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION **PROJECT DEVELOPMENT DIVISION**
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 10 39 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME _____ DATE _____

PLEASE PRINT ADDRESS _____

CITY/TOWN _____ STATE _____ ZIP CODE _____

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another house on Beltsay, 695 unless barriers are built prior to construction in areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION **PROJECT DEVELOPMENT DIVISION**
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 10 39 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME HODGE C. MORGAN DATE 3-5-90

PLEASE PRINT ADDRESS 3 BELLOWS COURT

CITY/TOWN TOLOSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Dulany Towers was constructed when the Beltsay only had 2 lanes each way. Now there are 3+ with great added traffic & noise.

No added construction on the Beltsay between Dulany & York roads should be constructed unless noise abatement barriers are built beforehand.

In some the area already exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Patrick F. Campbell DATE 5/1/90

PLEASE PRINT ADDRESS 33 Theo Lane

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I would like to take this opportunity to notify the
State Highway Administration that I finally
believe that the Balt way should not be
expanded without providing adequate noise
abatement barriers.

Thank you,

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME BESS A. STANAS
PAUL G. STANAS DATE 3-5-90

PLEASE PRINT ADDRESS 27 THEO LANE

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of
another lane to Baltimore Beltway #695
unless barriers are built prior to
construction in all areas that exceed
Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME PATRICIA M. & SAMUEL E. SIBAYAN DATE 3/5/90

PLEASE PRINT ADDRESS 14 ALAN BROOKE COURT

CITY/TOWN TOWSON STATE MARYLAND ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to Baltimore Beltway #695, unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

- Please add my/our name(s) to the Mailing List.*
- Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See Response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME T. REVE Y. DUNCAN DATE 3/1/90

PLEASE PRINT ADDRESS 19 BELLOWS COURT

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Since I 695 has 2 lanes on each side with a grass plot in the middle, I feel that the state has a responsibility to erect sound barriers before proceeding to increase the lanes to four on each side.

Since the funds are predominately Federal funds, Federal guidelines should be used to determine the locations. I continue to be appalled at the cost of the noise barriers from Providence Rd to Dulany Valley Rd on the north side of the Beltway - for protection of Hampton Mansion that with a residence that is a greater distance from the beltway than homes where barriers have been refused.

No barriers, please no widening

- Please add my/our name(s) to the Mailing List.*
- Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Madeline L. Artigiani DATE Mar. 6, 1990

PLEASE PRINT ADDRESS 15 BELLONS Ct.
CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of
another lane to Baltimore Beltway #695
unless barriers are built prior to
construction in all areas that exceed
"Federal Noise Guidelines."

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.
*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ALBERT L. OSBORNE DATE 3-6-90

PLEASE PRINT ADDRESS 45 THEO LANE
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

"WE OPPOSE ADDITIONAL CONSTRUCTION
ON THE BELTWAY #695 UNLESS
BARRIERS ARE BUILT PRIOR TO CONSTRUCTION
IN ALL AREAS THAT EXCEED FEDERAL
NOISE GUIDE LINES"

BUILD ANOTHER BELTWAY FURTHER
NORTH, BECAUSE YOU WILL NEED
ADDITIONAL LANES IN THE
COMING YEARS

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.
*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See response for Noise Abatement on P. V-2.
2. An outer beltway was not part of this project.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME CAROLYN LAZZARO DATE 3/5/90

PLEASE PRINT ADDRESS 42 BLANCKMERE CT - DWANEY TOWERS

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I think that it is disappointing that your Administration is planning to add another lane on #695 Beltway without first constructing noise barriers in the area. Sometime ago we were told that sound barriers were to be constructed, now they have been deleted in the program. Some of our roads around Camp get through the noise now - what will happen with this other lane being added? Our property values will then go down - I am sure - is certainly making backward decisions. I guess it's time to get some new blood and get these people absolutely out of office. I would appreciate a response.

Off J

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Betty Benson DATE 3/5/90

PLEASE PRINT ADDRESS 18 Ches Lane

CITY/TOWN Pawson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal noise guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

PROJECT DEVELOPMENT DIVISION

Mar 3 10 40 AM '90

1225

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 9 10 40 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PLEASE PRINT NAME George and Mary Homan DATE 31 March 1990
ADDRESS 34 Bardeen Court (Dulany Towers) T.H.
CITY/TOWN TOWSON STATE Maryland ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to Baltimore Beltway # 695 unless noise barriers are built prior to construction on all areas that exceed noise guidelines.

The Beltway as it is now is a health hazard as it is now with out the noise barriers. Way back about 1984 we were promised noise barriers by 1986. Yet we are still without them. Now you want the traffic to increase and the noise to increase and pollution to decrease what next.

To All Concerned

We have a vote and we will use it. For Fair Play.

Yours sincerely, George and Mary Homan

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PLEASE PRINT NAME D. Miricanyan DATE 3-6-90
ADDRESS 5 Bellows Ct (Dulany Towers Condos)
CITY/TOWN Towson STATE Md ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

Dear Sirs,

I am apposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Adding a 4th lane to the beltway means two things:

- 1) that we are gearing up for 33% increase in traffic
- 2) the beltway will get closer to the home by quite a few yards.

These two factors will add to the already unbearable noise. I read recently that a State Highway Engineer claims the above two factors will only add 1 decibel to the noise. As a fellow engineer, I must say that's an impossibility. Gentlemen, please erect the barriers before the construction starts. Thank you.

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. See response for Noise Abatement on P. V-2.

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2/22/90

V-28

Response:

1. No response required.

U.S. Department of Housing and Urban Development
Baltimore Office, Region III PROJECT DEVELOPMENT
The Equitable Building
3rd Floor, 10 North Calvert Street
Baltimore, Maryland 21202-1265

Mar 5 2 15 PM '90

March 7, 1990

Mr. Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, MD 21202

Dear Mr. Ege:

SUBJECT: Environmental Assessment/Section 4(f) Evaluation
I-695 from MD 140 to MD 702

Margaret Krangel, Regional Environmental Officer at the HUD Philadelphia Regional Office, has referred the above-mentioned assessment/4(f) evaluation to this office for review and comment.

In accordance with 40 CFR Chapter V of the Council on Environmental Quality regulations we wish to inform you that no HUD-assisted Community Development Block Grant activities or HUD-insured housing projects will be impacted by the proposed improvements to the Baltimore Beltway.

As a general comment, however, it is disappointing to note that even with the proposed beltway improvements, the level-of-service on the expressway would only temporarily be improved. The discussion of alternatives for solving anticipated future long term traffic congestion on the beltway appears to rule out all possible solutions. Only the consideration of constructing an elevated roadway above the existing beltway was not mentioned.

Given the above, we have no specific comments regarding the assessment.

Very sincerely yours,

Robert H. Herbert, Jr.
Robert H. Herbert, Jr.
Environmental Officer

cc: Margaret Krangel

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Response:

1. See response for noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME WENDY J. RUNDEL DATE 2/28/90

PLEASE PRINT ADDRESS 42 THEOLA

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I oppose any additional construction on
695 (beltway) at or near exit 27A/B
without first having noise abatement barriers
for our area.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472 Mar 9 9 54 AM '90
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME DR and MRS C.D. Stiles DATE 3/6/90

PLEASE PRINT ADDRESS 26 THEO LANE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to Baltimore Beltway at 695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

C.D. Stiles MD.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRLARS JAMES KRIMETIS DATE 3-5-90

PLEASE PRINT ADDRESS 21 THEO LANE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*ADDING ANOTHER LANE ON THE BELTWAY NEAR YORK ROAD AND DULANEY VALLEY ROAD WOULD INCREASE THE NOISE WHICH IS ALREADY TOO OFFENDING.
 IF YOU MUST WIDEN THE BELTWAY, PUT UP SOUND BARRIERS SO THAT WE CAN TOLERATE THE NOISE*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
 1. See response for Noise Abatement on P. V-2.

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Handwritten signature/initials

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME GEORGE KALLAP DATE 3/5/90

PLEASE PRINT ADDRESS 30 RAINBOW ST

CITY/TOWN TOWSON STATE MD ZIP CODE 21286

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF
ANOTHER LANE TO BALTIMORE BELTWAY #695
UNLESS NOISE ABATEMENT BARRIERS ARE BUILT
PRIOR TO CONSTRUCTION IN ALL AREAS THAT
EXCEEDS FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

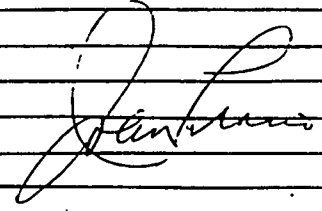
NAME JOHN PRANO DATE 3/5/90

PLEASE PRINT ADDRESS 10 BARDEEN CT

CITY/TOWN TOWSON STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to this project
unless sound barriers will
be provided before work
begins



Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

PROJECT
 DEVELOPMENT
 DIVISION

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Bette R Cary DATE 3/6/90

PLEASE PRINT ADDRESS 4 Choate Ct Apt F

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Please build noise barriers before any additional and various additional lanes to Baltimore Beltway are begun. Such a noise would exceed Federal Noise Guidelines. I would agree to widening the beltway only if sound barriers are first built.

Bette R Cary

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME ALICE W Gould DATE 3/5/90

PLEASE PRINT ADDRESS 35 CHIARA Ct

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Balto Beltway # 695 unless barriers are built prior to construction in all areas that exceed federal noise guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
 1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 3 9 55 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME JAMES F. O'SHEA DATE 3/5/90

PLEASE PRINT

ADDRESS 31 BELLOWS CT.

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines

James F. O'Shea

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

(45)

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 3 9 55 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRS GLORIA M McCANN DATE 3/9/90

PLEASE PRINT

ADDRESS 202 ONE SHETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MILDRED A. EDWARDS DATE _____

PLEASE PRINT ADDRESS 1102 ONE SUTTON PL.
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I am in favor of any improvement
to the Beltway.
I would suggest a sound barrier
in all areas that need Federal
Noise Guidelines*

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.
*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR. AND MRS. RICHARD H. MERCHANT DATE 3/7/90

PLEASE PRINT ADDRESS 27 BELLOWS COURT
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*We are opposed to the addition of another lane to Baltimore Beltway #695 unless
barriers are built prior to construction in all areas that exceed Federal Noise
Guidelines.*

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.
*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See response for Noise Abatement on P. V-2.

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V-34

3/20

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT
DEVELOPMENT
DIVISION

MAR 9 9 55

NAME ROBERT T. MOHRE DATE 3/7/90

PLEASE PRINT ADDRESS 1402 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO ADDITION OF ANOTHER LANE
ON ROUTE 695 UNLESS SUITABLE BARRIERS FOR NOISE
CONTROL ARE INSTALLED AT ALL AFFECTED AREAS.

Robert T. Mohre

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT
DEVELOPMENT
DIVISION

MAR 9 9 55 AM '90

NAME MRS. K.P. MACMILLAN DATE MAR. 6, 1990

PLEASE PRINT ADDRESS 17 THEO LANE

CITY/TOWN TOWSON STATE MARYLAND ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another
lane to Baltimore Beltway # 695 unless barriers
are built prior to construction in all areas
that exceed "Federal Noise Guidelines."

(Mrs. K.P.) Phyllis E. Macmillan

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472 Mar 9 9 55 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Sally Jane Ranson DATE 3/6/90

PLEASE PRINT ADDRESS 121 Swan Downer Dr.

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I will fight the addition of another lane to the Baltimore Beltway in the Towson area unless barriers are built prior to construction in our community because the present noise level exceeds the Federal Noise Guidelines now before construction.

Sally J. Ranson

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472 Mar 9 9 55 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRS Elizabeth Dukehart DATE 3/6/90

PLEASE PRINT ADDRESS 43 Chigra Ct

CITY/TOWN Balto STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane on the Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal noise guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

V-36

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2-25-90

Response:

- 1. I-83 (JFX) Option B was not selected.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MARCOS T. DOXARAS DATE 3/2/90

PLEASE PRINT ADDRESS 15 Roland Ct.
CITY/TOWN Baltimore STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:
 Comments regarding I-695 intersection I-83 options B&C:
 We respect the need for future planning for expanding needs of county-
 however, the main problem existing at this intersection is sunlight or
 glare due to Westbound beltway towards Pikeville. Widening the beltway
 plus the fly-bye ramp will not alleviate these problems. We realize you
 are planning for the future of existing travel patterns; however, there
 is rarely, if ever, a backup on I-83 north at this intersection. This
 is contrasted to the frequent and usual congestion problems on other
 areas of the beltway. I therefore, feel the enormous expense and in-
 convenience provided to those surrounding the immediate area of I-83
 Falls road and I-695 are not warranted.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response

- 1. I-83 (JFX) Option B was not selected
- 2. This project does not precede future HOV lanes.
- 3. The no-build alternate does not address the operational and safety needs/

**STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT DIVISION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 5 9 57 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME John Eckenrode sr DATE 3-3-90

PLEASE PRINT ADDRESS 6 Roland Ct. (Ruxton Hills)
CITY/TOWN Ruxton STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

- ① All H4 Property Owners in Ruxton Hill adjacent to Rt 83 are very much opposed to I-83/Ad25 Falls Rd Interchange Option B. For the following reasons.
 - A- The Fly over connection is Far too Expensive & will greatly increase noise level & Pollution which is Far too High now.
 - B- Will adversely effect Property Values in this area
 - C- We would lose The Euc trees along 83 that provide some screening from noise (ect)
 - D. The small wall along 83N would not screen out Noise & Pollution at the Flyover- The exit grade will produce Far more acceleration noise (ect) by truck & cars.

E. We believe your Environmental study is Faulty.

- ② The whole Project from md 140 to md 702 is only an Expensive Band Aid. and will adversely effect the whole area and will require a East-West connector rather than the Beltway in near future.
- ③ Reduce traffic Now- install High Speed Lanes for Busses other

Please add my/our name(s) to the Mailing List. * ^{copies not save money cities}

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

- ④ We are For No Build. The Federal government should be advised how you people are throwing Tax Payers \$ away.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 5 9 57 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 5 9 57 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME M/M FRANCIS H. WYATT DATE 3/4/90

NAME MARCIA SMITH DATE 3/7/90

PLEASE PRINT ADDRESS 6-B CHATE CT.

PLEASE PRINT ADDRESS 1201 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I/We wish to comment or inquire about the following aspects of this project:

WE ARE OPPOSED TO THE ADDITION OF
ANOTHER LANE TO BALTIMORE BELTWAY
I-695, UNLESS BARRIERS ARE BUILT IN
ALL AREAS WHERE THE FEDERAL NOISE
GUIDELINES ARE EXCEEDED.

I am opposed to the addition of another lane
to Baltimore Beltway #695 unless barriers are
built prior to construction in all areas that
exceed Federal Noise Guidelines.

Francis H. Wyatt

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

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2/29/90

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 3 9 57 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME M. AENES GRIFFIN DATE 3/9/90

PLEASE PRINT ADDRESS 1207 ONE SMETON PLACE

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to another lane to the Baltimore Beltway No 695 as it will increase the already intolerable noise which equates noise and you will create another mountain of noise. However, if business owners built to suppress the noise now, I am not opposed to another lane of the Beltway; however, I would need proof that the business would be installed first.

M. Aenes Griffin

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME SP'INACE GLORIOSO DATE 3/6/90

PLEASE PRINT ADDRESS 604 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Rte 695. I have had a long time prior to construction.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472 Mar 9 9 57 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME PETER & CATHERINE KOUSOURIS DATE MAR 6-1990

PLEASE PRINT ADDRESS 11 BELLOW'S CT.

CITY/TOWN BALTO STATE MD ZIP CODE 21204

We wish to comment or inquire about the following aspects of this project:

We are opposed to addition of another lane
to Beltway #695 unless barriers are built prior
to construction in all areas that exceed
Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochures through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472 Mar 9 9 58 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Joseph Paganini DATE 3-6-90

PLEASE PRINT ADDRESS 22 B Alsbauke Ct.

CITY/TOWN Towson STATE MD ZIP CODE 21204

We wish to comment or inquire about the following aspects of this project:

I AM opposed to the Addition of Another lane
to Balto Beltway #695 unless barriers are
Built prior to construction in all areas that
Exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Louise W. Gorfine (Mrs. Emanuel) DATE 3/7/90

PLEASE PRINT ADDRESS #1301 - One Sneton Place

CITY/TOWN Towson STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

(For your records, Mr. Gorfine is dead.)

I am opposed to the addition of another lane to Baltimore Beltway #695
unless barriers are built prior to construction in all areas that exceed
Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME HEBB DATE 3/7/90

PLEASE PRINT ADDRESS 6005 N. Charles St

CITY/TOWN Dttr STATE Md ZIP CODE 21212

I/We wish to comment or inquire about the following aspects of this project:

I live in the end house
#2 Bardem Court - Dulany
Towers. It is ridiculous
for you to spend money to widen
the exit to Dulany Valley Road.
The exit road already enters
my bedroom & the noise is
intolerable. We need sound
barriers!

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

JOHN H. HEBB
#2 Bardem
Towson 21202
MD

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 9 9 58 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME JOSEPH D. MILLER DATE MARCH 6, 1990

PLEASE PRINT ADDRESS 304 ONE SMETON PLACE
CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

We wish to comment or inquire about the following aspects of this project:

I BELIEVE THAT CONSTRUCTING AN ADDITIONAL ROADWAY
TO BELTWAY #695 BEFORE INSTALLATION OF A
SOUND BARRIER IS VERY WRONG.
I VERY MUCH OBJECT TO THIS ADDITIONAL ROADWAY
BECAUSE OF THIS.

Joseph D. Miller

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR. & MRS. JOHN E. BOERNER DATE 3-6-90

PLEASE PRINT ADDRESS 39 THEO LANE DULANEY TOWERS
CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

We wish to comment or inquire about the following aspects of this project:

WE ARE DECIDEDLY OPPOSED TO THE
ADDITION OF ANY FURTHER LANE TO
THE BALTIMORE BELTWAY #695 UNTIL
BARRIERS ARE PRIOR TO CONSTRUCTION IN
ALL AREAS THAT EXCEED FEDERAL NOISE
GUIDELINES.

MANY OTHER AREAS ALONG THE EASTERN
SECTION OF THE BELTWAY - HURD RD, BELAIR
RD ETC HAVE BEEN PROVIDED WITH BARRIERS.
WE ENCOUNTER JUST AS MUCH NOISE IN THESE
THE DULANEY VALLEY, JUMP ROAD SECTIONS.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT
DEVELOPMENT
DIVISION

MAR 5 9 53 AM '90

NAME MRS. EDYTH C. GELBERT DATE 3/7/90

PLEASE PRINT ADDRESS ONE SMETON PLACE #407

CITY/TOWN TOWSON STATE MD. ZIP CODE 21284

I wish to comment or inquire about the following aspects of this project:

I do oppose to the addition of another lane to
Beck. Beltway #695 unless barriers are built prior
to construction in all areas that would reduce
noise levels.

Edyth C. Gelbert

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT
DEVELOPMENT
DIVISION

MAR 5 9 53 AM '90

NAME MR. + MRS. HARRY S. HICKMAN DATE 3/7/90
805 One Smeton Place
Dulaney Towers
Towson, MD 21284

PLEASE PRINT ADDRESS _____

CITY/TOWN _____ STATE _____ ZIP CODE _____

I We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of
another lane to Bechtel Beltway #695
unless barriers are built prior to
construction in all areas that would reduce
noise levels.

Arbitrarily
OPPOSITION

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Thelma C. Eberwein DATE 3/6/90

PLEASE PRINT ADDRESS 22 Bardeen Ct
CITY/TOWN Balt STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I am opposed to the addition
of another lane to the Baltimore Beltway,
695, unless noise barriers are
built prior to construction in all
areas that exceed Federal noise
standards.*

T. Eberwein

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Rita P. HUNDLEY DATE 3-2-90

PLEASE PRINT ADDRESS 1028 MARLEIGH CIR
CITY/TOWN ROUSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*My home was built in 1959
when the beltway was mighty
quiet.
I request a speed up on
the noise barriers behind my
home & my neighborhood of
Ridderwood Hills. I seldom use
my patio due to the unnecessary
noise of traffic.*

Please add my/our name(s) to the Mailing List.* *Sign at hearing 2/28/90*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Handwritten mark

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 9 9 59 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Charlotte D Rice DATE 3-7-1990

PLEASE PRINT ADDRESS 27 Chiara Court

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

PROJECT DEVELOPMENT

Mar 9 9 59 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME STEVE KARAS DATE MARCH 7 1990

PLEASE PRINT ADDRESS 4 ALANCOCKE COURT

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF ANOTHER LANE TO BALTIMORE BELTWAY #695 UNLESS BARRIERS ARE BUILT BEFORE THE CONSTRUCTION IN ALL AREAS THAT EXCEED THE FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME BETTYE GOULD DATE 3-7-90

PLEASE PRINT ADDRESS 322 ALAN BROOKS CT
CITY/TOWN TOWSON STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I have a slip to the District and not able to open the sliding glass door. I am in the process of having a door installed. The door is not able to be closed with the opening of the District. It will be a barrier and something is definitely needed. It is necessary for something to be done to keep the tenants in the area. Thank you.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472 MAR 12 10 35 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MOHAMED HABEEB DATE 2-28-1990

PLEASE PRINT ADDRESS 2223 SPRING LAKE DRIVE
CITY/TOWN TIMONIUM STATE MD ZIP CODE 21093

I/We wish to comment or inquire about the following aspects of this project:

I wish to comment about the traffic on Beltway P 5-83 p lanes falls expressway is "built the Light Rail" or Subway that is the better solution. When I called to the department of Highways about the Sound Barrier behind my property 1031 Adcock Road in Lutherville they replied the Highway department did not have enough funds to put the Sound Barrier. If they get the money from federal Govt then they will put it. When they have money how they are planning to improve the Beltway, Love & falls expressway P 5-83 ex 16: P extending & widening to lanes. In my opinion this is not a good scheme a plan. It is prove if you built one highway one traffic of one car on the highway so better you "stop" building highways example is Toronto & West Germany.

If you want like to reduce the traffic on Beltway P 5-83 please put express "full" lane that is going to reduce the pollution, noise pollution and traffic.

My conclusion is before you do any plan please put the Noise Barrier behind my property because my house is built before the Beltway P it is my right & request as a citizen and as a tax payer.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Handwritten signature

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Response:

- 1. No response required.

H.J



U.S. Department of Housing and Urban Development
 Baltimore Office, Region III
 The Equitable Building,
 3rd Floor, 10 North Calvert Street
 Baltimore, Maryland 21202-1865

March 7, 1990

Mr. Louis H. Ege, Jr.
 Deputy Director
 Office of Planning and
 Preliminary Engineering
 State Highway Administration
 707 North Calvert Street
 Baltimore, MD 21202

Dear Mr. Ege:

SUBJECT: Environmental Assessment/Section 4(f) Evaluation
 I-695 from MD 140 to MD 702

Margaret Krangel, Regional Environmental Officer at the HUD Philadelphia Regional Office, has referred the above-mentioned assessment/4(f) evaluation to this office for review and comment.

In accordance with 40 CFR Chapter V of the Council on Environmental Quality regulations we wish to inform you that no HUD-assisted Community Development Block Grant activities or HUD-insured housing projects will be impacted by the proposed improvements to the Baltimore Beltway.

As a general comment, however, it is disappointing to note that even with the proposed beltway improvements, the level-of-service on the expressway would only temporarily be improved. The discussion of alternatives for solving anticipated future long term traffic congestion on the beltway appears to rule out all possible solutions. Only the consideration of constructing an elevated roadway above the existing beltway was not mentioned.

Given the above, we have no specific comments regarding the assessment.

Very sincerely yours,

Robert H. Herbert, Jr.
 Robert H. Herbert, Jr.
 Environmental Officer

cc: Margaret Krangel

Response:

- 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

Mar 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Janet M. Blackmon DATE March 5, 1990
 PLEASE PRINT ADDRESS 10 Alanbrooke Court
 CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our names to the Mailing List.*

Please delete my/our names from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mary C. Culbertson DATE 3-7-90

PLEASE PRINT ADDRESS 48 Theo Lane

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/Wa wish to comment or inquire about the following aspects of this project:

"I am opposed to the addition of another lane to the Baltimore Beltway #695 without first providing noise abatement barriers for our area. Thank you.

Mary C. Culbertson

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mr. & Mrs. Daniel Streib DATE 3/7/90

PLEASE PRINT ADDRESS 1 Smetan Pl Apt 607

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/Wa wish to comment or inquire about the following aspects of this project:

We are in opposition to the addition of another lane being added to Baltimore Beltway #695 without benefit of noise barriers being installed prior to construction.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See Response for Noise Abatement on P. V-2.

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Streib

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME A. OWEN HENNEGAN + Mrs. DATE MAR 6, 1990

PLEASE PRINT ADDRESS 31 Theo Lane

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to the Baltimore Beltway I 695 unless barriers are built prior to construction in all areas that exceed Federal noise guidelines.

*A Owen Hennegan
Catherine L Hennegan*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT DIVISION

CONTRACT No. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Dorothy M. Clark DATE 3/7/90

PLEASE PRINT ADDRESS 60 Theo Lane

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to additional lanes to the 695 (Baltimore Beltway) unless a permanent barrier against noise is erected in all areas. The noise is deafening now & with more lanes will definitely exceed the Federal limits.

Dorothy M. Clark

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mary A. Ruppert DATE March 7, 1990

PLEASE PRINT ADDRESS 801 Lantana Place

CITY/TOWN Brown STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I have agreed with the project to quiet down
my side noise on the "Log Lane" to be
more comfortable in some homes

Mary A. Ruppert

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 35 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME FRANK/KATHLYNE WAGNER DATE 3-5-90

PLEASE PRINT ADDRESS 44 THEO LANE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of
another lane to Baltimore Beltway
#695 unless barriers are built
prior to construction in all
areas that exceed Federal Noise
Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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8/16/90

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

**PROJECT
DEVELOPMENT
DIVISION**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME BEV MOORE DATE _____

PLEASE PRINT ADDRESS 188 One Smeton Place
Towson, Maryland 21204
(301) 828-7577

CITY/TOWN _____ STATE _____ ZIP CODE _____

I/We wish to comment or inquire about the following aspects of this project:

*I am opposed to the addition of another
lane to Baltimore Beltway #695
unless barriers are built prior to construction
in all areas that exceed Federal noise
guidelines*

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME AUDREY SCHWEMMER DATE 3/7/90

PLEASE PRINT ADDRESS 1 SMETON PLACE #1206

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I am opposed to the addition of another
lane to the Baltimore Beltway #695 unless
sound barriers are built prior to
construction in all areas that exceed
Federal noise guidelines; particularly the
west side of Deerley Valley Road area.*

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Edward Patrick McDermott DATE 3/7/90

PLEASE
PRINT

ADDRESS 29 Bellows Co

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of
another lane to the Beltway unless
or until barriers are built prior
to construction in all areas that exceed
Federal Noise Guidelines.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Judith R. Rigby DATE March 6, 1990

PLEASE
PRINT

ADDRESS One Bellows Court

CITY/TOWN Towson STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Gentlemen:

I have learned that an additional lane is planned for #695, the
Baltimore Beltway. I strongly urge the State Highway Administration
not to add another lane unless barriers are built prior to
construction in all areas that exceed Federal Noise Guidelines.

In my opinion, barriers should be an integral part of the planning
and construction of all additional lanes.

Sincerely,

Judith R. Rigby

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already
on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already
on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT DIVISION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472 MAR 12 10 37 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME SHOW - PIN #10 DATE 03/06/90

PLEASE PRINT ADDRESS 14 TIGEO LANE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Sound Barriers for our area are necessary.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mr. & Mrs. William A. Peat, Jr. DATE 9 MARCH 1990

PLEASE PRINT ADDRESS 1 Smeton Place - Unit 1101

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

WE FAVOR AN ADDITIONAL LANE BEING CONSTRUCTED ON BELTWAY #695, BUT ONLY AFTER COMPLETION OF NOISE BARRIERS IN THOSE AREAS WHERE THE NOISE CURRENTLY EXCEEDS FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

PROJECT
DEVELOPMENT
DIVISION

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mr & Mrs E.J. Pawlicki DATE 3/10/90

PLEASE
PRINT

ADDRESS 1 Theo Lane

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of
another lane to Interstate 695 in the
Towson area between Exits 26 and 27 unless
noise barriers are built prior to the
construction. It is critical that
these barriers be built in all areas
that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

PROJECT
DEVELOPMENT
DIVISION

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Stanley H. Dorney Jr DATE 3-9-90

PLEASE
PRINT

ADDRESS 25 CHIARA CT

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

WE ARE AGAINST ANOTHER LANE TO
BALTO. BELTWAY I-695 UNLESS BARRIERS
ARE INSTALLED PRIOR TO CONSTRUCTION
IN ALL AREAS THAT EXCEED FEDERAL
NOISE GUIDELINES.

ASD

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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ASD

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME H. C. Buceart DATE 3-7-90
ADDRESS 1 Smeton Pl.
CITY/TOWN Towson STATE Md ZIP CODE 21204

I wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane of the Baltimore Beltway (695) unless sound barriers are built prior to construction in all areas that exceed Federal Noise Guidelines. The constant noise is depending on the winter atmosphere.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mrs + Mrs Drafton Meyers DATE 3/9/90
ADDRESS One Smeton Place - Delaney Towers 1205
CITY/TOWN Towson STATE Md ZIP CODE 21204

I wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to the Baltimore Beltway, #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

C. Drafton Meyers

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702
Mar 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

H. EARLE GERDING, JR.

NAME SHIRLEY M. WARD DATE 3-06-90

PLEASE PRINT ADDRESS 8 BARDEEN COURT
CITY/TOWN TOWSON STATE MARYLAND ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am totally opposed to the addition of another
lane to the Baltimore Beltway #1405, (or
widening or similar changes) unless barriers
are built prior to construction in all areas
that exceed Federal Noise Guidelines.

Those in the SHH should experience first-hand
the effects of 24 hour/day noise within a few
feet of the Beltway - barriers would be
the true first priority.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already
on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702
Mar 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME LOUISE STREET DATE 3/6/90

PLEASE PRINT ADDRESS ONE SNETON PL. #1005
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to additional lanes to the
Beltway, without first providing sound barriers.

A. S. L.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already
on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DESIGN

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ELSE RHOADES DATE 3.7.90

PLEASE PRINT ADDRESS 704 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I AM opposed to the addition of another lane
to Baltimore Beltway #695 unless barriers
are built prior to construction in all
AREAS that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Robert A. Clark DATE 3/7/90

PLEASE PRINT ADDRESS 60 THEO LANE

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane
to Baltimore Beltway #695 unless barriers are
built prior to construction in all areas that
exceed Federal Noise Guidelines.
The noise now is deafing and dirty! the
option here it will be terrible.

Robert A. Clark

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Robert A. Clark

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472 Mar 12 10 37 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Debbie M Tegetmeier DATE 3-8-90

PLEASE PRINT

ADDRESS 306- One Smeton Place

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to the Baltimore Beltway # 695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Debbie Tegetmeier

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472 Mar 12 10 37 AM '90
BALTIMORE BELTWAY
MD-140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Marion F. Mudd DATE 3/8/90

PLEASE PRINT

ADDRESS 1 SMETON PLACE APT 601

CITY/TOWN Towson STATE md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

"I am opposed to the addition of another lane to the Baltimore Beltway, #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines."

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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MSL

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIV 5

CONTRACT NO. B 635-101-472 Mar 12 10 37 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME PAT FRANCE-Mentec DATE 3-9-90

PLEASE PRINT ADDRESS ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Adding a lane to 695 is great
but not without a noise
barrier!

[Handwritten signature]

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing list.

(101)

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIV 5

CONTRACT NO. B 635-101-472 Mar 12 10 37 AM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ALIDA Sommer DATE 3/8/90

PLEASE PRINT ADDRESS 307 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

PLEASE DO NOT BUILD AN ADDITIONAL LANE
TO BALTIMORE BELTWAY RTE #695 UNLESS
A SOUND BARRIER IS ERECTED FIRST.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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[Handwritten mark]

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Joseph B. Yalloway DATE 3/6/90

PLEASE
PRINT

ADDRESS 27 Theo Lane

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of
another lane to Baltimore Beltway
695 unless barriers are built prior
to construction in all areas that
exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME HARRIET M. LOVELESS DATE 3-6-90

PLEASE
PRINT

ADDRESS 6 CHATE CT. APT. C

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another
lane to Baltimore Beltway # 695 unless
barriers are built prior to construction
in all areas that exceed Federal Noise
Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise abatement on P. V-2.

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5/5/90

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MARGARET D POORBAUGH
W4 E. POORBAUGH DATE 3/7/90

PLEASE
PRINT

ADDRESS 19 TREE LA.
CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

My wife and I are opposed to widening 695, unless
sound barriers are erected before construction
begins

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Gloria Rose Benjamin H Beckwith DATE March 6 1990

PLEASE
PRINT

ADDRESS 23 Bellows Court
CITY/TOWN Towson STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to
Baltimore Beltway #695 unless barriers are built
prior to construction in all areas that exceed
Federal Noise Guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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T9-A

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ALLAN G. & ANI V. DOLK DATE 3-7-90

PLEASE PRINT ADDRESS 22 ALANBROOKE COURT APT. E

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

WE ARE OPPOSED TO THE ADDITION OF ANOTHER LANE
TO BALTIMORE BELTWAY #695 UNLESS BARRIERS ARE
BUILT PRIOR TO CONSTRUCTION IN ALL AREAS THAT
EXCEED FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT
CHARTER

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME JAMES E. WHITE JR. DATE March 6, 1990

PLEASE PRINT ADDRESS 44 ALANBROOKE CT.

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition
of another lane to the Baltimore
Beltway #695 unless barriers are
built prior to construction in
all areas that exceed Federal
noise guidelines.

James E. White

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ROLAND L. HARTMANN DATE 3-6-90

PLEASE PRINT ADDRESS 1000 ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 12 10 37 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME SAMUEL D. HERMAN DATE 3/6/90

PLEASE PRINT ADDRESS 21 CHIARA COURT

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway 695 until road barriers are built in all areas that exceed Federal Noise Guidelines.

Samuel D. Herman

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS**

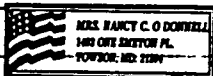
CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME _____ DATE 3/7/90

PLEASE PRINT

ADDRESS _____



CITY/TOWN _____ STATE _____ ZIP CODE _____

I/We wish to comment or inquire about the following aspects of this project:

Noise barriers are a necessity. Potions along 695 are unusable - No new construction should be allowed unless this situation is corrected.

- Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME William E. and Grace B. Ferguson DATE 3/9/90

PLEASE PRINT

ADDRESS 302 One Smeron Place

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We object to having another lane on the Baltimore Beltway #695 - this should have been included in your plans when you just recently worked on the Beltway West and East of Dulaney Valley Road. In addition, and most importantly - should another lane be added, barriers need to be built prior to construction in all areas that exceed Federal Noise Guidelines.

- Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702
MAR 12 10 33 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ROBERT BOOKER DATE 3/2/90

PLEASE PRINT ADDRESS 3 Theo Lane

CITY/TOWN Towson STATE MD. ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

For some time, we at Dulany Towers have been in need of noise barriers and I'd like to register my opposition for the construction of another lane to the Baltimore Beltway without first building the barriers.

*Sincerely yours
R. Booker*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702
MAR 12 10 33 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Patsy R. Kane DATE 3/7/90

PLEASE PRINT ADDRESS 1 Smelter Pl apt 873

CITY/TOWN Towson STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I have opposed to the addition of another lane to Baltimore's Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

P.S. Can not sleep now with wood windows open. day or night!

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Mar 13 4 19 PM '90

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Ms FLORENCE AND BARBARA ARMIGER DATE _____

PLEASE PRINT ADDRESS 24 C ALANDRUM CT

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines

As of now the noise is objectionable when the windows are open it is unbearable

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Alice N. Hilbert DATE 3/13/90

PLEASE PRINT ADDRESS One Smeton Place Apt 804

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to the Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines

Alice N. Hilbert

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

FEB 15 4 10 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME John E. Landerda DATE 3/9/90

PLEASE PRINT ADDRESS 402 One Linton Pl

CITY/TOWN Towson STATE Md. ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another
lane to Baltco. Beltway 695 unless business
are built to construction in all areas
that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOP.**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

FEB 15 4 10 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR & MRS Thomas Smeot DATE 3/11/90

PLEASE PRINT ADDRESS 40 Theo Lane

CITY/TOWN Towson STATE Md ZIP CODE 21284

X We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition
of another lane to Baltimore
Beltway #695 unless business
are built prior to construction in
all areas that exceed Federal
Noise Guidelines.

Mr Thomas Smeot
Mrs Thomas Smeot

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Richard O Gerry Warkang DATE 3-9-90

PLEASE PRINT ADDRESS 40 Bardeen Ct, Dulany Towers

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane
to Baltimore Beltway #695 unless barriers are
built prior to construction in all areas that
exceed Federal Noise Guidelines. In the 15 3/4
years we have lived here the noise and
dirt pollution have increased every year. Another
lane to #695 will just add to the empty
discomfort we are now subject to.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME M/M JOHN R. PAYNE DATE 3/8/90

PLEASE PRINT ADDRESS ONE SACKET PLACE #602

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

we are opposed to the addition of
another lane to Baltimore Beltway, unless
barriers are built prior to construction
in all areas that exceed Federal noise
guidelines.

John R. Payne
Elvira C. Payne

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

V-74

Response:

1. I-83 (JFX) Option B was not selected.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 16 1 15 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Manuele and Richard Wasserman DATE March 8, 1990

PLEASE PRINT ADDRESS 3 Rockland Vue Court
CITY/TOWN Baltimore STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We would like to go on record as being absolutely opposed to Option B under your proposal for the interchange at I-83. This ramp would

adversely impact the environment in a residential and historic community.

It would create noise problems for local residents along Old Court Road and

Falls Road as cars and trucks would accelerate on the ramp. In addition harmful toxic fumes would be released by vehicles as they climbed the fly

ramp. Moreover the ramp, estimated at 17 million dollars is prohibitively costly in comparison to the 4 million dollar alternative of option A. In

addition, we feel that this ramp will adversely affect property values

in an area where tax assessments are among the highest in the county. We will join with members of the Ruxton Hills community to oppose this ramp.

We further oppose option C as outlined in your proposal. We would be willing to support options A and D.

*Richard Wasserman
Manuele & Richard Wasserman*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 16 1 15 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Stephen Mealy DATE 3/8/90

PLEASE PRINT ADDRESS 35 Theo Lane
CITY/TOWN Towson STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the adding or restructuring of the now existing beltway or changes to the exit or entrance ramps at those areas that would directly or indirectly affect the area surrounding the Dulaney Towers' complex without the addition of noise barriers.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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V-75

PASTOR
WALTER C. SMITH, JR.
823-1004

The Towson United Methodist Church

501 HAMPTON LANE
TOWSON, MARYLAND 21284

ASSOCIATE PASTOR
JAMES M. WARNER
823-6811

March 12, 1990

Mr. Daniel Cheng
Maryland Department of Transportation
State Highway Administration
Office of Planning and Preliminary Engineering
Box 717
Baltimore, Maryland 21203

Re: Contract No. B 635-101-472
Baltimore Beltway
MD 140 to MD 702

Dear Sirs:

Representatives of the Church attended the public hearing at Loch Raven Senior High School on February 28, 1990. The Trustees of the Towson United Methodist Church are concerned and interested in the proposed changes to the exit ramp from the outer beltway (north) since the proposed ramp abuts the property line of the Church. We are also concerned about any changes made to the traffic pattern entering Hampton Lane.

We respectfully request that we be advised of any changes affecting the above noted interchange and that we be notified of any hearings so that our concerns can be made part of the public record. Any communication may be addressed to:

Chairman, Board of Trustees
Towson United Methodist Church
501 Hampton Lane
Towson, Maryland 21204

We appreciate the opportunity to express our views.

Sincerely yours,

Arthur R. Ransom, Jr.
Chairman
Board of Trustees

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STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS SECTION

CONTRACT NO. B 635-101-472 HR 14 1 51 PM '90
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRS STANLEY J. HOPE DATE 3/12/90
PLEASE PRINT ADDRESS 4 CHOATE COURT - APT 4B
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway # 695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. MD 146 Option was selected. Studies were investigated to reduce impact to the church.

Response:

1. See response for Noise Abatement on P. V-2.

V-76

12E

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY.
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME John P. Kraus DATE 3/10/90

PLEASE PRINT ADDRESS 1 Smetow Place #1300

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane
to the Baltimore Beltway # 485, unless barriers are
built prior to construction in all areas that exceed
the Federal Noise Guidelines.

J.P. Kraus

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. I-83 (JFX) Option B was not selected.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR. & MRS. STANLEY BIELAK DATE 3/12/90

PLEASE PRINT ADDRESS 1929 OLD COURT ROAD,

CITY/TOWN BALTIMORE, STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I would like to comment on the proposed project under consideration by the state
Highway Administration. Of particular interest to us is the I-83 (JFX) MD 25
(FALLS ROAD) interchange. Options A,B,C,D. As we live adjacent to the beltway and
I-83 north in Ruxton Hills we are opposed to building of the proposed fly ramps
and widening I-83 north. The natural barrier of trees will be eliminated with
road expansion causing the noise level to increase and we do not believe there
is a noise barrier tall enough to eliminate or even cut down the sounds that will be
generated from a fly ramp.

It has been the states position in the past that the road system was in place before
Ruxton Hill but the opposite is now true with your proposed ramp. We are sure the
states only concern is to move automobiles from point A to point B. But don't you
think it is time to begin considering the environment as well as the people?

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 14 1 32 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME THOMAS W. RIGNEY DATE 3/10/90

PLEASE PRINT ADDRESS 1 SMETON PLACE #904

CITY/TOWN BALTIMORE STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I am opposed to the addition of another
lane to Baltimore Beltway #695 unless barriers
are built prior to construction in all areas that
exceed Federal Noise Guidelines*

Thomas W. Rigney

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME WILLIAM E POORBAUGH II DATE 3-9-90

PLEASE PRINT ADDRESS 8 CHOATE CT APT D

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I AM OPPOSED TO THE ADDITION OF
ANOTHER LANE TO BALTO BELTWAY 695
UNLESS BARRIERS ARE BUILT PRIOR TO
CONSTRUCTION IN ALL AREAS THAT
EXCEED FEDERAL NOISE GUIDELINES.*

William E Poorbaugh II

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

V-78

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Edward Benkovic DATE 3-9-90

PLEASE PRINT ADDRESS 109 Swarthmore Drive

CITY/TOWN TOWSON STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all lanes that exceed Federal Noise Guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME VERNON M. SCHLUTTER DATE 3/12/90

PLEASE PRINT ADDRESS ONE SMETON PLACE UNIT 1106

CITY/TOWN BALTO STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF ANOTHER LANE TO BALTIMORE BELTWAY #695 UNLESS BARRIERS ARE BUILT PRIOR TO CONSTRUCTION IN ALL AREAS THAT EXCEED FEDERAL NOISE GUIDELINES

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 14 1 53 PM '90

847 Kellogg Rd.
Lutherville, MD 21093
March 14, 1990

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

Donald Honeywell, Project Manager
Office of Planning and Preliminary Engineering
State Highway Administration
P.O. Box 717
Baltimore, MD 21203

MR. + MRS.

NAME Richard W. Agresti DATE 3-10-90

PLEASE PRINT ADDRESS 26 BARDEEN Court
CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to the Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

The residents of Dulaney Towers feel we are being discriminated against since the barriers are in every direction E or West, and there are no few feet involved for Dulaney Valley area at Dulaney Towers.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

Mr. Honeywell:

As you may be able to tell from my address, I am one of the residents whose homes are adjacent to the Beltway (I-695). I am writing to you specifically about Contract # B635-151-472, PDMS # 031113, widening of the Beltway between Maryland routes 702 and 140. Perhaps more importantly, however, I want to go on record about the project to build sound barriers in my backyard.

Although the project itself will be an inconvenience while construction is going on (whenever that may come), the end will be worth the means. I am looking forward to the day when I will be able to have a conversation in my backyard without yelling over the noise of the traffic. I support this project wholeheartedly, even though it will take land away from my yard, because the end result will be worth it.

I know there are fairly specific plans for the project to widen the Beltway, although no one can tell me just how much of my yard you are going to take away. But the plans to build the sound walls seem to be ever distant. It's always "next year" or the "year after." Now we only joke about when they'll be built. But it's not really very funny, especially when we see walls going up all over the rest of the Beltway on property with houses that are not half as close to the road as we are. We've been hearing about these sound walls since we moved into our house in 1980.

And now I'm being told that you may widen the Beltway without building the sound walls? You want to cause more noise to come closer to our ears without the cushion the sound walls should provide? That will make the sound levels in our neighborhood that much higher. Does that make any sense? If anything, you should be building the sound walls first, and then go ahead and widen the Beltway. That would make the construction job for the widening far more tolerable.

But wouldn't it be better to do both jobs at one time? Surely it would save the taxpayers' money, not to mention save the minds of the residents involved. I don't know about you, but there are only a certain number of nights that I can go without proper sleep. And if you can sleep while road crews pound away with their jackhammers from 10 p.m. until 4 a.m., more power to you.

(14)

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08-A

Response:

1. See response for Noise Abatement on P. V-2.

Page 2

I know I can't. And I know I can't live through that kind of racket twice more, when it could be done once. However, I'm sure the noise from construction for the widening project would be significantly lower if the sound walls were already in place. Completing these projects the other way around would be ludicrous.

If you have any more concrete plans about the sound walls, please send them to me. I already have your propaganda-green booklet about the widening project. If you're going to take some of my yard away, I certainly want it done for some reason that's going to benefit me, and not just cause me more trouble.

Sincerely,

Sheryl Morris
Sheryl Morris



THE ORCHARD HILLS
COMMUNITY ASSOCIATION, INC.

Lutherville, Maryland 21093

March 9, 1990

Mr. Neil J. Pedersen
Director, Office of Planning and Engineering
The Maryland State Highway Administration
P.O. Box 717
Baltimore, Maryland 21203-0717

Dear Mr. Pedersen,

The Orchard Hills Community Association, Inc. wishes to express their concern for the proposed beltway modifications.

Although we can understand and appreciate the need for improvement to and widening of Interstate 695, this proposal would be detrimental to the "quality of life" in our community.

The noise level is already intolerable and adding a lane in each direction of the beltway would only increase this level of noise. Some of our residents can no longer enjoy their homes and surroundings because of the noise generated by the beltway. Orchard Hills Community Association, Inc. cannot endorse a project that compounds an existing problem which has no foreseeable solution.

Understanding that the sound barrier project is voluntary on the part of the State, and funded by the Federal government, we request and urge your assistance in having sound barriers installed. We desperately need and rightfully deserve sound barriers at Interstate 695 and Charles Street and York Road. (Project No. 20, Contract No. B-850 - 501-424).

With a projected cost of \$240 million dollars for the proposed beltway upkeep, the funds needed for our sound barriers seems to be a reasonable request in our efforts to preserve the "quality of life" in our community.

cc: Del. Ellen Sauerbrey

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY MAR 12 10 33 AM '90
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Beverly Barnes DATE 3/8/90

PLEASE PRINT ADDRESS One Ameton Place #1107

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another
lane to Baltimore Beltway #695 unless
barriers are built prior to construction
in all areas that exceed Federal
Noise Guidelines.

Beverly Barnes

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See Response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY MAR 12 10 33 AM '90
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR. MRS. HENRY F. DETLOF DATE MARCH 8 1990

PLEASE PRINT ADDRESS 8 F CHOATE CT.

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We have lived at this address for
17 years. At that time the noise in
the beltway was nil compared to what
it is now. We are opposed to widening
of the beltway unless the proper barrier
is put up to deaden the noise.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
1. See response for Noise Abatement on P. V-2.

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P-66

Response:

1. See response for Noise Abatement on P. V-2.

THE RUXTON - RIDERWOOD - LAKE ROLAND AREA
IMPROVEMENT ASSOCIATION, INC.

Box 204
Riderwood, Md. 21139

March 1, 1990

Mr. Donald G. Honeywell
Project Manager, Project Planning Division
State Highway Administration
307 North Calvert Street
Baltimore, Maryland 21202

Dear Mr. Honeywell:

I am writing as per your instructions following last evening's public hearing on proposed changes to the Baltimore Beltway to request an extension of the comment period beyond March 16, 1990 to at least April 10, 1990.

The issues raised during the presentation and subsequent public hearing require careful and thoughtful consideration.

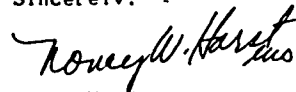
Our board is scheduled to meet March 26, 1990 and would like the opportunity to study the various options more closely.

In addition, we received the lengthy Environmental Assessment only last evening, and need time to peruse it in order to determine affects on the affected communities.

Please advise me of the approval or denial of our request as soon as possible.

Thank you for an informative meeting.

Sincerely,



Nancy W. Horst
Second Vice President

P.S. In looking over the distribution list enclosed with the environmental assessment report, I note that Ruxton-Riderwood-Lake Roland Area Improvement Association is not on your list, and, in fact, we did not receive a copy of the Beltway report until we saw an advertisement in the paper and called about it. Would you please see to it that we are added to your list. P.R.L.R.A.I.A., P. O. Box 204, Riderwood, MD 21139, phone 337-0792 for any future mailings.

Thank you.

Response:

1. See response for Noise Abatement on P. V-2.

Rec. 3/12/90

NANCY WORDEN HORST
7819 Ellenham Avenue
Ruxton, Maryland 21204

March 9, 1990

Dear Mr. Honeywell
I hope that by this time you have received a second copy of my letter mailed March 9, 1990, (Second letter mailed March 7, 1990), requesting an extension on the comment period until April 6, 1990, for the Ruxton - Riderwood Lake Roland Area Improvement Association.

Your telephone call on the sixth of March gave us a verbal extension, but I would appreciate having a written confirmation mailed to our organization at P.O. Box 204, Riderwood, Md. 21139.

Thank you for your assistance in this matter.

Sincerely Yours,
Nancy W. Horst

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V-67

845 Kellogg Road
Lutherville, MD 21093
March 5, 1990

PROJECT DEVELOPMENT
MAR 13 3 00 PM '90

Mr. Neil J. Pedersen, Director
Office of Planning and
Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21220

RE: Public Comment - Location/Design Public Hearing

Dear Mr. Pedersen,

For seventeen years we have been living in Longford, a community in Lutherville. Longford is bordered by I-83 south, the outer loop of I-695, and Thornton Road/ Seminary Avenue.

Several years ago we were told that a sound barrier was to be built to protect our community from the ever increasing noise created by the Baltimore Beltway. At a meeting at Loch Raven High School in October, 1985 we were told by representatives of the State Highway Administration that this sound barrier was scheduled to be built in 1988. We were also told that the scheduling of projects was based upon the age of the community and the decibel level of the sound bordering the highway. We were told that our community had one of the highest decibel levels in the state.

The Longford Community Association had representatives from the State Highway Administration speak to our group on two occasions. At one meeting, we were told that an additional lane was going to be built on the beltway but this would not delay the sound barriers. At another meeting, we were told that this decision had not been made.

On Wednesday, February 18, 1990 another public hearing was held on location/design for modifications to the Baltimore Beltway. We attended this meeting which was held at Loch Raven High School.

At this public hearing, the audience was told that an additional lane was being planned for the beltway. The accompanying materials documented the proposed plans. The audience, during the introductory remarks, was told that the sound barriers were contingent upon dwindling federal funds. We find it to be deplorable to consider adding another lane on the beltway unless sound barriers are planned at the same time.

To repeatedly inform our community that these two projects can not be discussed at the same time (since they come through different funding sources) does not solve the noise problem. Therefore, we support Alternative 1: No-Build until the state pursues other sources for the sound barriers or includes the cost of sound barriers in other alternatives.

Over the years, we have written numerous public and elected officials about our concerns including Governor Schaefer. We feel quite frustrated that most of the responses have been bureaucratic doubletalk and that it is very likely that the new lane on the beltway will come before the proposed sound barrier.

Thank you for your attention.

Sincerely,

Mr. and Mrs. James Hargest
Mr. and Mrs. James Hargest

cc. The Honorable William Donald Schaefer
The Honorable Dennis Rasmussen

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. The option suggested was investigated and dropped due to right-of-way impacts.

Response:

1. See response for Noise Abatements on P. V-2.

MR. NEIL J. PEDERSEN, DIRECTOR
 OFFICE OF PLANNING AND
 PRELIMINARY ENGINEERING
 STATE HIGHWAY ADMINISTRATION
 707 NORTH CALVERT STREET
 BALTIMORE, MD. 21202
 MARCH 7, 1990

DEAR MR. PEDERSEN:

I WAS UNABLE TO ATTEND THE MEETING AT LOCH RAVEN HIGH SCHOOL DUE TO PREVIOUS ENROLLMENT IN AN ADULT NIGHT SCHOOL CLASS.

MY HOUSE IS SITUATED ON THE CORNER OF HAMPTON LANE AND DULANEY VALLEY ROADS. I HAVE HAD MY PROPERTY SLICED AWAY TO ACCOMMODATE THE BELTWAY, HAMPTON LANE AND DULANEY VALLEY ROAD. NOW FURTHER PLANS ARE AFOOT TO TAKE MORE PROPERTY. ALSO TO TAKE THE PRIVACY SCREENING TREES OF MY NEIGHBOR. I REALIZE THAT THESE TAKINGS ARE ALREADY OWNED BY THE STATE DUE TO PRIOR CONDEMNATIONS FOR WHICH THERE WAS VIRTUALLY NO COMPENSATION. THIS TAKING WILL SHORTEN MY ALREADY DANGEROUS DRIVEWAY FROM SIXTY TO FIFTY FIVE FEET ON HAMPTON LANE AND THE PROPOSED DULANEY VALLEY ROAD MERGING TRAFFIC LANE WILL BE ONLY THIRTY SEVEN FROM THE SIDE OF MY HOME. AND I WILL HAVE MORE LAND TAKEN TO ALLOW FOR THE RELOCATION OF TRAFFIC LIGHT POLES AND STREET LIGHT POLES. THE ADDED NOISE, FUMES AND ROAD DIRT WILL INCREASE BY AT LEAST TEN PERCENT.

I HAVE BEEN INSTRUMENTAL IN THE PAST IN ALLEVIATING SOME OF THE TRAFFIC CONGESTION, AT THIS INTERSECTION, BY CALLING YOUR OFFICE AND REQUESTING THAT THE THE RIGHT HAND LANE ON HAMPTON LANE, AFTER EXITING THE BELTWAY, BE MADE A RIGHT HAND TURN ONLY LANE. I DID THIS EVEN THOUGH IT MEANT AN EXTRA RUSTED SIGN IN MY FRONT YARD.

NOW I HAVE ANOTHER SUGGESTION TO MAKE. THE FREE FLOWING LANES FROM THE BELTWAY, WITH HAMPTON LANE TRAFFIC GIVING WAY WAS AN IDEA I'D HAD FOR SOME TIME. THIS EXIT IS ONLY BUSY FROM 8 A.M. TO 10 A.M. AND FROM 3 P.M. TO 6 P.M. DAILY. WHY NOT LEAVE THE LAND AT 500 AND 502 HAMPTON LANE INTACT AND USE THE LAND NOW OCCUPIED BY THE TRAFFIC ISLAND DIRECTLY ACROSS FROM MY HOME? THE ADJACENT CLOVER-LEAF COULD BE MADE SLIGHTLY MORE OVAL, IF NECESSARY, THIS WOULD BENEFIT EVERYONE IN THAT IT WOULD STRAIGHTEN OUT HAMPTON LANE, REMOVING THE DANGEROUS BEND, SHRINK THE TRAFFIC ISLAND, WHICH IS USUALLY AN UN-MOWED EYESORE, AND WOULD BE MUCH CHEAPER SINCE NO POLES OR CURBS AND WALLS WOULD HAVE TO BE MOVED.

I AM A TRAVELING MAN AND I'VE SEEN JUST ABOUT ALL TYPES OF INTERSECTIONS, AND I KNOW THIS IS A WORKABLE PLAN. PLEASE BELIEVE ME WHEN I SAY THAT I'M NOT AGAINST PROGRESS, BUT I AM FOR MORE CONSTRUCTIVE PROGRESS THAT WILL BENEFIT THE MAJORITY.

I'D LIKE TO THANK YOU AND YOUR DEPARTMENT FOR THE CONSIDERATIONS PREVIOUSLY ACCORDED MY NEIGHBOR, DR. HAROLD H. BURNS M.D., AND MYSELF. WE ALSO WOULD LIKE TO MEET WITH YOU OR WITH YOUR DELEGATE FOR AN ON SITE DESCRIPTION.

SINCERELY YOURS,

James T. Clark
 JAMES T. CLARK

500 Hampton Lane Towson MD 21204

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STATE HIGHWAY ADMINISTRATION
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME Steven G. Stamas DATE 3/9/90

PLEASE PRINT ADDRESS 1 Smeton Place #1204

CITY/TOWN Towson STATE md. ZIP CODE 21204

If we wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway # 695 unless barriers are built prior to construction in all areas that exceed Federal noise guidelines.

Steve Stamas

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mrs. Charles J. Bell DATE MAR 9 1990

PLEASE PRINT ADDRESS 712 ONE SMITH RD.
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

In 1975 the Bell Family moved into the Dulaney Towers Complex building on the east side of Beltway intersection. Initially the noise level from Beltway traffic was not annoying.

When the additional 2 lanes were later added the noise level became unbearable. The fact is attested by the installation of sound-out noise barrier walls along both sides of the Beltway south and west of where we live.

The addition of 2 more traffic lanes (total 6) would undoubtedly again raise the noise level still further.

If noise barrier walls would be erected I would agree to the 2 lane addition. If not, my vote to the proposed 2 lane addition would be negative.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Richard J. Schumiger DATE 3/6/90

PLEASE PRINT ADDRESS 38 Theo Lane
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

The add'l lane on the Beltway will increase the noise factor of my neighborhood - the barriers are skewed so we can continue to pay our increasing taxes.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

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V-70

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

**PROJECT
DEVELOPMENT
DIVISION**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 14 1 13 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MR. & MRS. EDWARD V. COOLAHAN DATE MAR. 9, 1990

PLEASE
PRINT

ADDRESS DULANEY TOWERS #205 / ONE SMETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

//We wish to comment or inquire about the following aspects of this project:

AS RESIDENTS OF TOWSON, WE ARE OPPOSED TO THE
PROPOSED ADDITIONAL LANE TO THE BALTIMORE BELTWAY
#695 BETWEEN YORK ROAD AND DULANEY VALLEY
ROAD UNLESS PROPER ARCHITECTURAL DESIGNED
BARRIERS ARE BUILT PRIOR TO CONSTRUCTION
IN ALL AREAS THAT EXCEED FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME



Ellen T. Herby
703 One Smeton Place
Towson, MD 21204-2728

DATE 3/10/90

PLEASE
PRINT

ADDRESS

CITY/TOWN _____ STATE _____ ZIP CODE _____

//We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to
Baltimore Beltway #695 unless barriers are built prior
to construction in all areas that exceed Federal
Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

V-71

Handwritten signature

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 14 1 23 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Betty Jean Alexizatos DATE 3-9-90

PLEASE PRINT ADDRESS 10 THEO LANE DULANEY TOWERS

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Unless barriers are built prior to this major construction in all areas that exceed Federal Noise Guidelines, I am strongly opposed to the addition of another lane to Baltimore Beltway #695.

Betty Jean Alexizatos

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 14 1 23 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Wm. T. Eickelberg DATE 3/9/90

PLEASE PRINT ADDRESS 125 Swarthmore Drive (Dulaney Towers)

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I oppose adding lanes to Beltway #695 until sound barriers are completed where noise levels exceed Federal guidelines.

W.T. Eickelberg

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement P. V-2.

W.T.E.

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(127)

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Marcella L. Wittich DATE 3-7-90

PLEASE PRINT ADDRESS 4-A Chesate Court

CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/Wa wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT DEVELOPMENT DIVISION

Mar 14 1 18 PM '90

NAME Nelida Mungatroyd DATE 3/5/90

PLEASE PRINT ADDRESS 32 Slankwake Ct.

CITY/TOWN Towson STATE Md ZIP CODE 21204

I/Wa wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines"

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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V-73

Please include this letter as part of the "Public Hearing Transcript" of February 28, 1990. Thank you for your consideration.

Sincerely,

Stephanie J. Boblooch

Stephanie J. Boblooch
President
Orchard Hills Community
Association, Inc.

cc. County Executive Dennis Rasmussen
Councilwoman Barbara Bachur
State Delegate Michael Gisriel
State Delegate Martha Klima
State Delegate John Bishop
The Honorable Barbara Mikulski
The Honorable Paul Sarbanes
Senator F. Vernon Boozar

Response:

1. See response for Noise Abatement on P. V-2.

202 Otterbein St.
Baltimore, MD 21230
685-3309
March 12, 1990

Mr. Neil J. Pedersen, Director
Office of Planning & Preliminary
Engineering
State Highway Administration
P. O. Box 717
Baltimore, MD 21203-0717

Dear Mr. Pedersen:

I attended the Public Hearing on the Baltimore Beltway project on February 28, 1990. Since then I have read your green book distributed at the hearing. To a limited extent in your book you do address both of the major concerns of the majority of citizens who made public remarks: better noise abatement for adjoining property owners and alternatives to adding one lane in each direction between route 140 and route 702.

As a person who for more than fifteen years has been active in various environmental groups (e.g. President of the Maryland Conservation Council, 1980-1982, and Treasurer of the Baltimore Area Transit Association since its founding in 1985), I urge you to make sure that what you now propose to do does not preclude some future light rail options (as discussed on your page 4) for connecting the proposed station on the Central Corridor Light Rail Line near where I 83 intersects with I 695 with downtown Towson. In other words, be sure to coordinate your present efforts with the "Statewide Commuter Assistance Study," to leave open the option of using the I 695 right of way between I 83 and Dulaney Valley Road for a future light rail route.

In regard to noise abatement, for short times I have experienced terrific highway noise where my wife's parents used to live in Connecticut. Unabated highway noise demonstrably lowers the quality of life (and, therefore, property values) for persons living near a major highway. As a resident of downtown Baltimore near Martin Luther King, Jr., Blvd., I also can sympathize with the citizens who testified, urging that you abate their problems before adding to them.

I know that it is not the responsibility of a highway planner to solve the big problem of travel congestion, but I take this opportunity to urge you and your superiors in the State Highway Administration to suggest that the Maryland Transportation Department give more attention to using public transit modes in addressing the travel congestion of the next century. Your presentation admits that by 2015 the Beltway is likely to be as congested as it now is. I recognize that the big problem now and for the near future is moving people East and West, not North and South, and that the old

Response:

1. The selected build alternate does not preclude light rail into Towson.
2. See response for Noise Abatement on P. V-2.

Pederaen (page 2)

rail systems that probably would be the basis for development of light rail lines basically radiate like spokes from the center of Baltimore City. Dealing with population growth and urban sprawl is not easy I realize!

Thank you for giving attention to my concerns.

Sincerely,

William G. Wilson

William G. Wilson

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 15 4 09 PM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME *Mr & Mrs David L. Rawlings - Sr* DATE *3/9/90*

PLEASE PRINT ADDRESS *129 SWARTHMORE DR*

CITY/TOWN *Towson* STATE *MD* ZIP CODE *21204*

I/We wish to comment or inquire about the following aspects of this project:

We are in support of the addition of another lane to Baltimore Parkway + 695 between Harwin + ...

Mr & Mrs David L. Rawlings - Sr

Please add my(our) name(s) to the Mailing List.*

Please delete my(our) name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

V-83

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**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

DEVELOPMENT
PAGE 1 of 3

NAME "Ruxton Hill Association" DATE 3/2/90

PLEASE PRINT ADDRESS c/o 6 Johnson Mill Road

CITY/TOWN Ruxton STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We, the members of "Ruxton Hill Association," submit the following comments in reference to the I-83 (JFX) MD 25 (Falls Road) interchange:

- 1.) We are going on record that the "Ruxton Hill Association" prefers a no build option.
- 2.) However, in reviewing the options, we would be willing to discuss option A and option D.
- 3.) We also go on record that we strongly oppose option B, and option C. We intend to fight these options with all our resources.

Name	Address
William S. Keller	6 Johnson Mill Rd. Ruxton 21204
William S. Keller	2 Lyman Ct Ruxton 21204
William S. Keller	4 Rockland Vue Ct Ruxton Md 21204
Charles F. Smith	8 Rockland Vue Ct Ruxton Md 21204
Elias & Nadia Zerhouni	5 Rockland Vue Ct - Ruxton Md - 21204
Robert & Elizabeth Johnson	3 Rockland Ct Ruxton Md 21204
D. Lockwood	6 Rockland Ct Ruxton Md 21204

- Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. I-83 (JFX) Option B was not selected.
2. I-83 (JFX) Option C was selected over Option D for better traffic operations and safety.

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**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PAGE 2 of 3

NAME "Ruxton Hill Association" DATE 3/2/90

PLEASE PRINT ADDRESS c/o 6 Johnson Mill Road

CITY/TOWN Ruxton STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Name	Address	(cont'd.)
John Edmunds	6 Rockland Ct	Ruxton
Imogene & Richard Wasserman	3 Rockland Vue Ct	Ruxton Ct
Toby & Seymour Escobar	1 Rockland Vue Ct	Ruxton 21204
Stan & Janet Beckel	1927 Old Court Rd	21204
Marion & Elizabeth Johnson	15 Rockland Ct	Ruxton Md 21204
John Smith	12 Rockland Ct	Ruxton Md 21204
Nancy Angler	10 Johnson Mill Rd	Built MD 21204
Billie Fearing	3 Johnson Mill Rd	Built MD 21204
Joseph Thomas	8 Rockland Ct	Built 11/21/2004
John & Ann Heiser	20 Rockland Ct	Built MD 21204
John & Helen	19 Rockland Ct	Ruxton Md 21204
B. R. S.	1730-2000 B	Ruxton Md 21204
Conrad & Madeline	1931 Old Ct Rd	Built Md 21204
John & Mary (Hester)	15 Rockland Ct	Ruxton Md 21204
Marion Quinn	11 Ruxton Ct #301	Baltimore, Md. 21204
Ann & Pearl	11 Johnson Mill Rd	Baltimore, MD 21204
Frank & Ruth Robinson	8 Johnson Mill Road	Baltimore Md 21204

- Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Handwritten signature or initials.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PAGE 3 of

NAME "Ruxton Hill Association" DATE 3/2/90

PLEASE PRINT ADDRESS c/o 6 Johnson Mill Road

CITY/TOWN Ruxton STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Name:	Address:
Donald T. Busch	9 JOHNSON MILL RD. 21204
Helena Busick	9 Johnson Mill Rd. 21204
Edward Arrigone	2 Rockland Vue Ct 21204
Richard Arrigone	2 Rockland Vue Ct 21204
Samuel [unclear]	2 JOHNSON MILL RD. 21204
John Liberty [unclear]	2 Johnson Mill Rd 21204

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mr and Mrs Alexander H. Pappas DATE March 8, 1990

PLEASE PRINT ADDRESS 11 Shea Lane

CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane to Baltimore Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal Noise Guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME EDWARD HEINZ DATE 3/10/90

PLEASE PRINT ADDRESS 6 E. CHOATE CT.

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of
another lane to Baltimore Beltway #695
unless barriers are built prior to
construction in all areas that exceed
Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME W. Glenn + Dorothy F. Rennie DATE _____

PLEASE PRINT ADDRESS 22 C. Alanbrooke Ct.

CITY/TOWN Towson STATE Maryland ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are opposed to adding another lane to
Baltimore Beltway #695 unless you build sound
barriers before adding the lane.
The Beltway noise now is so terrible that
we can not sit on our balcony and converse
properly with anyone. In fact, with windows
open in good weather the noise in our
living room is frustrating.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Response:

1. See response for Noise Abatement on P. V-2.

1009

88-A

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME SAMUEL J. & CATHERINE M. PICCO DATE MARCH 12, 1990

PLEASE PRINT ADDRESS 131 SWARTHMORE DRIVE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

OUR HOME IS LESS THAN A BLOCK INSIDE THE BELTWAY WEST OF DULANEY VALLEY ROAD. NOISE BARRIERS ARE ESSENTIAL FOR OUR COMMUNITY OF DULANEY TOWERS.

TO CONSIDER ADDING ANOTHER LANE TO THE BELTWAY IN THIS AREA "BEFORE" BARRIERS ARE INSTALLED WOULD BE PLANNING WITHOUT CONSIDERING THE EFFECT OF ADDITIONAL TRAFFIC NOISE ON THE NEAR-BY RESIDENTS.

PLEASE, PLEASE, PLEASE PUT THE BARRIERS UP, FIRST!

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

15!

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT DEVELOPMENT DIVISION
MAR 15 4 11 PM '90

NAME Me & MRS EDWARD F WELLER JR DATE 3/12/90

PLEASE PRINT ADDRESS 1104 ONE SNETON PLACE

CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are in complete accord with your long range plan to widen the beltway but only after you provide sound barriers in the Dulaney Valley Village areas.

If barriers are not installed please consider my comments as a negative vote.

Edward F Weller Jr.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

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Handwritten initials

68-A

GLENN P. HARE
4 BARROW CT.
TOWSON, MD. 21204
(301) 494-1499

March 13, 1990

Mr. Donald G. Honsywell
Project Manager
Project Planning Division
707 N. Calvert St.
Baltimore, Md. 21202

Re: Public Commsnts
Beltway Expansion Project

Dear Mr. Honeywell:

I attended the Public Hearing at Loch Raven High School on February 28, 1990 regarding the proposed plan to expand the Baltimore Beltway from MD140 to MD702. I did not publicly comment at that time but I will do so now -- because I do not feel that the State Highway Administration ("SHA") has adequately considered the negative impact that the 695 Beltway has had on the neighborhoods through which it passes.

My property, and the area of great concern, is immediately adjacent to the Beltway at its heaviest traveled section between the I-83 intersects--(I'm located just west of Thornton Road on the south side of 695).

At current levels of traffic, the Beltway has already become a nightmare for me and my family. The problems are well beyond the nuisance level. I am convinced that there are tangible health problems being created by the Beltway.

Expansion before the current problems are fully addressed is unacceptable and inhumane; expansion without additional protection against the ravishes of increased and closer traffic would be unconscionable.

You may already be aware of the problems, but I want the record to reflect that my family and I have personally experienced the following problems as a direct result of the Beltway being in our back yard:

1. Constant loud noise that makes it impossible to talk at a normal conversational level outside the house;

2. Constant loud noise that makes it impossible to sleep at nightThis noise is quite possibly causing gradual hearing loss as well --(were this private industry making the noise the government would shut the Beltway down instantly);
3. Constant vibration that upsets wall fixtures;
4. Unacceptable levels of pollution (auto emissions) and soot that (1) leave the exterior of my house constantly grimy, (2) cause cumulative damage to my lungs and to the lungs of my wife and two young children, and (3) exacerbate our allergies; and
5. Rats -- (Yes, the Department of Health has specifically verified this and has attributed them to your Beltway). We maintain a meticulously clean house and yard.

I do not want to hear more obtuse arguments about (1) date of construction, (2) cost of sound barriers, or (3) artificial state or federal guidelines. These are excuses for inaction despite the existence of a real health problem. I do want the SHA to immediately address the aforementioned problems in a positive and comprehensive manner without further excuses or "back passing," even if it means "no build," "rebuild" or "relocate" the Beltway.

At a minimum, I expect the SHA "with all due haste" to erect state-of-the-art sound barrier walls around every community that currently has Beltway noise levels exceeding 67 decibels. No excuses -- You have found money in the past for everything but the health and safety of those who live by the Beltway. I am certain you can help us if you tried.

Until your highway becomes "quiet," I will continue to pursue my complaints "loud" and clear. There are thousands of families out there that feel exactly like I do and our voices will be heard above your noise.

This callous abuse of our neighborhood by your highway borders on criminal neglect. "Progress at any price," as measured by lanes of traffic, is too costly for me and my family.

Very truly yours,


Glenn P. Hare

Response:

1. See response for Noise Abatement on P. V-2.

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06-A

Response:

1. No response required.

GLENN P. HARE
4 BARROW CT.
TOWSON, MD. 21204
(301) 494-1499

March 13, 1990

Mr. Donald G. Honeywell
Project Manager
Project Planning Division
707 N. Calvert St.
Baltimore, Md. 21202

Re: Public Comments
Beltway Expansion Project

Dear Mr. Honeywell:

I want to thank you for taking the time yesterday to speak with me on the Beltway Expansion Project that was the focus of a Public Hearing at Loch Raven High School on February 28, 1990. I also want to thank you for extending the time for submitting written comments for the "Public Hearing Transcript" by one week from the original March 16 cutoff date.

I understand that this extension will apply to the following homeowner/improvement associations:

Greenway Garth
Village Green
Thornleigh
Longford
Seminary Ridge
Heatherfield
Ruxton Green
Ruxton Hill

Your cooperation is most appreciated.

Sincerely yours,


Glenn P. Hare
Greenway Garth

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Response:

1. See response within Agency Coordination.

H 1



DEPARTMENT OF THE ENVIRONMENT

2500 Broening Highway, Baltimore, Maryland 21224

Area Code 301 • 631-

William Donald Schaefer
Governor

Martin W. Walsh, Jr.
Secretary

March 9, 1990

Mr. Louis H. Ege, Jr., Deputy Director
Office of Planning and Engineering
Maryland State Highway Administration
707 N. Calvert Street
Baltimore, Maryland 21202

RE: Environmental Assessment Improvement of I-695 from East of MD 140 to West of MD 702 in Baltimore County, Contract Number B 635-151-472.

Dear Mr. Ege:

We are in receipt of the above-referenced document and have reviewed the potential water quality and resource impacts of the proposal. Considering the scope of the project, the primary impacts to state wetlands and waterways appear to be minimal. However, the potential for secondary impacts appears to be significant. The Division of Standards and Certification has no objections to this proposal provided the following conditions are satisfied.

1. Interchange options for I-83 JFX which result in the greatest impacts of the project should be further reduced if possible. The waters in this area are Class III, and are afforded the highest level of protection.
2. Mitigation for Class III and IV impacts should include restoration of streams and riparian habitat in addition to a minimum of 1:1 wetland creation.
3. Areas bound by access ramps should not be used for mitigation areas.
4. All newly constructed impervious areas shall be subject to stormwater management of a minimum of the first one-half inch of runoff in uplands. Vegetated medians and swales removed for road widening

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Mr. Louis H. Ege, Jr.
Page 2 of 2

State Highway Administration
Office of Planning and
Preliminary Engineering
P.O. Box 717
Baltimore, Maryland 21203

March 14, 1990

and which serve as stormwater attenuating areas for existing road surfaces shall be compensated for by providing alternative methods of stormwater quality management for this runoff. Infiltration is preferred.

5. All work in wetlands and waterways is prohibited from October 1 to April 30 in Class III waters and from March 1 to May 31 in Class IV waters.
6. Wet ponds may not be used for stormwater control if they discharge to Class III and IV waters. Infiltration of a minimum of the first one-half inch of runoff is the preferred method of stormwater management in these areas.
7. Naturally occurring State wetlands and waterways shall not be impounded for the purposes of stormwater retention or mitigation enhancement.

We hope that this information is helpful. If you have any questions, please contact me at (301) 631-3609.

Sincerely,

Andrew T. Der
Andrew T. Der
Standards and Certifications Division

cc: Linda Milchling
James Tielt

ATD/lg

Re: Contract No. B 635-101-472
~~BALTIMORE BELTWAY~~

Dear Sirs:

After attending the public hearing and examining the project booklet, I would like to offer the following comments:

LOCH RAVEN @ BELTWAY

I concur with the design. However, I disagree with the location of the storm water management facility. As previously suggested, a possible location could be inside the ramps in the northwest quadrant. This area is already depressed and could serve as an ideal location for a storm water management facility. The State already owns this land, therefore, no land would have to be acquired. In addition, water quality should be done by using infiltration or extended detention. No permanent pool of water is desired because of safety considerations between wildlife and high speed traffic.

PROVIDENCE @ BELTWAY

I concur with this design also. However, would it be possible to locate the storm water management facility in Campus Hills Park? This could reduce the cost of land acquisition because the State already owns the property.

It may be possible to save the existing bridge and providing a modification as shown on the attached sheet.

BELAIR @ BELTWAY

I work for Baltimore County, but am pursuing this intersection change as a private citizen and an engineer. Baltimore County through its Department of Environmental Protection and Resource Management will be evaluating Stemmers Run for water quality retrofit. They will be looking at Double Rock Park for this work. Because of the close proximity of the Beltway to the park, a joint effort may be the best solution for this area. In addition, the County has downstream flooding complaints on record. If the State acquires the four properties, could one or more of these properties be added to Double Rock Park?

V-92

DULANEY VALLEY @ BELTWAY

There is a serious flaw in this design. It occurs with the eastbound Beltway on and off ramps. The major concern is the eastbound off ramp which ties into Hampton Lane. The design has a stop sign installed to stop westbound Hampton Lane. This is a good idea, but what about eastbound Hampton Lane from Dulaney Valley Road? There is insufficient distance to queue cars. This could lead to a traffic backup on Dulaney Valley Road. A possible solution is: The elimination of all rampage in the northeast quadrant. The exit ramp would start as a long deceleration lane and become a double exit ramp, as shown on the attached print. A traffic signal already exists at this intersection. A left turn lane already exists on the Dulaney Valley Road Bridge. The traffic signal could be coordinated by the use of a loop detector on the exit ramp and giving favorable conditions to the exit ramp. This will eliminate potential conflicts with Hampton Lane and minimize the delays which could be experienced on Dulaney Valley road. I ask that you consider this recommendation as the solution at this intersection.

YORK @ BELTWAY

I concur with part of your design. A flaw exists in the eastbound off ramp which ties into West Road. The exit ramp as shown will not correct an existing problem. In fact, it may worsen the problem, especially in consideration of development on the southwest corner of York Road and West Road. Additional vehicles will be generated when this site is developed. The recommended solution is to eliminate the intersection of York and West Roads at its present location and shift it as shown on the attached print. Close the section of West Road shown and shift the location of the development to the north. Provide a double exit ramp similar to that recommended for Dulaney Valley Road. I also hope that you consider this recommendation very seriously. In addition, this configuration will also help with the entrance ramps from Charles Street and I-83 by providing longer merge lane distance for weaving.

GENERAL

I also ask that you look at the areas inside of cloverleaf type entrance and exit ramps for the possible location of storm water management facilities. No permanent pools of water should be established in the storm water management facilities because of the conflict between the high speed traffic of the Beltway and wildlife.

As always, I thank you for keeping me advised of any activity on this project. If I think of further ideas which could reduce the cost of this project, I would like to present them to you. Once again, I thank you for allowing me to comment on this project. Because of my interest in the Baltimore Beltway, I would like to be kept informed of any studies or projects relating to any part of the Beltway.

Very truly yours,

John Frisk III
John Frisk, III

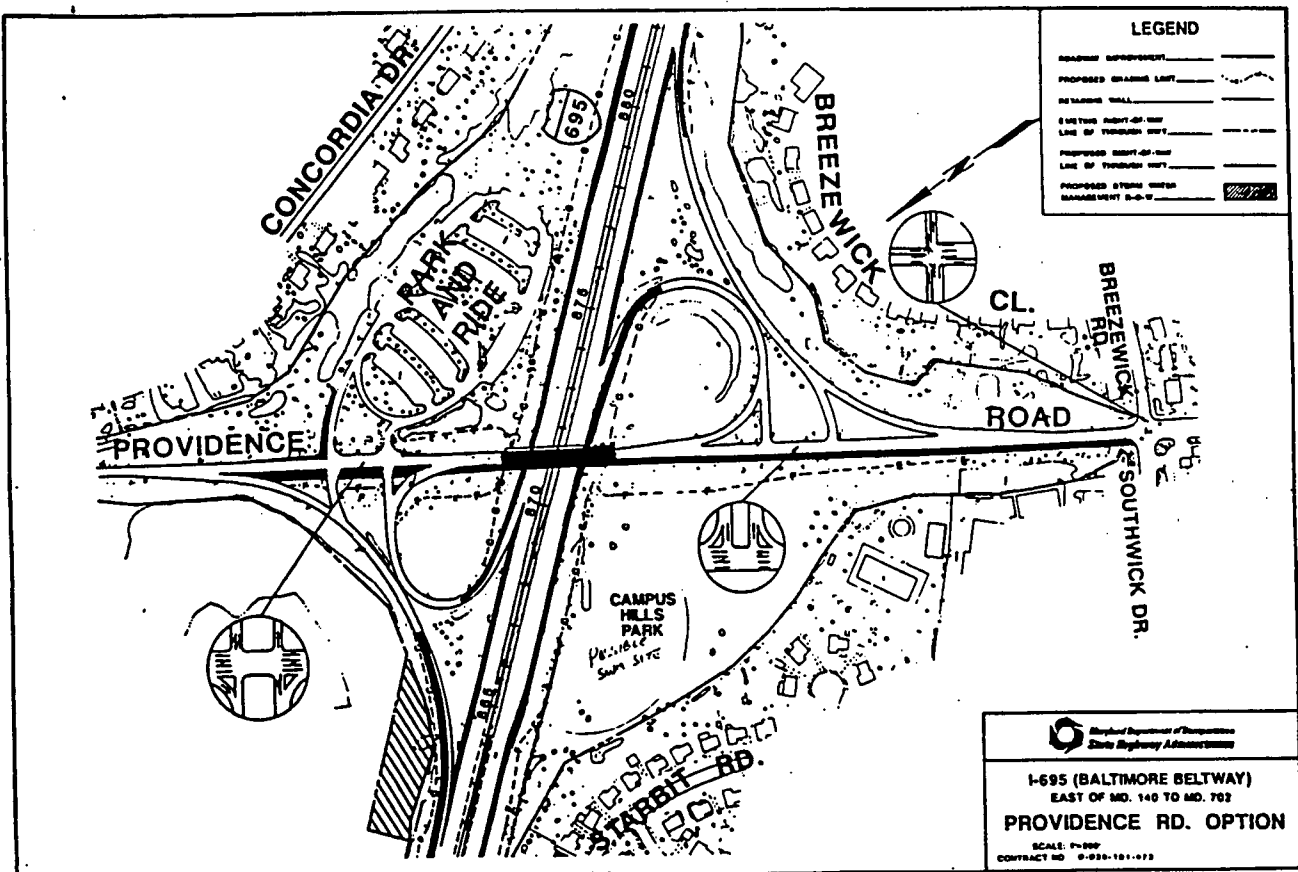
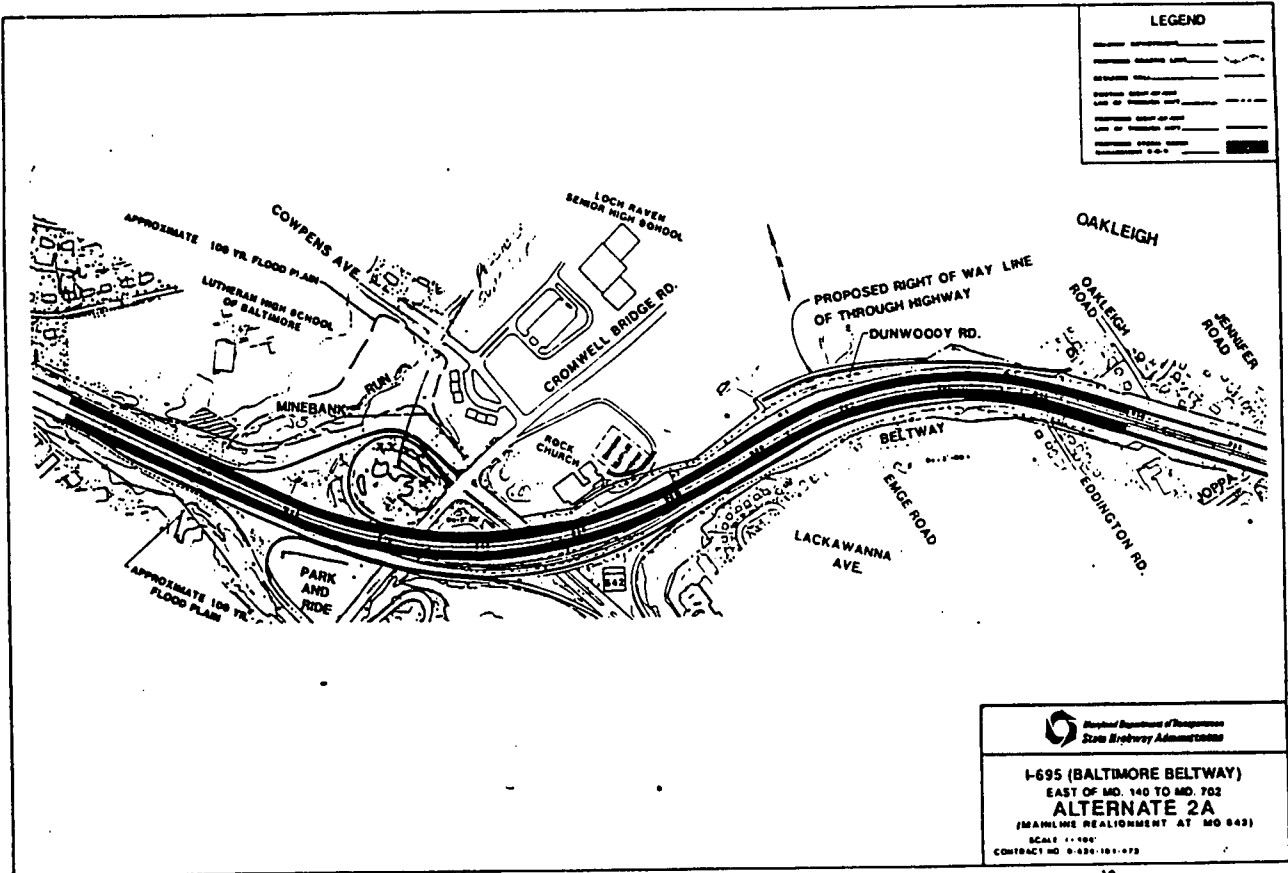
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attachments
cc: file

Response:

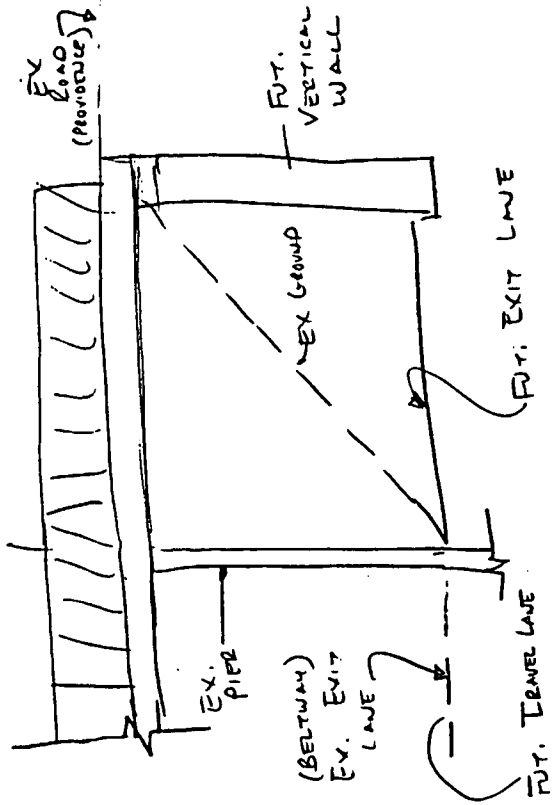
1. S.W.M. location will be finalized during highway design.
2. The selected build alternate will not impact Double Rock Park. A modification of the option will not require acquisition of the four properties.
3. The option at MD 146 would not provide adequate traffic operations.
4. The option at MD 45 was investigated and dropped due to right-of-way impacts.

585

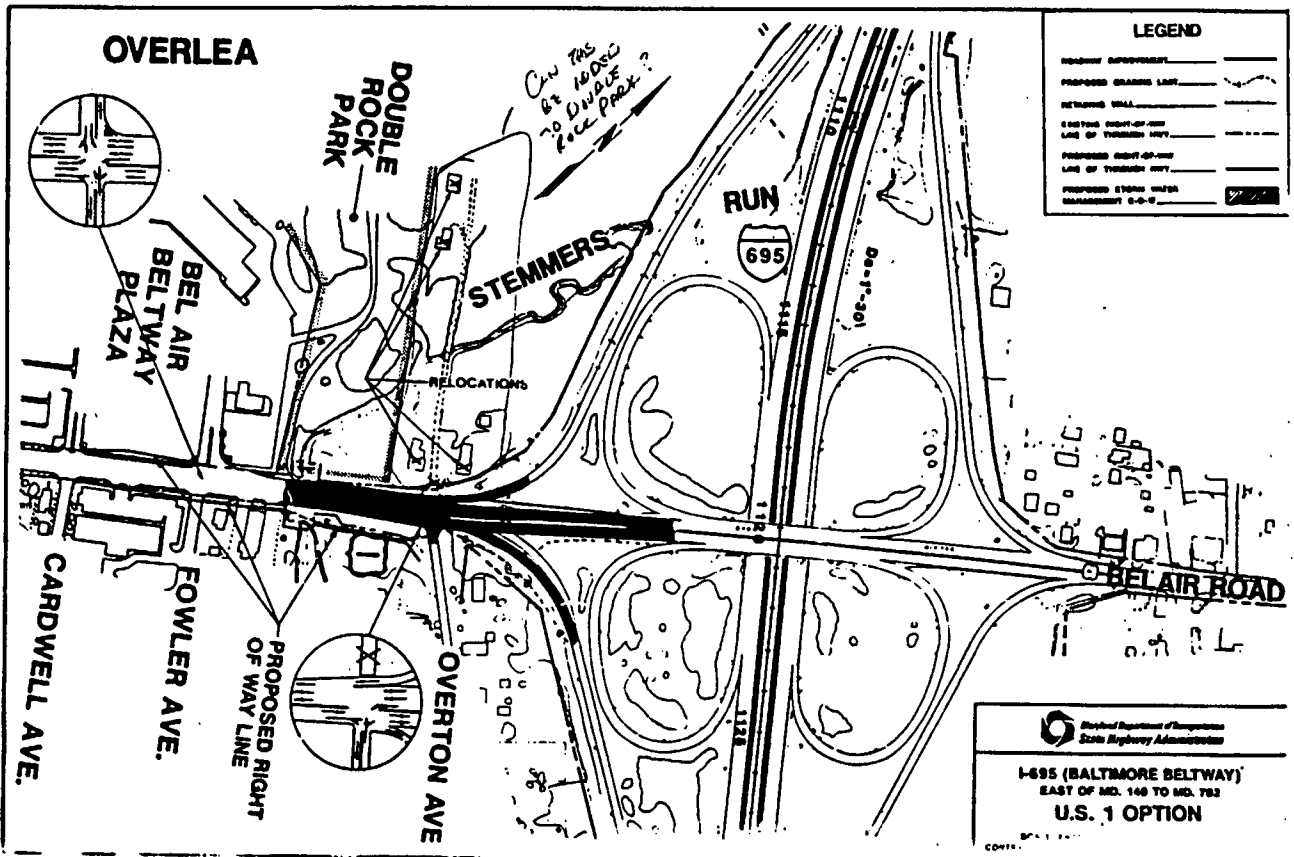
250



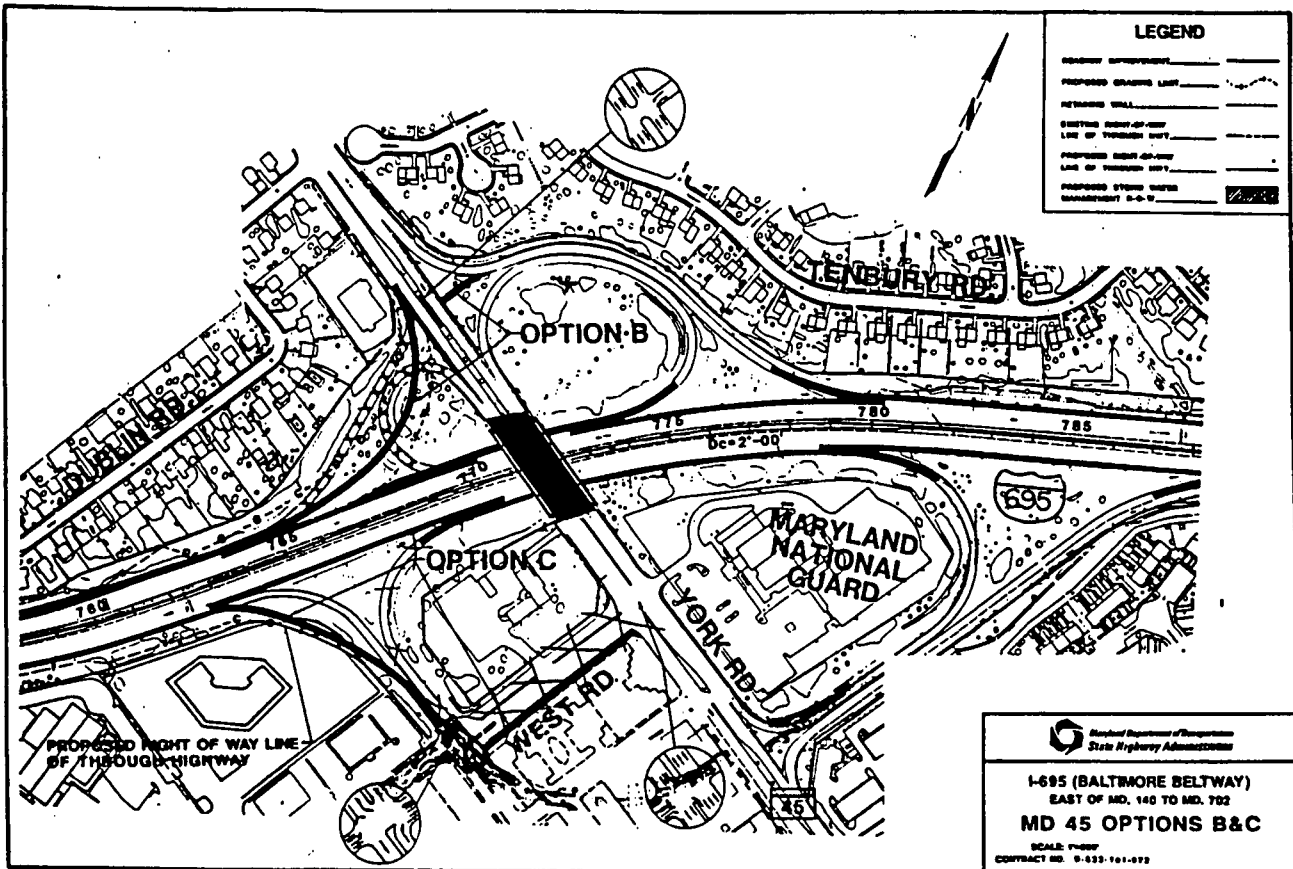
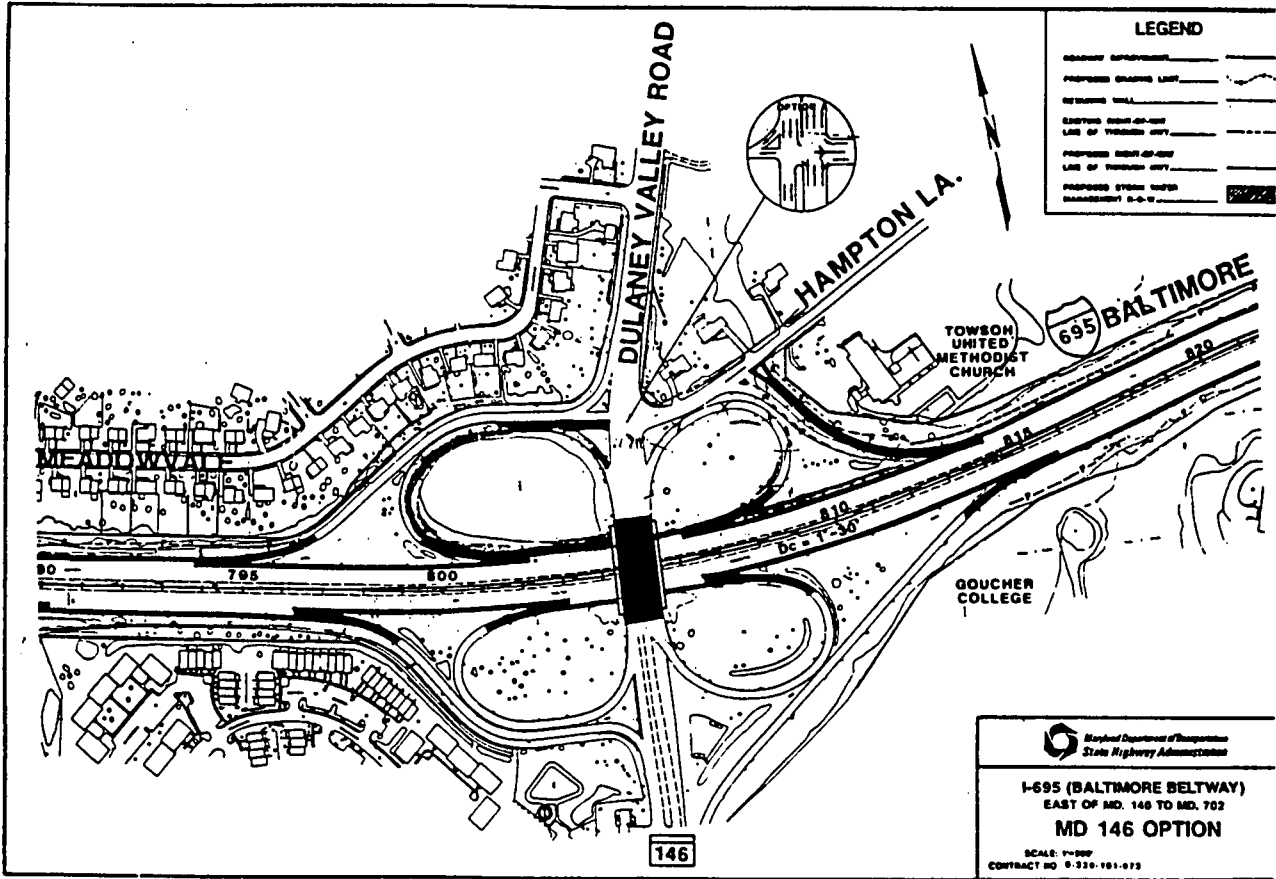
123



PROVIDENCE ROAD
BRIDGE MODIFICATION
NOT TO SCALE



202



Response:

1. See response for Noise Abatement on P. V-2.

Rec. 3/16/90

March 12, 1990

Irene MacCarron
14 Barrow Court
Towson, MD 21204

Mr. Donald G. Honeywell
Project Manager
Project Planning Division
707 North Calvert Street
Baltimore, Maryland 21202

Re: Public Comments, Beltway Expansion Project

Dear Mr. Honeywell:

Ever since I attended the Public Hearing at Loch Raven High School on February 28th I have been contemplating what I could possibly say to make the State Highway Administration understand what it is like to live near the beltway and the unbearable thought of what it would be like to add to that.

I do not live directly on the beltway as many of my neighbors do, I am lucky enough to have a flood plain in the middle. However, I do have a beautiful home with a 40 ft. deck across the back that cannot be used. Each time we use our deck I must remove black soot from the surface of every chair and our white pic nic table that cost me \$700. The noise level is so intense it is like eating dinner on the median strip. You must yell at each other in order to be heard. The dust level in my home was at such a point that my husband and I purchased a \$450 air filter to try to get a grip on it. I could dust and vacuum and then write my name on every piece of furniture in the house within 24 hours. In the winter months when the foliage dies on the trees we can count every truck that passes and tell what gear they are shifting into. What is really worse is when the trucks are empty because the beds bounce all over the road; they sound like explosions.

My husband and I worked very hard to build this house. We even suffered a loss of \$30,000 because our builder went bankrupt. We are very happy with the way the house turned out, we love the school district for our son and best of all we have wonderful neighbors. It is easy for the people at the Highway Administration to say if you don't like the noise move. But you see we would be giving up much more than the beltway, we would be giving up a whole way of life. The people in this community work hard to maintain their homes. You can drive through here any day in the summer and find people gardening, trimming and mowing their lawns and generally taking very good care of their property.

We are asking with all sincerity that you please provide this community with sound barriers now and then come up with a rational plan to keep the flow of traffic moving along the beltway. We are not trying to hold back progress, we are simply trying to have happier, healthier lives.

Thank you for your immediate attention to the health and welfare of our community.

Sincerely,
Irene V. MacCarron
Irene V. MacCarron

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Response:

1. See response for Noise Abatement on P. V-2.

Dr. and Mrs. Nell M. Scheffler
2810 Grasty Woods Lane
Baltimore, Maryland 21208

March 13, 1990

Donald G. Honeywell
Project Planning Division
State Highway Administration
P.O. Box 717
Baltimore, Maryland 21203-0717

Dear Mr. Honeywell:

It was a pleasure speaking with you again earlier today. Thanks for the wealth of information that you have supplied to me regarding the Beltway expansion and noise and pollution abatement for our area.

As I explained, all of our neighbors met on March 5th, following the hearing that was held on February 28th. Reports were heard and lengthy discussions followed. Our unanimous feelings follow below.

Needless to say we are VERY concerned about the health effects the Beltway will exert on our families. As you know noise level readings taken at Mr. and Mrs. Snyder's house (2808 Grasty) show that the noise levels have already increased from below health-risk levels (from earlier tests) to levels that are considered unsafe (your latest readings). Increased traffic on the Beltway, repaving and legislation allowing bigger trucks to pass our homes may be contributing factors. With or without the Beltway expansion plans we can expect even more disruption of our dangerous environment. We would like to minimize that disruption.

We believe that the only way for the State to fully protect our safety would be with a sound barrier and would like to be considered for the same. We understand, however, that alternative methods may be used in some areas. These methods may include landscaping alternatives such as dirt and/or trees to block and absorb noise and pollution. If true structural barriers are not approved for our area because some criteria are not met please help us by means of an alternative method.

162
162

V-97

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

MAR 16 10 01 AM '90

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

As I mentioned during our conversation today, there are other environmental matters that we would like addressed. First of all are our resident falcons. A pair of these endangered birds live in our neighborhood. Please consider them in your impact studies. Mr. Charles Green (2800 Grasty; 484-4954) may be better equipped than I to give you more information about the birds.

We are also concerned about increased runoff from the road. Grasty Woods Lane has flooded on numerous occasions. Previous efforts to alleviate the situation have not worked. Trees that have been uprooted along the stream (Slaughterhouse) by previous flooding were not replaced. Since this is the only way out of the street we must be assured that flooding will not increase. Possibly the landscaping measures that will block the noise and air pollution will also help in this regard.

Once again thank you for your assistance in this matter.

Sincerely,

Dr. Neil M. Scheffler

NAME DEBRA FANGIHKIS DATE 3/5/90
 PLEASE PRINT ADDRESS 109 SWARTHMORE DR
 CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Before 695 expansion begins, all Noise Barrier should be completed. I am under the understanding that the Towson section of Noise Barrier is not to be erected. This seems unfair, since other areas have had barriers. Where housing has not been as close (as in the Towson area) to the beltway, i.e. Hampden Lane + Howbery should a barrier site where no one resides have barriers + some hundred of feet away from the beltway have had the ~~barrier~~ over Towson + Cluloney + York Rd? or on the west side @ Wilkins where at least a hill provides some noise relief.

I think we need to be reasonable + finish one job before we begin another. Residents have been patient + waited long enough.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

16
 J. G. H.

V-96

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

PROJECT
DEVELOPMENT
DIVISION

MAR 15 10 01 AM '90

NAME Mrs. M. L. Wilson DATE 3-5-90

PLEASE PRINT ADDRESS 20 Barber Court

CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*Consideration of building another lane
to 695 until sound barriers have been
constructed is absurd. I live on the
Dulany Valley Rd. Street (exit 27) and can
even see the telephone at the rear of the
house because of traffic noise. Further
consideration of barriers between York Rd.
and Dulany Valley Rd. is of the utmost
importance.
I never miss an opportunity to vote.*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Vienna L. Aiello DATE 3/11/90

PLEASE PRINT ADDRESS 21 Bellows Ct

CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

*I am opposed to the addition
of another lane to Balto. Beltway #695
unless barriers are built
prior to construction in all areas
that exceed Federal noise level limits*

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

66-A

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**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Ms. Betty D. Davis DATE 3/12/90

PLEASE PRINT ADDRESS PA Choate Ct.

CITY/TOWN Towson STATE md ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

No expansion without guarantee of noise control barriers
Other things which must be considered during peak use periods -
car pool lanes; diversion of truck traffic.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.
2. The selected build alternate does not preclude future HOV lanes.

ORIGINAL
DEADLINE

MARCH 16, 1990

V-100

3/12

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

Rec. 3/19/90

March 14, 1990

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 13 10 14 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRS BETTY R. MURPHY DATE 3/9/90

PLEASE PRINT ADDRESS 24-B ALAN BROOKE CT.

CITY/TOWN JAWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another
lane to Baltimore Beltway by 695 unless
sound barriers are built prior to
construction.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Gentlemen:

With regard to the proposed beltway widening, especially near the northeast Bl 83 junction, please consider this a registration of strong opposition. Until such time as noise barriers have been erected in this area, additional traffic allowed by this expansion will cause increased aggravation, noise and air pollution and discomfort to the residing property owners in this district.

Yours very truly,
Dana H. MacIndosh
7 Bannow Court
Baltimore, Maryland 21204

Handwritten initials

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME M. Jane Herlach DATE 3/13/90

PLEASE PRINT ADDRESS 16 Borden Ct.

CITY/TOWN Towson STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I find it extremely difficult to understand how this project is receiving funding when the Dulles/Towson Development can not receive a major business.

Besides we were promised an outer beltway at one time. All these projects either fall through or are delayed.

I am certainly against any funding or projects until the outer beltway is put in.

To date nothing we have been promised has become a reality. I feel that our increased taxes and bringing in nothing and increasing gas tax has only added more taxes. Taxes are the basis for the road.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME MRS. Doris Page DATE MARCH-11-1990

PLEASE PRINT ADDRESS 1006 MARLEIGH CIRCLE

CITY/TOWN TOWSON STATE MD ZIP CODE #21204

I/We wish to comment or inquire about the following aspects of this project:

I believe in progress (of changes being made for the good of the public) but I also believe in changes for the good of the tax payer. What I am saying is our community (along the beltway) needs the wall more than ever. We are just about on the beltway as it is.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. See response for Noise Abatement on P. V-2.

V-102

PROJECT DEVELOPMENT
MARCH 1990

255

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 13 10 15 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Henry E. Boone DATE 3-9-1990

PLEASE PRINT ADDRESS 143 THEO LANE

CITY/TOWN Towson STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF
ANOTHER LANE TO BALTIMORE BELTWAY #695
UNLESS BARRIERS ARE BUILT PRIOR TO CONSTRUCTION
IN ALL AREAS THAT EXCEED FEDERAL NOISE
GUIDELINES

Henry E. Boone

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

PROJECT
DEVELOPMENT
DIVISION

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

Mar 19 10 15 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Jan B. Sterling DATE 3-13-90

PLEASE PRINT ADDRESS 29 Theo Lane

CITY/TOWN Towson STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I am against the construction of an additional
lane to Baltimore Beltway #695 unless
barrriers are built prior to construction
in areas that exceed the Federal
noise guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
 QUESTIONS AND/OR COMMENTS DIVISION

CONTRACT NO. B 635-101-472 MAR 19 10 15 AM '90
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

RECEIVED

Sony R. Florendo's

RESTAURANTS
 BALTIMORE, MARYLAND

MAR 19 1990
 DIRECTOR, OFFICE OF
 PLANNING & PRELIMINARY ENGINEERING

7718 Belair Road
 Baltimore Md. 21236
 March 15, 1990

PLEASE PRINT NAME Mr. William Kerner, Jr. DATE 3/13/90
 ADDRESS 113 Sparrowhawk Drive
Towson, MD 21204
 CITY/TOWN _____ STATE _____ ZIP CODE _____

Mr. Neil J. Pedersen, Director
 Office of Planning and
 Preliminary Engineering
 State Highway Administration
 707 N. Calvert St.
 Balto. Md. 21202

Dear Mr. Pedersen,

Thank you for your presentation of the Project Planning Study of I-695 (Baltimore Beltway), from east of Md. 140 (Reisterstown Rd.) to Md. 702 last February 28, 1990 at the Loch Raven Senior High School auditorium. We take this opportunity to comment upon the engineering, social, economic, environmental and right-of-way aspects of the alternative under consideration.

I am Sony (Resurrection) Florendo, owner of 7718 Belair Road known as Lodge Cliff. I represent its interests and am an active participant in the community.

Our comments follow.

- We strongly endorse the major objectives of the US 1 Option A for
 increased safety
 increased traffic accommodation
 increased capacity for economic and social activities
 We strongly request the earliest possible implementation of these objectives at this particular interchange because lives and property are at risk of being damaged and lost at an alarmingly increasing rate.

- 7718 Belair Road (Lodge Cliff) is the site of a minority owned business and residence.

 Lodge Cliff is zoned commercial with residence allowed in the premises.

Lodge Cliff does not have to be relocated even if the access

Party & Catering Consultants

PHILIPPINE-ASIAN RESTAURANT
 324 PARK AVENUE
 BALTIMORE, MARYLAND 21201
 (301) 539-2466

HARBORPLACE
 2nd FLOOR
 LIGHT STREET PAVILLION
 539-2467

OWINGS MILLS TOWNE CENTER
 383-8116
 7718 BELAIR ROAD
 661-7444

I am opposed to the addition of another lane to Baltimore Beltway # 695 unless barriers are built first. Construction in all areas that speed Federal River Bureliaw

Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:
 1. See response for Noise Abatement on P. V-2.

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V-104

Handwritten signature/initials

to the Right-of Way to the other business and residences as shown in the page 25 drawing is denied because Lodge Cliff has its main access 100 ft. further south of the property. This main access to these commercial sites is situated relative to the traffic lanes similar to the accesses to the commercial site shown in the Page 24, Md. 147 (Harford Rd) Options A & B drawing: NONE of which are being relocated.

3. Lodge Cliff is developing programs enriching

cross cultural awareness
international understanding
children and youth development and exposure to global ideas

Lodge Cliff is a location for meetings of professional and civic groups. Lodge Cliff is a place where Filipino-Americans find their identity and interact.

These business plans can be made available to the proper agencies upon request. See addendum for highlight of the special projects.

4. The following Proposals are being presented for your kind consideration. Your reply and comments to the is requested and greatly appreciated.

Drawings: See attached

Advantages achieved by the proposals:

Greater safety
Smoother merging with Belair Road traffic
Higher elevation of entry onto Belair Road resulting in increased visibility of traffic conditions.
Left turn into Overton Avenue will not be restricted.

No minority businesses and residences have to be relocated.
No displacement of employment for approximately 50 people.
No reduction in government tax income from affected properties and businesses.
Very significant reduction in the estimated \$2.9 million cost of U.S.1 Option A as presently outlined.

Action:

Adopt/modify Proposal A or B

As an immediate partial solution to the hazardous traffic condition now, a traffic control device can be installed at the present intersection. Such traffic device now will surely provide more safety to travelers compared to no traffic device at all.

Pay 7718 (Lodge Cliff) property to grant the other residences and businesses Right of Way to its South access.

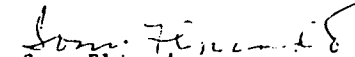
Pay the properties for the decrease in value to the properties.

Pay the Park if access has to be located further south and connected to southeast corner of 7718 (Lodge Cliff) property.

Implement at the earliest possible schedule placing the proper high priority due to the present existing danger.

Thank you for your kind consideration.

Sincerely yours,


Sony Florendo

cc: Governor William Donald Schaefer
Senator Barbara Mikulski

Addendum:

Highlights of Special Projects

From 1988, the 10 year plan for Lodge Cliff is as follows:

1988 - Started using the facility as a base for food research and development. Products developed are sold at presently owned fast food facilities in Baltimore. Many more products and systems are being developed, some are in their final stages for sale to others in the food industry. Because of the excellent location and access to Interstate Routes, Lodge Cliff is ideal for a business office or interstate trading.

1988 and on - Programs for the following are either on-going or in its final stages for implementation.

- 1.) Cross cultural awareness through food
- 2.) Internation understanding through and other cultural activities.
- 3.) Special programs and activities for children and youth to foster understanding of the people around them. These programs will address youngsters both from the public and private schools.

This program will allow students in the food industry, sociology, and international studies (college levels) to be part of the implementation of this program. Dialoguas with appropriate school officials and ethnic groups along these lines began as early as 1987.

- 4.) Lodge Cliff is available to the Filipino-American community as their gathering place to showcase and share to the rest of the community their heritage.

The 10 Year Plan for Lodge Cliff is a very important commitment of Sony Florendo to the community and to the State of Maryland in terms of economic growth and cultural understanding.

Sony Florendo has a professional degree in Foods, Nutrition and Dietetics and in Institution Management. Her work in food research and development is recognized in the different food establishments she has in Baltimore. Her work with children and youth, both in nutrition and internation understanding are appreciated by her professional groups as well

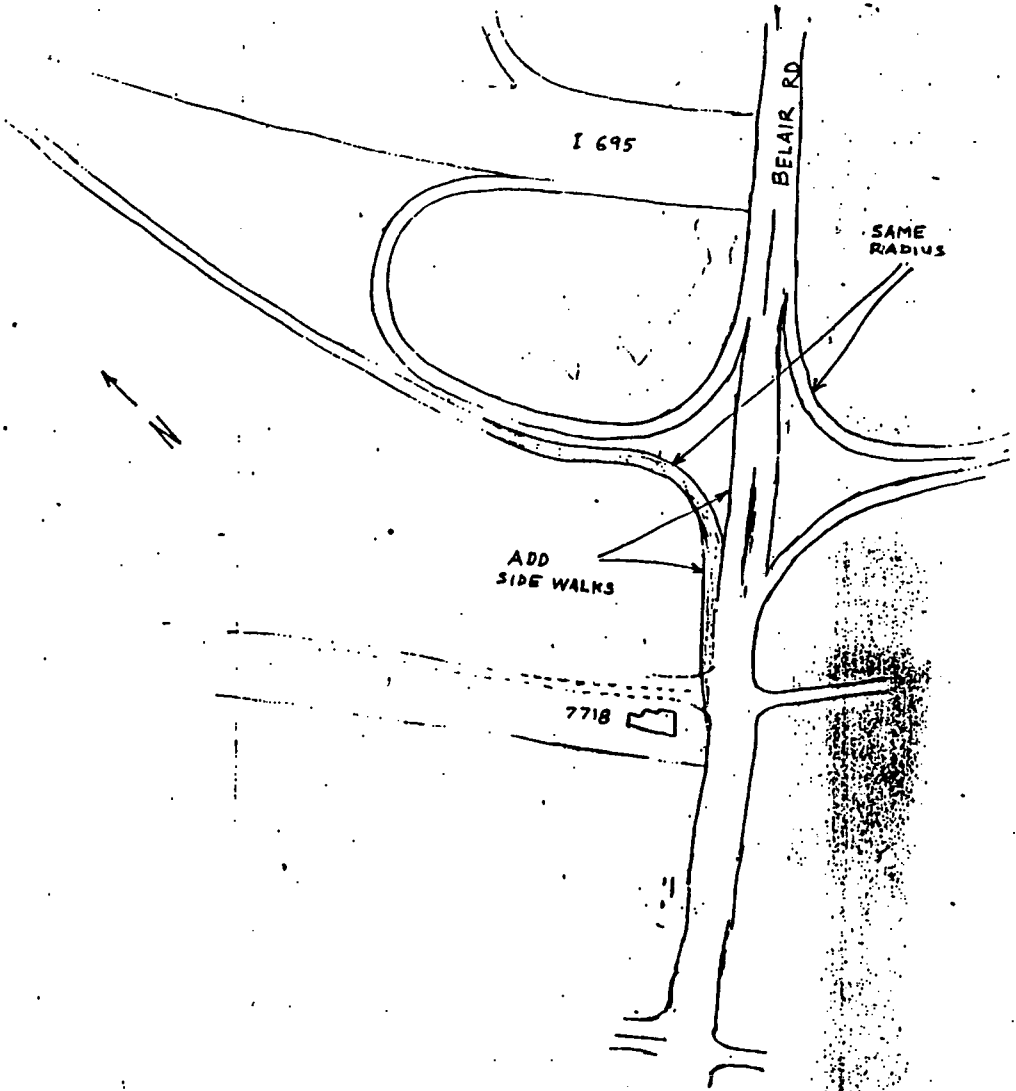
as the community. Her continued participation in the area of ethnic and global understanding through foods is naturally a very strong commitment. Lodge Cliff, at its strategic location, is very vital in the success of these projects.

Response:

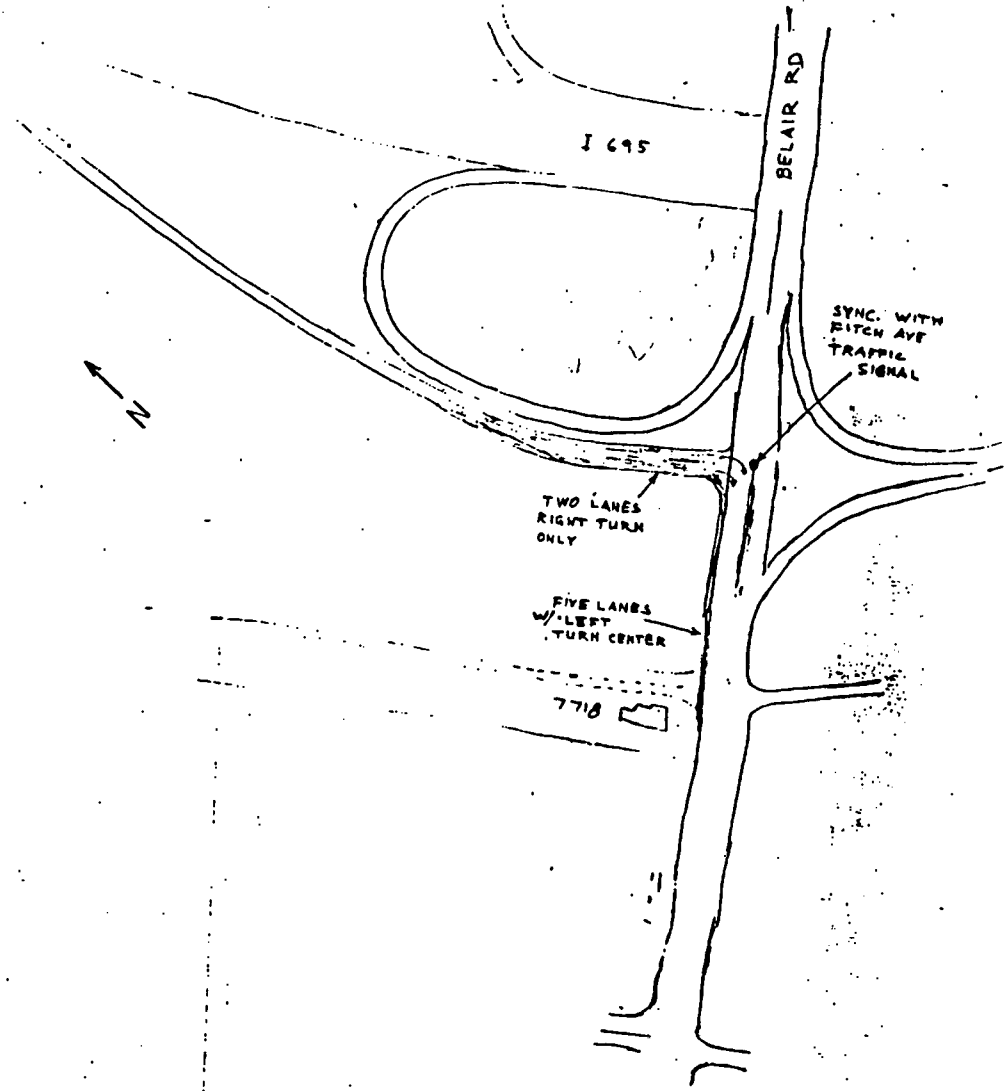
1. U.S. 1 Option has been selected.
2. Modifications of the option have been selected which will not require acquisition of the four properties.

200

V-107



PROPOSAL A



PROPOSAL B

333

801-A

10 Barron Court
Town, Md 21204
March 13, 1990

State Highway Administration
Office of Planning and Preliminary Engineering
Box 717
Baltimore, Md 21203

Sir:
On record we would like to
state that we do not wish
any further expansion of the
Beltway, unless some form of
sufficient noise reduction (barrier
wall) is incorporated into the
Beltway expansion plans.
Thank you. Sincerely,
Lynette or Richard Goodney

Response:
1. See response for Noise Abatement on P. V-2.

175

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT
MAR 13 2 01 PM '90

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

RECEIVED
MAR 16 P 3:13
PLEASE PRINT
NAME Mr & Mrs Joseph W. D. Wyatt DATE 3-3-90
ADDRESS 41 Theo Lane
CITY/TOWN Towson STATE Md ZIP CODE 21204

If we wish to comment or inquire about the following aspects of this project:

We are opposed to the addition of another lane
to Baltimore Beltway #695 unless business area
shut goes to construction in all areas that exceed
National Noise Guidelines

Response:
1. See response for Noise Abatement on P. V-2.

176

304

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2.

March 12, 1990

Christopher and Barrie Brown
3 Barrow Court
Towson, Maryland 21284

State Highway Administration
Office of Planning and Preliminary Engineering
Box 7114
Baltimore, Maryland 21203

To whom it may concern:

We are residents of a neighborhood that borders the Beltway and are extremely opposed to the plans for expansion of the Beltway parallel to our home. We oppose this expansion as the noise level is already at an unbearable level and will increase with this expansion. We feel that a sound barrier should be a mandatory part of this plan. The beltway is jeopardizing our health and our personal property and we find expansion without a sound barrier unacceptable and without consideration to the families that will be affected.

Sincerely, Barrie and Chris Brown

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 20 10 45 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mrs. P. E. Jeanneret DATE 3/7/90

PLEASE PRINT ADDRESS 906 One Smeton Pl

CITY/TOWN Towson STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane
in Baltimore's Beltway #695 unless barriers
are built prior to construction in all areas
that exceed Federal Noise Guidelines

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

305

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 20 10 45 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Mrs. L. D. Kennedy DATE 3/15/90

PLEASE PRINT ADDRESS 15 THEO LANE
CITY/TOWN TOWSON STATE Md. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

If the State finds it necessary to increase the present Beltway by two additional lanes, I certainly hope that you will give the neighborhood of Lindey Tower the same consideration that was given to the Hampton Mansion property. I personally feel that this noise protection should have been forthcoming to this development while all of the equipment was so readily available to those "open spaces" rather than our neighborhood.

Thank you for your serious consideration regarding this project, since it is a problem that affects the life style as well as the value of our properties.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT
DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 20 10 45 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ANNA D GEIGER & ANN MARIE BAYNE DATE 3/14/90

PLEASE PRINT ADDRESS 1010 Marleigh Circle
CITY/TOWN TOWSON STATE Md ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Sound Barrier Our back property backs up to Beltway. It is also bottom of incline to West Road exit. The trucks shift gear at that spot and the noise is unbearable. The empty dump truck bodies bounce on the incline and they are deafing. It is impossible to hold a conversation on our patio unless we scream. We can't even keep doors and windows of the house open. If the 2nd lane of West Road exit is put in the situation will be worse.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. See response for Noise Abatement on P. V-2.

180

306

011-A

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT
QUESTIONS AND/OR COMMENTS DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

MAR 20 10 45 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Ruth M. Kelley DATE 3/14/90

PLEASE PRINT ADDRESS 12 ALAN BROOK ST.

CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

I am opposed to the addition of another lane to Balto. Beltway #695 unless barriers are built prior to construction in all areas that exceed Federal noise guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. U.S. 1 Modified Option has been selected. This will not require the acquisition of four properties.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME RICHARD L. NAEGELE DATE 3-14-90

PLEASE PRINT ADDRESS 4210 OVERTON AVENUE

CITY/TOWN BALTO STATE MO ZIP CODE 21236

I/We wish to comment or inquire about the following aspects of this project:

I WAS INFORMED BY A NEIGHBOR YESTERDAY 012
SHA PLANS FOR ROAD WIDENING & RELOCATION
012 RAMP AT BELAIR RD # 695.

THIS CHANGE HAS DIRECT BEARING ON MY
HOUSE AND I SHOULD HAVE BEEN INFORMED
OF YOUR FEB 28, 1990 HEARING.

I REQUEST DETAIL PLANS OF ROAD AND
R/W CHANGES ADJACENT TO MY HOUSE.
MY PHONE IS 825-6622 AND I CAN BE
REACHED AFTER 5PM WEEK DAYS

Richard L. Naegle

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

307

V-1111



Campus Hills

COMMUNITY ASSOCIATION, INC.
P.O. BOX 9776 EUDOWOOD STATION
TOWSON, MARYLAND 21204

March 13, 1990

Maryland Department of Transportation
State Highway Administration
PO Box 717/707 North Calvert Street
Baltimore, Maryland 21203-0717

Attention: Mr. Hsi Kassooff
Administrator

Reference: I-695 / Providence Road Interchange

Dear Sir,

The Campus Hills Community Association, a community of more than 350 homes, wishes to comply with the March 16th deadline for comments by submitting the following statements. Our proposals refer to the re-construction of I-695 in general and the Option for Providence Road particularly.

Association representatives have attended public hearings conducted at Loch Raven Senior High school, have reviewed the state's green proposal booklet and environmental studies. The data presented has been studied in committee with the best interests of our residents as our primary concern. The overall concept does not appear to address the main problem of origin and destination flow. It is feared that costly expansion will produce more construction congestion and noise and will not meet the state's traffic projection when completed.

After studying the proposal for Providence Road and the history at this interchange, the Association is opposed to the ramp modifications and widening of the bridge and its approaches as suggested. There is not and has not been any evidence which suggest a widening is justified and there is ample evidence that such an improvement would create serious problems north and particularly south of the limits of improvement.

A topic of concern to our residents is the inefficiency of the sound barrier on I-695 and the Association would like to see it addressed in the very near future. We are aware of earlier correspondence and phone discussions from our residents and the Association to members of the state regarding possible solutions. It is obvious from the comments at your hearings that this is a very emotional issue. However, the Association does feel that as constructed, the barrier fails to perform as presented and has contributed additional noise levels not present before barrier construction.

Thank you for the opportunity to study the proposal and render our opinions. We are available to discuss our rationale. Please advise us of your reaction to our comments and any future planning or proposals that would effect our community.

Very truly yours,

Michael Mulford
Michael Mulford,
President

MM/jgc

cc: Marths S. Klime, Del. 9th District
Berbra F. Bechur, Councilwoman, 4th District

Response:

1. The Providence Road Option was not selected.

V-112

228

Response:

- 1. The Providence Road Option was not selected due to public opposition and a lack of traffic operations need.

Response:

- 1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT No. B 635-191,422
 BALTIMORE BELTWAY
 MD 140 TO MD 702

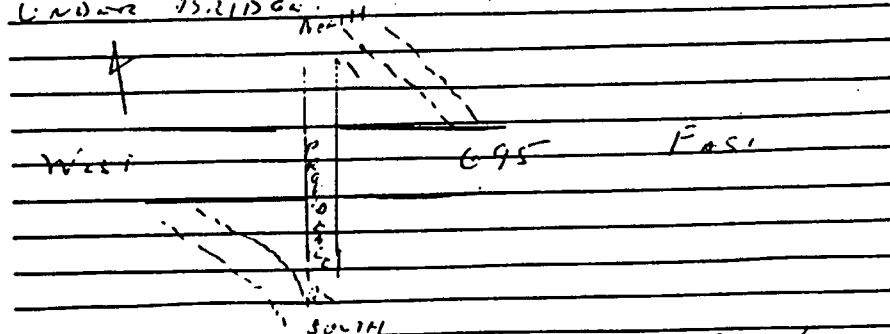
LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME James W Clifford DATE 3/19/90

PLEASE PRINT ADDRESS 922 STARBIT RD (Campus Hills
 CITY/TOWN Towson STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I UNDERSTAND IT IS THE PLAN TO WIDEN
PROVIDENCE BRIDGE AGAIN. I THINK THE MOST
IMPORTANT THING TO DO (AT ONCE), IS TO MAKE
THE INTERCHANGE MORE SAFE BY COMING OFF
*695 GOING EAST BEFORE THE BRIDGE TO PROVIDENCE
RD. SOUTH WOULD ELIMINATE GOING UNDER BRIDGE
AND HIT THE PROVIDENCE RD (NORTH & SOUTH) ALSO
ALLOW TRAFFIC GOING WEST ON 695 TO GET OFF
TO GO NORTH ON PROVIDENCE RD BEFORE GOING
UNDER BRIDGE.



Please add my/our name(s) to the Mailing List. SHOULD BE ON
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

March 15, 1990

Mr. Donald G. Honeywell
 Project Manager
 Project Planning Division
 STATE HIGHWAY ADMINISTRATION
 707 N. Calvert Street
 Baltimore, MD 21202

Dear Mr. Honeywell:

The neighborhood of Thornleigh, represented by The Thornleigh Improvement Association, hereby makes the following written testimony regarding the beltway expansion project.

The impacted neighborhoods have many unresolved issues. These issues include family health problems, quality of life, peace and quiet and property values. These concerns need to be addressed and resolved before expansion takes place.

With the light rail running through our community and with the widening of beltway, we want to be assured all measures will be taken to preserve the quality of life.

Thornleigh recognizes the need for this expansion, but the affected property owners' rights must also be recognized and protected.

Thornleigh's position on the beltway expansion is Type II noise barriers be installed before construction begins on the beltway.

Thank you for your cooperation.

Sincerely,

Carol Seward

Carol Seward
 President
 Thornleigh Improvement Association

CS/djb

V-113

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

1123 22 11 03 AM '90

March 14, 1990

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME Dr. MARIA E. Stoffers DATE 3-16-90
ADDRESS 36 Bardeen Ct. Dulaney Towers
CITY/TOWN TOWSON STATE Md. ZIP CODE 21204

PLEASE PRINT

I/We wish to comment or inquire about the following aspects of this project:

I bought the condo/condominium in May 1988 with the understanding that sound barriers will be built on the property is close by to 30 ft from the Beltway. Shortly after I moved in I communicated with Mr. Trauman, the secretary and was told that because the construction built after the beltway the barriers will not be erected. Shortly the beltway was NOT in 1975 what it is today - I can't stay in the house for more than 2 days in a row - my blood pressure is way up and I cannot sleep - I think this is a gross injustice - discrimination - to put it mild. I have been trying to sell again but can't not even if I had a big loss - and the county tax office has raised my taxes for 35-40% will be 60% for 1991 - so that fair please we are talking about a small section of the beltway and we are older people on a limited income - Do I have to go to Washington to get justice?

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

if necessary I will organize

Gentlemen:

With regard to the proposed beltway widening, especially near the northeast Rt 83 junction, please consider this a registration of strong opposition. Until such time as noise barriers have been erected in this area, additional traffic allowed by this expansion will cause increased aggravation, noise, air pollution and discomfort to the retiring property owners in this district.

Yours very truly
Dana H. MacIntosh
7 Barrow Court
Baltimore, Maryland 21204

Response:

1. See response for Noise Abatement on P. V-2.

Response:

1. See response for Noise Abatement on P. V-2

g/p

V-114



DEPARTMENT OF THE ARMY
BALTIMORE DISTRICT, CORPS OF ENGINEERS
P.O. BOX 1718
BALTIMORE, MARYLAND 21203-1718

March 16, 1990

REPLY TO ATTENTION OF:
Planning Division

Mr. Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Dear Mr. Ege:

Reference the letter dated February 6, 1990, from Mr. Neil J. Pedersen, requesting Baltimore District comments regarding the Environmental Assessment/Section 4(f) Evaluation to improve safety and capacity of I-695 (Baltimore Beltway) between MD 140 and MD 702. The comments provided below address the Corps of Engineers (Corps) areas of concern, including direct and indirect impacts on existing and/or proposed Corps projects, flood control hazard potential, and permit requirements under Section 404 of the Clean Water Act.

There are no existing or proposed Corps projects that would be affected by the work. In accordance with the subject report, portions of the proposed work will be located within the flood plain. New construction or major replacements within the flood plain require full compliance with Executive Order (E.O.) No. 11988, Flood Plain Management, May 24, 1977; Federal Emergency Management Agency (FEMA) regulations; and other Federal, State, and local flood plain regulations. The objectives of the E.O. and the other flood plain regulations are to avoid the adverse effects of occupying and modifying the flood plain and to avoid direct and indirect support of development in the flood plain. The E.O. requires that activities not be located in the flood plain unless it is the only practicable alternative. Activities which must be located in the flood plain must incorporate measures to: (1) reduce the hazard and risks associated with floods, (2) minimize the adverse effects on human health, safety, and welfare; and (3) restore and preserve the natural and beneficial values of the flood plain.

Certain activities in the waters of the United States, and jurisdictional wetlands, require Department of the Army permits from the Corps of Engineers. Corps regulations (33 CFR 320 through 330 and 33 CFR 230 and 325 (Appendix B)) require full compliance with the National Environmental Policy Act (NEPA) of 1969 during the review and evaluation of permit applications. To the maximum extent possible, the Corps will accept the information presented in NEPA documents for evaluating permit applications. The report was reviewed by the District's Regulatory Branch and their comments are as follows:

a. The Interstate Route 695 Baltimore Beltway, Maryland 140 to Maryland 702 Corridor, was field inspected by Mr. Jon Romeo of the Regulatory Branch in March of 1988 in order to verify the wetlands delineation completed by SHA's environmental consultant. The delineation of Waters of the United States, including jurisdictional wetlands is correct.

b. The report states that alternate 2 is the only practicable option to provide the needed capacity on the Beltway and is fixed in alignment by the existing travel lanes. It also states that measures have been taken to minimize wetland impacts. Road surface runoff should be directed to stormwater management basins located on uplands before its release into wetlands and streams.

c. In general, it is recommended that wetland impacts due to interchange improvements be kept to a minimum, where practicable. At wetland sites 4, 5, 6, 7, 8, 9, 10, 22, 23, and 25 it is recommended that the options with the least wetland impacts be chosen for the final design.

d. If, during the permit review process, the Corps concludes that the project complies with Section 404 (b)(1) guidelines, and that avoidance and minimization will be done to the greatest extent practicable, mitigation will be required to replace the values and functions of impacted wetlands.

If you have any questions or need additional information on permits, the point of contact is Mr. Tom Filip, Assistant Chief, Regulatory Branch, Operations Division, at (301) 962-3671.

If you have any other questions on this matter, please call me or my action officer, Mr. John Brzezanski, at (301) 962-4997.

Sincerely,

James F. Johnson

James F. Johnson
Chief, Planning Division

Response:

1. See response within Agency Coordination.

Rec. 3/29/90



HOUSE OF DELEGATES

ANNAPOLIS, MARYLAND 21401-1991

March 27, 1990

ELLEN R. SAUERBREY
MINORITY LEADER

SUITE 312
LOWE HOUSE OFFICE BUILDING
841-3401
1800-492-7132

DISTRICT OFFICE
4122 SWEET AIR ROAD
BALDWIN, MARYLAND 21013
592-2200

Mr. Hal Kasoff
State Highway Administrator
Maryland Department of Transportation
707 N. Calvert Street
Baltimore, Maryland 21202

Dear Hal:

As you know, our constituents whose homes back up to the Beltway are very concerned about the proposal for widening I-695 before the sound barriers are put into place.

I certainly understand the fiscal problem that you are dealing with, and I am aware of the priority list that determines in what order these projects will be funded. However, I also share the concern expressed by residents who will have to live with the construction project in the middle of the night and who also believe that it would be more cost effective to put the sound barriers up at the time that the Beltway construction is occurring, not at a later date.

I would appreciate your getting back to me with the projected construction schedule of the Beltway in the 10th District and your current projection as to when the sound barriers will be funded. I would urge you to find a way to ensure that the sound barriers precede the highway construction.

Sincerely,

Ellen R. Sauerbrey

ERS:elw

Response:

1. Final staging and funding of the improvements have not been finalized.



COUNTY COUNCIL OF BALTIMORE COUNTY

COURT HOUSE, TOWSON, MARYLAND 21204

BARBARA F. BACHUR
COUNCILWOMAN, FOURTH DISTRICT

COUNCIL OFFICE: 887-3388
887-2188

March 23, 1990

Mr. Neil Pedersen
Director
Office of Planning and Engineering
The Maryland State Highway Administration
P.O. Box 717
Baltimore, MD 21203

Dear Mr. Pedersen:

I have received a copy of a letter sent to you by the president of the Orchard Hills Community Association regarding sound barriers on Interstate 695.

I would like to offer my support to their position. I am a firm believer in reducing the negative effects that progress sometimes brings to our communities. While the widening of the Beltway is a necessary step in the growth of our county, all efforts should be made to minimize the impact this will have on the adjacent communities.

The community of Orchard Hills is requesting that sound barriers be placed on Interstate 695 at Charles Street and York Road (Project No. 20, Contract No. B-850-501-424). I agree fully with this proposal and hope that you will take the appropriate steps to have these barriers erected.

Sincerely,

Barbara F. Bachur
Councilwoman, Fourth District

BFB:pah

cc: Stephanie J. Bobloch, President
Orchard Hill Community Association

Response:

1. See response for Noise Abatement on P. V-2.

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3/29

V-116

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
 QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472 APR 2 3 32 AM '90
 BALTIMORE BELTWAY
 MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME W. GRENNON DATE 3/24/90

PLEASE PRINT ADDRESS 6 THEO LANE

CITY/TOWN TOWSON STATE MD ZIP CODE 21284

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF ANOTHER LANE TO BALTIMORE BELTWAY #695 UNLESS BARRIERS ARE BUILT PRIOR TO CONSTRUCTION IN ALL AREAS THAT EXCEED FEDERAL NOISE GUIDELINES.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

1. I-83 (HX) Option D was selected.
2. Staging of this improvements has not been finalized.

STATE HIGHWAY ADMINISTRATION PROJECT DEVELOPMENT
 QUESTIONS AND/OR COMMENTS

CONTRACT No. B 635-101-472
 BALTIMORE BELTWAY
 MD 140 TO MD 702

APR 2 3 32 AM '90

LOCATION/DESIGN PUBLIC HEARING
 WEDNESDAY, FEBRUARY 28, 1990
 LOCH RAVEN SENIOR HIGH SCHOOL

NAME RON & LAUREN NOZAK DATE 3-6-90

PLEASE PRINT ADDRESS 7202 North Charles Street

CITY/TOWN Lutherville STATE MD ZIP CODE 21093

I/We wish to comment or inquire about the following aspects of this project:

Our home is @ the end of Charles Street - the cul-de-sac across Ballora. So we are right @ the Exit 25 project location. Our backyard vias the on ramp/access road that Ballora turns into. Needless to say, we are very concerned about the noise that will affect our lifestyle/home/property as a result of the Beltway project @ Exit 25. Any information / transcripts that provide detailed information regarding project scope (in addition to map-study document), noise abatement, project timeframes, etc. will be greatly appreciated.

Thank you
 (Ron & Lauren Nozak)

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

ZIT-A

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

PROJECT DEVELOPMENT

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

APR 2 3 32 AM '90

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ^{Constance &} Robert Walkingshaw DATE 3/29/90

PLEASE PRINT ADDRESS 1921 OLD COURT ROAD
CITY/TOWN Towson/Ruthtown STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

We are very concerned about the "Flyway" option for the expansion of the Jones Falls Expressway (JFX) Northbound connecting to 695 West, for the following reasons:

1) The tremendous increase in noise level for all residents of Ruxton Hill/ Ruxton Green, Ruxton, Brooklandville. There is no "barrier" remedy for the noise level increase. This will cause tremendous decrease in quality of life and property value.

2) The "Flyway" option is three times as expensive as the alternate option.

We do not need the Flyway option which is 3XS as expensive and could potentially ~~drive~~ drive our community.

Thank you for listening.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. I-83 (JFX) Option B was not selected.

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STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME ^{LOUIS &} SUSAN L. ZARANSKI DATE 4/2/90

PLEASE PRINT ADDRESS 3 WOOD HOLME VILLAGE CT.
CITY/TOWN BALTIMORE STATE MD ZIP CODE 21208

I/We wish to comment or inquire about the following aspects of this project:

PLEASE ADVISE US IF AND WHEN A MUCH NEEDED CONCRETE WALL WILL BE ERECTED BETWEEN EXIT 20 AND NW EXP. 795.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Response:

- 1. See response for Noise Abatement on P. V-2.

19

3/14

811-11

Response:

- 1. See response for Noise Abatement on P. V-2.

**THE RUXTON - RIDERWOOD - LAKE ROLAND AREA
IMPROVEMENT ASSOCIATION, INC.**

Box 204
Riderwood, Md. 21139

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Robert E. Scott, Jr.
Er Office

April 2, 1990

Mr. Donald G. Honeywell
Maryland Department of Transportation
State Highway Administration
Office of Planning & Preliminary Engineering
Box 717
Baltimore, Maryland 21203-0717

Dear Mr. Honeywell:

The Board of Governors of the Ruxton-Riderwood-Lake Roland Area Improvement Association wishes to go on record as being in full support of the communities involved in the "Noise Abatement Task Force" regarding their position that "Any expansion or modification of Interstate 695 prior to the construction of sound abatement barriers along the boundaries of our communities is unacceptable."

We feel quite strongly that you owe it to the affected communities (Thornleigh, Longford, Greenway Garth, Heatherfield, Seminary Ridge, Village Green and Ruxton Hill, etc.) to meet with them personally to see what tangible steps the State plans to take to oblivate noise on their properties. I would welcome the opportunity to assist in setting up a meeting along these lines. Please let me hear from you at your earliest convenience.

Very truly yours,

Christopher R. West

Christopher R. West
President

cc: George Arconti
John Dahne
Marc Doxanas
John MacCarron
Jorgen Jensen
Stan Hamilton

Charlotte Patrone
H. Lee Boatwright, III
Wayne Keller
John Eckenrode, Jr.
John Eckenrode, Sr.
Mrs. Richard Wasserman

195

Response:

- 1. Added to mailing list.

John R. Johnson

2009 Hillenwood Rd.

Baltimore, Maryland 21239-362

MARYLAND DEPARTMENT OF TRANSPORTATION

STATE HIGHWAY ADMINISTRATION

OFFICE OF PLANNING AND PRELIMINARY ENGINEERING

PO. BOX 717

BALTIMORE, MARYLAND 21203

Gentlemen:

I understand several major changes and improvements to the northern section of Interstate 695 are currently under study by the State Highway Administration. As a very frequent driver of the northern half of the Beltway and Harrisburg Expressway, I am very interested in the progress of these improvements.

Please enter my name and address to your mailing list of interested parties for these and any other major projects in this region.

Thank you very much in advance.

Sincerely,

John R. Johnson

John R. Johnson

196

3/5

**STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS**

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME C. BOSWELL DATE 3-28-90
ADDRESS 6F (DATE COURT, DULANEY TOWER APTS
CITY/TOWN TOWSON STATE Md ZIP CODE 21204

PLEASE
PRINT

I/We wish to comment or inquire about the following aspects of this project:

I AM OPPOSED TO THE ADDITION OF ANOTHER LANE
OF BALTIMORE BELTWAY '695' UNLESS BARRIERS ARE
BUILT PRIOR TO CONSTRUCTION IN ALL AREAS THAT EXCEED
FEDERAL NOISE GUIDELINES


3-28-90

- Please add my/our name(s) to the Mailing List.*
 Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

197

Response:

1. See response for Noise Abatement on P. V-2.

**THE RUXTON - RIDERWOOD - LAKE ROLAND AREA
IMPROVEMENT ASSOCIATION, INC.**

Box 204
Riderwood, Md. 21139

Board of Governors
Christopher R. West
President
Louise H. Hildreth
1st Vice President
Nancy Worden Horst
2nd Vice President
John C. Ruxton
Treasurer
E. Richard Watts, Jr.
Secretary

Betty C. Bixler
Richard C. Burch
Nancy Cammack
Suzanne E. Chapelle
Hobart V. Fowlkes
Dorothy S. Fravel
Edith Hoyt Garrett
Robert O. Kan
Sarah F. Lord
Joan Swift Lucas
James H. Magee, III
Herbert B. Mittenenthal
William D. Naughton
Paula Davis Noell
Cail B. O'Donovan
Roy C. Parsons
Bonnie S. Serpick
Gordon B. Shelton
Marjorie Sinton

Robert E. Scott, Jr.
Ex Officio

April 4, 1990

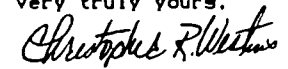
Mr. Donald G. Honeywell
Maryland Department of Transportation
State Highway Administration
Office of Planning & Preliminary Engineering
Box 717
Baltimore, Maryland 21203-0717

Dear Mr. Honeywell:


The Board of Governors of the Ruxton-Riderwood-Lake Roland Area Improvement Association wishes to go on record as being opposed:

- 1) to any fly-over bridge from the northbound I-83 (JFX) to the westbound Beltway, unless the State can produce compelling evidence that the existing circular cloverleaf has been found to be dangerous, and
- 2) to the proposed modified interchange involving additional lanes.

Very truly yours,



Christopher R. West
President

12:30 PM
4/9/90 Strike item 2 per President West. 

cc: Marc Doxanas

198

Response:

1. I-83 (JFX) Option B was not selected.

3/10

V-120

Response:

1. See response for Noise Abatement on P. V-2.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME SUE E. SMALL DATE 3-5-90

PLEASE PRINT ADDRESS 17 BELLOWS CT
CITY/TOWN TOWSON STATE MD ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

Please consider the erection of barriers adjacent to Baltimore Beltway 695 prior to any attempts to add another lane to the highway. I am opposed to highway construction without barriers in all areas that exceed Federal Noise Guidelines.

Please add my/our name(s) to the Mailing List.*

Please delete my/our name(s) from the Mailing List.

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

199

Response:

1. The selected build alternate does not preclude future light rail or HOV lanes.

STATE HIGHWAY ADMINISTRATION
QUESTIONS AND/OR COMMENTS

CONTRACT NO. B 635-101-472
BALTIMORE BELTWAY
MD 140 TO MD 702

LOCATION/DESIGN PUBLIC HEARING
WEDNESDAY, FEBRUARY 28, 1990
LOCH RAVEN SENIOR HIGH SCHOOL

NAME NANCY W. HORST DATE 4/7/90

PLEASE PRINT ADDRESS 7819 ELLENHAM AVE
CITY/TOWN TOWSON STATE MD. ZIP CODE 21204

I/We wish to comment or inquire about the following aspects of this project:

connecting light rail transportation, and other means of mass transportation, at or on the Beltway, rather than merely widening the Interstate.

The Tenth District Light Rail Task Force of which I am a member, has met with M.T.A. officials and selected officials, including Governor Wilentz and Schaefer, to discuss the possibility of running the light rail on the west side of I-83 (The Toll Falls Expressway) at its intersection with the Beltway where it could terminate or else run up the median of I-83 to Hunt Valley.

If the four proposed new lanes of the Beltway were used for light rail or exclusive bus lanes, I believe H.S.V. lanes this would help alleviate recurring problems of beltway congestion which are proposed for the year 2015, or even earlier.

I would greatly appreciate the opportunity to Please add my/our name(s) to the Mailing List.* Please delete my/our name(s) from the Mailing List. expand on these ideas - please

*Persons who have received a copy of this brochure through the mail are already on the project Mailing List.

Contact me at the above address or at 821-5394
Thank you Nancy W. Horst 200

V-121

→

7819 Ellenham Avenue
Baltimore, Maryland 21204
April 9, 1990

Mr. Hal Kassoff, State Highway Administrator
Maryland Department of Transportation
707 North Calvert Street
Baltimore, Maryland 21202

Dear Mr. Kassoff:

The Tenth District Task Force on Light Rail supports residents living near the Beltway in their plea for sound barriers to be erected prior to any widening of I-695.

However, this issue demands that a longer view, encompassing the entire Department of Transportation, be contemplated.

In January of this year, the Task Force presented plans for an alternate Light Rail route to the Governor and our elected officials. The following is a capsule version of that plan.

The alternate route would leave the existing Conrail line at the city line and run north along the west side of I-83 (which would allow access from U.S.F. & G. and three large apartment complexes) to a possible terminus at the Beltway.

This alternate route, linking existing major thoroughfares, has the advantage of either eliminating the necessity for adding another lane by using existing space (the median) for Light Rail or "Maglev" trains, or, if additional lanes were deemed necessary, they could be used for dedicated busways or exclusive H.O.V. lanes. This would encourage people to get out of their cars and use public transportation. Adding lanes to the Beltway merely encourages motorists to use their cars in ever increasing numbers, and will lead to worsening noise and pollution problems, as well as traffic jams, by the turn of the century.

The Light Rail line could be extended, as funding permits, along the Beltway to I-83 (The Harrisburg Expressway), running along the median to Hunt Valley. This has the additional advantage of including a possible stop near Orgon Ridge, a major cultural attraction of the Baltimore Symphony Orchestra, which is experiencing poor parking and access problems.

One final point - funding. With the Beltway widening projected at \$208.71 million, and the Light Rail currently at \$446.3 million, there could be some savings to both the Transportation and State Highway Departments if they worked in concert. This may be especially urgent in view of the Federal Government's proposal to slash Federal Highway monies.

We would appreciate your consideration of our proposal, and welcome your comments.

Sincerely yours,
Nancy W. Horst
Nancy W. Horst
for the Task Force

cc: Rep. Helen Bentley
Rep. Ben Cardin
Mr. Ron Hartman

Response:

1. The selected build alternate does not preclude future light rail or HOV lanes.

4/7/90

Dear Mr. Honeywell:

I would greatly appreciate the opportunity for the Task Force and the Kenton-Ridwood Lake Colored Area Improvement Association representatives to discuss these ideas with you at more length. Please contact

me at: 7819 Ellenham Ave.
Towson, Md. 21204
or call me at: 821-5344
Thank you for your continued interest in the excellence of this project.
Sincerely,
Nancy W. Horst

Response:

1. Discussion occurred.

201

201

8/3/8

V-122

APR 11 1990

Mr. Micheal Mulford, President
Campus Hills Community Association, Inc.
Post Office Box 9776
Eudowood Station
Towson, Maryland 21204

Dear Mr. Mulford:

Thank you for your recent letter in response to the February 28th location/design public hearing for the Baltimore Beltway from MD 140 to MD 702.

The widening of Providence Road through the Beltway Interchange was proposed to alleviate the conflict with left turns into Beltway ramps, as well as the entrance to the park and ride lot. We expect these conflicts to increase with traffic volumes. We would only intend to implement these improvements if the problem significantly worsens. Your association's opposition to the proposed improvements will be given serious consideration in our reaching a final decision at this location.

Our Landscape Architecture Division will be investigating your concerns about the effectiveness of the existing noise barrier adjacent to your community. Mr. Charles Adams, Chief of the Landscape Architecture Division, will be contacting you with the results by late summer. If you would like to discuss your concerns with Mr. Adams, his telephone number is (301) 333-8063.

Thank you for letting us know your views on this project.

Sincerely,

ORIGINAL SIGNED BY:
HAL KASSOFF
Hal Kassoff
Administrator

HK/t

cc: The Honorable Martha Kima
The Honorable Barbara Bachur
Mr. Charles B. Adams

bcc: Mr. Neil J. Pedersen
Mr. C. Robert Olsen
~~Mr. Robert W. Ege, Jr.~~

EXTENDED
DEADLINE
APRIL 10, 1990

V-124

203

3/19

Response:

- 1. See response for Noise Abatement on P. V-2.

NOTES

4/6/90

Mr. Neil J. Pedersen,

Please help in fixing up my records for the state's sound barrier project. (Project 20, Contract # 8850-501-424). This would be helpful so that my community does not hear the noise from the highway.

Thank you,

Thomas L. Schmitt
35 Carver Drive
Lutherville, MD. 21093

V-125

International
Association
for Financial
Planning



204

Response:

- 1. Contact was made with Orchard Hills Community Association.

PAUL S. SARBANES
MARYLAND

United States Senate
WASHINGTON, DC 20510-2002

March 19, 1990

PLEASE REPLY TO:
1818 GLENN FALLON FEDERAL BUILDING
31 MADISON PLAZA
BALTIMORE, MD 21201
(301) 952-4438

RECEIVED

MAR 23 1990

DIRECTOR, OFFICE OF
PLANNING & PRELIMINARY ENGINEERING

Mr. Hal Kassoff
State Highway Administrator
707 North Calvert Street
Baltimore, Maryland 21202

Dear Mr. Kassoff,

From time to time my office is contacted by constituents who bring to my attention matters which do not fall within the responsibility of the federal government. In this respect, I have enclosed correspondence I received from Ms. Stephanie Bobloch and Ms. Elaine Smyth, representing the Orchard Hills Community Association, Inc., in Lutherville, Maryland 21093 which I believe is self-explanatory.

It would be appreciated if you would review this matter and respond directly to them. Thank you very much for your assistance. I have written to them of this action.

With best regards,

Sincerely,



Paul S. Sarbanes
United States Senator

PSS/bbs
Enclosure

205

320

Rec. 4/16/90



THE ORCHARD HILLS
COMMUNITY ASSOCIATION, INC.

Lutherville, Maryland 21093

7819 Ellenham Avenue
Baltimore, Maryland 21204
April 9, 1990

Mr. Hal Kasaoff, State Highway Administrator
Maryland Department of Transportation
707 North Calvert Street
Baltimore, Maryland 21202

March 13, 1990

The Honorable Paul S. Sarbanes
United States Senator
District Office
1518 Federal Office Building
Baltimore, Maryland 21204

Re: Sound Barrier Project #20
Contract # B 850-501-424

Dear Mr. Kasaoff:

The Tenth District Task Force on Light Rail supports residents living near the Beltway in their pleas for sound barriers to be erected prior to any widening of I-695.

However, this issue demands that a longer view, encompassing the entire Department of Transportation, be contemplated.

In January of this year, the Task Force presented plans for an alternate Light Rail route to the Governor and our elected officials. The following is a capsule version of that plan.

The alternate route would leave the existing Conrail line at the city line and run north along the west side of I-83 (which would allow access from U.S.F. & G. and three large apartment complexes) to a possible terminus at the Beltway.

This alternate route, linking existing major thoroughfares, has the advantage of either eliminating the necessity for adding another lane by using existing space (the median) for Light Rail or "Maglev" trains, or, if additional lanes were deemed necessary, they could be used for dedicated busways or exclusive H.O.V. lanes. This would encourage people to get out of their cars and use public transportation. Adding lanes to the Beltway merely encourages motorists to use their cars in ever increasing numbers, and will lead to worsening noise and pollution problems, as well as traffic jams, by the turn of the century.

The Light Rail line could be extended, as funding permits, along the Beltway to I-83 (The Harriaburg Expressway), running along the median to Hunt Valley. This has the additional advantage of including a possible stop near Orgon Ridge, a major cultural attraction of the Baltimore Symphony Orchestra, which is experiencing poor parking and access problems.

One final point - funding. With the Beltway widening projected at \$208.71 million, and the Light Rail currently at \$446.3 million, there could be some savings to both the Transportation and State Highway Departments if they worked in concert. This may be especially urgent in view of the Federal Government's proposal to slash Federal Highway monies.

We would appreciate your consideration of our proposal, and welcome your comments.

Sincerely yours,

Nancy W. Horst
for the Task Force

cc: Rep. Helen Bentley
Rep. Ben Cardin
Mr. Ron Hartman

Response:

1. The selected build alternate does not preclude future light rail or HOV lanes.

206

V-126

Dear Senator Sarbanes.

We are enclosing a copy of our letter to the Maryland State Highway Administration expressing our concern about the proposed beltway widening and complete lack of sound barriers in our area. (I-695 at York Road and Charles Street)

The Orchard Hills Community Association, Inc. implores you to assist the various agencies who are responsible for this project in completing the funding, design and construction of these sound barriers.

Should you have any questions or comments, please contact us at 115 Hedgewood Road, Lutherville, Maryland 21093 or by telephoning 296-3017.

Thank you for your attention.

Sincerely,

Stephanie J. Bobloch
President

Elaine D. Smyth
Vice-President

SJB/eds

Response:

1. See previous coorespondence.

Response:

1. See response for Noise Abatement on P. V-2.



United States Department of the Interior

OFFICE OF THE SECRETARY
WASHINGTON, D.C. 20240

PROJECT REVIEW
APR 10 2 50 PM '90

H. Fillmore and Mary Ellen Schmidt
1310 Warwick Drive
Lutherville, Maryland 21093

L74(MAR-PD)
ER-90/117

APR 13 1990

April 13, 1990

Mr. Neil J. Pedersen
Office of Planning & Engineering
The Maryland State Highway Administration
P. O. Box 717
Baltimore, Maryland 21203

A. Porter Barrows
Division Administrator
Federal Highway Administration
711 West 40th Street, Suite 220
Baltimore, MD 21211

Dear Mr. Barrows:

This responds to a request for the Department of the Interior's comments on the Environmental Assessment/Section 4(f) Evaluation for I-695 (East of SR-140 to West of SR-702), Baltimore County, Maryland.

Dear Mr. Pedersen:

My family has lived in the Orchard Hills community of Baltimore County for twenty-six years. Our home is just a few short blocks from the beltway and I am very concerned about the possibility of additional lanes being added to this roadway.

With the proposed lane additions to the beltway, I feel strongly that sound barriers are needed to protect the Orchard Hills community. I urge you to obtain federal funds to complete the state's sound barrier project on this section of the beltway.

Thank you for your help on behalf of all Orchard Hills residents.

Sincerely,

Mary Ellen Schmidt

SECTION 4(f) STATEMENT COMMENTS

We concur that, if transportation objectives are to be achieved, there is no feasible and prudent alternative to the proposed use of land within public parkland and property listed in the National Register of Historic Places. We are also in agreement with selection of the recommended plan. We concur that the proposed mitigation, which includes erection of a noise wall adjacent to the Hampton National Historic Site, is appropriate, and recommend the continued coordination and consultation with the National Park Service and the Maryland State Historic Preservation Officer.

ENVIRONMENTAL ASSESSMENT COMMENTS

Fish and Wildlife Resources - We do not endorse the proposed interchange I-83 (Jones Falls Expressway) Option C alternative. Option C would impact 0.916 acres of wetlands along Jones Falls, a stream which has a naturally reproducing brown trout population. The other improvement options (A, B, D) for this interchange would only impact 0, 0.203, and 0.543 acres of wetlands, respectively. We recommend that these other options be considered for improving the geometrics of this intersection. There are no objections to any of the other interstate improvement proposals.

4.20
Don do we have a problem with it?

V-127

372

We endorse the State Highway Administration's (SHA) plan to control stormwater management in the Jones Falls watershed (pg IV-38). This management would include the construction of subsurface trenches throughout the watershed for augmenting the infiltration of stormwater. SHA also plans to construct 24-hour detention basins for attenuating flood heights and reducing the flood water's erosive force. We condone this use of 24-hour basins as opposed to 48-hour or 72-hour detention basins which would introduce heated water into the trout stream.

The Department recommends that all unavoidable wetland losses be replaced on a 2:1 basis for palustrine forested wetlands and on a 1:1 basis for all other wetlands types. The 2:1 replacement ratio for forested wetlands will help compensate for the time lag of 40 to 50 years which are required for planted seedlings to reach maturity. This ratio will also help compensate for the risk associated with trying to create forested wetlands. The techniques for creating forested wetlands have not been fully developed.

Mineral Resources - Owing to the nature of the project, (widening of the existing Baltimore Beltway mainline and upgrading specific substandard interchanges), no impact to mineral resources apparently is anticipated by the preparers of the document; accordingly, mineral resources are not discussed. A search of our data files leads us to concur that the project would not impact mineral resources.

FISH AND WILDLIFE COORDINATION ACT COMMENTS

The U.S. Fish and Wildlife Service's most probable position on any Section 404 permits for this project would most likely be no objection provided the I-83 (Jones Falls Expressway) Option C interchange is not selected for construction and provided an acceptable compensation plan is submitted and a viable site is identified with the Section 404 application.

SUMMARY COMMENTS

The Department of the Interior offers no objection to Section 4(f) approval of the recommended plan, provided wetlands at Option C interchange are avoided and the other measures mentioned above are included in the plans for project implementation.

As this Department has a continuing interest in this project, we are willing to cooperate and coordinate with you on a technical assistance basis in further project evaluation and assessment. For matters pertaining to recreational and cultural resources, please contact the Regional Director, National Park Service, Mid-Atlantic

Response:

1. See response within Agency Coordination.

323

V. CORRESPONDENCE

B. Elected Officials



325
RECEIVED
DEVELOPMENT
OFFICE

COUNTY COUNCIL OF BALTIMORE COUNTY 85 17 80
COURT HOUSE, TOWSON, MARYLAND 21204

BARBARA F. BACHUR
COUNCILWOMAN, FOURTH DISTRICT

COUNCIL OFFICE: 887-3389
887-3198

March 23, 1990

Mr. Neil Pedersen
Director
Office of Planning and Engineering
The Maryland State Highway Administration
P.O. Box 717
Baltimore, MD 21203

Dear Mr. Pedersen:

I have received a copy of a letter sent to you by the president of the Orchard Hills Community Association regarding sound barriers on Interstate 695.

I would like to offer my support to their position. I am a firm believer in reducing the negative effects that progress sometimes brings to our communities. While the widening of the Beltway is a necessary step in the growth of our county, all efforts should be made to minimize the impact this will have on the adjacent communities.

The community of Orchard Hills is requesting that sound barriers be placed on Interstate 695 at Charles Street and York Road (Project No. 20, Contract No. B-850-501-424). I agree fully with this proposal and hope that you will take the appropriate steps to have these barriers erected.

Sincerely,

Barbara F. Bachur
Councilwoman, Fourth District

BFB:pah

cc: Stephanie J. Bobloch, President
Orchard Hill Community Association

Rec. 3/29/90
326



HOUSE OF DELEGATES
ANNAPOLIS, MARYLAND 21401-1991

March 27, 1990

DISTRICT OFFICE:
4122 SWEET AIR ROAD
BALDWIN, MARYLAND 21013
592-2200

ELLEN R. SAUERBREY
MINORITY LEADER

SUITE 312
LOWE HOUSE OFFICE BUILDING
841-3401
1-800-492-7122

Mr. Hal Kassoff
State Highway Administrator
Maryland Department of Transportation
707 N. Calvert Street
Baltimore, Maryland 21202

Dear Hal:

As you know, our constituents whose homes back up to the Beltway are very concerned about the proposal for widening I-695 before the sound barriers are put into place.

I certainly understand the fiscal problem that you are dealing with, and I am aware of the priority list that determines in what order these projects will be funded. However, I also share the concern expressed by residents who will have to live with the construction project in the middle of the night and who also believe that it would be more cost effective to put the sound barriers up at the time that the Beltway construction is occurring, not at a later date.

I would appreciate your getting back to me with the projected construction schedule of the Beltway in the 10th District and your current projection as to when the sound barriers will be funded. I would urge you to find a way to ensure that the sound barriers precede the highway construction.

Sincerely,

Ellen R. Sauerbrey

ERS:elw

PAUL S. SARBANES
MARYLAND

PROCESSED
SERIALIZED
MAR 24 10 20 AM '90

United States Senate
WASHINGTON, DC 20510-2002

PLEASE REPLY TO:
1618 G.H. FALLON FEDERAL BUILDING
31 HOPKINS PLAZA
BALTIMORE, MD 21201
(301) 962-4436

327

RECEIVED

MAR 23 1990

DIRECTOR, OFFICE OF
PLANNING & PRELIMINARY ENGINEERING

March 19, 1990

Mr. Hal Kassoff
State Highway Administrator
707 North Calvert Street
Baltimore, Maryland 21202

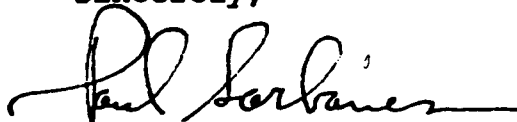
Dear Mr. Kassoff,

From time to time my office is contacted by constituents who bring to my attention matters which do not fall within the responsibility of the federal government. In this respect, I have enclosed correspondence I received from Ms. Stephanie Bobloch and Ms. Elaine Smyth, representing the Orchard Hills Community Association, Inc., in Lutherville, Maryland 21093 which I believe is self-explanatory.

It would be appreciated if you would review this matter and respond directly to them. Thank you very much for your assistance. I have written to them of this action.

With best regards,

Sincerely,



Paul S. Sarbanes
United States Senator

PSS/bbs
Enclosure

328

V. CORRESPONDENCE

C. Agency Coordination

329

V. CORRESPONDENCE

Community and agency participation is an integral and essential part of the alternatives development and evaluation process. Since 1983, these project activities have been supervised by the Maryland State Highway Administration (SHA).

In Stage II of the project, alternatives were defined for the environmental assessment, and the assessment performed. Comments from the public were reviewed and considered in this process. Ongoing coordination regarding noise impacts and barriers has also been conducted with several community groups. Further contact with local, state and federal agencies was performed throughout the project as well. Coordination letters appear at the end of this section.

Agencies contacted for scoping or information include:

- ◊ Maryland Department of Natural Resources
- ◊ Maryland Historical Trust
- ◊ U.S. Department of Agriculture Soil Conservation Service
- ◊ National Park Service
- ◊ U.S. Fish and Wildlife Service
- ◊ U.S. Army Corps of Engineers
- ◊ Providence Volunteer Fire Co., Inc.
- ◊ Baltimore County Fire Department
- ◊ Baltimore County Department of Planning and Zoning
- ◊ Baltimore County Parks and Recreation
- ◊ Baltimore County Economic Development Commission
- ◊ Baltimore Regional Planning Council

Copies of the Air Quality Technical Report were made available to the USEPA and the Maryland Air Management Administration for review.



Maryland Historical Trust

March 24, 1986

PROJECT
DEVELOPMENT
DIVISION
Mar 31 4 20 PM '86

Mr. Louis H. Ege, Jr., Deputy Director
Project Development Division
State Highway Administration
P.O. Box 717
707 North Calvert Street
Baltimore, Maryland 21203-0717

Re: Contract No. B 635-151-472
I-695 (Baltimore Beltway) from
MD Rt. 140 to I-95
P.D.M.S. No. 031113
Baltimore County, Maryland

Dear Mr. Ege:

Thank you for your letter of 26 February 1986, regarding the above-referenced project. This office concurs that archeological investigations are not warranted for this particular project, unless property acquisition of the two sensitive areas identified by the Maryland Geological Survey is required. In that event, an archeological survey would be recommended for the area near Brooklandville and the area adjoining the Hampton Historic Site.

If you have any questions, please contact Ms. Beth Brown of our staff at (301) 269-2438.

Sincerely,

Richard B. Hughes
State Administrator of Archeology

RBH/BCB/bjs

cc: Mr. Tyler Bastian
Mr. Charles L. Wagandt
Mr. Paul McKean

Inaw House, 21 State Circle, Annapolis, Maryland 21401 (301) 269-2212, 269-2438, 269-2850
Department of Economic and Community Development Adm S & P TPS
V-3

Response:

1. See Additional coorespondence following.



Maryland Historical Trust

RECEIVED
84169
MAR 8 1987
D.A.
PROJECT DEVELOPMENT DIVISION

February 24, 1987

PROJECT DEVELOPMENT DIVISION
MAR 2 2 33 PM '87

Mr. Louis M. Ege, Jr.
Deputy Director
Project Development Division
State Highway Administration
P. O. Box 717
707 North Calvert Street
Baltimore, Maryland 21203-0717

RE: Contract No. B 635-151-472
I-695 (Baltimore Beltway) from
MD Rt. 140 to MD Rt. 702
PDMS No. 03113

Response:

1. No response required.

Dear Mr. Ege:

Thank you for your letter of 3 February 1987 regarding the above-referenced project.

We concur that Phase I archeological investigations are not warranted for the two parcels, constituting 1.25 acres, slated for easement acquisition.

Sincerely,

Richard B. Hughes
State Administrator of Archeology

RBH/BCD/mmc

cc: Dr. Jody Hopkins
Mr. Tyler Bastian
Ms. May C. Robinson
Mr. Paul McKean

NOTE: This letter was written as a response to SHA's 2/3/87 letter. That letter requested that the MHT concur with the SHA's determination that two areas (of + 1.25 acres) did not retain sufficient integrity to warrant Phase I archeological testing due to extensive disturbance connected with Beltway construction.

331



PROJECT
DEVELOPMENT
DIVISION
Dec 16 2 58 PM '87

William Donald Schafer
Governor

Jacqueline H. Rogers
Secretary, DHCD

December 14, 1987

Ms. Cynthia Simpson, Chief
Environmental Management
Maryland Department of Transportation
State Highway Administration
P. O. Box 717
707 North Calvert Street
Baltimore, MD 21203-0717

RE: Contract No. B 635-151-472
Interstate Route 695
(Baltimore Beltway)
from Maryland Route 140 to
Maryland Route 702
PDMS No. 03113

Response:

1. See additional coorespondence following.

Dear Ms. Simpson:

Thank you for your letters of February 23rd, October 8th and October 26th, 1987 concerning the subject project. Our office concurs with the following determinations of effect:

Alt 2 - Greenspring H.D. - NE
Alt 2(A) - Greenspring H. D. - NAE
Alt 2(B) - Greenspring H.D. - NAE
Alt 2 - Lutherville H.D. - NE
Alt 2(A) - Lutherville H.D. - NE
Alt 2 - Rockland Farm - NE
Alt 2 - Rockland H.D. - NE
Alt 2 - Hunt's Meeting H.D. - NE

We disagree with your determinations for the following which we consider to be affected, but not adversely:

Alt 2(C) Lutherville H.D. - NAE
Alt 2 Hampton N.H.L. - NAE

We also consider that the nature of the improvements, and the taking of portions of residential yards, makes the following adverse effects:

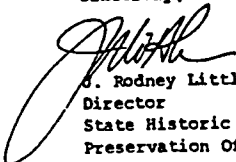
Maryland
Department of Housing and Community Development
State House, 21 State Circle, Annapolis, Maryland 21401 (301) 974-4450, 757-9000
Temporary Address: Arnold Village Professional Center, 1517 Rector Highway, Arnold, Maryland 21012

Ms. Cinthia Simpson, Chief
Environmental Management
December 14, 1987
Page 2

Alt 2(D), 3 Lane, - Lutherville H.D. - Adv
Alt 2(D), 4 Lane, - Lutherville H.D. - Adv

Thank you for your continued cooperation. If you have any questions, feel
free to contact Al Luchenbach at 974-4450.

Sincerely,



J. Rodney Little
Director
State Historic
Preservation Officer

JRL/AHL/eib

cc: Paul Wettlauffer (FHWA)
Rita Suffness (SHA)
May C. Robinson
Paul McKean



PROJECT DEVELOPMENT
DIVISION
JAN 5 11 55 AM '89

William Donald Schaefer
Governor

Jacqueline H. Rogers
Secretary, DHCD

January 3, 1989

RECEIVED

W.D.S.

Ms. Cynthia D. Simpson, Chief
Environmental Management
Maryland Department of Transportation
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21203-0717

Re: Contract No. B 635-151-472
Interstate Route 695
Baltimore Beltway from Maryland
Route 140 to Maryland Route 702
PDMS No. 03113

Response:

1. No response required.

Dear Ms. Simpson:

Based on additional information submitted by Ms. Rita Suffness, of your office, concerning the above referenced project, the Maryland Historical Trust is reversing its prior determination of adverse effect (letter dated December 14, 1987), for the following project alternates:

Alt 2 (D), 3 Lane, - Lutherville Historic District

Alt 2 (D), 4 Lane, - Lutherville Historic District

This office now considers these two alternates to have no adverse effect on the Lutherville Historic District. Should you have any questions, please contact Michael Day at 974-5000.

Sincerely,

George J. Andreve
Project Review and
Compliance Administrator
Office of Preservation Services

GJA/HKD/meh

cc: Ms. Rita Suffness
Mrs. Lauri Fitzgerald
Mr. Paul McKean

Department of Housing and Community Development
Shaw House, 21 State Circle, Annapolis, Maryland 21401 (301) 974-5000

734



PROJECT DEVELOPMENT
William Donald Schaefer
Governor

APR 19 11 44 AM '89
Jacqueline H. Rogers
Secretary, DHCD

April 3, 1989

Ms. Cynthia D. Simpson, Chief
Environmental Management
Maryland Department of Transportation
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21203-0717

Re: Contract No. B 635-151-472
I-695 (Baltimore Beltway)
from MD 140 to MD 702
PDMS No. 03113

Response:

1. No response required.

Dear Ms. Simpson:

Thank you for your letter of March 9, 1989 concerning the above referenced project.

Concerning the non-historic buildings that now exist along I-695 just within the boundaries of the Lutherville Historic District, this office feels that our opinion regarding their significance was made clear in our January 3, 1989 letter in which we stated that Alternates 2(D)-3 lane and 2(D)-4 lane would have no adverse effect. Certainly, if these had been historic buildings, our previous determination of adverse effect (letter of December 14, 1987) would not have changed.

For the record, we concur with your opinion that the new construction located along I-695 and on the fringe of the Lutherville Historic District, does not contribute to the significance of the district. Further, we concur with your opinion that Alternate 2 (E) will have no adverse effect on the Lutherville Historic District.

Maryland
Department of Housing and Community Development
Shaw House, 21 State Circle, Annapolis, Maryland 21401 (301) 974-5000

335

Ms. Cynthia D. Simpson
April 3, 1989
Page 2

Should you have any questions concerning this review, please
contact Michael Day at 974-5000.

Sincerely,

George J. Andreve
George J. Andreve
Project Review and
Compliance Administrator
Office of Preservation Services

GJA/MKD/meh
cc: Ms. Rita Suffness
Mrs. Lauri FitzGerald
Mr. Paul McKean

V-140



Maryland Department of Transportation
State Highway Administration

Richard H. Trainor
Secretary
Hal Kassoff
Administrator

November 27, 1989

RE: Contract No. B 635-151-472
I-695 (Baltimore Beltway)
from MD 140 to MD 702
PDMS No. 03113

Mr. J. Rodney Little
State Historic Preservation Officer
Maryland Historical Trust
21 State Circle
Annapolis, Maryland 21401

Dear Mr. Little:

On February 23, 1987 we described the possible effects of Alternate 2, plus Options A Modified and B on the Greenspring Valley Historic District. Since that time we have learned that stormwater management areas would be developed along the right-of-way line which is coterminous with the boundary of the historic district. Initial estimates indicate that 0.5 acres would be required from the Valley Inn property, and 0.6 acres from the adjacent tax parcel (#206). As this area is quite distant from the Inn, as shown on the attached map, we believe that confirmation of your December 14, 1987 determination of no adverse effect is appropriate and thus seek your concurrence by January 4, 1990.

Should you have any questions, please call Ms. Suffness on 333-1183.

Very truly yours,

Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering

by: Cynthia D. Simpson
Cynthia D. Simpson
Assistant Division Chief
Project Planning Division

LHE:RS:cd
Attachment
cc: Mr. Don Honeywell
Mr. Howard Johnson

My telephone number is (301) 333-1177

V-33
Teletypewriter for Impaired Hearing or Speech
383-7555 Baltimore Metro - 585-0451 O.C. Metro - 1-800-492-5082 Statewide Toll Free
707 North Calvert St., Baltimore, Maryland 21203-0717

Response:

1. See coorespondence following.

171-A

333



William Donald Schaefer
Governor

Jacqueline H. Rogers
Secretary, DHCD

December 21, 1989

Ms. Cynthia Simpson, Chief
Maryland Department of
Transportation
State Highway Administration
Project Development Division
707 N. Calvert Street
Baltimore, Maryland 21203-0717

Re: Contract No. B 635-151-472
I-695 (Baltimore Beltway)
from MD 140 to MD 702
PDMS No. 03113

Dear Ms. Simpson:

Thank you for your November 27th letter regarding the project listed above. We agree with State Highway Administration (SHA) that the storm water management areas shown on your map will have no adverse effect on the Greenspring Valley Historic District.

Sincerely,

George J. Andreve
Project Review and
Compliance Administrator
Office of Preservation Services

GJA/meh

Response:

1. No response required.

Department of Housing and Community Development
Shaw House, 21 State Circle, Annapolis, Maryland 21401 (301) 974-5000



217234

TORREY C. BROWN M.D.
SECRETARY
JOHN R. GRIFFIN
DEPUTY SECRETARY

STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
MARYLAND GEOLOGICAL SURVEY

KENNETH H. WEAVER
DIRECTOR
MARYLAND GEOLOGICAL SURVEY
EMERY T. CLEAVES
DEPUTY DIRECTOR

THE ROTUNDA
711 W. 40TH STREET, SUITE 440
BALTIMORE, MARYLAND 21211

Division of Archeology
338-7236

15 August 1985

Ms. Rita Suffness
Environmental Evaluation
State Highway Administration
P.O. Box 717/707 N. Calvert Street, Room 314
Baltimore, Maryland 21203-0717

RE: I-695, Baltimore Co.
Archeological Assessment

Response:
1. See additional coorespondence.

Dear Rita:

I have reviewed our site files for the I-695 project from Falls Road to Harford Road and find only two known sites near the project area: Hampton mansion (BA95) at the Hampton National Historical Site, and site BA177 (a small prehistoric artifact scatter with little research potential).

Highlighted on the enclosed maps are two areas that may possess moderate potential for site location. The small area noted near Brooklandville does not appear built up on the latest (1974) USGS quadrangle, and is situated between two streams. The section noted near Hampton, in addition to adjoining the Hampton Historic Site, also appears relatively undisturbed and is cross-cut by a small stream. More modern (i.e., post-1974) development in either of these areas, if any, would have reduced this predicted potential.

As for the remainder of the I-695 project area, the prehistoric potential must be characterized as being very low because of the massive disturbance which has resulted from both road and housing construction. Likewise, historic archeological potential (aside from that associated with possible 19th century standing structures) is low.

If I can provide additional information on this matter, please let me know.

Sincerely,

Dennis C. Curry
Dennis C. Curry
Archeologist

DCC:lw

TELEPHONE 301-251-7236
V-11

217234



TORREY C. BROWN MD
SECRETARY
JOHN R. GRIFFIN
DEPUTY SECRETARY

STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
MARYLAND GEOLOGICAL SURVEY
THE ROTUNDA
711 W. 40TH STREET, SUITE 440
BALTIMORE, MARYLAND 21211

KENNETH H. WEAVER
DIRECTOR
MARYLAND GEOLOGICAL SURVEY
EMERY T. CLEAVES
DEPUTY DIRECTOR

Division of Archeology
338-7236

13 May 1986

Mr. Louis H. Ege, Jr.
Deputy Director
Division of Project Development
State Highway Administration
P.O. Box 717/707 North Calvert Street
Baltimore, Maryland 21203-0717

RE: I-695 from Harford Road to MD 7
Baltimore County

Response:

1. No response required.

Dear Mr. Ege:

I have reviewed this project area with respect to archeological resources. There are no known archeological sites in the study area. Much of this area has been previously surveyed as shown on the attached map. The potential for archeological sites in the area along I-695 in this area is moderate at best in view of the results of other surveys and the construction that has taken place in this area.

If I may be of further assistance on this matter, please let me know.

Sincerely,
Lori Frye
Lori Frye
Archeologist

LF:lw

Attachment

cc: Rita Suffness
Cynthia Simpson

340

TORREY C. BROWN, M.D.
SECRETARY
JOHN R. GRIFFIN
DEPUTY SECRETARY



STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
MARYLAND GEOLOGICAL SURVEY

2300 ST. PAUL STREET
BALTIMORE, MARYLAND 21218

Don
cc: Eitan Hyles, ECC
KENNETH H. WEAVER
DIRECTOR
MARYLAND GEOLOGICAL SURVEY
EMERY T. CLEAVES
DEPUTY DIRECTOR

Division of Archeology
(301) 554-5530
6 January 1957

JAN 8 9 32 AM '57
PROJECT
DEVELOPMENT
DIVISION

Mr. Louis H. Ege, Jr.
Deputy Director
Division of Project Development
State Highway Administration
P.O. Box 717/707 North Calvert Street
Baltimore, Maryland 21203-0717

RE: Baltimore Beltway, from I-695 to
Maryland 702

Response:
1. No response required.

Dear Mr. Ege:

I have reviewed the above-referenced project with regard to archeological resources. There are no known archeological sites in the project area. However, at Exit 36 of I-695 at Northeast Creek, a boat (details unavailable in Division of Archeology files) was found during highway construction in 1970.

The potential for prehistoric sites is moderate along portions of Northeast Creek provided construction has not already destroyed them. For historic sites, the potential is poor based on the lack of settlement noted in early maps and recent development that has occurred in the area.

If there are any additional questions regarding this matter or if I can be of further assistance, please do not hesitate to contact me.

Sincerely,

Hettie L. Ballweber
Archeologist

HLB:lw

cc: Rita Suffness
Cynthia D. Simpson

TELEPHONE: 301-554-5500
V-13

348



TORREY C. BROWN, M.D.
SECRETARY
JOHN R. GRIFFIN
DEPUTY SECRETARY

STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
CAPITAL PROGRAMS ADMINISTRATION
TAWES STATE OFFICE BUILDING
ANNAPOLIS, MARYLAND 21401

FRED L. ESKEW
ASSISTANT SECRETARY
FOR CAPITAL PROGRAMS

February 28, 1985

Mr. Richard Rein
DeLew, Cather & Company
Suite 500
1201 Connecticut Avenue, NW
Washington, D.C. 20036

Subject: I-695 Project, I-695 between Reisterstown Road
and I-95 in Baltimore County

Dear Mr. Rein:

The Maryland Natural Heritage Program has no record of any rare species, unusual habitat or other significant natural feature in the immediate vicinity of the portion of I-695 identified above. Nor, are any sites identified in the Upland Natural Areas Survey located in this area. As long as proposed improvements will be generally in the location of existing rights-of-way and intersections, I see no potential impact to any site considered significant by this Program.

Sincerely,

Arnold Norden

Arnold Norden
Maryland Natural Heritage Program

AWN:mle

Response:

1. No response required.

347



JERREY C. BROWN, M.D.
SECRETARY

JOHN R. GRIFFIN
DEPUTY SECRETARY

STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
TIDEWATER ADMINISTRATION
TAWES STATE OFFICE BUILDING
ANNAPOLIS 21401

March 11, 1985

TO: W.R. Carter, Environmental Review,
Fisheries Division

FROM: Charles R. Gougeon

SUBJECT: Fish List and "High Priority" Streams
Along Baltimore Beltway (Md. Rt. 695)

Response:

1. I-83 (JFX) Option C is part of the selected build alternate. This option will bridge the Jones Falls. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance, including use of snow fencing to mark areas, a double row of silt fencing and straw bales intercepting runoff, immediate removal of dredged material from the floodplain, and revegetation of disturbed areas with natural vegetation.

1) See attached fish list - freshwater fish

- 2) a) By far, the most important stream proposed to be crossed by the Rt. 695 construction will be Jones Falls. Surprisingly, Jones Falls has maintained a high degree of water/habitat quality. In fact, the stream presently sustains a "wild" reproducing brown trout population that extends from its headwaters above Green Spring Valley Road and continues inside the Baltimore Beltway. The water quality remains suitable for year-round trout survival down to Lake Roland. Waters below Lake Roland are designated as Class IV (Recreational Trout Waters) and are stocked with hatchery trout each year in the spring.
- b) Stemmers Run and all tribs is also designated as a Class IV stream. At this time, the stream is not stocked with hatchery trout. No wild trout are known to exist in this stream. Surveys of the stream system indicate moderately degraded conditions (water quality/habitat). All efforts should be directed towards preserving/enhancing stream conditions through implementation of strict sediment control measures.

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DEPARTMENT OF NATURAL RESOURCES
Maryland Forest, Park & Wildlife Service
TAWES OFFICE BUILDING
ANNAPOLIS, MARYLAND 21401

TORREY C. BROWN, M.D.
SECRETARY

DONALD E. McLAUGHLIN
DIRECTOR

March 13, 1985

Mr. Richard Rein
DeLew, Cather & Co.
Suite 500
1201 Connecticut Ave., N.W.
Washington, D.C. 20036

Re: Baltimore Beltway Widening

Dear Mr. Rein:

As discussed on our phone conversation of 14 February 1985, I have enclosed a list of standard references which you could consult in compiling a list of species representative of the project area.

I have no record of any unique or critical wildlife habitats in the area.

If you need additional information, please let us know.

Sincerely,

Sean McKewen
Project Leader Technical Services

Response:

1. No response required.

SM/dec

Telephone 269-3195
TTY FOR DEAF: STATEWIDE 1-800-492-5062; BALTIMORE 269-2609



TORREY C. BROWN M.D.
SECRETARY
JOHN R. GRIFFIN
DEPUTY SECRETARY

STATE OF MARYLAND
DEPARTMENT OF NATURAL RESOURCES
CAPITAL PROGRAMS ADMINISTRATION
TAWES STATE OFFICE BUILDING
ANNAPOLIS, MARYLAND 21401

FRED L. ESKEW
ASSISTANT SECRETARY
FOR CAPITAL PROGRAMS

September 26, 1985

Mr. Louis Ege
Bureau of Project Planning
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21203

Subject: Improvements to I-695 from Md. Route 140 to
I-95, Baltimore County

Dear Mr. Ege:

Baltimore County in the vicinity of this portion of I-695 contains numerous recorded localities for rare, threatened and endangered species. However, none of these locations are close enough to the existing roadway to be impacted by widening or interchange improvements generally in existing location. If I can be of further assistance, please do not hesitate to contact me.

Sincerely,

Arnold W. Norden
Arnold W. Norden
Maryland Natural Heritage Program

AWN:mle

Response:

1. No response required.

245

671-A



Department of Natural Resources
MARYLAND FOREST, PARK & WILDLIFE SERVICE
Tawes Office Building
Annapolis, Maryland 21401

TORREY C. BROWN MD
SECRETARY

DONALD E. MACLAUCHLAN
DIRECTOR

June 12, 1986

Ms. Cynthia D. Simpson, Chief
Environmental Management
Maryland Department of Transportation
P.O. Box 717
707 North Calvert Street
Baltimore, Maryland 21203-0717

RE: Contract No. B-635-151-472
I-695 from Md Route 140 to
Md Route 702

Dear Ms. Simpson:

Your request for information we may have concerning threatened or endangered species has been reviewed by Gary J. Taylor.

There are no known populations of threatened or endangered species within the area of project influence in Baltimore County.

Sincerely,

James Burtis, Jr.
Assistant Director

JB:emp

CC: G. Taylor
C. Brunori

Response:

1. No response required.

346



rec'd 9/18/86

Department of Natural Resources
MARYLAND FOREST, PARK & WILDLIFE SERVICE
Tawes Office Building
Annapolis, Maryland 21401

DONALD E. MACLAUGHLIN
DIRECTOR

TORREY C. BROWN, M.D.
SECRETARY

September 3, 1986

Annette Mercer
DeLew Cather & Co.
Suite 500
1201 Connecticut Avenue, N.W.
Washington, D.C. 20036

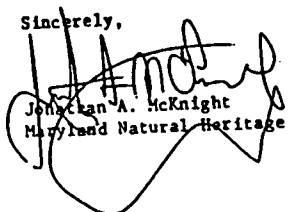
Subject: I-695: Reistertown Road to Interchange 36

Dear Ms. Mercer,

The Maryland Natural Heritage Program has no record of any rare species or unique habitat at or in the vicinity of this project site. However, in the absence of a recent site review, we cannot show that such species or habitats are not present.

Species and habitats of special concern to the state are listed and discussed in the following 1984 Department of Natural Resources publication: Threatened and Endangered Plants and Animals of Maryland, available through this office. A site evaluation should include a consideration of these species and their habitats.

Sincerely,


Jonathan A. McKnight
Maryland Natural Heritage Program

JAM:nlc

Response:

1. No response required.

V-151

347

PROVIDENCE VOLUNTEER FIRE COMPANY, INC.

1416 Providence Rd. • P.O. Box 9702 • Towson, Md. 21284

Telephone
829-9029

Emergency
911

September 5, 1986

Ms. Cynthia Simpson, Acting Chief
Environmental Management
Maryland State Highway Administration
707 North Calvert Street
P. O. Box 717
Baltimore, Maryland 21203-0717

PROJECT
DEVELOPMENT
DIVISION
SEP 11 10 33 AM '86

Dear Ms. Simpson:

Thank you for your letter to Tom Hill dated August 23, 1986. Let me begin by advising you that Tom has relocated in Colorado and that I am now the President of the company. The previous plans submitted to us for review were of a nature that we had only one comment. We are very concerned about the Beltway interchange with Loch Raven Blvd. as this has proven to be over the years one of the most accident occurrences on 695. Since the inner loop of the portion of the road is our responsibility, we want to see the changes made here reduce dramatically the accident rate. We do not believe the work currently being completed at Loch Raven Blvd. will significantly change these statistics.

The supplemental information that you sent us in your August 23, 1986 letter greatly concerns us. By closing the Providence Road bridge for any period of time greatly endangers the public safety of the area. As you may recall, several years ago, the Providence Road bridge was reconstructed and only north bound traffic was maintained during this period. After a number of letters and discussions, emergency vehicles were permitted to proceed south over the bridge after the bridge was cleared. In addition, Providence Road is the primary access road to the area for fire apparatus responding in to the area from stations one and eleven of the Baltimore County Fire Department.

It should also be noted that the Loch Raven Drive bridge which services the Providence Road area from the north has been restricted for a number of years prohibiting vehicles having the substantial weight of fire apparatus from using the bridge. This restriction prevents equipment from the Long Green and Jacksonville Volunteer Fire Companies from having a direct access to the Providence area.

Response:

1. Reconstruction of the Providence Road bridge will occur in halves, allowing emergency vehicles to cross the Beltway if needed.

Ms. Cynthia Simpson
September 5, 1986
Page 2

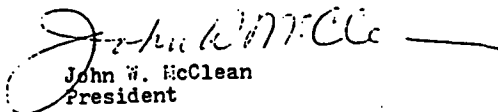
Reconstruction of the Loch Raven Drive bridge is anticipated in 1987 and 1988. However, this is the first of several bridge replacement projects planned for the Loch Raven Reservoir area which will change traffic patterns to the Towson, Parkville, Lutherville and Cockeysville areas. Consideration must be given to these projects in the overall picture.

A further consideration is the ongoing sewer construction which is taking place in the Hampton area. Roads are closed frequently for the construction work taking place for this sewer installation. At one point during the Providence Road bridge reconstruction, Seminary Avenue was also closed for a period of time, isolating the entire Providence-Hampton area.

We oppose any plan which calls for the closing of the Providence Road bridge even for one day. We are a primary response unit to the Parkville, Hillendale and Towson areas which in addition to access to our area by responding apparatus would be severely restricted by any closing of the bridge. I am sure that there are alternative plans which would accomplish the same result without the closing of the Providence Road bridge.

We are looking forward to reviewing alternate plans for the improvement of the Baltimore Beltway and in particular the Providence Road bridge.

Yours truly,


John W. McClean
President



BALTIMORE COUNTY
FIRE DEPARTMENT
TOWSON, MARYLAND 21204-2586
494-4500

PAUL H. REINCKE
CHIEF

September 8, 1986

SEP 10 9 50 AM '86
PROJECT
DEVELOPMENT
DIVISION

Ms. Cynthia Simpson, Acting Chief
Environmental Management
Maryland State Highway Administration
707 North Calvert Street
P.O. Box 717
Baltimore, MD 21203-0717

Re: Possible Construction Modification
of I-695; Potential Impact to
Emergency Vehicle Response Time

Dear Ms. Simpson:

With respect to Alternate 2, Beltway Widening mentioned in DeLeuw, Cather and Company's 8/28/86 letter, below are our concerns about each of the bridges over I-695 which would be completely closed for several months:

- a. Falls Road Bridge and ^{old} Harford Road Bridge

The complete closing of these bridges would cause major interruptions to our service delivery since they are utilized on approximately 45% of our responses in those areas. Would it be possible to upgrade them half at a time, thereby making emergency apparatus access still available?

- b. Providence Road Bridge

The complete closing of the Providence Road Bridge would have no major effect on our service in that area. We were without its use for an extended period last year.

The "NO BUILD OPTION" obviously would have no effect to our emergency vehicle response time. With regard to the other alternatives and options, may we assume that the affected bridges would all still be available to emergency vehicle traffic? If that is the case, other than dealing with possible increased traffic congestion during construction, no other significant effect on our emergency vehicle response time is anticipated. Any temporary inconvenience will eventually be offset by increased, faster traffic flow. Thus, as long as we are kept advised of the projects, our response routes could be adjusted accordingly.

Sincerely,

Paul H. Reincke
Paul H. Reincke, Chief
Fire Department

PHR:RAW

Copy: Executive Staff

V-22

Response:

1. The Falls Road and Old Harford Road Bridge may be closed to traffic during reconstruction either in their entirety or in part. If the closing of either bridge is required, coordination with emergency facilities will be completed.
2. The Providence Road bridge will be reconstructed in halves to maintain a crossing of the Beltway.

350



BALTIMORE COUNTY
 FIRE DEPARTMENT
 TOWSON, MARYLAND 21204-2586
 494-4500

PAUL H. REINCKE
 CHIEF

September 9, 1986

PROJECT
 DEVELOPMENT
 DIVISION
 SEP 11 10 33 AM '86

Ms. Cynchia Simpson, Acting Chief
 Environmental Management
 Maryland State Highway Administration
 707 North Calvert Street
 P.O. Box 717
 Baltimore, MD 21203-0717

RE: Our 9/8/86 Response to The Potential Impact
 to Emergency Vehicle Response Time of
 Possible Construction Modifications to I-695

Response:

1. No response required.

Dear Ms. Simpson:

Please correct our September 8 letter to read in item
 a. "Falls Road Bridge and Old Harford Road Bridge."

Thank you. We regret the inconvenience.

Sincerely,

Lydia M. Yates

(Mrs.) Lydia M. Yates, Adm. Secy. II
 Office of the Chief
 Fire Department

351



United States Department of the Interior

NATIONAL PARK SERVICE

MID-ATLANTIC REGION
143 SOUTH THIRD STREET
PHILADELPHIA, PA. 19106

IN REPLY REFER TO:

L7621 (MAR-PD)

30 SEP 1986

PROJECT
DEVELOPMENT
DIVISION
OCT 2 2 01 PM '86

Neil J. Pedersen, Director
Office of Planning and Preliminary Engineering
Maryland Department of Transportation
P.O. Box 717, 707 North Calvert Street
Baltimore, Maryland 21203-0717

Dear Mr. Pedersen:

In response to your letter of September 15, 1986 concerning noise mitigation in the area of Hampton National Historic Site, the Department of the Interior concurs that with the provision of the noise wall along the right-of-way line in front of the historic site and adjacent residential areas there will not be a Section 4(f) use of the site as a result of the widening of the Baltimore Beltway. Further, we do not object to the widening of the Baltimore Beltway in this area.

However, as there are important historic resources located adjacent to the fence line opposite Stations 840+00 and 841+00 as identified on the enclosed map, we would caution the State Highway Administration to refrain from crossing onto the site with heavy equipment. The area will be flagged by the National Park Service, and coordination and consultation should be continued with Superintendent Karen Wade (535 Hampton Lane, Towson, Maryland 21204 (telephone 301/823-7054).

I wish to express my appreciation for your sensitivity and cooperation in protecting this historic resource.

Sincerely,

James W. Coleman, Jr.
James W. Coleman, Jr.
Regional Director

Enclosure

*Honywell
Simpson*

RECEIVED

#939
OCT 2 1986

DIRECTOR, OFFICE OF
PLANNING & PRELIMINARY ENGINEERING

RECEIVED

OCT 8 1986

JOHNSON, MERRICK & THOMPSON

Dan
cc DeLoach
Eileen Hughes

Response:

1. A noise wall has been constructed in the area of Hampton National Historic site.

V-156

352



United States Department of the Interior

FISH AND WILDLIFE SERVICE
DIVISION OF ECOLOGICAL SERVICES
1825B VIRGINIA STREET
ANNAPOLIS, MARYLAND 21401

October 30, 1986

Ms. Annette Mercer
Environmental Planner
DeLew Cather & Co.
Suite 500
1201 Connecticut Avenue, N.W.
Washington, D.C. 20036

Dear Ms. Mercer:

This is in response to your request for endangered species information for the area of the proposed improvements to the Baltimore beltway (your reference 3663-40). We are enclosing a copy of our September 24, 1985 letter to the Maryland Department of Transportation commenting on this project. As you will note, our records indicate no endangered, threatened or other rare species in the project vicinity.

Regarding the planned expansions to BWI, we know of no Federal candidate species in the project area other than Helonias bullata. However, the Maryland Heritage Program has data on several state-rare species in the vicinity. For further information, you should contact Mr. D. Daniel Boone, MD Heritage Program, Taves State Office Bldg., 580 Taylor Avenue, Annapolis, MD 21401 (tele. 301/269-2870).

You should be aware that we plan to propose threatened status for Helonias bullata in the coming year. Therefore, the needs of this plant should be carefully considered in the preparation of the BWI master plan. For further information, please contact Ms. Judy Jacobs of our staff, telephone 301/269-6324.

We appreciate your concern for endangered species.

Sincerely yours,

S. A. Moser

Glenn Kinser
Supervisor
Annapolis Field Office

Enclosure

Response:

1. No response required.

353



DEPARTMENT OF THE ENVIRONMENT
201 WEST PRESTON STREET • BALTIMORE, MARYLAND 21201
AREA CODE 301 • 225-5275

William Donald Scheffer
Governor

Martin W. Walsh, Jr.
Secretary

August 26, 1987

Ms. Cynthia O. Simpson, Chief
Environmental Management
Project Development Division
7D7 North Calvert Street, Room 310
Baltimore, Maryland 21202

RE: Interstate Route 695 from
Maryland Route 140 to MD Route 702
PDMS No. D31113
Contract No. B 635-151-472

Dear Ms. Simpson:

I have reviewed the air impact analysis performed for the proposed improvements of Interstate 695 between Maryland Route 140 and Maryland Route 702 in Baltimore County and concur with its conclusions.

Given the expected increase in traffic predicted for the region, the Department believes that any built alternate will yield the best air quality for the area.

The proposed project is consistent with the transportation control portion of the State Implementation Plan for the Metropolitan Baltimore Intrastate Air Quality Control Region. Furthermore, adherence with the provisions of COMAR 10.18.06.03D will ensure that the impact from the construction phase of this project will be minimal.

Thank you for the opportunity to review this analysis.

Sincerely,

Mario E. Jorquera, Chief
Division of Air Quality Planning
and Data Systems
Air Management Administration

MJ:dsd

V-26

PROJECT
DEVELOPMENT
DIVISION
SEP 1 2 14 PM '87

Response:

1. A build alternate was selected for the mainline.

V-158

354



United States Department of the Interior
PROJECT DEVELOPMENT DIVISION

FISH AND WILDLIFE SERVICE
DIVISION OF ECOLOGICAL SERVICES
1825 VIRGINIA STREET
ANNAPOLIS, MARYLAND 21401

AUG 15 9 53 AM '88

July 29, 1988

Ms. Cynthia D. Simpson
Maryland Department of Transportation
707 N. Calvert Street
Baltimore, Maryland 21203

Dear Ms. Simpson:

This responds to your July 26, 1988 request for information on the presence of species which are Federally listed or proposed for listing as endangered or threatened within the area of the proposed improvements to MD 695, Baltimore County, Maryland. We have reviewed the information you enclosed and are providing comments in accordance with Section 7 of the Endangered Species Act (87 Stat. 884, as amended; 16 U.S.C. 1531 et seq.).

Except for occasional transient individuals, no Federally listed or proposed endangered or threatened species are known to exist in the project impact area. Therefore, no Biological Assessment or further Section 7 Consultation is required with the Fish and Wildlife Service (FWS). Should project plans change, or if additional information on the distribution of listed or proposed species becomes available, this determination may be reconsidered.

This response relates only to endangered species under our jurisdiction. It does not address other FWS concerns under the Fish and Wildlife Coordination Act or other legislation.

Thank you for your interest in endangered species. If you have any questions or need further assistance, please contact Judy Jacobs of our Endangered Species staff at (301) 269-5448.

Sincerely yours,

Judy Jacobs
Judy Jacobs
Supervisor
Annapolis Field Office

Response:

1. No response required.

6/29/88

Baltimore County
Department of Recreation & Parks
Towson, Maryland 21204
494-3817
494-3058 (Deaf/TDD) Sep 7 10 39 AM '88

Robert R. Staab
Director

cc Dan
Rin
Annette
Meyer
OCC

August 31, 1988



Dennis F. Rasmussen
County Executive

Ms. Cynthia D. Simpson, Chief
Environmental Management
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21203-0717

RECEIVED

SEP 8 1988

Dear Ms. Simpson:

PERSON, HUMAN & THINGS

Re: Contract No. B 635-101-472
I-695 (Baltimore Beltway)

This is in response to your recent letter relative to the relocation of the entrance road to Double Rock Park off Belair Road.

A number of years ago, our Department negotiated with the owners of Belair Plaza an additional ingress/egress that presently traverses the Shopping Center, using the existing signalized intersection.

We do not have a concern with the closing of the park entrance a few hundred feet north of the signal light; however, we are troubled with your proposed plan locating the new road through park property, which we imagine would be a public road since it provides access to Cliff Lodge and private residences. If this were to become a reality, the property to the east, adjacent to Belair Road, would not have any Recreation and Parks value; and you will have destroyed the continuity of the existing park. In your planning process, has it been brought to your attention that the park property in question was purchased in 1976 for Recreation and Park purposes only, by funds provided by Program Open Space? The law states that property acquired with P.O.S. funds "may not be converted from public recreation or open space use to any other use without written approval of the Secretary of the Department of Natural Resources."

You suggested in your July 26 letter that the state has "adjacent excess land" which could be used to replace park property. If the State property in question is north of Double Rock Park, adjacent to the Beltway, we feel this does not have any Recreation and Parks value, and we would not be interested in same.

Response:

1. U.S. 1 Option has been selected. The previous option to provide access through the park property has been dropped.

8/5/88

V-160

Ms. Cynthia D. Simpson

-2-

August 31, 1988

However, we would be interested in discussing the possibilities of your replacing the property you are taking at Double Rock Park with State property currently under your control at Chesaco Avenue. See attached Exhibit "A".

Enclosed are two copies of our Master Plan for Double Rock Park, showing existing and proposed facilities. The Park is one of the County's most heavily used facilities, with an annual attendance of 100,000.

If you have any questions, please do not hesitate to contact me at 494-3805.

Sincerely,



Harry G. Coulter, Jr.
Assistant Director/
Facility Development
and Conservation

HGC:dv

Enc.

CC: Mr. R. Staab

357



Maryland Department of Transportation
State Highway Administration

H 7

O. James Lightfoot
Secretary
Hal Kassoff
Administrator

October 25, 1991

RE: Contract No. B 635-101-472
I-695 from MD 140 to MD 702
Baltimore County, Maryland

Mr. Wayne R. Harman, Director
Baltimore County
Department of Recreation and Parks
301 Washington Avenue
Towson MD 21204

ATTN: Mr Charles Kines

Dear Mr. Harman:

The Maryland State Highway Administration (SHA) is finalizing its study to widen the Baltimore Beltway (I-695) from MD 140 to the vicinity of MD 702 (see attachment). The proposed improvements will require a temporary construction easement at Woodcroft Park. The proposed temporary easement area will consist of approximately 0.5 acre which is required to construct an earth-retaining jersey barrier. Construction of the jersey barrier will eliminate the need for any fee-simple acquisition in this area. Construction would also require the temporary removal of the existing noise wall which is within existing SHA right-of-way. Any area impacted will be restored to its preconstruction appearance.

We seek your concurrence with our determination that the proposed temporary use of Woodcroft Park will: (a) be of short duration and less than the time needed for construction of the project, (b) not change the ownership or result in the retention of long-term or indefinite interests in the land for transportation purposes, (c) not result in any temporary or permanent adverse change to the activities, features, or attributes which are important to the purposes or functions of the park, and (d) include only a minor amount of land.

(301) 333-1177

My telephone number is _____

Teletypewriter for Impaired Hearing or Speech
383-7555 Baltimore Metro - 565-0451 D.C. Metro - 1-800-492-5062 Statewide Toll Free
707 North Calvert St., Baltimore, Maryland 21203-0717

Response:

1. See coorespondence following.

V-162

5252

Mr. Wayne Harman
October 25, 1991
Page 2

We seek your concurrence in our determination by October 30, 1991. Should you have any questions, please call Mr. Howard Johnson at 333-1179.

Thank you for your assistance in this matter.

Very truly yours,

Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering

by: Cynthia D. Simpson
Cynthia D. Simpson
Deputy Division Chief
Project Planning Division

LHE:HJ:cd

Attachments

cc: Ms. Shirley Murphy
Mr. Robert Sanders

V-163

10/25/91

Baltimore County Government
Department of Recreation and Parks



301 Washington Avenue
Towson, MD 21204

887-3817
Fax 825-3305
Deaf/TDD 887-5319

November 1, 1991

Mr. Louis H. Ege, Jr., Deputy Director
Office of Planning and Preliminary Engineering
Maryland Department of Transportation
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21203-0717

RE: Contract No. B 635-101-472
I-695 from MD 140 to MD 702
Baltimore County, Maryland

Response:

1. Prior to construction activity, Baltimore County Land Acquisition will be contacted.

Dear Mr. Ege:

Confirming my recent telephone conversations with your Mr. Howard Johnson and your letter of October 25, 1991, we have no reason to withhold or deny your request for a temporary construction easement at Woodcroft park during the widening of the (I-695) Baltimore Beltway.

In that we have no active programs at Woodcroft Park and you will restore the area to its pre-construction appearance, there will be minimal disruption during construction.

Prior to the construction activity, the temporary easement must be processed by Ms. Shirley Murphy, Chief of Baltimore County Land Acquisition Bureau. By a copy of this letter, I will notify her of the impending construction and widening of the Beltway.

If the Department of Recreation and Parks can be of any further assistance, please feel free to contact me at 887-3813.

Sincerely,

Charles L. Kines
Superintendent
Northern Area

CLK:rw

3600



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION III

841 Chestnut Building
Philadelphia, Pennsylvania 19107

SEP 2 1987

Cynthia D. Simpson
Chief
Environmental Management
Project Development Division (Rm. 310)
Maryland State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Re: Interstate Route 695
Maryland Route 140 to
Maryland Route 702

Dear Ms. Simpson:

In accordance with the National Environmental Policy Act (NEPA) and Section 309 of the Clean Air Act, EPA has reviewed the Draft Air Quality Analysis for the above referenced project. The results of the microscale analysis demonstrate that none of the build alternatives will violate the State or National Ambient Air Quality Standards for the the design year, 2015. The No-Build Alternate will violate the 1-hour standard for CO in the design year, although the the standard will not be exceeded by more than 1 ppm.

EPA is satisfied with the method of analysis and is pleased that the EPA Mobile 3 computer program was used for calculating emission factors.

Thank you for providing EPA with the opportunity to comment on this document. If you have any questions or if we can be of further assistance, please contact Lynn Rothman at 215/597-7336.

Sincerely,


Jeffrey M. Alper, Chief
NEPA Compliance Section

SEP 10 9 59 AM '87
PROJECT DEVELOPMENT DIVISION

Response:
1. No response required.

591-4

8/22/87



United States
Department of
Agriculture

Soil
Conservation
Service

10 W. College Terrace
Room 235
Frederick, MD 21701

October 3, 1988

Mr. Charles P. Butler
Environmental Manager
Johnson, Mirmiran, and Thompson, P.A.
810 Gleneagles Court, Suite 200
Baltimore, MD 21204

RECEIVED

84169
OCT 8 1988
Chuck

FOR THE DIRECTOR

Re: I-695 Environmental Assessment Document
JMT Job No. 84169

Response:

1. No response required.

Dear Mr. Butler:

Although you did an excellent job in providing the Soil Conservation Service with detailed acreages of prime farmland soils and soils of statewide importance for completion of an AD-1006 form (Farmland Conversion Impact Rating), the information you provided by telephone regarding zoning of the areas in question precludes the Farmland Protection Policy Act.

You indicated that available zoning information shows the subject areas to be zoned residential, commercial and industrial. Consequently, this overrides any soil classification and the site is said to contain "no" prime, unique, statewide or local important farmland soils.

Enclosed is an AD-1006 with the appropriate part completed by SCS which covers the project areas outlined in your letter.

If we can be of further assistance please contact our office in Cockeyville at 666-1188.

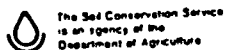
Sincerely,

Carl E. Robinette

CARL E. ROBINETTE
Soil Scientist

Enclosure

CC:
Ricky R. Dilis, District Consvst., SCS, Cockeyville, MD



362

FARMLAND CONVERSION IMPACT RATING

Part I (To be completed by Federal Agency)

Date Of Land Evaluation Request: 9/12/88

Name Of Project: I-695 Improvements

Federal Agency Involved: Federal Highway Administration

Proposed Land Use: Highway

County And State: Baltimore, MD

Date Request Received By SCS: 9/22/88

Part II (To be completed by SCS)

Does the site contain prime, unique, statewide or local important farmland? (If no, the FPPA does not apply - do not complete additional parts of this form). Yes No

Acres Irrigated: None

Average Farm Size: 98

Major Crops: Corn, Small Grain, Soybeans, Hay

Farmable Land In Govt. Jurisdiction: Acres: 195,200 % 51

Amount Of Farmland As Defined In FPPA: Acres: 154,000 % 40.3

Name Of Local Site Assessment System: Balto. Co. Land Evaluation

Date Land Evaluation Returned By SCS: 10/3/88

Use FPPA System

Part III (To be completed by Federal Agency)

	Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly	13.11	1.33	1.45	
B. Total Acres To Be Converted Indirectly				
C. Total Acres In Site				

Part IV (To be completed by SCS) Land Evaluation Information

A. Total Acres Prime And Unique Farmland

B. Total Acres Statewide And Local Important Farmland

C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted

D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value

Part V (To be completed by SCS) Land Evaluation Criterion

Relative Value Of Farmland To Be Converted (Scale of 0 to 100 Points)

Part VI (To be completed by Federal Agency)

Site Assessment Criteria (These criteria are explained in 7 CFR 350.516)

Criteria	Maximum Points
1. Area In Nonurban Use	
2. Perimeter In Nonurban Use	
3. Percent Of Site Being Farmed	
4. Protection Provided By State And Local Government	
5. Distance From Urban Builtup Area	
6. Distance To Urban Support Services	
7. Size Of Present Farm Unit Compared To Average	
8. Creation Of Nonfarmable Farmland	
9. Availability Of Farm Support Services	
10. On-Farm Investments	
11. Effects Of Conversion On Farm Support Services	
12. Compatibility With Existing Agricultural Use	
TOTAL SITE ASSESSMENT POINTS	160

Part VII (To be completed by Federal Agency)

Relative Value Of Farmland (From Part VI)	100
Total Site Assessment (From Part VI above or a local site assessment)	160
TOTAL POINTS (Total of above 2 lines)	260

Site Selected: _____ Date Of Selection: _____

Was A Local Site Assessment Used? Yes No

Legend For Selection

*Site A = Stormwater Management areas

Site B = Relocation of I-695

Site C = Interchange Options

V-167

Handwritten signature and date



United States Department of the Interior

OFFICE OF THE SECRETARY
WASHINGTON, D.C. 20240



FEB 15 1990

ER 90/117

Mr. A. Porter Barrows
Division Administrator
Federal Highway Administration
711 West 40th Street, Suite 220
Baltimore, Maryland 21211

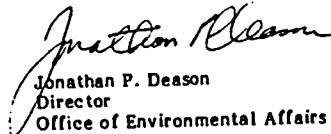
Dear Mr. Barrows:

This is in regard to the request for the Department of the Interior's comments on the Draft Environmental Statement concerning I-695 (east of SR-140 to west of SR-702), Baltimore County, Maryland.

This is to inform you that the Department will have comments, but will be unable to reply within the allotted time. Please consider this letter as a request for an extension of time in which to comment on the statement.

Our comments should be available about late March.

Sincerely,


Jonathan P. Deason
Director
Office of Environmental Affairs

cc: Mr. Neil J. Pedersen
Director
Office of Planning and Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Response:

1. See coorespondence following.

10

364



United States Department of the Interior

OFFICE OF THE SECRETARY
WASHINGTON, D.C. 20240

APR 13 1990

L74 (MAR-PD)
ER-90/117

A. Porter Barrows
Division Administrator
Federal Highway Administration
711 West 40th Street, Suite 220
Baltimore, MD 21211

Dear Mr. Barrows:

This responds to a request for the Department of the Interior's comments on the Environmental Assessment/Section 4(f) Evaluation for I-695 (East of SR-140 to West of SR-702), Baltimore County, Maryland.

SECTION 4(f) STATEMENT COMMENTS

We concur that, if transportation objectives are to be achieved, there is no feasible and prudent alternative to the proposed use of land within public parkland and property listed in the National Register of Historic Places. We are also in agreement with selection of the recommended plan. We concur that the proposed mitigation, which includes erection of a noise wall adjacent to the Hampton National Historic Site, is appropriate, and recommend the continued coordination and consultation with the National Park Service and the Maryland State Historic Preservation Officer.

ENVIRONMENTAL ASSESSMENT COMMENTS

Fish and Wildlife Resources - We do not endorse the proposed interchange I-83 (Jones Falls Expressway) Option C alternative. Option C would impact 0.916 acres of wetlands along Jones Falls, a stream which has a naturally reproducing brown trout population. The other improvement options (A, B, D) for this interchange would only impact 0, 0.203, and 0.543 acres of wetlands, respectively. We recommend that these other options be considered for improving the geometrics of this intersection. There are no objections to any of the other interstate improvement proposals.

4.10
Don-do
we have
a pool
off?

Response:

1. I-83 (JFX) Option C has been selected. This will include a bridge crossing of the Jones Falls. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance, including the use of snow fencing to mark areas, a double row of silt fencing and straw bales intercepting runoff, immediate removal of dredged material from the floodplain, and revegetation of disturbed areas with natural vegetation.

3/15

691-A



U.S. Department of Housing and Urban Development
Baltimore Office, Region III PROJECT
The Equitable Building, BALTIMORE
3rd Floor, 10 North Calvert Street
Baltimore, Maryland 21202-1865

Mar 9 2 15 PM '90

March 7, 1990

Mr. Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, MD 21202

Dear Mr. Ege:

SUBJECT: Environmental Assessment/Section 4(f) Evaluation
I-695 from MD 140 to MD 702

Response:

1. No response required.

Mergeret Krengel, Regional Environmental Officer at the HUD Philadelphia
Regional Office, has referred the above-mentioned assessment/4(f) evaluation
to this office for review and comment.

In accordance with 40 CFR Chapter V of the Council on Environmental Quality
regulations we wish to inform you that no HUD-assisted Community Development
Block Grant activities or HUD-insured housing projects will be impacted by the
proposed improvements to the Baltimore Beltway.

As a general comment, however, it is disappointing to note that even with the
proposed beltway improvements, the level-of-service on the expressway would
only temporarily be improved. The discussion of alternatives for solving
anticipated future long term traffic congestion on the beltway appears to rule
out all possible solutions. Only the consideration of constructing an elevated
roadway above the existing beltway was not mentioned.

Given the above, we have no specific comments regarding the assessment.

Very sincerely yours,

Robert H. Herbert, Jr.
Robert H. Herbert, Jr.
Environmental Officer

cc: Mergeret Krengel

3/6/90

V-170



DEPARTMENT OF THE ENVIRONMENT

2500 Broening Highway, Baltimore, Maryland 21224
Area Code 301 • 631•

William Donald Schaefar
Governor

Martin W. Walsh, Jr.
Secretary

March 9, 1990

Mr. Louis H. Ege, Jr., Deputy Director
Office of Planning and Engineering
Maryland State Highway Administration
707 N. Calvert Street
Baltimore, Maryland 21202

RE: Environmental Assessment Improvement of I-695 from East of MD 140 to
West of MD 702 in Baltimore County,
Contract Number B 635-151-472.

Dear Mr. Ege:

We are in receipt of the above-referenced document and have reviewed the potential water quality and resource impacts of the proposal. Considering the scope of the project, the primary impacts to state wetlands and waterways appear to be minimal. However, the potential for secondary impacts appears to be significant. The Division of Standards and Certification has no objections to this proposal provided the following conditions are satisfied.

1. Interchange options for I-83 JFX which result in the greatest impacts of the project should be further reduced if possible. The waters in this area are Class III, and are afforded the highest level of protection.
2. Mitigation for Class III and IV impacts should include restoration of streams and riparian habitat in addition to a minimum of 1:1 wetland creation.
3. Areas bound by access ramps should not be used for mitigation areas.
4. All newly constructed impervious areas shall be subject to stormwater management of a minimum of the first one-half inch of runoff in uplands. Vegetated medians and swales removed for road widening

Response:

1. I-83 (JFX) Option C is part of the selected build alternate. this option will bridge the Jones Falls. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance, including use of snow fencing to mark areas, a double row of silt fencing and straw bales intercepting runoff, immediate removal of dredged material from the floodplain, and revegetation of disturbed areas with natural vegetation.
2. Wetland mitigation will be provided in kind wetland recreation at a minimum of 1:1. Wetland mitigation sites have not located.
3. Stormwater management will be prepared in final design in coordination with the Department of the Environment.
4. Any construction in Class III or Class IV waters will be restricted as required.

Mr. Louis H. Ege, Jr.
Page 2 of 2

and which serve as stormwater attenuating areas for existing road surfaces shall be compensated for by providing alternative methods of stormwater quality management for this runoff. Infiltration is preferred.

5. All work in wetlands and waterways is prohibited from October 1 to April 30 in Class III waters and from March 1 to May 31 in Class IV waters.
6. Wet ponds may not be used for stormwater control if they discharge to Class III and IV waters. Infiltration of a minimum of the first one-half inch of runoff is the preferred method of stormwater management in these areas.
7. Naturally occurring State wetlands and waterways shall not be impounded for the purposes of stormwater retention or mitigation enhancement.

We hope that this information is helpful. If you have any questions, please contact me at (301) 631-3609.

Sincerely,

Andrew T. Der *ATD*
Andrew T. Der
Standards and Certifications Division

cc: Linda Milchling
James Tiett

ATD/lg

368



DEPARTMENT OF THE ARMY
BALTIMORE DISTRICT, CORPS OF ENGINEERS
P.O. BOX 1715
BALTIMORE, MARYLAND 21203-1715

March 16, 1990

REPLY TO ATTENTION OF:

Planning Division

Mr. Louis H. Ege, Jr.
Deputy Director
Office of Planning and
Preliminary Engineering
State Highway Administration
707 North Calvert Street
Baltimore, Maryland 21202

Dear Mr. Ege:

Reference the letter dated February 6, 1990, from Mr. Neil J. Pedersen, requesting Baltimore District comments regarding the Environmental Assessment/Section 4(f) Evaluation to improve safety and capacity of I-695 (Baltimore Beltway) between MD 140 and MD 702. The comments provided below address the Corps of Engineers (Corps) areas of concern, including direct and indirect impacts on existing and/or proposed Corps projects, flood control hazard potential, and permit requirements under Section 404 of the Clean Water Act.

There are no existing or proposed Corps projects that would be affected by the work. In accordance with the subject report, portions of the proposed work will be located within the flood plain. New construction or major replacements within the flood plain requires full compliance with Executive Order (E.O.) No. 11988, Flood Plain Management, May 24, 1977; Federal Emergency Management Agency (FEMA) regulations; and other Federal, State, and local flood plain regulations. The objectives of the E.O. and the other flood plain regulations are to avoid the adverse effects of occupying and modifying the flood plain and to avoid direct and indirect support of development in the flood plain. The E.O. requires that activities not be located in the flood plain unless it is the only practicable alternative. Activities which must be located in the flood plain must incorporate measures to: (1) reduce the hazard and risks associated with floods, (2) minimize the adverse effects on human health, safety, and welfare; and (3) restore and preserve the natural and beneficial values of the flood plain.

Certain activities in the waters of the United States, and jurisdictional wetlands, require Department of the Army permits from the Corps of Engineers. Corps regulations (33 CFR 320 through 330 and 33 CFR 230 and 325 (Appendix B)) require full compliance with the National Environmental Policy Act (NEPA) of 1969 during the review and evaluation of permit applications. To the maximum extent possible, the Corps will accept the information presented in NEPA documents for evaluating permit applications. The report was reviewed by the District's Regulatory Branch and their comments are as follows:

Response:

1. Encroachment on area floodplains is minimal and is not anticipated to affect floodplain values or benefits. Increased flooding is not anticipated.
2. During the final design phase, stormwater management for quality control and quantity control will be designed and reviewed with the appropriate agencies.
3. Wetland mitigation will occur for impacted wetlands.
4. The I-83 (JFX) Option C has been selected. This will include a bridge crossing of the Jones Falls. Particular mitigation at this stream would include careful pier placement to minimize impacts, construction scheduling in accordance with stream restrictions, on-site construction management to minimize disturbance, including the use of snow fencing to mark areas, a double row of silt fencing and straw bales intercepting runoff, immediate removal of dredged material from the floodplain, and revegetation of disturbed areas with natural vegetation.

We endorse the State Highway Administration's (SHA) plan to control stormwater management in the Jones Falls watershed (pg IV-38). This management would include the construction of subsurface trenches throughout the watershed for augmenting the infiltration of stormwater. SHA also plans to construct 24-hour detention basins for attenuating flood heights and reducing the flood water's erosive force. We condone this use of 24-hour basins as opposed to 48-hour or 72-hour detention basins which would introduce heated water into the trout stream.

The Department recommends that all unavoidable wetland losses be replaced on a 2:1 basis for palustrine forested wetlands and on a 1:1 basis for all other wetlands types. The 2:1 replacement ratio for forested wetlands will help compensate for the time lag of 40 to 50 years which are required for planted seedlings to reach maturity. This ratio will also help compensate for the risk associated with trying to create forested wetlands. The techniques for creating forested wetlands have not been fully developed.

Mineral Resources - Owing to the nature of the project, (widening of the existing Baltimore Beltway mainline and upgrading specific substandard interchanges), no impact to mineral resources apparently is anticipated by the preparers of the document; accordingly, mineral resources are not discussed. A search of our data files leads us to concur that the project would not impact mineral resources.

FISH AND WILDLIFE COORDINATION ACT COMMENTS

The U.S. Fish and Wildlife Service's most probable position on any Section 404 permits for this project would most likely be no objection provided the I-83 (Jones Falls Expressway) Option C interchange is not selected for construction and provided an acceptable compensation plan is submitted and a viable site is identified with the Section 404 application.

SUMMARY COMMENTS

The Department of the Interior offers no objection to Section 4(f) approval of the recommended plan, provided wetlands at Option C interchange are avoided and the other measures mentioned above are included in the plans for project implementation.

As this Department has a continuing interest in this project, we are willing to cooperate and coordinate with you on a technical assistance basis in further project evaluation and assessment. For matters pertaining to recreational and cultural resources, please contact the Regional Director, National Park Service, Mid-Atlantic

97D

APPENDIX

"SUMMARY OF THE RELOCATION ASSISTANCE PROGRAM OF THE
STATE HIGHWAY ADMINISTRATION OF MARYLAND"

All State Highway Administration projects must comply with the provisions of the "Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970" (Public Law 91-646) and/or the Annotated Code of Maryland, Real Property, Title 12, Subtitle 2, Sections 12-201 thru 12-212. The Maryland Department of Transportation, State Highway Administration, Bureau of Relocation Assistance, administers the Relocation Assistance Program in the State of Maryland.

The provisions of the Federal and State Law require the State Highway Administration to provide payments and services to persons displaced by a public project. The payments that are provided include replacement housing payments and/or moving costs. The maximum limits of the replacement housing payments are \$15,000 for owner-occupants and \$4,000 for tenant-occupants. Certain payments may also be made for increased mortgage interest costs and/or incidental expenses, provided that the total of all housing benefits does not exceed the above mentioned limits. In order to receive these payments, the displaced person must occupy decent, safe and sanitary replacement housing. In addition to the replacement housing payments described above, there are also moving cost payments to persons, businesses, farms and non-profit organizations. Actual moving costs for residences include actual moving costs up to 50 miles or a schedule moving cost payment, including a dislocation allowance, up to \$500.

The moving cost payments to businesses are broken down into several categories, which include actual moving expenses and payments "in lieu of" actual moving expenses. The owner of a displaced business is entitled to receive a payment for actual reasonable moving and related expenses in moving his business, or personal property; actual direct losses of tangible personal property; and actual reasonable expenses for searching for a replacement site.

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The actual reasonable moving expenses may be paid for a move by a commercial mover or for a self-move. Generally, payments for the actual reasonable expenses are limited to a 50 mile radius. The expenses claimed for actual cost commercial moves must be supported by receipted bills. An inventory of the items to be moved must be prepared in all cases. In self-moves, the State will negotiate an amount for payment, not to exceed the lowest acceptable bid obtained. The allowable expenses of a self-move may include amounts paid for equipment hired, the cost of using the business' own vehicles or equipment, wages paid to persons who physically participate in the move, the cost of actual supervision of the move, replacement insurance for the personal property moved, costs of licenses or permits required, and other related expenses.

In addition to the actual moving expenses mentioned above, the displaced business is entitled to receive a payment for the actual direct losses of tangible personal property that the business is entitled to relocate but elects not to move. These payments may only be made after an effort by the owner to sell the personal property involved. The costs of the sale are also reimbursable moving expenses. If the business is to be reestablished, and the personal property is not moved but is replaced at the new location, the payment would be the lesser of the replacement cost minus the net proceeds of sale (or trade-in value) or the estimated cost of moving the item. If the business is being discontinued or the item is not to be replaced in the reestablished business, the payment will be the lesser of the difference between the value of the item for continued use in place and the net proceeds of the sale or the estimated cost of moving the item. When personal property is abandoned without an effort by the owner to dispose of the property for sale, unless permitted by the State, the owner will not be entitled to moving expenses, or losses for the item involved.

The owner of a displaced business may be reimbursed for the actual reasonable expenses in searching for a replacement business up to \$1,000. All expenses must be supported by receipted bills. Time spent in the actual search may be reimbursed on an hourly basis, within the maximum limit.

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In lieu of the payments described above, the business may elect to receive a payment equal to the average annual net earnings of the business. Such payment shall not be less than \$2,500 nor more than \$10,000. In order to be entitled to this payment, the State must determine that the business cannot be relocated without a substantial loss of its existing patronage, the business is not part of a commercial enterprise having at least one other establishment in the same or similar business that is not being acquired, and the business contributes materially to the income of a displaced owner during the two taxable years prior to displacement.

Considerations in the State's determination of loss of existing patronage are the type of business conducted by the displaced business and the nature of the clientele. The relative importance of the present and proposed locations to the displaced business, and the availability of suitable replacement sites are also factors.

In order to determine the amount of the "in lieu of" moving expenses payment, the average annual net earnings of the business is considered to be one-half of the net earnings, before taxes, during the two taxable years immediately preceding the taxable year in which the business is relocated. If the two taxable years are not representative, the State may use another two-year period that would be more representative. Average annual net earnings include any compensation paid by the business to the owner, his spouse, or his dependents during the period. Should a business be in operation less than two years, the owner of the business may still be eligible to receive the "in lieu of" payment. In all cases, the owner of the business must provide information to support its net earnings, such as income tax returns, for the tax years in question.

For displaced farms and non-profit organizations, the actual reasonable moving costs generally up to 50 miles, actual direct losses of tangible personal property, and searching costs are paid. The "in lieu of" actual moving cost payments provide that the State may determine that a displaced farm may be paid from a minimum of \$2,500 to a maximum of \$10,000, based upon the net income of the farm, provided that the farm has been discontinued or relocated. In some cases, payments "in lieu of" actual moving costs may be made to farm operations that are affected by a partial acquisition. A non-profit organization is eligible to receive "in lieu of" actual moving cost payments, in the amount of \$2,500.

A more detailed explanation of the benefits and payments available to displaced persons, businesses, farms, and non-profit organizations is available in Relocation Brochures that will be distributed at the public hearings for this project and will also be given to displaced persons individually in the future along with required preliminary notice of possible displacement.

In the event comparable replacement housing is not available to rehouse persons displaced by public projects or that available replacement housing is beyond their financial means, replacement "housing as a last resort" will be utilized to accomplish the rehousing. Detailed studies must be completed by the State Highway Administration before "housing as a last resort" can be utilized.

The "Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970" requires that the State Highway Administration shall not proceed with any phase of any project which will cause the relocation of any persons, or proceed with any construction project, until it has furnished satisfactory assurances that the above payments will be provided and that all displaced persons will be satisfactorily relocated to comparable decent, safe, and sanitary housing within their financial means or that such housing is in place and has been made available to the displaced person.



Transmittal

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Johnson, Mirmiran and Thompson, P.A.
PLANNERS ENGINEERS LANDSCAPE ARCHITECTS SURVEYORS

72 LOVETON CIRCLE • SPARKS, MD. • 21152 • (301) 329-3100

TO: SHA

DATE: 2/11/92

JOB: 87112-09

707 N. CALVERT

PROJECT: I-695

ATTN: HOWARD JOHNSON

GENTLEMEN:

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APPLICATIONS OTHER

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REMARKS:

PLEASE NOTIFY US AT ONCE IF ENCLOSURES ARE NOT AS NOTED.

Johnson, Mirmiran and Thompson, P.A.

CC: _____

BY: MICHAEL J. ROTHENBERGER, P.E.